

- SCOPE OF WORK
- 1) REMOVE EXISTING CONCRETE PADS AND ASPHALT FROM OLD SHED AREA.
 - 2) BOX OUT 13.5 INCHES FROM PROPOSED FINISH GRADE, PROVIDE NEW LANDSCAPE AREAS.
 - 3) BOX OUT NEW PAVEMENT AREA 13.5 INCHES ON EASTERLY SIDE NEAR GOLFVIEW ROAD
 - 4) PUT DOWN 12 INCHES OF COMPACTED LIMEROCK OR SHELLROCK IN TWO 6 INCH LIFTS. 95% DENSITY, AASHTO T-180 LIMEROCK BEARING RATIO PAVE WITH 1.5 INCHES OF SP-9.5 HOT ASPHALT.
- GEO-TECH TESTING REQUIRED IN ACCORDANCE WITH HIGHLANDS COUNTY TECHNICAL STANDARDS MANUAL, SECTION TWO - TRANSPORTATION SYSTEM DESIGN AND CONSTRUCTION STANDARDS - SECTION 02.207 INSPECTIONS, FIELD MEASUREMENTS AND LAB TEST.
- 5) POUR NEW CONCRETE AREA BY GOLF CART WASH AREA. 6" STABILIZED SUB-BASE TO 95% DENSITY, AASHTO T-180 4" CONCRETE, 3000 PSI W/ 6x6 W1.4xW1.4 W.W.M. 1/8" WIDE x 1" DEEP EXPANSION JOINT, CUT 10' O.C.
- GEO-TECH TESTING REQUIRED IN ACCORDANCE WITH HIGHLANDS COUNTY TECHNICAL STANDARDS MANUAL, SECTION TWO - TRANSPORTATION SYSTEM DESIGN AND CONSTRUCTION STANDARDS - SECTION 02.207 INSPECTIONS, FIELD MEASUREMENTS AND LAB TEST.
- 6) INSTALL WASHED STONE BORDER AREAS AS SHOWN
 - 7) INSTALL SOD IN ALL NEW LANDSCAPE AREAS AS WELL AS ALL DISTURBED AREAS
 - 8) STRIPE PARKING LOT AS SHOWN.

CREATING 3,580 SQUARE FEET OF NEW LANDSCAPE AREAS. ADDING 2,000 SQUARE FEET OF OF ADDITIONAL ASPHALT AND CONCRETE AREAS. THIS YIELDS A NET REDUCTION OF 1,580 SQUARE FEET OF PAVEMENT. (3,580 - 2,000 = 1,580 SQ. FT. OF NEW GREEN AREA)

THE TWO (2) PROPOSED GOLF CART BUILDINGS WILL BE REPLACING EXISTING BUILDINGS AND EXISTING ASPHALT.

QUANTITIES SHOWN ARE ENGINEERS ESTIMATES ONLY
 CONTRACTOR SHALL DO THEIR OWN TAKEOFF AND/OR MEASUREMENTS FOR THE QUANTITIES USED IN THE BIDDING. THE CITY DESIRES A LUMP SUM BID FOR EACH TASK AND THE JOB AS A WHOLE.

DATE	REMARK	DR	CH
29-AUG-18	BID DOCUMENT	RDP	

CERTIFICATE OF AUTHORIZATION # 6684
 ROGER DALE POLSTON P.E. # 33222
 MARVIN LUTHER WOLFE P.E. # 48030

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 CIVIL ENGINEERING CONSULTANTS
 2925 KENILWORTH BLVD., SEBRING, FLORIDA, 33870
 888-365-5564 PHONE - 888-365-5462 FAX

CADDY SHACK PARKING
 CITY OF SEBRING
 SITE DEVELOPMENT PLANS
 SCOPE OF WORK HIGHLIGHTED AREAS

ENGINEER JOB # **18022**

DRAWING SCALE
1"=20'
 SHEET
 1 OF 1

PLANS OF PROPOSED
**CITY OF SEBRING
 PARKING LOT RESTORATION
 CADDY SHACK / SEBRING MUNICIPAL GOLF COURSE
 HIGHLANDS COUNTY, FLORIDA**

DATE OF PREPARATION: 10-AUGUST-2018
 DEVELOPMENT: CITY OF SEBRING PROPOSED PARKING LOT RENOVATION
 SEC. 36, TWP. 34E, RGE. 28E
 OWNER: CITY OF SEBRING
 368 SOUTH COMMERCE AVE.
 SEBRING, FL 33870
 863-471-5100
 ENGINEER: ROGER DALE POLSTON, P.E.
 POLSTON ENGINEERING, INC.
 2925 KENILWORTH BOULEVARD
 SEBRING, FL 33870
 (863) 385-5564
 (863) 385-2462 FAX
 SURVEYOR: GERMAINE SURVEYING, INC.
 3803 KENILWORTH BLVD.
 SEBRING, FLORIDA 33870
 (863) 385-6856
 (863) 382-4531

