

ADDENDUM NO. 2
KINGMAN MUNICIPAL AIRPORT
RUNWAY 3-21 REHABILITATION PROJECT
FAA AIP NO.: 3-04-0021-TBD

In accordance with the FAA General Provisions, Subsection 20-15, Discrepancies and Omissions, the following revisions to the Plans and Specifications shall become a part of the Contract Documents and each bidder shall acknowledge receipt thereof on page PROPOSAL-18 of the Proposal Forms.

GENERAL

- ITEM 1:** Included in this addendum is **Attachment ‘A’**, Pre-Bid Meeting Minutes and Sign-In Sheet.
- ITEM 2:** Clarification: Proposals can be dropped off at the City Clerk’s Office during normal business hours of 7 a.m. – 6 p.m., Monday – Thursday.
Also, bidders will be able to attend the bid opening (to be held in the council chambers). Everyone will be asked to follow social distancing guidelines.

SPECIFICATIONS

- ITEM 3:** PROPOSAL FORM, page Proposal-4. Modify the quantity for Bid Alternate No. 2, Item 21 as follows: Cross out “20 EA” quantity and replace it with “**22 EA**”. All computations for L-858 Sign Panel Replacement. Remove and Replace Existing Sign Panels With New Sign Panels shall be based on the revised 22 EA quantity.
- ITEM 4:** FAA TECHNICAL PROVISIONS, after page TS P-620-6, insert **Attachment ‘B’** Item L-125 Airport Lighting Systems.
- ITEM 5:** Section 80 Execution and Progress, page GP 80-11, section 80-08 Failure to complete on time. In the table, under Allowed Construction Time cross out “30 Calendar Days” and replace it with “33 Calendar Days”
- ITEM 6:** ITEM P-401, Subsection 401-2.3 Asphalt binder. Delete the Asphalt Binder PG Plus Test Requirements table in its entirety.
- ITEM 7:** ITEM P-401, Subsection 401-4.4.1 Material transfer vehicle (MTV). Delete “Material transfer vehicles are not required.” and replace it with the following:

“Material transfer vehicles used to transfer the material from the hauling equipment to the paver, shall use a self-propelled, material transfer vehicle with a swing conveyor that can deliver material to the paver without making contact with the paver. The MTV shall be able to move back and forth between the hauling equipment and the paver providing material transfer to the paver, while allowing the paver to operate at a constant speed. The Material Transfer Vehicle will have remixing and storage capability to prevent physical and thermal segregation.”

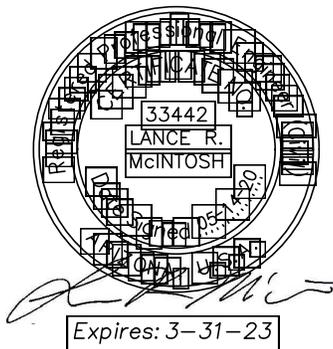
- ITEM 8:** CONSTRUCTION AND SAFETY PHASING PLAN (CSPP)
3.14 RUNWAY AND TAXIWAY VISUAL AIDS., Page 16,
a. General, cross out “Closed runway markings and lighting are not required.” and replace with “Closed runway markings and lighting are required. Lighted Closed Runway Markers shall be provided by, and maintained by the Contractor for the duration of the project.”
c. Lighting and visual NAVAIDs, Replace the entire paragraph with “All runway edge lights and visual NAVAIDs of runways closed to aircraft traffic shall be de-energized by the contractor for the duration of the runway closure.”

PLANS

- ITEM 9:** Replace sheet CM-101 with **Attachment ‘C’**.
ITEM 10: Replace sheet CM -102 with **Attachment ‘D’**.
ITEM 11: Replace sheet CM -103 with **Attachment ‘E’**.
ITEM 12: Replace sheet CM -104 with **Attachment ‘F’**.

ADDITIONAL QUESTIONS

- ITEM 13:** Question: Does this project include Herbicide?
Answer: No.



C&S ENGINEERS, INC.

ATTACHMENT 'A'

PRE-BID MEETING MINUTES FOR CONSTRUCTION OF RUNWAY 3-21 REHABILITATION PROJECT AT KINGMAN MUNICIPAL AIRPORT

I. INTRODUCTION

- A. This is the Pre-Bid Meeting for the Runway 3-21 Rehabilitation Project at Kingman Municipal Airport.
- B. Meeting Attendees.
All those in attendance for this meeting were asked to send an email containing attendees name, company, phone number, and email address to Rbehm@cscos.com - to be included on the sign-in sheet.

II. PROJECT DESCRIPTION

- A. General Description: This project is to construct the rehabilitation of Runway 3-21 which generally involves milling 1.5 inches of existing asphalt concrete pavement from the runway surface, the placement of 3 inches of new P-401 asphalt pavement, and the painting of interim and final runway markings and some connector taxiway paintstriping.

The project is being broken into bid alternates consisting of:

Base Bid – the first half of the runway reconstruction.

Bid Alternate 1 – the second half of the runway reconstruction.

Bid Alternate 2 – sign panel replacement.

- B. Special Items:
 - 1. P-401 FAA asphalt pavement
 - 2. CSPP Document in proposal – Be sure to read the CSPP carefully to understand responsibilities and complete the Safety Plan Compliance Document Certification.
 - 3. Lighted Runway X's - (4) must be provided by, and maintained by the contractor for the duration of the project.
 - 4. Flaggers required for active area crossings during Phase 1
 - 5. The Proposal Form (included in the contract specifications) needs to be completely filled out and accompanied with your 10% bid bond. Because this is a Federal AIP Project there are additional forms and certifications which will need to be filled out completely and signed, some of the forms require notarization. Be sure to list the addendum numbers in the Certification for Receipt of Addenda. Make sure all forms are completed and included with bid.

