

**ADDENDUM NO. TWO (2)
TO
COMMERCIAL APRON EXPANSION
HILTON HEAD ISLAND AIRPORT
HILTON HEAD ISLAND, SOUTH CAROLINA
IFB # 060420HXD
TBE PROJECT NO. 2119-1906**

June 5, 2020

From: Talbert, Bright & Ellington, Inc.
2000 Park Street, Suite 101
Columbia, SC 29201

To: **All Bidders of Record**

This Addendum is hereby made a part of the contract documents and specifications of the above referenced project. All other requirements of the original specification shall remain in effect in their respective order. Acknowledge receipt of this addendum by inserting its number and date in the proposal form.

SPECIFICATIONS

1. In section P-152 of the Technical Specifications, in section 152-2.2 Excavation (b.) Undercutting, **DELETE** the words “Unsuitable materials to be disposed off the airport.” and **INSERT** the revised words “Unsuitable materials to be disposed of in waste areas as shown on the plans. Unsuitable soils shall be placed in separate waste area from the suitable soils waste area in a location or locations approved by the Owner.”
2. In section P-152 of the Technical Specifications, in section 152-2.2 Excavation (b.) Undercutting, **DELETE** the words “Undercutting will be paid as unclassified excavation”, and **INSERT** the revised words “Undercutting will be paid as unsuitable excavation.”

PLANS

1. **REMOVE AND REPLACE** the original plan sheet 150 with the revised plan sheet 150 attached to this Addendum.
2. **MODIFY** plan sheet 451 by **ADDING** the detail shown on “Underdrain Detail - Addendum 2” attached to this Addendum.
3. **REMOVE AND REPLACE** the original plan sheet 650 with the revised plan sheet 650 attached to this Addendum.

QUESTIONS AND ANSWERS

1. Question: The sequence of construction states only clearing & grubbing in areas of basin are to be completed prior to clearing of the remaining areas. During the pre-bid it was stated; “All clearing and grubbing could be completed in one phase” Please confirm all clearing and grubbing and be completed in the initial phase?
Answer: The statement made during the Pre-Bid was, since all of the clearing and grubbing area falls within the designated Phase 1 limits, all of the clearing and grubbing needs to be completed during Phase 1 of this project.
2. Question: Will a FOD barrier be required around the perimeter of the site adjacent to Taxiway F and Parking Apron?
Answer: See revised plan sheet 150 attached to this Addendum for the FOD barrier requirements associated with the temporary SIDA fence shown on the revised sheet 150.
3. Question: Please provide information regarding the requirements for personnel to have completed the “Airports Ground Vehicle Driver Training Program”. (Duration time for training and any fees required to be paid by contractor).
Answer: The Contractor awarded the construction contract for this project will be required to schedule his/her key field personnel to attend the Owner’s Vehicle Driver Training Program and successfully pass this Program prior to receiving driver privileges in the active Aircraft Operations Area. There is no fee required by the Owner for this program. The total amount of time is dependent on the total number of Contractor employees who attend the Program. Typically, one day should be allotted for the Program. However, Contact the Airport Director for specific timeframes required due to COVID-19 policies currently in place.
4. Question: Specification section 152-2.2 Excavation (b.) Undercutting, States; “Unsuitable materials to be disposed off the airport”. During pre-bid it was stated the unsuitable excavation was to be stockpiled at the “Waste Area Stockpile”. Please clarify.
Answer: See item 1 under the “Specifications” section above for the clarification.
5. Question: Specification section 152-2.2 Excavation (b.) Undercutting, States; “Undercutting will be paid as unclassified excavation”. Please clarify this is incorrect and will be paid under the pay item # 13 “Unsuitable Excavation”.
Answer: See item 2 under the “Specifications” section above for the clarification.
6. Question: Is the contractor responsible for conducting the weekly inspections and reporting for the SWPPP requirements or will this be provided by the owners testing agency?
Answer: The Contractor is responsible for conducting the weekly inspections and the related documentation/reporting requirements for the SWPPP.

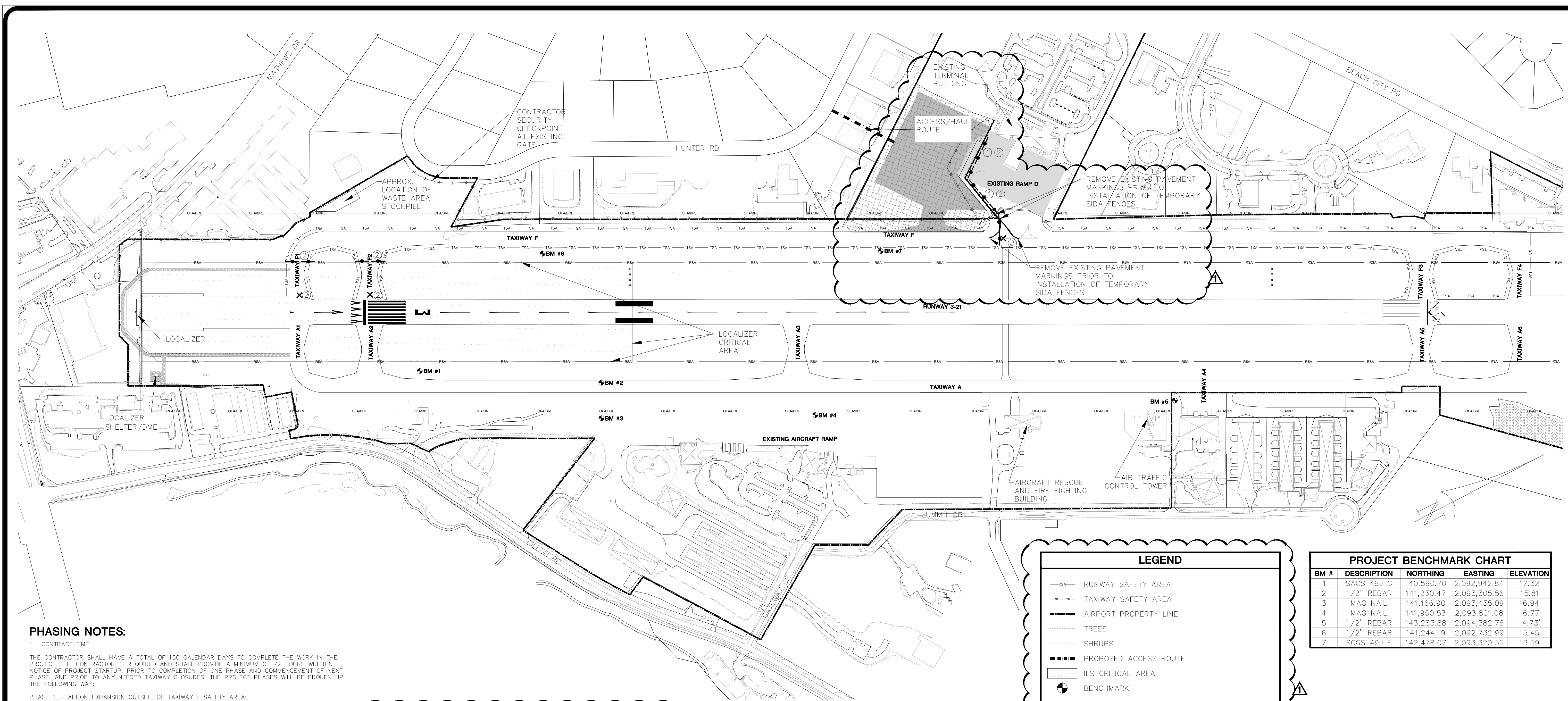
7. Question: There is no detail in the plans for the 4" Perforated under-drain pipe.
- Is stone/granular material required around the pipe?
 - Is a cleanout needed at the end of the pipe?
 - Please provide details.
- Answer: See "Underdrain Detail - Addendum 2" attached to this Addendum as addressed in item 2 under the "Plans" section above.*
8. Question: Will the owners testing representative also be doing the weekly SWPPP inspection and reporting?
- Answer: No. See question and answer number 6 above.*
9. Question: If there are any noise restrictions for this project, please provide requirements?
- Answer: There are no known noise restrictions known for this project at this time. However, it is each bidder's responsibility to determine if any work proposed for this project will be subject to noise restrictions from the Town of Hilton Head Island.*
10. Question: Can grinders be used on site for chipping the clearing debris, prior to hauling off?
- Answer: Yes, provided the debris from the grinding operation does not impact normal operation of the Airport including, but not limited to, foreign object debris (FOD) on the airfield/aircraft parking areas, or excessive dust creation. Additionally, the grinding operation shall be performed during normal business hours.*
11. Question: Did not see any daily hours of operation. Are there any time restraints for working on site?
- Answer: There are no time restraints established for working onsite. Any nighttime work must be coordinated with the Owner/Engineer in advance in writing by the Contractor and the Contractor shall be responsible for providing adequate artificial lighting for the proper performance, inspection and testing of the work in accordance with the Contract Documents and industry standards when natural lighting is insufficient for proper performance, inspection and testing of the work. The Contractor is responsible for submitting an artificial lighting plan to the RPR for review well in advance of any scheduled nighttime work.*
12. Question: Is weekend work allowed?
- Answer: Yes.*

