



St. Johns River

Water Management District

Ann B. Shortelle, Ph.D., Executive Director

525 Community College Parkway S.E. • Palm Bay, FL 32909 • 321-984-4940
On the internet at www.sjrwmd.com.

DATE: February 13, 2019
TO: Prospective Respondents
FROM: Amy Lucey, Procurement Specialist
SUBJECT: Addendum #4 to IFB # 34093, C40 Plugs Enhancements

As a result of inquiries, the following clarifications/changes are provided for your information. Please make all appropriate changes to your bid documents.

- Q1: What pay item(s) should the cost for the erosion control work be put in?
- A1: The contractor may work the cost of erosion control into the bid as deemed appropriate.
- Q2: How will the unknown quantity of unsuitable soil removal be paid for? The soil borings show quite a bit of muck/peat. Project documents say it has to be removed.
- A2: Removal of unsuitable material should be included in the pay item for Excavation and Fill. The District has roughly estimated the quantity of unsuitable material to be removed at 771 CY. However, this is a rough estimate.
- Q3: How will the unknown quantity of additional fill material that will replace the unsuitable soil removed be paid for?
- A3: The "borrow" fill shall be included in the pay item for Excavation and Fill. The District has roughly estimated the quantity of "borrow" fill at 399 CY. However, this value is rough estimate.
- Q4: Is there a bedding the pipe needs to be placed on? If so would the bedding be an aggregate base or flowable fill? There is no detail in the plans for it.
- A4: Sheet C3 – NOTES AND SPECIFICATIONS. Under the EARTHWORK – EXCAVATION AND EMBANKMENT SECTION, Item 4 indicates "EXCAVATION AND FILL SHALL BE IN ACCORDANCE WITH SECTION 125 – EXCAVATION FOR STRUCTURES AND PIPE OF THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (LATEST EDITION). Within this section, subsection 125-8.3.2 provides guidance on pipe bedding. Specifically:

125-8.3.2 Material:

125-8.3.2.1 Lowest Zone: Backfill areas undercut below the bedding zone of a pipe with coarse sand, or other suitable granular material, obtained from the grading operations on the project, or a commercial material if no suitable material is available.

125-8.3.2.2 Soil Envelope: In both the bedding zone and the cover zone of the pipe, backfill with materials classified as A-1, A-2, or A-3. Material classified as A-4 may be used if the pipe is concrete pipe.

125-8.3.2.3 Top Zone: Backfill the area of the trench above the soil envelope of the pipe with materials allowed on Standard Plans, Index 120-001.

In addition to the above, general guidelines for the compaction and fill are also provided within the geotechnical engineering reports for each of the plugs.

- Q5: In addendum #2 it still appears the start date and finish date are somewhat nebulous. Other than the actual work, this project has at least 2 time challenges, engineering and submittal approval for the cofferdams and the submittal, fabrication and delivery of the gates. For example slide gates typically take 12 weeks or more from submittal approval to get delivered to the project. What is the number of contract days the District anticipates for this contract?
- A5: Barring unforeseen or extended wet weather, the District considers 4-6 months a reasonable timeframe for this construction.
- Q6: Due to the difficulty getting to the project when it rains, does it make sense to be starting this project in the spring knowing it will extend well into the rainy season? The burden on the contractor to finish this project by September 30th will be something to consider. Given a normal rainy season the contractor for this project will likely have to work in poor conditions to get through the rainy season and finish by September 30th. Any consideration for postponing the project and putting it out to bid in November to take advantage of the dry season?
- A6: The District is constrained to start construction on this contract within FWC's current fiscal year which ends June 30, 2019. For this reason, a project postponement until after the 2019 rainy season is unfeasible..
- Q7: Could you add a line item be added to the bid form for the proposed riprap at each end of the culverts?
- A7: A rip/rap line item will be added to the schedule of values.
- Q8: Will the "Critical Path" scheduling method be applicable for this project as shown on sheet 29 of the Invitation to bid package?
- A8: The District will accept any form of schedule provided by the contractor.
- Q9: Will 3/8" thick gasket instead of 1/8" be acceptable for the flanges?
- A9: Referring to S3/1 Aluminum Flange Detail, the District will accept the substitution of a 3/8" gasket for the specified 1/8" gasket.
- Q10: Can the Walkways be galvanized steel instead of aluminum?