III. SAFETY AND OPERATIONAL REQUIREMENTS

- A. Safety during construction is the No. 1 priority for the protection of the Airport users, employees and the contractor's employees. The Contractor is responsible for safety during Construction on Airport Projects. Contractor to prepare proposal exercising all safety requirements and required barricades
 - 1. The Contractor is responsible for his employees and for compliance with OSHA standards, rules and regulations.
 - 2. Barricades must be placed prior to start of construction.

ATTACHMENT 'A'

3. No deviations of men and equipment from designated work areas and access routes will be allowed. The construction safety phasing plan (CSPP) must be followed as approved by the FAA. Workers must remain in the designated work areas at all times.
 4. Unless otherwise shown on the Plans or Specifications, under no circumstances will the contractor be allowed to travel on or across active airport operating surfaces.
- B. Prevention of Foreign Object Damage (FOD) is extremely important as ingestion of material into jet engines and prop-wash will cause extensive damage.
1. Work areas must be kept clean and pavements must be continually swept to prevent the accumulation of dirt and debris, with particular attention payed to the crossings of active areas.
 2. Dust control measures must be implemented by the Contractor.
 3. Trash and debris must be placed in appropriate containers.
- C. Security
1. If a gate is left open, the contractor must post a guard to ensure no unauthorized entry.
 2. Open, unattended gates are subject to fines by the FAA of up to \$10,000.00. Fines incurred by the Owner will be passed on to and become the responsibility of the contractor.
- D. Construction safety phasing plan (CSPP)
1. Phasing plan description
 - a. Base Bid and Bid Alternate 1 are being broken out into two phases to allow Runway 17-35 to remain open and accessible for the majority of the project. Please review the CSPP carefully to understand phasing.

IV. TIME SCHEDULE & LIQUIDATED DAMAGES

- A. Phase 1 - 33 calendar days & \$1,000 per day
- B. Phase 2 - 48 hours & \$1,000 per hour

V. DBE & CIVIL GOALS

- A. DBE participation goal for this contract is 10%. These are goals only.
- C. EEO participation goals for this contract are 19.6% minority and 6.9% female. Goals must be met unless otherwise approved in writing.
- D. Other civil rights requirements are as designated in the AC 150/5100-15 (available at faa.gov).
- E. Contractor can reference the ADOT website for firms that are qualified DBE within the state. This is a goal and if it is not met, the contractor will need to provide evidence of an effort to meet or exceed the goal.

VI. LABOR REQUIREMENTS

- A. Contractor must pay the higher prevailing wage rate as a minimum in accordance with the current Federal Wage Rate Schedules.
- B. Other labor requirements are as contained in the AC 150/5100-6 (available at faa.gov)

ATTACHMENT 'A'

- C. Wage rate interviews will be conducted as part of this project.

VII. ADDENDA

- A. Any changes or modifications identified at this meeting will be made by addendum to all prospective bidders.
- B. Contractor's Responsibility to Monitor Plan Room for Addendums (individual notification may not be made).
- C. Other questions raised after this meeting must be submitted by email to C&S Attn: Lance McIntosh (lmcintosh@cscos.com) to be considered for future addendum.
- D. No interpretation of the meaning of the Contract Documents, Contract Drawings or other portions of the Contract will be made orally. Every request for such interpretation must be in writing or email and addressed to Lance McIntosh, P.E. (lmcintosh@cscos.com) C&S Engineers, Inc., located at 2575 E. Camelback Rd., Suite 740, AZ 85016, and to be given consideration must be received at the above address at least seven (7) days prior to the date fixed for opening of bids. Any supplemental instructions will be in the form of written addenda, which, when issued, will be sent and or posted not later than twenty-four (24) hours prior to the date fixed for the opening of bids. All addenda so issued shall become Part of the Contract and acknowledged by Bidder. Failure of any Bidder to receive any such addenda or interpretation shall not relieve said Bidder from any obligation under his bid submitted.

VIII. QUESTIONS & GENERAL DISCUSSION

- A. Bids are due Thursday, May 21, 2020 at 3:00 P.M. AZ Time. at the City Clerk's Office, 310 N Fourth St. Kingman, AZ 86401. Bids will be read aloud at the City Clerk's Office.
- B. Due to current "social distancing" requirements, all proposals shall be submitted via mail, courier, or shipping service. Each proposal submitted shall be placed in a sealed envelope plainly marked with the project number, location of airport, and name and business address of the bidder on the outside and then enclosed in an additional envelope for mailing or shipping.
 - * clarification was provided by the City after the meeting: proposals can be dropped off at the City Clerk's Office between the hours of 7 a.m. – 6 p.m., Monday – Thursday.
- C. Question: What is the time frame for work the Phase 2 areas?
Answer: 48 hours.
- D. Discussion about time schedule: for clarification the time allowed for completion of the project will be 30 calendar days regardless of how it is awarded. There will be a time suspension to allow for pavement cure before final paint marking, and also to allow for potential delays in sign panel delivery.
The attendees were asked if 30 calendar days was an adequate amount of time to complete this project – no one expressed concern.
- E. Question: Does the 30 day schedule include the time for P-401 testing?
Answer: yes

ATTACHMENT 'A'