- SCOPE OF WORK
 1) REMOVE EXISTING CONCRETE PADS AND ASPHALT. (SEE EXEMPTION NOTE BELOW)
 2) BOX OUT 13.5 INCHES FROM PROPOSED FINISH GRADE.
 3) CONSTRUCT A NEW PARKING LOT AND OTHER IMPROVEMENTS.
 4) REMOVE AND RELOCATE POWER POLES.

APPLYING FOR PERMIT EXEMPTION.
 REMOVAL OF EXISTING PAVEMENT : 3,580 SQUARE FEET OF ASPHALT
 CREATING 3,580 SQUARE FEET OF NEW LANDSCAPE AREAS.
 ADDING 2,000 SQUARE FEET OF OF ADDITIONAL ASPHALT FOR GOLF
 CART PARKING, PAVING AN EXISTING LANDSCAPED AREA TO CREATE A
 DRIVE AISLE FOR TRAFFIC FLOW. THE TWO (2) PROPOSED GOLF CART
 BUILDINGS WILL BE REPLACING EXISTING BUILDINGS AND EXISTING
 ASPHALT. THIS YIELDS A NET REDUCTION OF 1,580 SQUARE FEET OF
 PAVEMENT. (3,580 - 2,000 = 1,580 SQ. FT. OF NEW GREEN AREA)

PARKING:
 EXISTING PARKING SPACES : 70 + 6 A.D.A. SPACES
 PROPOSED NEW PARKING SPACES: 75
 TOTAL AFTER IMPROVEMENTS: 145 PARKING SPACES + 6 A.D.A. SPACES= 151 TOTAL
 NO ADDITIONAL A.D.A. SPACES NEEDED.

SPECIAL CONSTRUCTION NOTES:

--THE TOPOGRAPHY SHOWN FOR PROJECT IS A REPRESENTATION
 OF FIELD CONDITIONS. THE DESIGN OF THIS PROJECT IS BASED ON
 THE TOPOGRAPHY PROVIDED. IT WILL BE THE RESPONSIBILITY OF
 THE CONTRACTOR TO VERIFY INFORMATION IN FIELD. ANY
 DISCREPANCIES BETWEEN THE DESIGN TOPOGRAPHY AND FIELD
 CONDITIONS DURING CONSTRUCTION SHOULD BE BROUGHT TO THE
 ENGINEER OF RECORDS ATTENTION IMMEDIATELY SO THAT
 ALTERATIONS IN THE DESIGN CAN BE MADE TO KEEP THE FINAL
 SITE CONSTRUCTION IN COMPLIANCE WITH THE GOVERNING
 AGENCIES REGULATIONS.
 --THESE PLANS HAVE BEEN DRAWN TO DEPICT THE REQUIRED
 CONSTRUCTION WITHIN THE PROJECT AREA. IN CERTAIN CASES
 THE SIZE AND/OR LOCATION OF PROPOSED CONSTRUCTION HAS
 BEEN BLOWN-UP TO SHOW ITEMS. THEREFORE, LOCATIONS ON
 THE DRAWINGS MAY NOT BE EXACT AND SHOULD NOT BE SCALED
 FOR CONSTRUCTION. ITEMS SUCH AS INLETS, CULVERTS, MITERED
 END SECTIONS, SIGNS, STRIPING, TURN LANES, SLOPES ETC. WILL
 NEED TO BE CONSTRUCTED USING EXISTING SITE CONDITIONS AND
 CURRENT HIGHLANDS COUNTY CONSTRUCTION REQUIREMENTS AND
 REGULATIONS.
 --THE INFORMATION AND DESIGN SHOWN ON THESE DRAWINGS IS
 BASED ON THE BEST AVAILABLE INFORMATION PROVIDED FOR
 DESIGN. THE DRAWING IS TO SCALE AS MUCH AS POSSIBLE.
 HOWEVER NO MEASUREMENTS SHOULD BE MADE BY SCALING FROM
 THESE DRAWINGS AS SOME ITEMS MAY BE NOT TO SCALE FOR
 DRAWING CLARITY. ANY QUESTIONS OR CONFLICTS SHOULD BE
 BROUGHT TO THE ENGINEER OF RECORDS ATTENTION IMMEDIATELY
 FOR CLARIFICATION OR RESOLUTION. POLSTON ENGINEERING INC.
 SHALL NOT BE RESPONSIBLE FOR ANY ERRORS MADE BY OTHERS
 CAUSED BY MAKING ASSUMPTIONS OR ERRORS CAUSED BY
 SCALING THE PLANS. ALL CONSTRUCTION SHALL FOLLOW THE
 ACCEPTED SAFETY PROCEDURES AND CONSTRUCTION TECHNIQUES
 AS REQUIRED BY ANY APPLICABLE GOVERNMENT STANDARDS.