- A10: Due to the potential for galvanic corrosion from dissimilar metals, the District will not allow the use of galvanized steel for the walkways.
- Q11: It appears there is active construction on Structure S-96B. Could you please confirm the Contractor will have uninterrupted access to and from the site at all times over this structure and any delays due to interference by others be compensable by the Owner?
- A11: The District is requiring the contractor for the S-96B construction to provide unimpeded access across that structure at all times during construction.
- Q12: What will the clear travel width over Structure S-96B be during this Contract?
- A12: The travel width of the S-96B bridge deck is 12' curb to curb.
- Q13: Would the District allow the Contractor to make temporary modifications to widen the turning points in the road way north and south of Structure S-96B in order to accommodate heavy equipment and material deliveries to the project sites?
- A13: The District will consider temporary modifications to widen the turning points in the road way north and south of Structure S-96B. Any turning points modifications made must be coordinated with the contractor for S-96B to ensure that contractor is not impeded. The areas of the temporary modification must be returned to existing or better conditions at the conclusion of this contract. If the contractor and the District mutually agree to leave the turn around modifications in place, the affected areas must be seeded/sodded and slopes no steeper than 3H:1V to preserve mowing access.
- Q14: Referencing Q&A #3 on Addendum #2 – It is stated that no off-road dump trucks will be allowed but during our site visit it was stated that smaller dump trucks would be allowed if Contractor did not damage the existing dikes. Could the Contractor use smaller sized off-road dump trucks if dike damage is mitigated?
- A14: Smaller off-road dump trucks are allowed. The contractor shall be required to maintain the levee to pre-existing conditions during construction. This may include daily maintenance/grading to keep ruts from holding water on the crown of the levee. Any amount of truck use will create ruts within the levee and if not graded regularly will/can cause the integrity of the levee to degrade quickly especially if it rains. Access on this levee is difficult when wet. The allowance of small off-road trucks will enhance the wet access to the jobsites but will also increase the damage to the levee during wet periods. The District will be monitoring the condition of the levee daily during the prosecution of work.
- Q15: Given the bids will be submitted in electronic format please confirm they will still be publicly opened and read aloud at the bid opening location listed in the IFB.
- A15: Yes

Q16: Is this project sales tax exempt?

A16: Yes

Q17: Please confirm that the itemized portion of the bid form shall be lump sum prices for items #1-#6 for structures E-3, E-4 and E-6. Also, the total bid prices have been auto-filled with \$0 for each structure. Can you please re-issue the bid form with blanks for these cells.

A17: A revised bid form with blanks and a line item for rip/rap has been included as an attachment.

Q18: Since the Contractor will not be able to control award and notice to proceed dates, could the Owner please provide a total contract day performance period (notice to proceed to substantial completion) in lieu of obligating the Contractor to a finish date of September 30, 2019.

A18: The September 30, 2019 finish date corresponds with the District's end of fiscal year and would put the construction period at six (6) months. Barring unforeseen or extended wet weather, we feel this is an adequate timeframe for this construction. Extension of the contract expiration maybe considered on a as-needed basis depending on circumstances.

Q19: Since liquidated damages are 0.5% of the contract value, which based on the engineers estimate of \$585,255 equals \$2,926 per day, and given the fast-paced performance period of the project and potential delays for dike conditions, would the Owner be amenable to reducing the liquidated damage amount for this project?

A19: Reduction of the liquidated damage percentage will require review and approval from the District's Office of General Counsel. District staff will work with contractor to adjust the expiration date of the contract based on circumstances.

Q20: Referencing Pages C5, C7 and C9 of the plans. It appears that the cofferdam footprint is much larger for Structure E-3 in comparison to E-4 and E-5. Is there a particular reason for this and does the contractor have to follow the footprints shown on the plans or as required by his cofferdam engineer to safely execute the work?

A20: The design of the cofferdam is the contractor's responsibility. The configuration of the sheet piling is at the contractor's discretion and should be designed such that the site can be properly dewatered and safe to execute the work.

Q21: It is our understanding that suitable Owner furnished backfill materials exist north of the project sites and will be available to the Contractor for this project, so no Contractor imported fill is required. Please confirm this is correct and provide this location on a map and the allowable cut area and depth.