- F. Question: Are the three days for final paint striping included in the 30 days or are they additional.
Answer: They were intended to be included, but 3 days can be added as additional for final paint striping .
- G. Question: When do you anticipate NTP?
Answer: possibly August.
- H. Question: Is a Material Transfer Vehicle required for P-401 on this project?
Answer: A material transfer vehicle is required. The P-401 Specification will be modified via addendum.
- I. Question: Is there a restriction on Mobilization %?
Answer: Mobilization shall be limited to 10 percent of the total project cost.
- J. Question: Is there an engineer's estimate available?
Answer: yes, the engineers estimate is between \$3m and \$3.9m.
- K. Question: Is a site visit possible (to look at cracks)?
Answer: yes, however everyone will be required to practice social distancing and travel in their own vehicles, under escort. The site visit has been scheduled for Monday, May 18th at noon. Everyone interested in a site visit is advised to meet in the airport administration building parking lot. 7000 Flightline Drive, Kingman, AZ 86401.
- L. Concern was expressed about trusting the mail or shipping service deliver the proposals on time and the question was asked if it is possible to electronically submit the proposal?
Answer: After the meeting we were informed by the City that proposals can be dropped off at the City Clerk's Office during normal business hours of 7 a.m. – 6 p.m., Monday – Thursday.
- M. Question: Will the bid opening be live? How will people be able to view the bid opening?
Answer: yes the bid opening will be live. After the meeting we were informed by the City that bidders will be able to attend the bid opening to be held in the council chambers. Everyone will be asked to follow social distancing guidelines.
- N. Question: Will there be a clause for price adjustment of bituminous materials?
Answer: No, the FAA does not allow.
- O. Question: Is there a location at the airport where a QC lab trailer can be setup?
Answer: Yes, there is a location adjacent to the airport administration building. The contractor will be responsible for securing electrical power.
- P. Clarification: the millings are to remain on site, location to be shown in a future addendum.
- Q. Question: Is there an alternative to the P-401 asphalt pavement analyzer specification?
Answer: The P-401 Specification will be modified to delete this requirement.

ATTACHMENT 'A'



ATTENDEES LIST
KINGMAN MUNICIPAL AIRPORT
RUNWAY 3-21 REHABILITATION PROJECT
PRE-BID MEETING

May 12, 2020

NAME	AFFILIATION	E-MAIL ADDRESS	PHONE
Lance McIntosh	C & S Engineers	lmcintosh@cscos.com	602-422-8620
Randy Behm	C & S Engineers	rbehm@cscos.com	623-512-7982
Steve Johnston	City of Kingman	sjohnston@cityofkingman.gov	928-565-1420
Andre Roen	McCormick Construction Company	aroen@nicnd.com	928-763-2272
Sarah Gray	FNF Construction, Inc.	sgray@fnfinc.com	480-929-6747
Amy Rosar	FNF Construction, Inc.	arosar@fnfinc.com	602-318-9332
Michael Fann	Fann Contracting, Inc.	mfann@fanncontracting.com	928-778-0170
Nate Gorrocino	Sunland Asphalt	ngorrocino@sunlandasphalt.com	602-288-5567

Item L-125 Airport Lighting Systems

DESCRIPTION

125-1.1 This item shall consist of runway and taxiway edge lighting systems, runway centerline and touchdown zone lighting systems, taxiway lighting systems, runway and taxiway guidance signs, runway distance remaining signs, and economy approach lighting aids furnished and installed in accordance with this specification, referenced specifications and applicable Advisory Circulars. The lighting systems, signs and approach lighting aids shall be installed at the locations and in accordance with the dimensions, design, and details shown in the Contract Drawings. This item shall include all excavation, backfilling, and restoration of surfaces and the furnishing of all equipment, materials, services, and incidentals necessary to place the systems in operation as completed units to the satisfaction of the Engineer.

Where indicated on the Contract Drawings, this item shall also include the removal, modification, or relocation of existing lights, signs and approach lighting aids.

125-1.2 Additional details pertaining to a specific systems covered in this specification are contained in the Advisory Circulars 150/5340-30, Design and Installation Details for Airport Visual Aids and 150/5340-18, Standards for Airport Sign Systems.

EQUIPMENT AND MATERIALS

125-2.1 GENERAL.

- a. Airport lighting equipment and materials covered by Federal Aviation Administration Specifications shall be certified in accordance with Advisory Circular 150/5345-53, Airport Lighting Equipment Certification Program latest edition.
- b. All other equipment and materials covered by other referenced Specifications shall be subject to acceptance through the manufacturer's certification of compliance with the applicable Specifications.
- c. Lists of the equipment and materials required for a particular system are contained in the applicable Advisory Circulars.
- d. Contractor shall have experience installing airport lights, signs and approach lighting aids or the manufacturer's representative shall be on-site during installation. Experience shall be documented by the Contractor by providing the Engineer with a list of three previous installations of the equipment being installed.

125-2.2 RUNWAY AND TAXIWAY SIGNS. Runway and Taxiway Signs shall conform to the requirements of 150/5345-44 and be listed in the current version of Appendix 3 to AC 150/5345-53.

- a. L-858 Airfield Guidance Signs - -as Scheduled

CONSTRUCTION METHODS

125-3.1 GENERAL. The installation and testing details for the systems shall be as specified in the applicable Advisory Circulars, manufacturer's requirements and as shown on the Contract Drawings or directed by the Engineer.

125-3.2 REPLACING GUIDANCE SIGN PANELS. The airport sign panels shall be installed at the approximate location indicated in the Contract Drawings. The exact location shall be as directed by the RPR. The sign panels shall be installed in accordance with the Contract Drawings and the manufacturer's recommendations.

Contractor shall have experience installing guidance sign panels, or the manufacturer's representative shall be onsite during installation. Experience shall be documented by the Contractor by providing the Engineer with a list of three previous installations of the equipment being installed.

METHOD OF MEASUREMENT

125-4.1 The quantity of replacement guidance sign panels to be paid for under this item shall be the number of sign panels replaced, ready for operation and accepted by the Engineer. This item shall also include replacement of sign dividers.

BASIS OF PAYMENT

125-5.1 Payment will be made at the Contract unit price for each replacement sign panel, regardless of size or type, furnished and installed in place by the Contractor and accepted by the RPR. This price shall be full compensation for furnishing all materials and for all preparation, assembly and installation of these materials and for all labor, equipment, tools and incidentals necessary to complete this item.