SITE PREPARATION

WE RECOMMEND NORMAL GOOD-PRACTICE SITE PREPARATION PROCEDURES. THESE PROCEDURES INCLUDE
 CLEARING THE SITE OF ABOVE AND BELOW GRADE IMPROVEMENTS, PROOF-ROLLING AND PROOF-COMPACTING THE
 SUBGRADE, AND FILLING TO GRADE WITH ENGINEERED FILL AS NEEDED.
 A MORE DETAILED SYNOPSIS OF THIS WORK IS AS FOLLOWS:
 1. IF REQUIRED, PERFORM REMEDIAL TEMPORARY DEWATERING PRIOR TO ANY EARTHWORK OPERATIONS TO
 REDUCE THE LIKELIHOOD OF PUMPING OF THE SHALLOW SUBGRADE SOILS DURING NORMAL CONSTRUCTION
 OPERATIONS. MAINTAIN GROUNDWATER LEVELS AT LEAST 24-INCHES BELOW THE LOWEST ANTICIPATED CUT
 AND/OR ALL COMPACTION SURFACES.
 2. STRIP THE PROPOSED CONSTRUCTION LIMITS OF ALL EXISTING PAVEMENT SECTIONS (INCLUDING BASE
 MATERIAL, WHERE PRESENT), TREES, GRASS, ROOTS, TOPSOIL, CONSTRUCTION DEBRIS, AND OTHER DELETERIOUS
 MATERIALS WITHIN AND 5 FEET BEYOND THE PERIMETER OF THE PROPOSED BUILDING AREAS, PAVED AREAS, OR
 OTHER AREAS RECEIVING IMPROVEMENTS. EXPECT CLEARING AND GRUBBING TO DEPTHS OF 6 INCHES, ON
 AVERAGE.
 3. PROOF-ROLL THE SUBGRADE WITH A HEAVILY LOADED, RUBBER-TIRED VEHICLE. PROOFROLLING WILL HELP
 LOCATE ANY ZONES OF ESPECIALLY LOOSE OR SOFT SOILS NOT ENCOUNTERED IN THE SOIL TEST BORINGS. THEN
 UNDERCUT, OR OTHERWISE TREAT THESE ZONES AS RECOMMENDED BY THE ENGINEER.
 4. PRIOR TO ANY FILLING OF THE SITE, PROOF-COMPACT THE SUBGRADE FROM THE SURFACE USING SUITABLE
 COMPACTION EQUIPMENT, UNTIL YOU OBTAIN A MINIMUM DENSITY OF 95% MPMDD TO A DEPTH OF 1 FOOT BELOW
 STRIPPED GRADE. IN ORDER TO ACHIEVE THE REQUIRED DEGREE OF COMPACTION, THE SOILS MAY NEED TO BE
 MOISTURE CONDITIONED UNTIL THE IN-SITU WATER CONTENT IS WITHIN ±2% OF THE OPTIMUM MOISTURE CONTENT
 (OMC).
 5. TEST THE SUBGRADE FOR COMPACTION AT A FREQUENCY OF NOT LESS THAN ONE TEST PER 2,500 SQUARE
 FEET PER FOOT OF DEPTH IMPROVEMENT IN THE BUILDING AREAS OR AT A MINIMUM OF TWO TEST LOCATIONS,
 WHICHEVER IS GREATER.
 6. PLACE FILL MATERIAL, AS REQUIRED. THE FILL SHOULD CONSIST OF FINE TO MEDIUM SAND WITH LESS THAN
 5 PERCENT SOIL FINES. YOU MAY USE FILL MATERIALS WITH SOIL FINES BETWEEN 5 AND 12 PERCENT, BUT STRICT
 MOISTURE CONTROL MAY BE REQUIRED. PLACE FILL IN UNIFORM 10 TO 12 INCH LOOSE LIFTS AND COMPACT EACH
 LIFT TO A MINIMUM DENSITY OF 95% MPMDD AT A MOISTURE CONTENT OF ±2% OF OPTIMUM (OMC).
 7. PERFORM COMPLIANCE TESTS WITHIN THE FILL AT A FREQUENCY OF NOT LESS THAN ONE TEST PER 2,500
 SQUARE FEET PER LIFT IN THE BUILDING AREAS, OR AT A MINIMUM OF TWO TEST LOCATIONS, WHICHEVER IS
 GREATER.
 8. TEST ALL FINAL FOOTING CUTS FOR COMPACTION TO A DEPTH OF 1 FOOT. ADDITIONALLY, WE RECOMMEND
 TESTING EVERY COLUMN FOOTINGS, AND AT LEAST ONE TEST PER EVERY 50 LINEAL FEET OF WALL FOOTING.
 USING VIBRATORY COMPACTION EQUIPMENT AT THIS SITE MAY DISTURB NEARBY STRUCTURES. VIBRATORY
 COMPACTION WITHIN 50 FEET OF EXISTING STRUCTURES IS NOT RECOMMENDED. FURTHERMORE, WE RECOMMEND
 YOU MONITOR NEARBY STRUCTURES BEFORE AND DURING ANY COMPACTION OPERATIONS ON-SITE. IF DISTURBANCE
 IS NOTED, HALT VIBRATORY COMPACTION AND INFORM THE ENGINEER OF RECORD IMMEDIATELY. WE WILL REVIEW
 THE COMPACTION PROCEDURES AND EVALUATE IF THE COMPACTIVE EFFORT RESULTS IN A SATISFACTORY
 SUBGRADE COMPLYING WITH OUR ORIGINAL DESIGN ASSUMPTIONS.