**ADDENDUM NO. TWO (2)
TO
COMMERCIAL APRON EXPANSION
HILTON HEAD ISLAND AIRPORT**

ATTACHMENTS

1. Revised plan sheet 150
2. Underdrain Detail - Addendum 2
3. Revised plan sheet 650

END OF ADDENDUM NO. TWO (2)

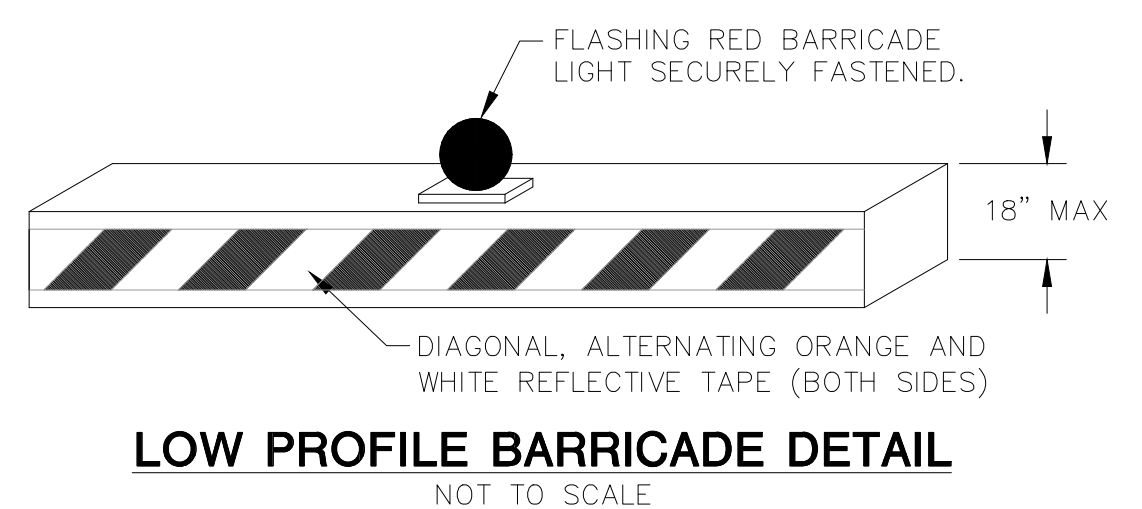


PHASING NOTES:

- CONTRACT TIME
- THE CONTRACTOR SHALL HAVE A TOTAL OF 150 CALENDAR DAYS TO COMPLETE THE WORK IN THE PROJECT. THE CONTRACTOR IS REQUIRED AND SHALL PROVIDE A MINIMUM OF 72 HOURS WRITTEN NOTICE OF PROJECT STARTUP, PRIOR TO COMPLETION OF ONE PHASE AND COMMENCEMENT OF NEXT PHASE, AND PRIOR TO ANY NEEDED TAXIWAY CLOSURES. THE PROJECT PHASES WILL BE BROKEN UP THE FOLLOWING WAY:
 - PHASE 1 - APRON EXPANSION OUTSIDE OF TAXIWAY F SAFETY AREA.
 - ALL WORK IN THIS PHASE MAY COMMENCE AND BE COMPLETED ANYTIME WITHIN THIS PHASE. THE CONTRACTOR IS NOTIFIED OF THE PRESENCE OF THE ACTIVE COMMERCIAL AIRCRAFT APRON ADJACENT TO THE WORK AREA. PARTICULAR ATTENTION SHALL BE PROVIDED TO DUST AND FOREIGN OBJECT DEBRIS (FOD) CONTROL. IT IS POSSIBLE THAT AIRCRAFT ACTIVITY FROM THE ADJACENT COMMERCIAL AIRCRAFT PARKING APRON WILL PRODUCE JET BLAST FROM AIRCRAFT ENTERING AND EXITING THE EXISTING APRON, AND CARE SHALL BE TAKEN TO PROPERLY SECURE MATERIALS AND EQUIPMENT FROM BEING MOVED BY SUCH JET BLAST DURING AND AFTER WORK HOURS.
 - PHASE 2 - APRON EXPANSION INSIDE TAXIWAY F SAFETY AREA.
 - THE CONTRACTOR WILL BE ALLOWED TO CLOSE TAXIWAY F DURING THIS PHASE TO MAKE REQUIRED TIE-INS TO TAXIWAY F, ALONG WITH ALL OTHER WORK WITHIN THE TAXIWAY F SAFETY AREA. ONCE THIS PHASE IS CLOSED TO AIRCRAFT WITH REQUIRED BARRICADES AND CONTRACTOR COMMENCES WORK IN THIS PHASE, ALL PAVEMENT REMOVAL AND OTHER WORK WITHIN THE CLOSED PHASE AREA MAY COMMENCE AND BE COMPLETED ANYTIME WITHIN THIS PHASE.
- CLOSED AIRFIELD MARKERS AND DE-ENERGIZED AND/OR COVERED TAXIWAY LIGHTS SHALL BE INSTALLED AS THE FIRST TASK IN EACH PHASE REQUIRING A TAXIWAY CLOSURE. CLOSED AIRFIELD MARKERS SHALL BE REMOVED AS THE LAST TASK IN EACH PHASE REQUIRING A TAXIWAY CLOSURE. TRAFFIC BARRICADES SHALL NOT BE REMOVED UNTIL PAVEMENT AND TAXIWAY SAFETY AREAS ARE IN SERVICEABLE CONDITION.
- THE CONTRACTOR SHALL SWEEP ALL AIRFIELD PAVEMENT PRIOR TO OPENING, TO CLEAR ANY FOREIGN OBJECT DEBRIS CAUSED BY THE WORK. THE SWEEPING WILL CONTINUE UNTIL APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL FOLLOW THE SAFETY GUIDELINES SET FORTH IN AC 150/5370-26 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- AT THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING THE NEW DOUBLE SWING GATES INSTALLED IN THE EXISTING AIRPORT FENCE FOR TEMPORARY CONSTRUCTION ACCESS POINTS AND FOR THE INSTALLATION OF NEW CHAIN LINK FENCE OR THE UNDAUNTED EXISTING CHAIN LINK FENCE REMOVED PRIOR TO THE TEMPORARY GATE INSTALLATION, IN ORDER TO RETURN THE AIRPORT FENCE TO ITS ORIGINAL SECURED CONDITION PRIOR TO THIS PROJECT.