Payment will be made under:

Item L-125-5.1	L-858 Sign Panel Replacement. Remove and Replace Existing Sign Panels with New Sign Panels.	– per each
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SUBMITTALS AND CERTIFICATIONS

125-6.1 The following "Shop and Setting Drawings", "Working Drawings", Catalogue Data" and "Certifications" shall be submitted for review:

- Evidence of Contractor's experience installing sign panels.
- Catalogue Data for each type of Airport lighting equipment specified, including all components which are used to assemble the equipment.
- Certification for each type of Airport lighting equipment specified in accordance with AC 150/5345-53.

FEDERAL SPECIFICATIONS

Number	Title
WW-C-581	Conduit, Metal Rigid; and Coupling, Elbow; and Nipple, Electrical Conduit: Zinc-coated.

FAA SPECIFICATIONS

Number	Title
AC 150/5340-30	Design and Installation Details for Airport Visual Aids.
AC 150/5340-18	Taxiway Guidance Sign System.
AC 150/5345-53	Airport Lighting Equipment Certification Program.
AC 150/5345-42	FAA Specification L-867, Airport Light Bases, Transformer Housing and Junction Boxes.
AC 150/5345-44	Specification for Runway and Taxiway Signs.

END OF ITEM L-125

SIGN DATA							
SIGN #	MANUFACTURER	SIZE	STYLE	CLASS	TYPE	SOUTH FACING PANEL(S)	NORTH FACING PANEL(S)
1	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	NOT REPLACING	A D4 B ↓
2	LUMA CURVE	5	2 (4.8-6.6A)	2	(DISTANCE REMAINING) L-858B	5	1

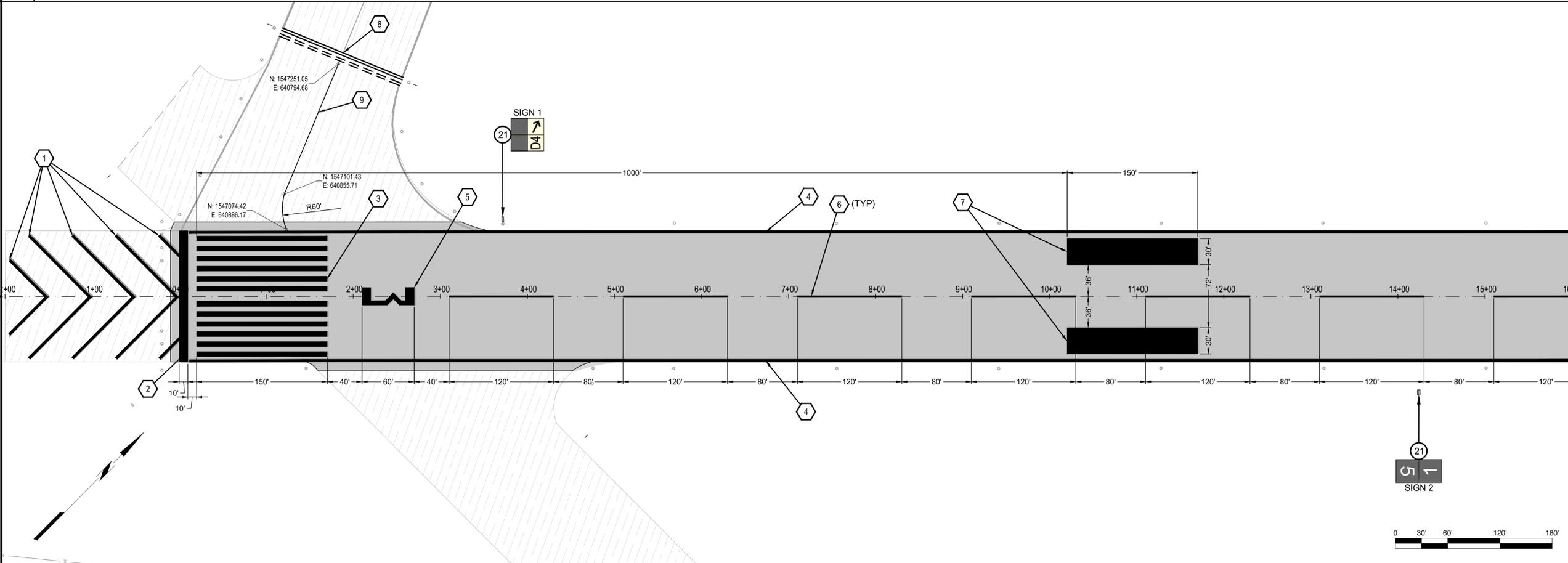


C&S Engineers, Inc.
2575 East Camelback Road
Suite 740
Phoenix, Arizona 85016
Phone: 602-997-7536
Fax: 602-997-7592
www.cscos.com



RUNWAY 3-21 REHABILITATION PROJECT
KINGMAN MUNICIPAL AIRPORT
CITY OF KINGMAN
KINGMAN, ARIZONA

D1 SIGN PANEL REPLACEMENT DETAILS
NOT TO SCALE



B1 MARKING PLAN
SCALE: 1"=50'

(21) PROVIDE AND INSTALL REPLACEMENT SIGN PANELS, PER SIGN PANEL REPLACEMENT DETAILS D1, THIS SHEET.

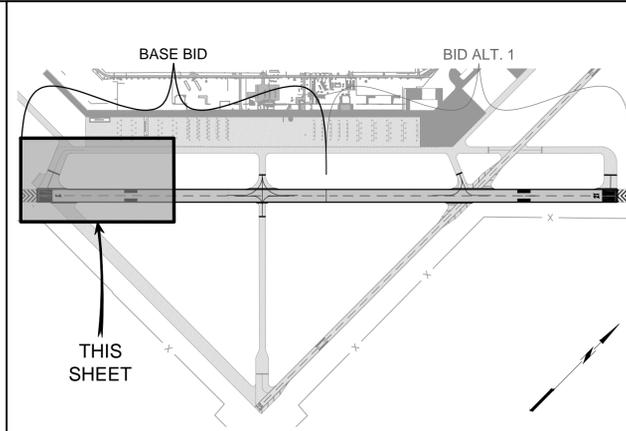
INTERIM MARKINGS SHALL BE APPLIED AT HALF RATE, WITHOUT GLASS BEADS PRIOR TO OPENING TO AIRCRAFT TRAFFIC.