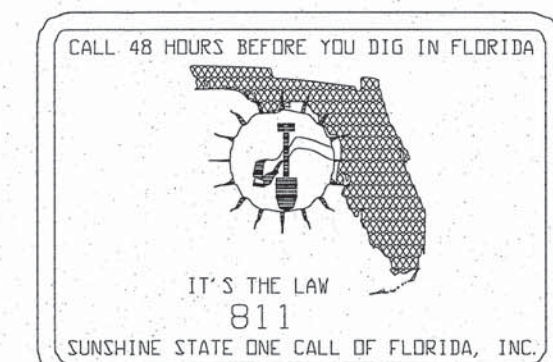
DEMOLITION OF THE EXISTING CONCRETE, ASPHALT AND DEBRIS TO CONSTRUCT
 AN IMPROVED PARKING LOT TO SERVICE THE CADDY SHACK RESTAURANT AND
 THE SEBRING MUNICIPAL GOLF COURSE.
 LOCATED IN SECTION 36, TOWNSHIP 34S, RANGE 28E



LOCATION MAP

THE CITY OF SEBRING
 SCOTT NOETHLICH, CITY ADMINISTRATOR
 368 SOUTH COMMERCE AVE.
 SEBRING, FLORIDA 33870
 (863) 471-5100

COUNTY NOTE:
 ALL CONSTRUCTION WITHIN THE COUNTY
 RIGHT-OF-WAY IS TO BE IN ACCORDANCE
 WITH THE CURRENT HIGHLANDS COUNTY
 STANDARD SPECIFICATIONS FOR ROADWAY
 CONSTRUCTION AND THE ROADWAY AND
 THE F.D.O.T. TRAFFIC DESIGN STANDARD
 INDEX.