TEMPORARY SIDA FENCING NOTES

- THE CONTRACTOR SHALL INITIALLY CLEAR AND GRUB ONLY ENOUGH TREES ALONG THE SOUTHERN PROPERTY LINE TO INSTALL THE TEMPORARY CONSTRUCTION ENTRANCE, SEDIMENT AND EROSION CONTROL MEASURES REQUIRED ALONG THE SOUTHERN PROPERTY LINE, AND THE PROPOSED AIRPORT SECURITY FENCE/DOUBLE SWING GATE. ONCE THESE ITEMS ARE COMPLETE AND THE AIRPORT SECURITY IS APPROVED BY THE OWNER, THE CONTRACTOR SHALL REMOVE THE EXISTING AIRPORT SECURITY FENCE SHOWN TO BE REMOVED ON SHEET 200. THE CONTRACTOR SHALL THEN INSTALL THE TEMPORARY SIDA FENCE INSTALLED IN PAVEMENT AND THE TEMPORARY MOVABLE SIDA FENCE DEPICTED ON THIS SHEET. THE TEMPORARY SIDA FENCE INSTALLED IN PAVEMENT AND THE TEMPORARY MOVABLE SIDA FENCE SHALL BE CONSTRUCTED WITH THE SAME FENCE POST, CHAIN LINK FENCE FABRIC, BARBED WIRES/SUPPORT ARMS AND RELATED HARDWARE AS SHOWN ON SHEET 650. EXCEPT THESE TEMPORARY SIDA FENCES MAY BE CONSTRUCTED WITH STANDARD GALVANIZED COATINGS. ALL SIDA FENCES SHALL HAVE A SINGLE ROW OF TEMPORARY SIDA FENCE INSTALLED AND SECURED ALONG THE BOTTOM PORTION OF THE CHAIN LINK FENCE FABRIC AS A FOREIGN OBJECT DEBRIS (FOD) BARRIER. THIS FOD BARRIER SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE PROJECT. ANY TEARS/HOLES/ETC. SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST. USE SILT FENCE FABRIC MEETING THE REQUIREMENTS SHOWN ON SHEET 401.
- ALL WORK/MATERIALS/MEN/EQUIPMENT/ALL INCIDENTALS ASSOCIATED WITH PROVIDING AND INSTALLING THE TEMPORARY SIDA FENCES, MAKING MULTIPLE MOVEMENTS OF THE TEMPORARY MOVABLE SIDA FENCE DURING CONSTRUCTION, SIDA FENCE/FOD BARRIER MAINTENANCE, REMOVAL OF ALL TEMPORARY SIDA FENCES AND DISPOSAL OFF-AIRPORT, AND RESTORATION OF EXISTING PAVEMENTS AS LISTED IN THE "TEMPORARY SIDA FENCE IN EXISTING PAVEMENTS DETAIL" ON SHEET 650 SHALL BE INCIDENTAL TO THE "REMOVE EXISTING FENCE" ITEM OF WORK. NO SEPARATE PAYMENT WILL BE MADE FOR TEMPORARY SIDA FENCES.



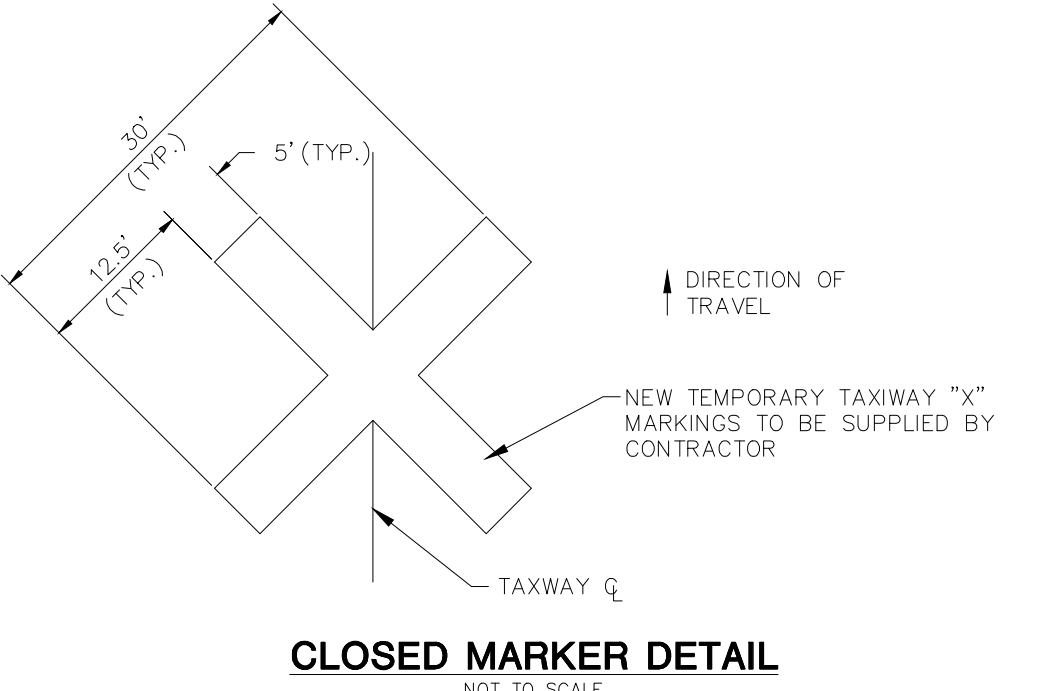
- NOTES:**
- BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
 - THE CONTRACTOR SHALL PROVIDE A SUFFICIENT NUMBER OF BARRICADES TO COMPLETE THE WORK. THIS ITEM IS INCIDENTAL TO THE PROJECT, INCLUDING MULTIPLE RELOCATIONS OF THE BARRICADES AS REQUIRED. NO SEPARATE PAY ITEM, AND ALL WORK ASSOCIATED WITH THIS ITEM SHALL BE INCIDENTAL TO THE PROJECT.
 - BARRICADES SHALL BE CONSTRUCTED OF MATERIAL THAT IS LOW IN MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE TO WITHSTAND HIGH WINDS AND/OR JET BLAST.
 - SPACING OF BARRICADES SHALL ENSURE THAT THE DISTANCE BETWEEN LIGHTS IS NO MORE THAN 10 FEET. INSTALLATION OF MULTIPLE LIGHTS ON A SINGLE BARRICADE MAY BE REQUIRED. THE BARRICADES SHALL BE A MINIMUM OF 8' IN LENGTH.
 - THE CONTRACTOR SHALL MAINTAIN AND MAKE FREQUENT INSPECTION OF THE BARRICADES THROUGHOUT THE DURATION OF THE PROJECT. PROMPT REPAIRS INCLUDING REPLACEMENT OF FLASHING RED LIGHTS SHALL BE MADE AS NECESSARY.
 - KNOWN MANUFACTURER AND MODEL: OTW SAFETY, AR10X96 O V.2.