FINAL MARKINGS SHALL BE APPLIED AT FULL RATE WITH GLASS BEADS, 30 DAYS AFTER FINAL DAY OF SURFACE TREATMENT.

INTERIM MARKINGS CONSIST OF:

- THRESHOLD BAR (CONSTRUCTION NOTE 2)
- RUNWAY DESIGNATOR MARKING (CONSTRUCTION NOTE 5)
- RUNWAY CENTERLINE MARKING (CONSTRUCTION NOTE 6)

- 1 RE-STRIPED EXISTING CHEVRON MARKINGS (YELLOW REFLECTORIZED) IN EXISTING LOCATIONS
- 2 PAINT WHITE REFLECTORIZED THRESHOLD BAR PER DETAIL A3, SHEET CD-501
- 3 PAINT WHITE REFLECTORIZED THRESHOLD MARKINGS PER DETAIL B1, SHEET CD-501
- 4 PAINT WHITE REFLECTORIZED RUNWAY EDGE MARKING PER DETAIL B2, SHEET CD-501
- 5 PAINT WHITE REFLECTORIZED RUNWAY DESIGNATOR MARKING PER DETAIL A4, SHEET CD-501
- 6 PAINT WHITE REFLECTORIZED RUNWAY CENTERLINE MARKING PER DETAIL A2, SHEET CD-501
- 7 PAINT WHITE REFLECTORIZED RUNWAY AIMING POINT MARKINGS PER DETAIL B3, SHEET CD-501
- 8 RE-PAINT YELLOW REFLECTORIZED HOLDING POSITION MARKING PER DETAIL A1, SHEET CD-501
- 9 PAINT YELLOW REFLECTORIZED TAXIWAY CENTERLINE MARKING PER DETAILS B4, AND A2, SHEET CD-501



A1 SIGN PANEL REPLACEMENT CONSTRUCTION NOTES
NOT TO SCALE

A2 INTERIM MARKING NOTES
NOT TO SCALE

A3 MARKING CONSTRUCTION NOTES
NOT TO SCALE

A4 KEYMAP
SCALE: 1"=1,000'

MARK	DATE	DESCRIPTION

PROJECT NO:	K32007001
DATE:	MAY 2020
DRAWN BY:	NGM
DESIGNED BY:	NGM
CHECKED BY:	LRM

MARKING PLAN (BASE BID) & SIGN PANEL REPLACEMENT PLAN (BID ALT. 2)

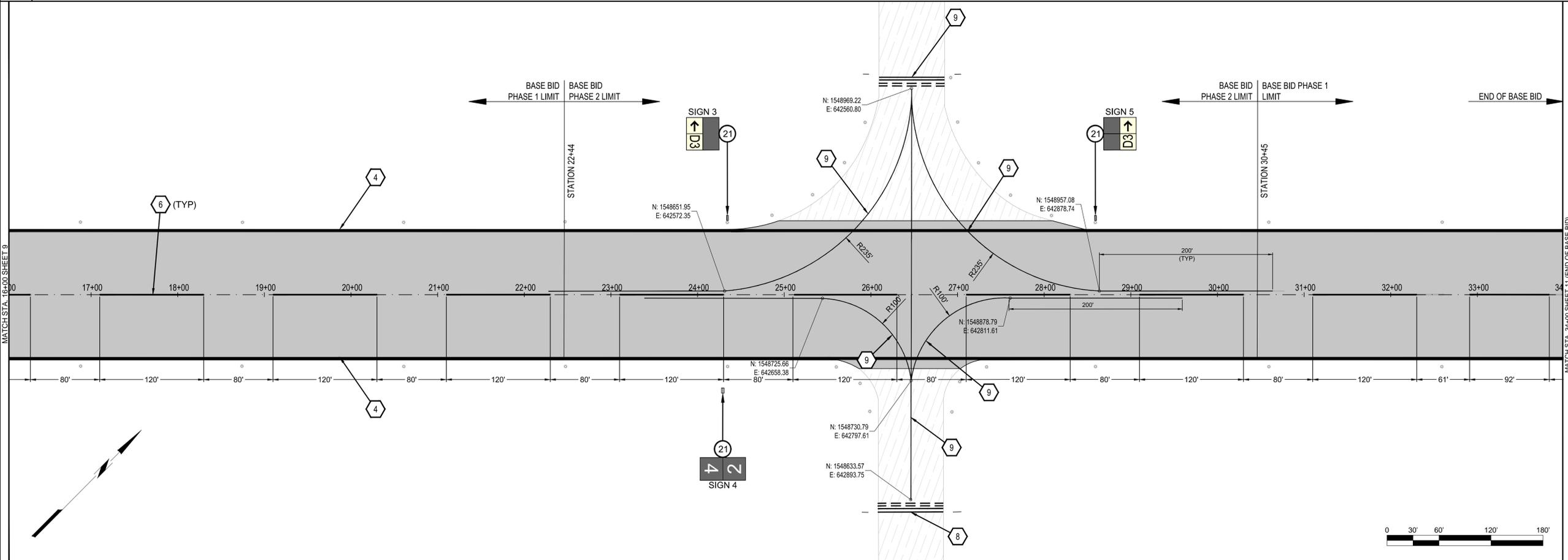
CM-101

9 of 13

May 14, 2020 - 11:25am
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SIGN DATA									
SIGN #	MANUFACTURER	SIZE	STYLE	CLASS	TYPE	SOUTH FACING PANEL(S)		NORTH FACING PANEL(S)	
						A	B	A	B
3	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	←	D3	NOT REPLACING	
4	LUMA CURVE	5	2 (4.8-6.6A)	2	(DISTANCE REMAINING) L-858B	4		2	
5	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	NOT REPLACING		D3	→

D1 SIGN PANEL REPLACEMENT DETAILS
NOT TO SCALE



B1 MARKING PLAN
SCALE: 1"=50'

(21) PROVIDE AND INSTALL REPLACEMENT SIGN PANELS, PER SIGN PANEL REPLACEMENT DETAILS D1, THIS SHEET.