DATE	REMARK
07-AUG-18	PRELIMINARY SUBMITTAL

CERTIFICATE OF AUTHORIZATION # 5684
 ROGER DALE POLSTON P. E. # 33222
 MARVIN LUTHER WOLFF P. E. # 40600

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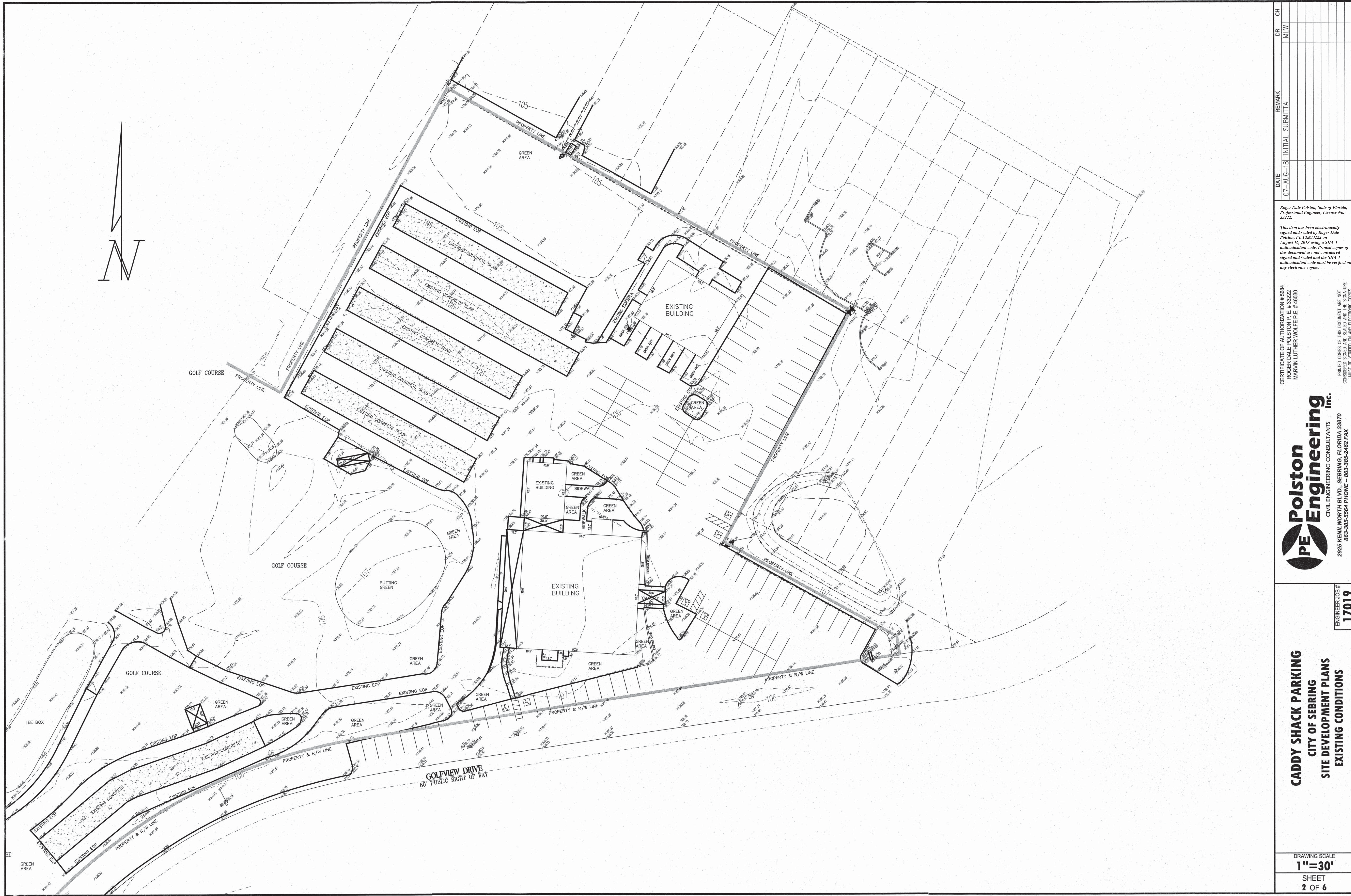
2018.08.16 14:44:17 -0400

Polston Engineering Inc.
 CIVIL ENGINEERING CONSULTANTS
 2925 KENILWORTH BLVD., SEBRING, FLORIDA 33870
 863-385-5564 PHONE - 863-385-2462 FAX

CADDY SHACK PARKING
 CITY OF SEBRING
 SITE DEVELOPMENT PLANS
 COVER SHEET / GENERAL INFORMATION

ENGINEER JOB #
17019

DRAWING SCALE
NTS
 SHEET
1 OF 6



DATE	REMARK
07-AUG-18	INITIAL SUBMITTAL

Roger Dale Polston, State of Florida, Professional Engineer, License No. 33222.