LEGEND

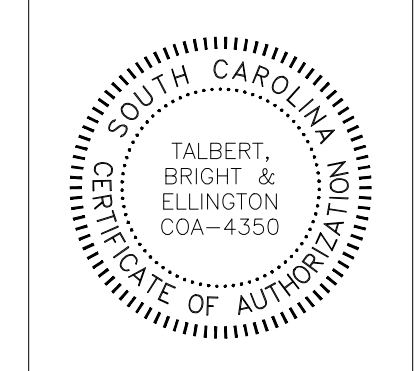
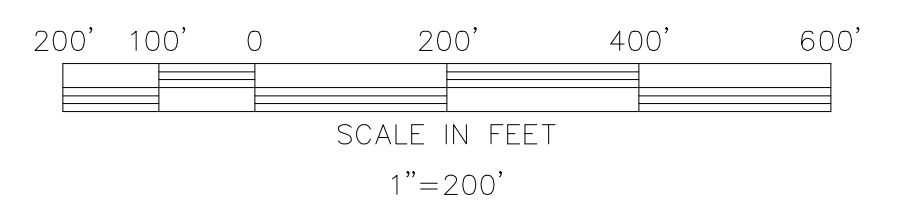
- RSA RUNWAY SAFETY AREA
- TAXIWAY SAFETY AREA
- AIRPORT PROPERTY LINE
- TREES
- SHRUBS
- PROPOSED ACCESS ROUTE
- ILS CRITICAL AREA
- BENCHMARK
- ② LOW PROFILE BARRICADES w/PHASE DESIGNATION
- ▨ DEMO, GRADING, DRAINAGE, PAVING PHASE 1
- ▩ DEMO, GRADING, DRAINAGE, PAVING PHASE 2
- ▤ WETLAND
- EXISTING FENCE
- TEMPORARY SIDA FENCE INSTALLED IN PAVEMENT
- TEMPORARY MOVABLE SIDA FENCE

PROJECT BENCHMARK CHART

BM #	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	SACS 49J G	140,590.70	2,092,942.84	17.32
2	1/2" REBAR	141,230.47	2,093,305.56	15.81
3	MAG NAIL	141,166.90	2,093,435.09	16.94
4	MAG NAIL	141,950.53	2,093,801.08	16.77
5	1/2" REBAR	143,283.88	2,094,382.76	14.73
6	1/2" REBAR	141,244.19	2,092,732.99	15.45
7	SCGS 49J F	142,478.07	2,093,320.35	13.59



- NOTES:**
- CLOSED MARKERS SHALL BE INSTALLED AS THE FIRST ITEM OF WORK.
 - THE CONTRACTOR SHALL PROPERLY SECURE THE MARKINGS, AND MAKE FREQUENT INSPECTION OF THE MARKINGS AND MAKE PROMPT REPAIRS AS NECESSARY.
 - SEE ADVANCE NOTICE REQUIREMENTS FOR TAXIWAY CLOSURE PROVIDED ON THIS SHEET IN THE PHASING NOTES.
 - TAXIWAY CLOSED MARKERS SHALL BE PAY ITEMS.



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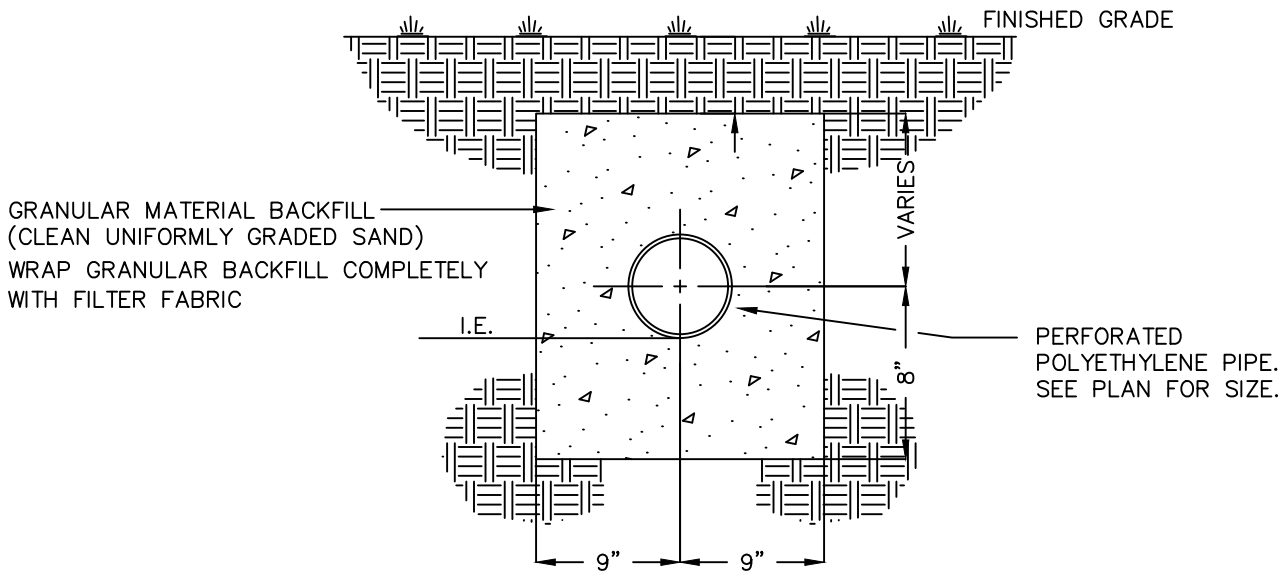
CONSTRUCTION SAFETY AND PHASING PLAN
COMMERCIAL APRON EXPANSION

DESIGNED BY: PET
PROJECT ENG.: JAS
PROJECT NO.: 2119-1906
DRAWN BY: PET
CHECKED BY: JAS
DATE ISSUED: MAY 2020

DATE: 06-05-2020
REVISIONS: 1 APPENDUM NUMBER TWO (2)

FILE NAME: 1906-PHASE
SCALE: 1"=200'