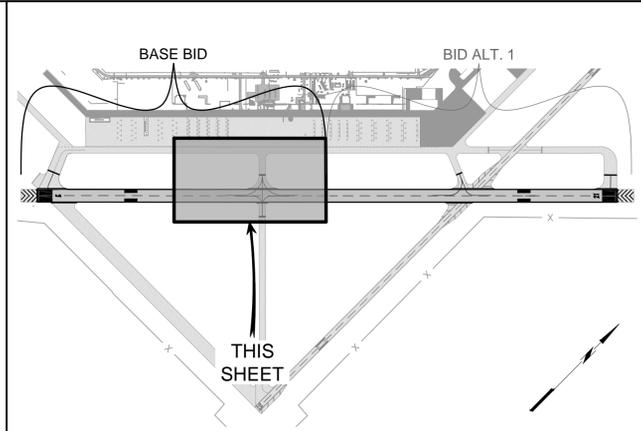
INTERIM MARKINGS SHALL BE APPLIED AT HALF RATE, WITHOUT GLASS BEADS PRIOR TO OPENING TO AIRCRAFT TRAFFIC.

FINAL MARKINGS SHALL BE APPLIED AT FULL RATE WITH GLASS BEADS, 30 DAYS AFTER FINAL DAY OF SURFACE TREATMENT.

INTERIM MARKINGS CONSIST OF:

- THRESHOLD BAR (CONSTRUCTION NOTE 2)
- RUNWAY DESIGNATOR MARKING (CONSTRUCTION NOTE 5)
- RUNWAY CENTERLINE MARKING (CONSTRUCTION NOTE 6)

- 4 PAINT WHITE REFLECTORIZED RUNWAY EDGE MARKING PER DETAIL B2, SHEET CD-501
- 6 PAINT WHITE REFLECTORIZED RUNWAY CENTERLINE MARKING PER DETAIL A2, SHEET CD-501
- 8 RE-PAINT YELLOW REFLECTORIZED HOLDING POSITION MARKING PER DETAIL A1, SHEET CD-501
- 9 PAINT YELLOW REFLECTORIZED TAXIWAY CENTERLINE MARKING PER DETAILS B4, AND A2, SHEET CD-501



A1 SIGN PANEL REPLACEMENT CONSTRUCTION NOTES
NOT TO SCALE

A2 INTERIM MARKING NOTES
NOT TO SCALE

A3 MARKING CONSTRUCTION NOTES
NOT TO SCALE

A4 KEYMAP
SCALE: 1"=1,000'



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RUNWAY 3-21 REHABILITATION PROJECT
KINGMAN MUNICIPAL AIRPORT
CITY OF KINGMAN
KINGMAN, ARIZONA

MARK	DATE	DESCRIPTION
Δ	5/14/2020	ADDENDUM No. 1
REVISIONS		
PROJECT NO: K32007001		
DATE: MAY 2020		
DRAWN BY: NGM		
DESIGNED BY: NGM		
CHECKED BY: LRM		

MARKING PLAN (BASE BID) & SIGN PANEL REPLACEMENT PLAN (BID ALT. 2)

CM-102

May 14, 2020 - 11:24am F:\Project\A32 - Kingman Airport, AZ\K32007001 - RW 3-21 Rehab\Design\CADD\Sheet Files\K32007001_09_CM102_MARKING & SIGN PLAN.dwg

SIGN DATA							
SIGN #	MANUFACTURER	SIZE	STYLE	CLASS	TYPE	SOUTH FACING PANEL(S)	NORTH FACING PANEL(S)
6	LUMA CURVE	5	2 (4.8-6.6A)	2	(DISTANCE REMAINING) L-858B	3	3
7	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	A ↙ B D2	NOT REPLACING
8	LUMA CURVE	5	2 (4.8-6.6A)	2	(DISTANCE REMAINING) L-858B	2	4