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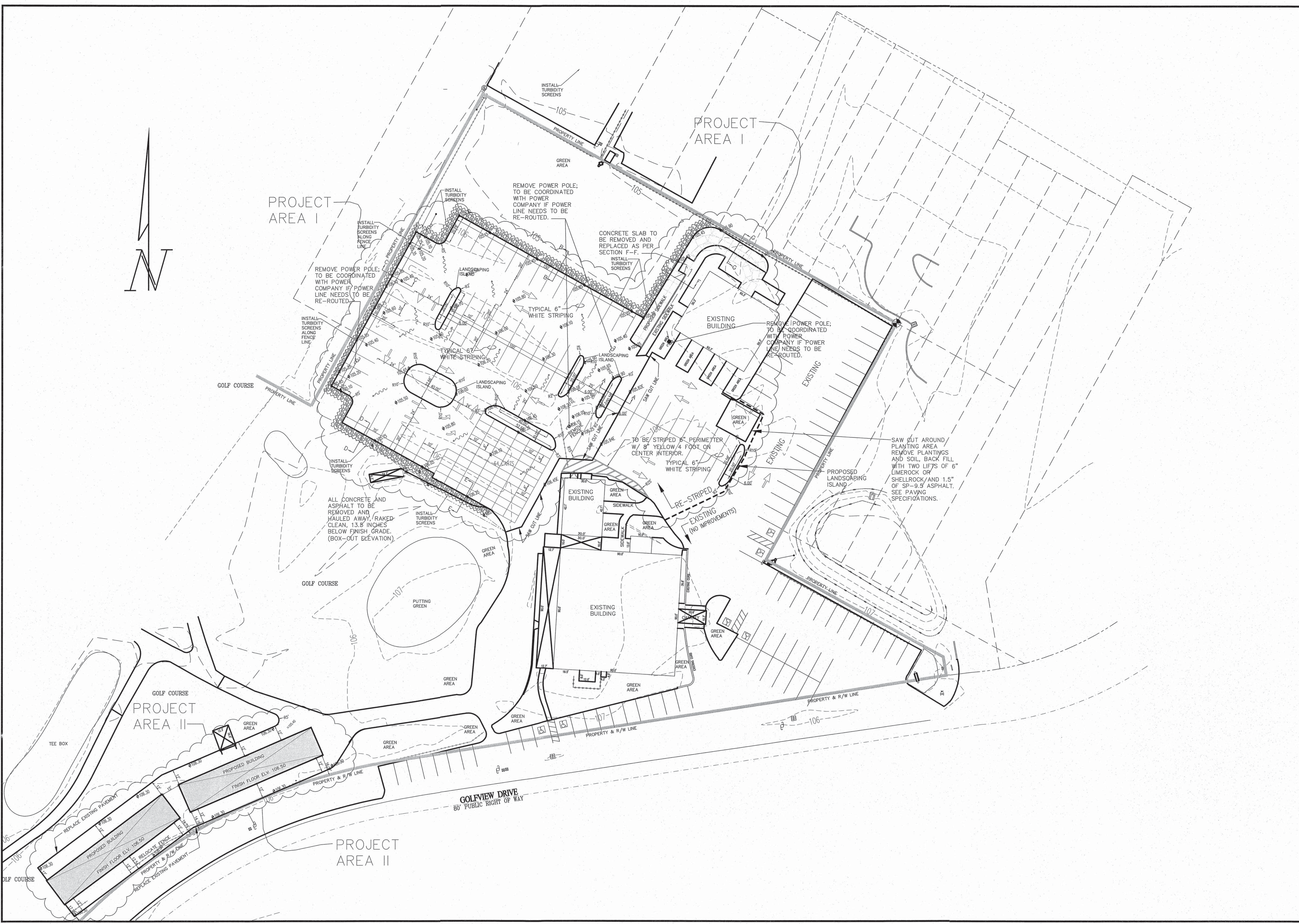
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 MARVIN LUTHER WOLFE P.E. #48030

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CADDY SHACK PARKING
CITY OF SEBRING
SITE DEVELOPMENT PLANS
EXISTING CONDITIONS