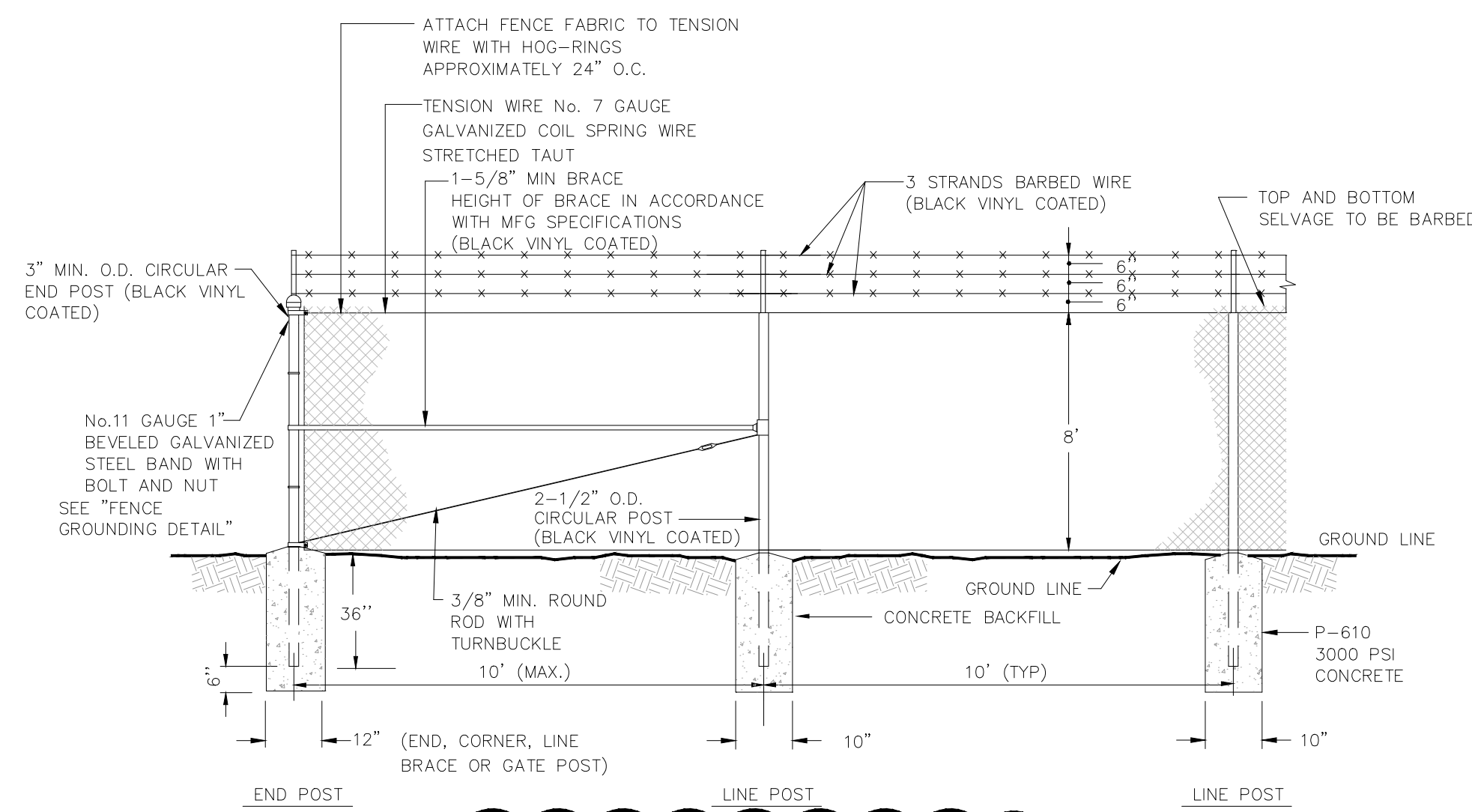
SHT. **150**



PROVIDE 4" CLEANOUT AT THE END OF THE UNDERDRAIN PIPE

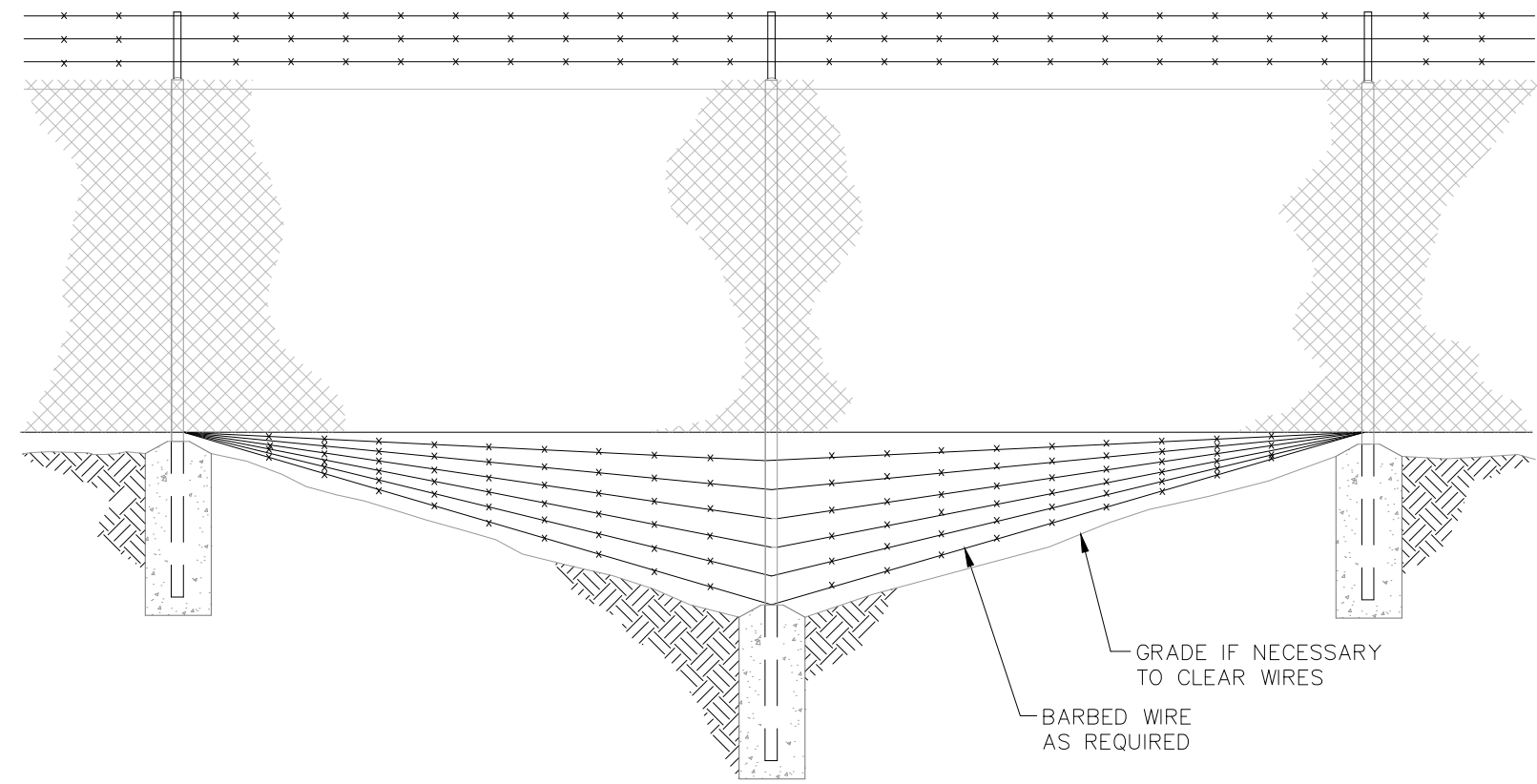
UNDERDRAIN DETAIL - ADDENDUM 2

DETAIL 02630-014



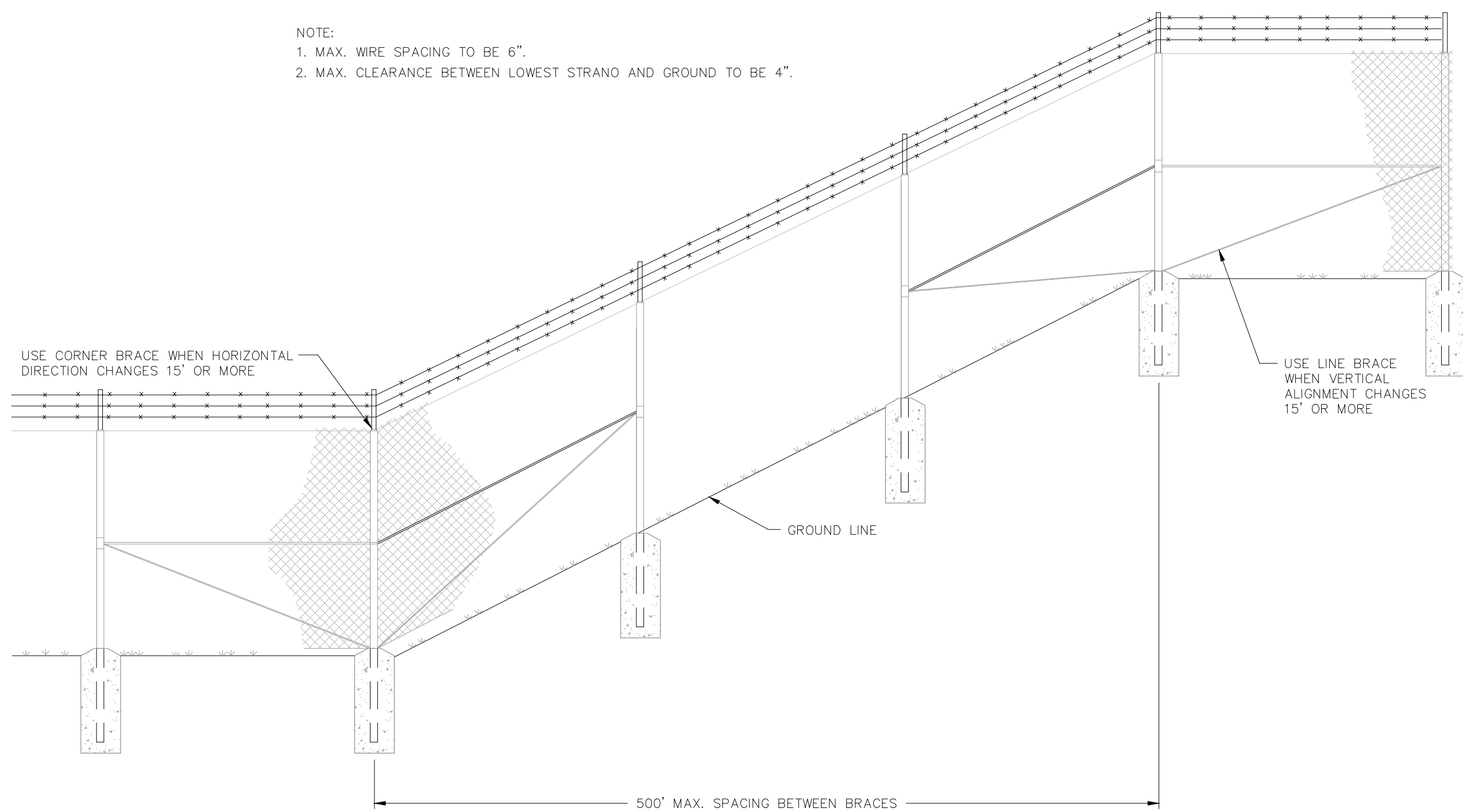
CHAIN LINK FENCE DETAIL
NOT TO SCALE

NOTE:
1. A MOISTURE-EXCLUDING CAP IS REQUIRED ON LINE POSTS. TUBULAR METAL MATERIAL FOR CAP SHALL CONFORM TO THE ALLOWABLE TYPES FOR OTHER LISTED FITTINGS.
2. PROPOSED FENCE, CHAIN LINK FABRIC, POSTS, POST CAPS, AND RAILS SHALL BE BLACK VINYL COATED.