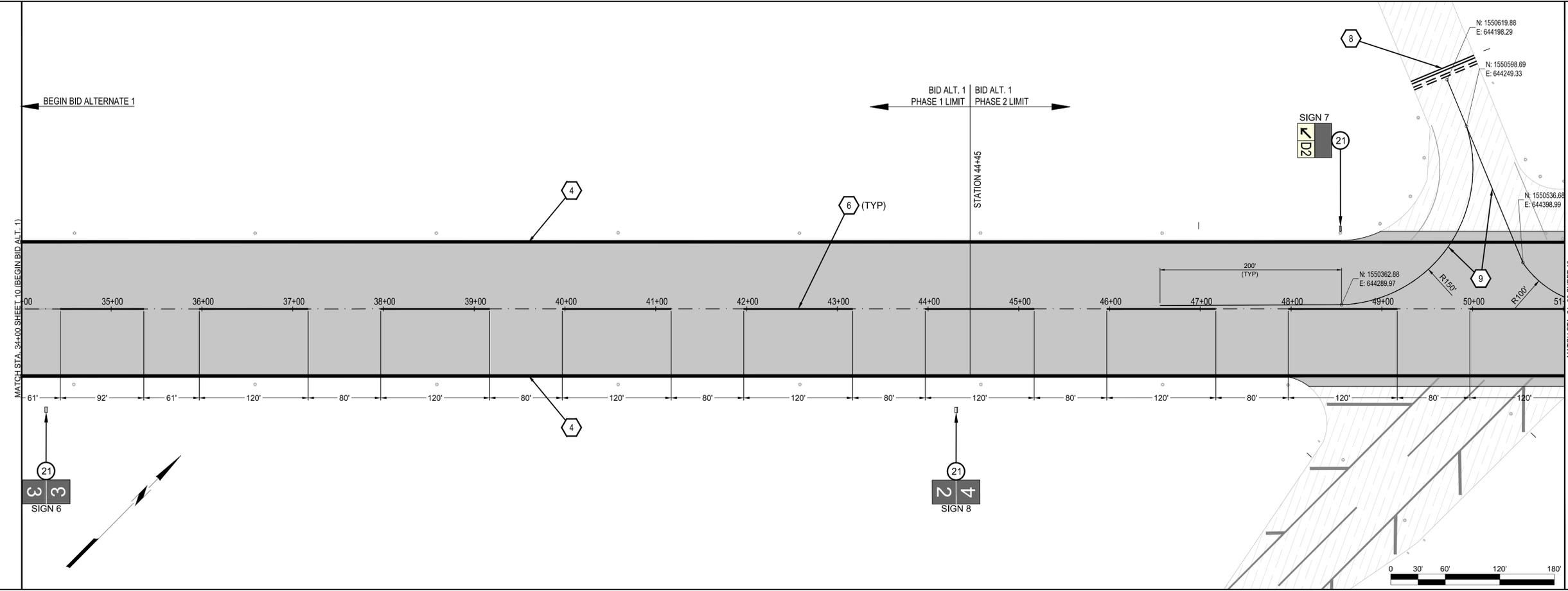


C&S Engineers, Inc.
2575 East Camelback Road
Suite 740
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Phone: 602-997-7536
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KINGMAN MUNICIPAL AIRPORT
CITY OF KINGMAN
KINGMAN, ARIZONA

D1 SIGN PANEL REPLACEMENT DETAILS
NOT TO SCALE



B1 MARKING PLAN
SCALE: 1"=50'

(21) PROVIDE AND INSTALL REPLACEMENT SIGN PANELS, PER SIGN PANEL REPLACEMENT DETAILS D1, THIS SHEET.

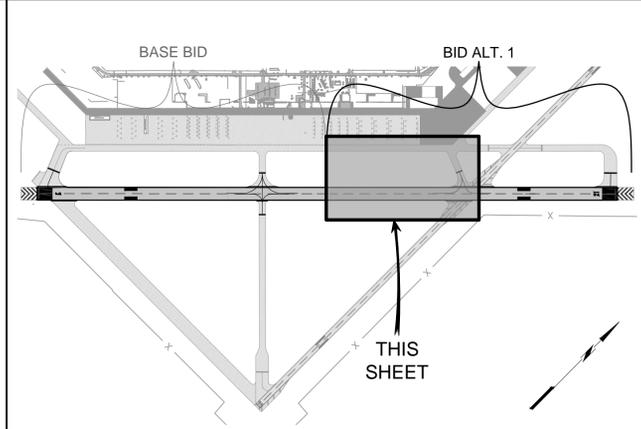
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- RUNWAY CENTERLINE MARKING (CONSTRUCTION NOTE 6)

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- (6) PAINT WHITE REFLECTORIZED RUNWAY CENTERLINE MARKING PER DETAIL A2, SHEET CD-501
- (8) RE-PAINT YELLOW REFLECTORIZED HOLDING POSITION MARKING PER DETAIL A1, SHEET CD-501
- (9) PAINT YELLOW REFLECTORIZED TAXIWAY CENTERLINE MARKING PER DETAILS B4, AND A2, SHEET CD-501



A1 SIGN PANEL REPLACEMENT CONSTRUCTION NOTES
NOT TO SCALE

A2 INTERIM MARKING NOTES
NOT TO SCALE

A3 MARKING CONSTRUCTION NOTES
NOT TO SCALE

A4 KEYMAP
SCALE: 1"=1,000'

MARK	DATE	DESCRIPTION
ADDENDUM No. 1	5/14/2020	
REVISIONS		
PROJECT NO:	K32007001	
DATE:	MAY 2020	
DRAWN BY:	NGM	
DESIGNED BY:	NGM	
CHECKED BY:	LRM	

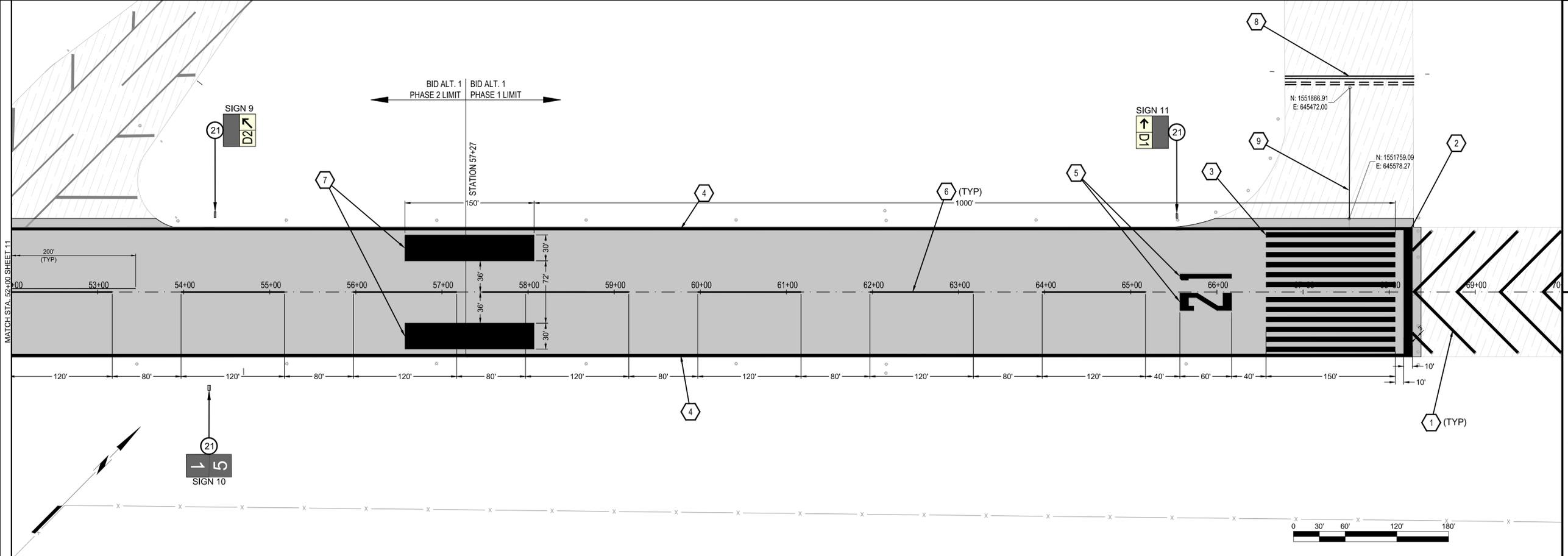
MARKING PLAN (BID ALT. 1) & SIGN PANEL REPLACEMENT PLAN (BID ALT. 2)