ENGINEER JOB # 17019



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07-AUG-18	INITIAL SUBMITTAL

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ROGER DALE POLSTON P. E. # 33222
MARVIN LUTHER WOLFE P. E. # 46030

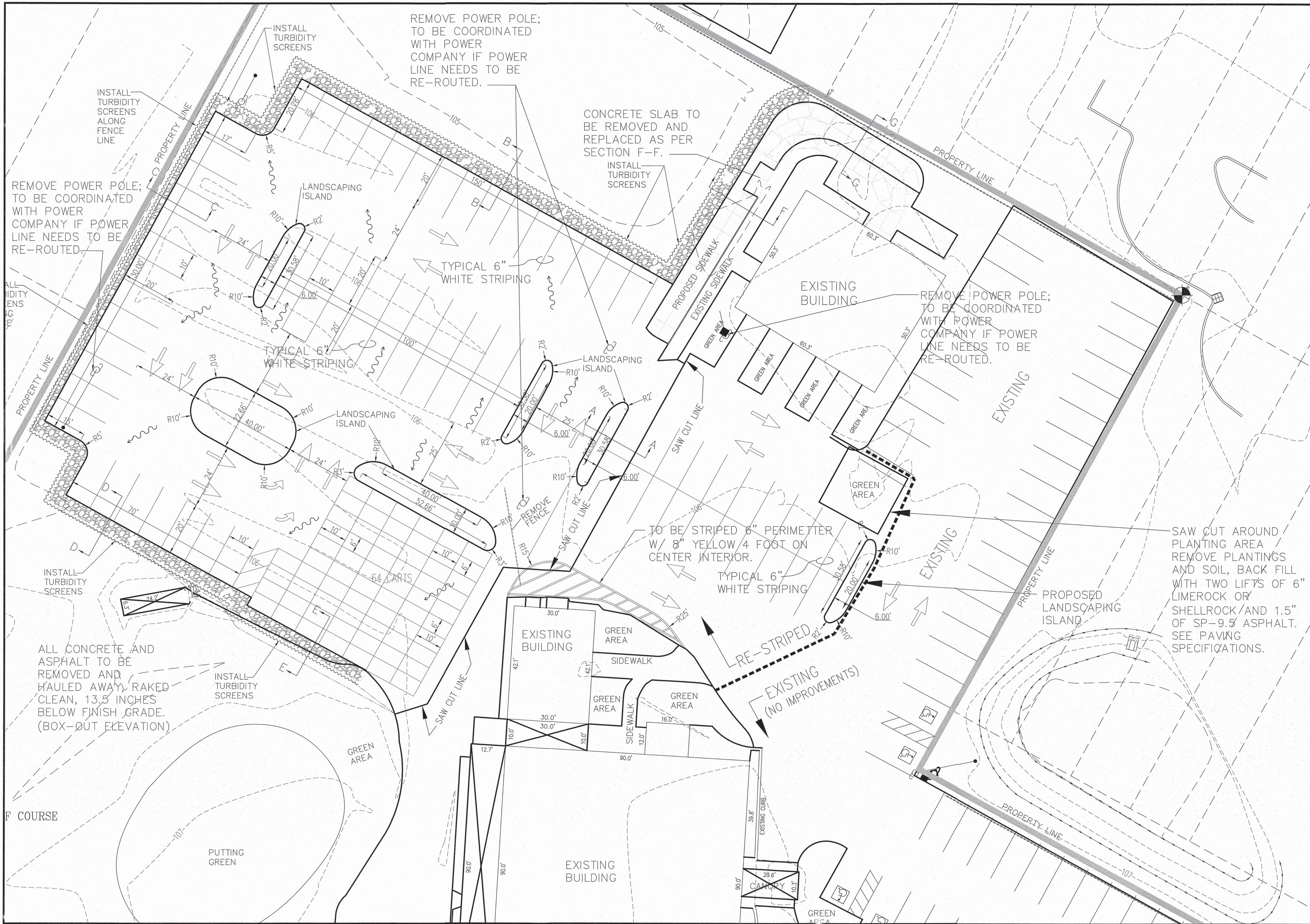
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ENGINEER JOB #
17019

**CADDY SHACK PARKING
CITY OF SEBRING
SITE DEVELOPMENT PLANS
PROPOSED LAYOUTS**

DRAWING SCALE
1" = 30'
SHEET
3 OF 6



DATE	REMARK	CH
07-AUG-18	INITIAL SUBMITTAL	

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 ROGER DALE POLSTON P.E. # 33222
 MARVIN LUTHER WOLFE P.E. # 48030

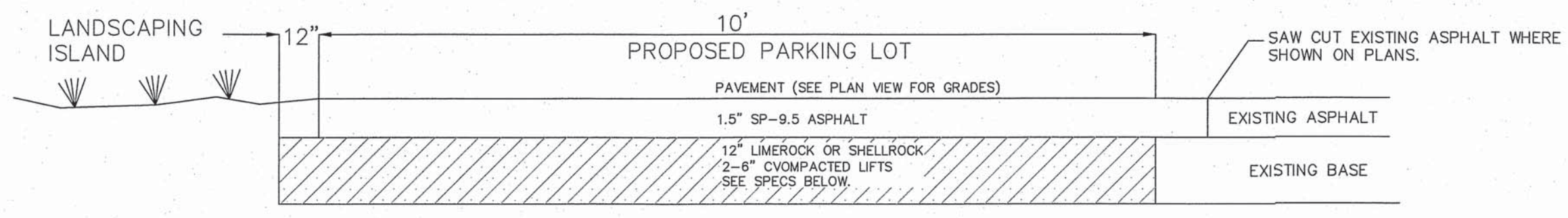
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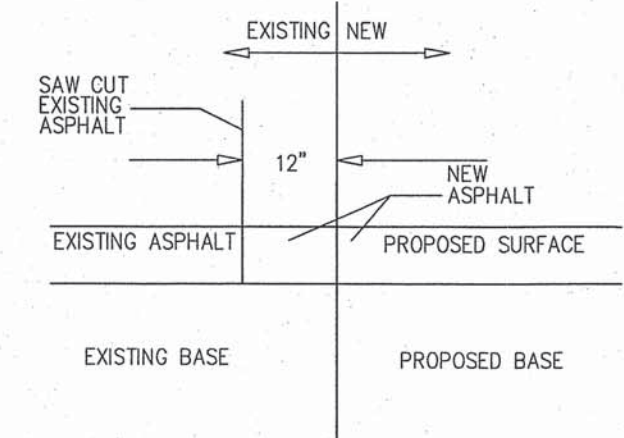
ENGINEER JOB # **17019**