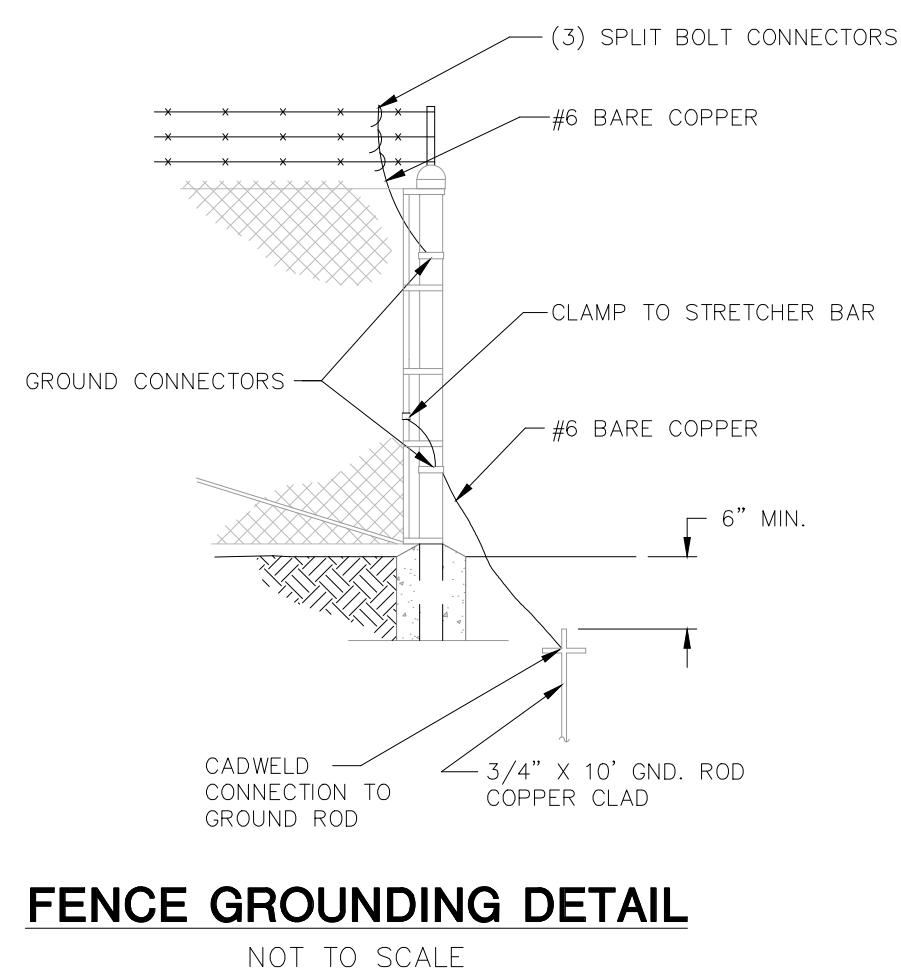


DETAIL OF DITCH CROSSING
NOT TO SCALE

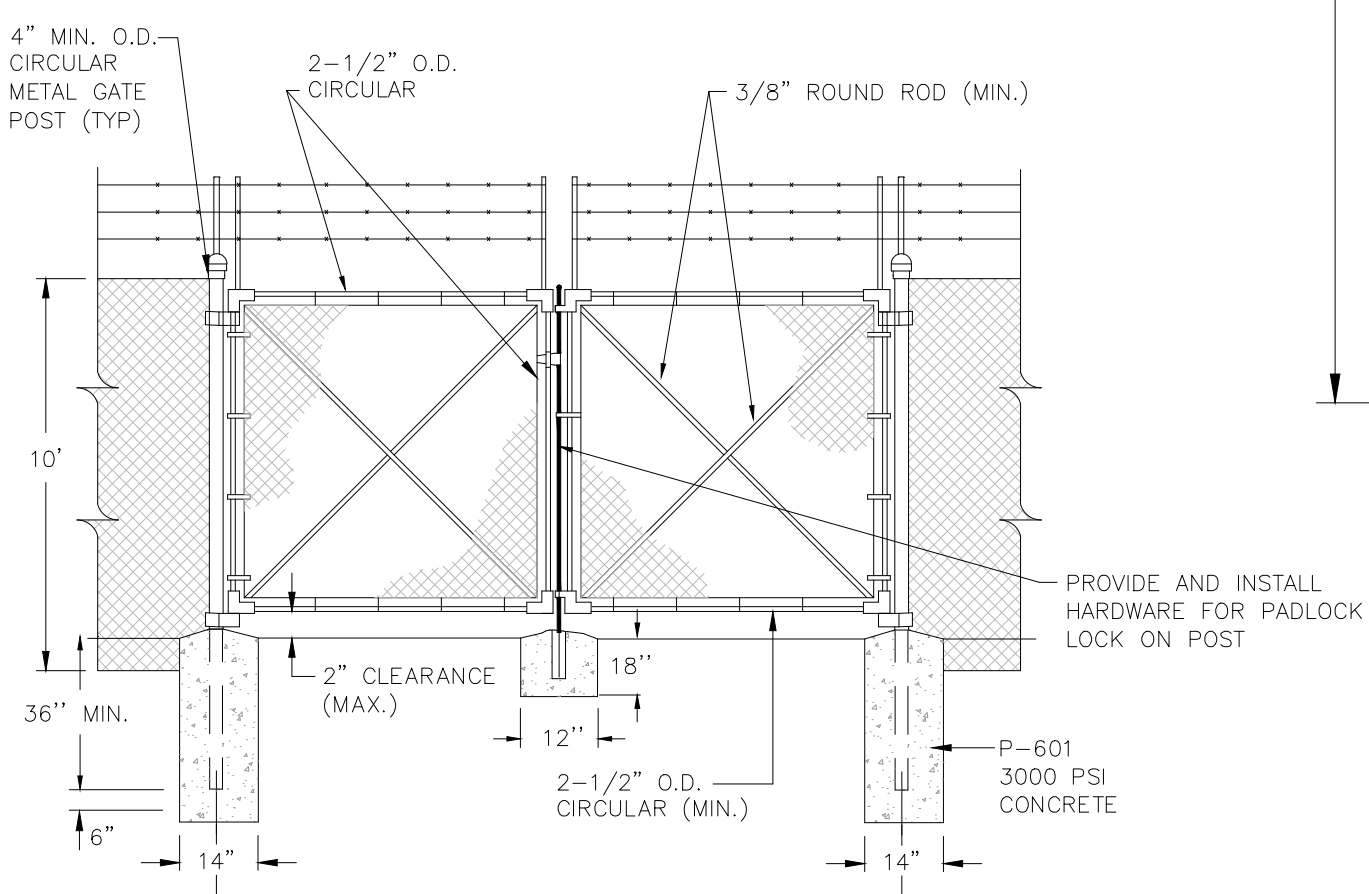
- NOTE:
1. MAX. WIRE SPACING TO BE 6".
2. MAX. CLEARANCE BETWEEN LOWEST STRAND AND GROUND TO BE 4".