- A21: As discussed at the site visit on February 6, 2019, the District believes there is ample suitable fill in the remnant levee located along the north side of the S-256 flowway. The borrow area is approximately 850' long and width ranging from 15' at the top to 30' near the water. The top of the remnant levee is 3'-4' above the water level. The contractor may take material down to the water level. If borrow is needed from this area, the specific boundaries will be staked by District staff. The borrow area is approximately 0.2 miles from turn around 2. Any damage to the haul route during the borrow activity must be restored to existing or better condition. See Figure 1: General location of borrow in the Appendix section.
- Q22: Are the Owner furnished materials backfill considered FDOT Group 1 or Group 2? This is important for estimating lift thickness per FDOT specification 125-8.1.6.1
- A22: The District estimates that these materials are Group 2, and therefore the FDOT Specification suggests lift thickness of 6" around the pipes.
- Q23: Please provide further explanation on over-excavation. It is stated on page C3 – Earthwork, Section 4 that “Muck/Peat (PT) and/or soft sediments present at subgrade levels shall be completely removed”. Does “and soft sediments” include the thick sandy clay to clay (CL/CH) layer directly below the Muck/Peat (PT), and if so how deep must the Contractor over excavate below bottom of pipe before backfill can begin?
- A23: The geotechnical engineering reports indicate removal of Stratum 8 (as shown on the boring profile), thus this does not include the CL/CH layer. In areas where muck is located at and below the invert of the proposed pipe, the District recommends removal of at least two (2) additional feet of muck below the invert elevation. Contractor shall coordinate with the District inspector regarding limits of muck removal
- Q24: Please provide locations of upland disposal areas for unsuitable materials.
- A24: Unsuitable and/or excess material shall be disposed by spreading on the eastern slope of the C-40 levee in the immediate area of the plug. The disposal of material shall be place below the top of eastern slope and may not go beyond the existing toe of eastern slope. Images below show the general location of disposal areas for each plug. See Figure 2: General location of disposal area for unsuitable/excess material in the Appendix section
- Q25: It is our understanding that hp pilings and substructure beams are to be A36 plain carbon with no coating. The C-channel superstructure, handrails, toe plates and grating are to be aluminum. Please confirm.
- A25: The superstructure is all aluminum. The steel members should be coated. We need to add the following note to the drawings:

“PROTECTIVE COATING FOR STRUCTURAL STEEL SHALL BE AS MANUFACTURED BY SHERWIN-WILLIAMS, OR EQUAL, AS FOLLOWS:
 FIRST COAT (PRIMER): DURA-PLATE 235, 4-8 MILS DFT
 SECOND COAT: DURA-PLATE 235, 4-8 MILS DFT

THIRD (FINAL) COAT: DURA-PLATE 235, 4-8 MILS DFT
COLOR SHALL BE LIGHT GRAY. SURFACE PREPARATION AND APPLICATION SHALL BE IN
ACCORDANCE WITH THE PAINT MANUFACTURER SPECIFICATIONS.”

Q26: We respectfully request a 1-week extension to the bid date.

A26: In order to award this Contract at the March Board Meeting, the bid due date must remain Wednesday, February 20th at 2:00PM.

Q27: What are the loading requirements over the structure adjacent to the boat ramp and the structure currently under construction approximately 2 miles west of that entrance point?

A27: The District has been recommending a H20 loading for the bridge decks of the spillway structures.

NOTE: The Bid Due Date remains 2:00 p.m., Wednesday, February 20, 2019

Attachments:

Figure 1 General location of borrow

Figure 2 General location of disposal area for unsuitable fill

Cost Schedule, page 12, Revised Addendum 4

Please acknowledge receipt of this Addendum on the BID FORM provided in the bid package.

If you have any questions, please e-mail me at alucey@sjrwm.com.