CM-103

May 14, 2020 - 11:23am
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SIGN DATA							
SIGN #	MANUFACTURER	SIZE	STYLE	CLASS	TYPE	SOUTH FACING PANEL(S)	NORTH FACING PANEL(S)
9	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	NOT REPLACING	A D2 B ↗
10	LUMA CURVE	5	2 (4.8-6.6A)	2	(DISTANCE REMAINING) L-858B	1	5
11	LUMA CURVE	1	2 (3)	2	(DIRECTION) L-858Y	A ← B D1	NOT REPLACING

D1 SIGN PANEL REPLACEMENT DETAILS
NOT TO SCALE



B1 MARKING PLAN
SCALE: 1"=50'

(21) PROVIDE AND INSTALL REPLACEMENT SIGN PANELS, PER SIGN PANEL REPLACEMENT DETAILS D1, THIS SHEET.

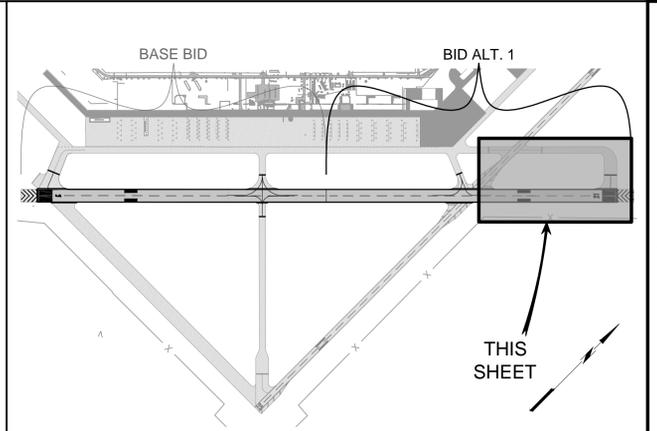
INTERIM MARKINGS SHALL BE APPLIED AT HALF RATE, WITHOUT GLASS BEADS PRIOR TO OPENING TO AIRCRAFT TRAFFIC.

FINAL MARKINGS SHALL BE APPLIED AT FULL RATE WITH GLASS BEADS, 30 DAYS AFTER FINAL DAY OF SURFACE TREATMENT.

INTERIM MARKINGS CONSIST OF:

- THRESHOLD BAR (CONSTRUCTION NOTE 2)
- RUNWAY DESIGNATOR MARKING (CONSTRUCTION NOTE 5)
- RUNWAY CENTERLINE MARKING (CONSTRUCTION NOTE 6)

- 1 RE-STRIPING EXISTING CHEVRON MARKINGS (YELLOW REFLECTORIZED) IN EXISTING LOCATIONS
- 2 PAINT WHITE REFLECTORIZED THRESHOLD BAR PER DETAIL A3, SHEET CD-501
- 3 PAINT WHITE REFLECTORIZED THRESHOLD MARKINGS PER DETAIL B1, SHEET CD-501
- 4 PAINT WHITE REFLECTORIZED RUNWAY EDGE MARKING PER DETAIL B2, SHEET CD-501
- 5 PAINT WHITE REFLECTORIZED RUNWAY DESIGNATOR MARKING PER DETAIL A4, SHEET CD-501
- 6 PAINT WHITE REFLECTORIZED RUNWAY CENTERLINE MARKING PER DETAIL A2, SHEET CD-501
- 7 PAINT WHITE REFLECTORIZED RUNWAY AIMING POINT MARKINGS PER DETAIL B3, SHEET CD-501
- 8 RE-PAINT YELLOW REFLECTORIZED HOLDING POSITION MARKING PER DETAIL A1, SHEET CD-501
- 9 PAINT YELLOW REFLECTORIZED TAXIWAY CENTERLINE MARKING PER DETAILS B4, AND A2, SHEET CD-501



A1 SIGN PANEL REPLACEMENT CONSTRUCTION NOTES
NOT TO SCALE

A2 INTERIM MARKING NOTES
NOT TO SCALE

A3 MARKING CONSTRUCTION NOTES
NOT TO SCALE

A4 KEYMAP
SCALE: 1"=1,000'

C&S COMPANIES
C&S Engineers, Inc.
2575 East Camelback Road
Suite 740
Phoenix, Arizona 85016
Phone: 602-997-7536
Fax: 602-997-7592
www.cscos.com



RUNWAY 3-21 REHABILITATION PROJECT
KINGMAN MUNICIPAL AIRPORT
CITY OF KINGMAN
KINGMAN, ARIZONA

MARK	DATE	DESCRIPTION
ADDENDUM No. 1	5/14/2020	

MARKING PLAN (BID ALT. 1) & SIGN PANEL REPLACEMENT PLAN (BID ALT. 2)

CM-104
12 of 13

May 14, 2020 - 11:22am
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