CHANGE IN FENCE ALIGNMENT DETAIL
NOT TO SCALE



FENCE GROUNDING DETAIL
NOT TO SCALE



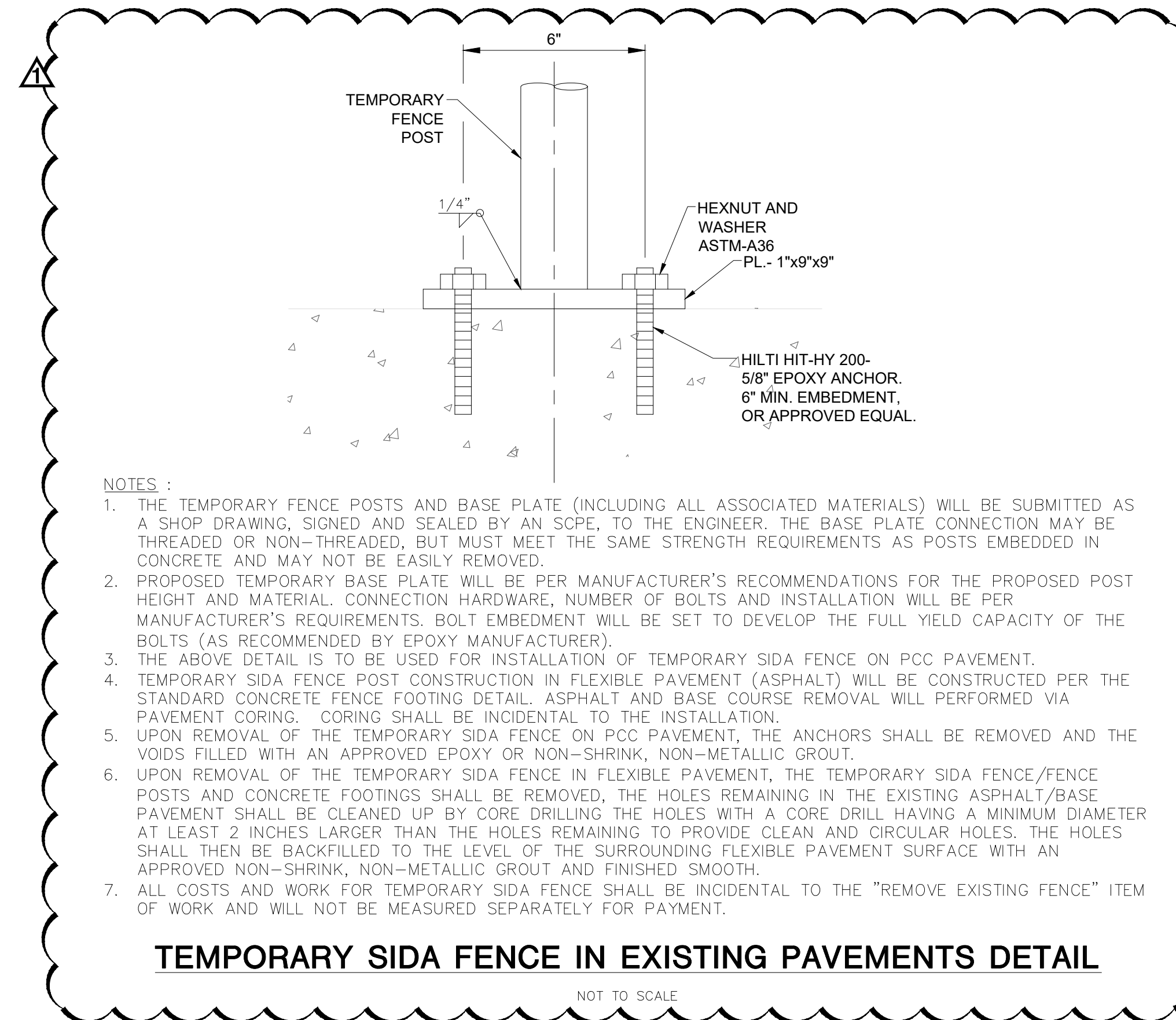
DOUBLE SWING GATE
NOT TO SCALE

NOTE:
1. PROPOSED FENCE, CHAIN LINK FABRIC, POSTS, POST CAPS, AND RAILS SHALL BE BLACK VINYL COATED.



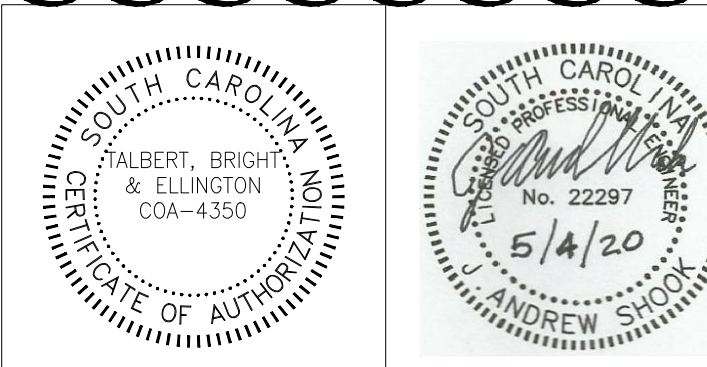
- NOTES:
1. THE FULL SIGN (BOTH BACKGROUND AND TEXT) SHALL BE RETROREFLECTIVE.
2. THE CONTRACTOR SHALL FIELD VERIFY EXISTING SIGNS AND ATTEMPT TO MATCH.

SECURITY FENCE SIGN DETAIL
NOT TO SCALE



- NOTES:
1. THE TEMPORARY FENCE POSTS AND BASE PLATE (INCLUDING ALL ASSOCIATED MATERIALS) WILL BE SUBMITTED AS A SHOP DRAWING, SIGNED AND SEALED BY AN SCOPE, TO THE ENGINEER. THE BASE PLATE CONNECTION MAY BE THREADED OR NON-THREADED, BUT MUST MEET THE SAME STRENGTH REQUIREMENTS AS POSTS EMBEDDED IN CONCRETE AND MAY NOT BE EASILY REMOVED.
2. PROPOSED TEMPORARY BASE PLATE WILL BE PER MANUFACTURER'S RECOMMENDATIONS FOR THE PROPOSED POST HEIGHT AND MATERIAL. CONNECTION HARDWARE, NUMBER OF BOLTS AND INSTALLATION WILL BE PER MANUFACTURER'S REQUIREMENTS. BOLT EMBEDMENT WILL BE SET TO DEVELOP THE FULL YIELD CAPACITY OF THE BOLTS (AS RECOMMENDED BY EPOXY MANUFACTURER).
3. THE ABOVE DETAIL IS TO BE USED FOR INSTALLATION OF TEMPORARY SIDA FENCE ON PCC PAVEMENT.
4. TEMPORARY SIDA FENCE POST CONSTRUCTION IN FLEXIBLE PAVEMENT (ASPHALT) WILL BE CONSTRUCTED PER THE STANDARD CONCRETE FENCE FOOTING DETAIL. ASPHALT AND BASE COURSE REMOVAL WILL PERFORMED VIA PAVEMENT CORING. CORING SHALL BE INCIDENTAL TO THE INSTALLATION.
5. UPON REMOVAL OF THE TEMPORARY SIDA FENCE ON PCC PAVEMENT, THE ANCHORS SHALL BE REMOVED AND THE VOIDS FILLED WITH AN APPROVED EPOXY OR NON-SHRINK, NON-METALLIC GROUT.
6. UPON REMOVAL OF THE TEMPORARY SIDA FENCE IN FLEXIBLE PAVEMENT, THE TEMPORARY SIDA FENCE/FENCE POSTS AND CONCRETE FOOTINGS SHALL BE REMOVED. THE HOLES REMAINING IN THE EXISTING ASPHALT/BASE PAVEMENT SHALL BE CLEANED UP BY CORE DRILLING THE HOLES WITH A CORE DRILL HAVING A MINIMUM DIAMETER AT LEAST 2 INCHES LARGER THAN THE HOLES REMAINING TO PROVIDE CLEAN AND CIRCULAR HOLES. THE HOLES SHALL THEN BE BACKFILLED TO THE LEVEL OF THE SURROUNDING FLEXIBLE PAVEMENT SURFACE WITH AN APPROVED NON-SHRINK, NON-METALLIC GROUT AND FINISHED SMOOTH.
7. ALL COSTS AND WORK FOR TEMPORARY SIDA FENCE SHALL BE INCIDENTAL TO THE "REMOVE EXISTING FENCE" ITEM OF WORK AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.

TEMPORARY SIDA FENCE IN EXISTING PAVEMENTS DETAIL
NOT TO SCALE

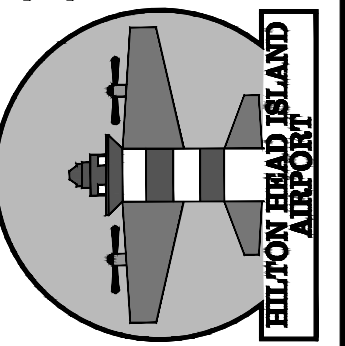


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DATE	REVISIONS
06-05-2020	1 APPENDUM NUMBER TWO (2)
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	4
	5

DESIGNED BY: CEV	PROJECT ENG.: PET	PROJECT NO.: 2119-1906	DRAWN BY: JPB	CHECKED BY: JAS	DATE ISSUED: MAY 2020
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HILTON HEAD ISLAND AIRPORT
"Fly to the Fun!"



MISCELLANEOUS DETAILS
COMMERCIAL APRON EXPANSION
SCALE: N.T.S.
FILE NAME: 1906-MISCDET

SHT. **650**