APPENDIX

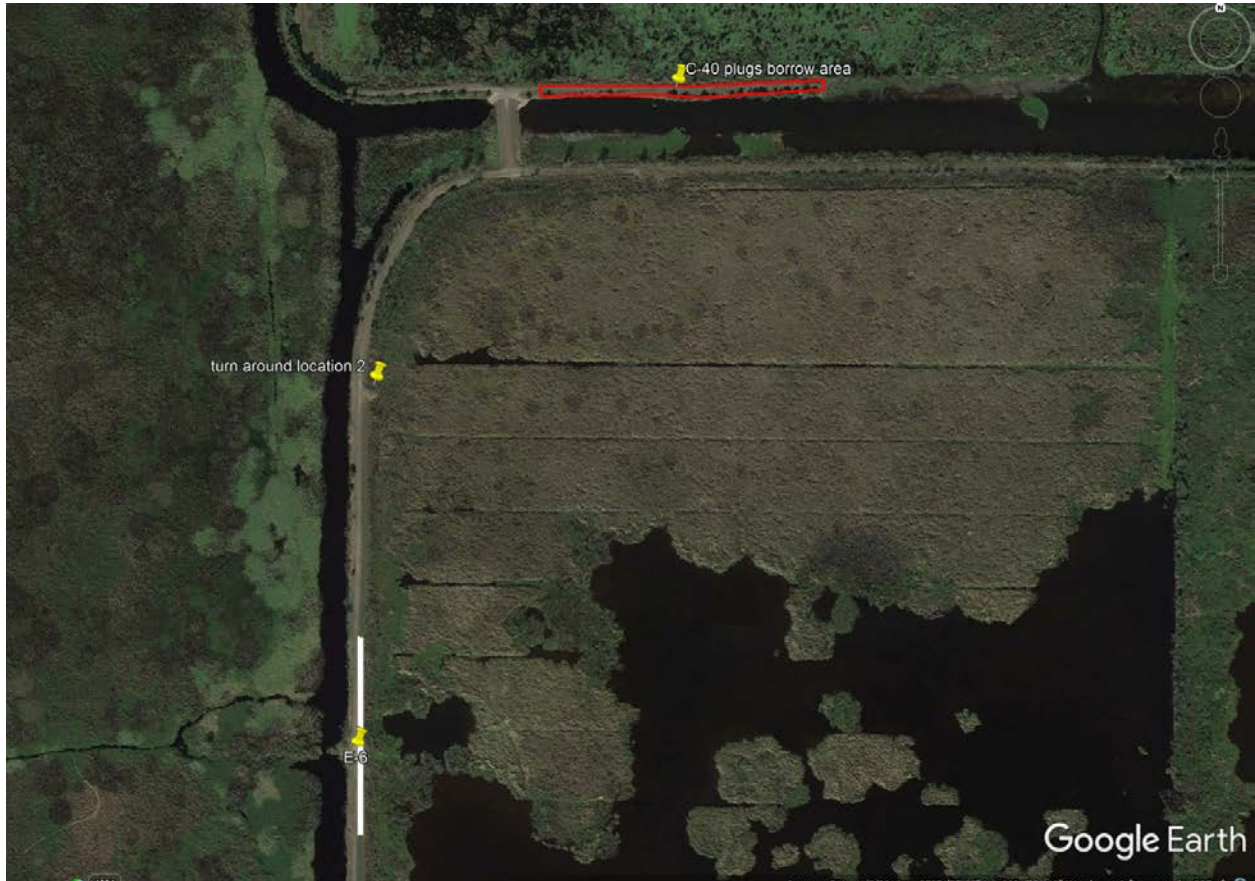


Figure 1: General location of borrow



Figure 2: General location of disposal area for unsuitable/excess material

COST SCHEDULE

Include this form in the response

Bid to be opened at 2:00 p.m., February 20, 2019

To: ST. JOHNS RIVER WATER MANAGEMENT DISTRICT

In accordance with the advertisement requesting bids for the St Johns Marsh Conservation Area Restoration - C-40 plugs enhancements, subject to the terms and conditions of the Agreement, the undersigned proposes to perform the Work for the price contained in the following schedule (fill in all blanks).

If said bid exceeds the estimated amount previously provided, the District expressly reserves the right to increase, decrease, or delete any class, item, or part of the Work, as may be determined by the District.

Respondents are reminded to refer to "PREPARATION AND ORGANIZATION OF BID DOCUMENTS" for information to be included with the bid package.

The bid will be awarded to the lowest responsive and responsible Respondent for items one through six .
RESPONDENTS MUST PROVIDE COSTS FOR ALL LISTED ITEMS.

Total Bid Cost: \$ _____

Total Bid Cost in words: _____

ITEM	E-3	E-4	E-6
mobilization/demobilization			
sheet pile coffer dam/dewatering			
excavation/backfill			
36" aluminum culvert			
36" aluminum sluice gate			
Aluminum walkway			
rip/rap			
TOTALS			

PROJECT TOTAL:

Cost schedule continued on the next page.