CADDY SHACK PARKING
 CITY OF SEBRING
 SITE DEVELOPMENT PLANS
 PROPOSED LAYOUT

DRAWING SCALE
1" = 30'
 SHEET
 4 OF 6



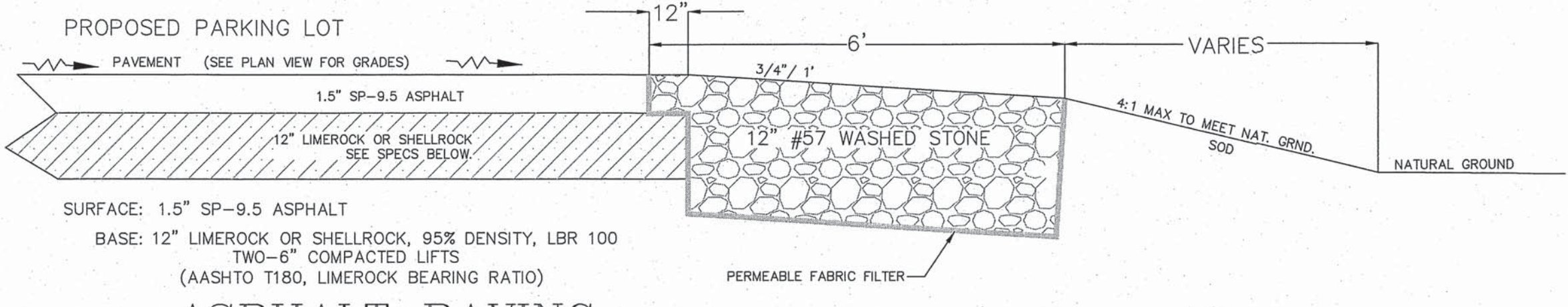
SECTION A-A
 SURFACE: 1.5" SP-9.5 ASPHALT
 BASE: 12" LIMEROCK OR SHELLROCK, 95% DENSITY, LBR 100
 TWO-6" COMPACTED LIFTS
 (AASHTO T180, LIMEROCK BEARING RATIO)



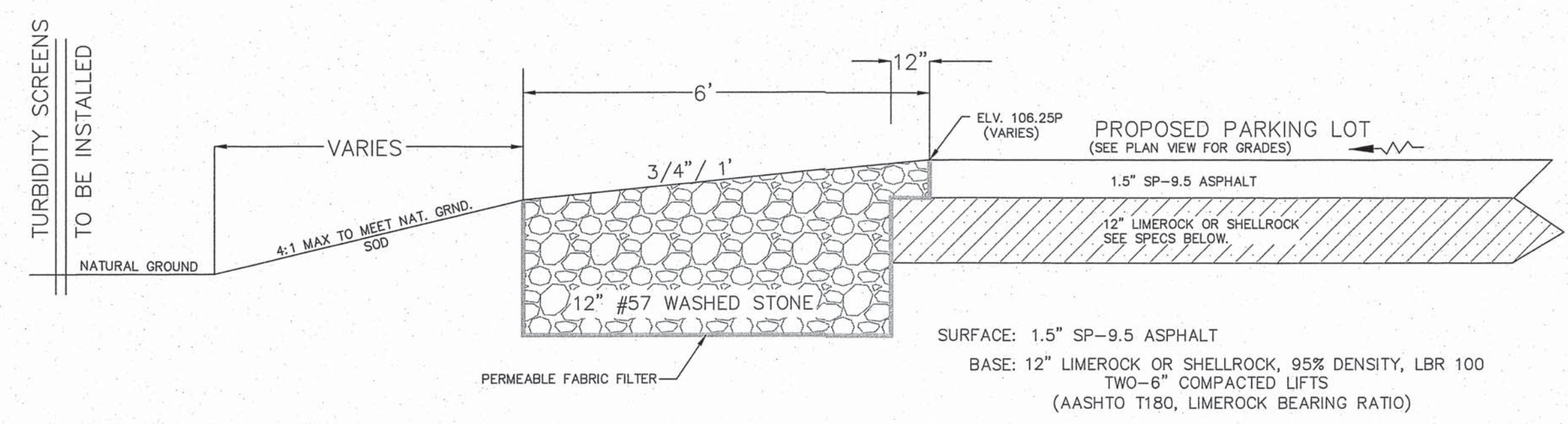
CONNECTION TO EXISTING PAVEMENT
 PROPOSED PARKING LOT DETAIL



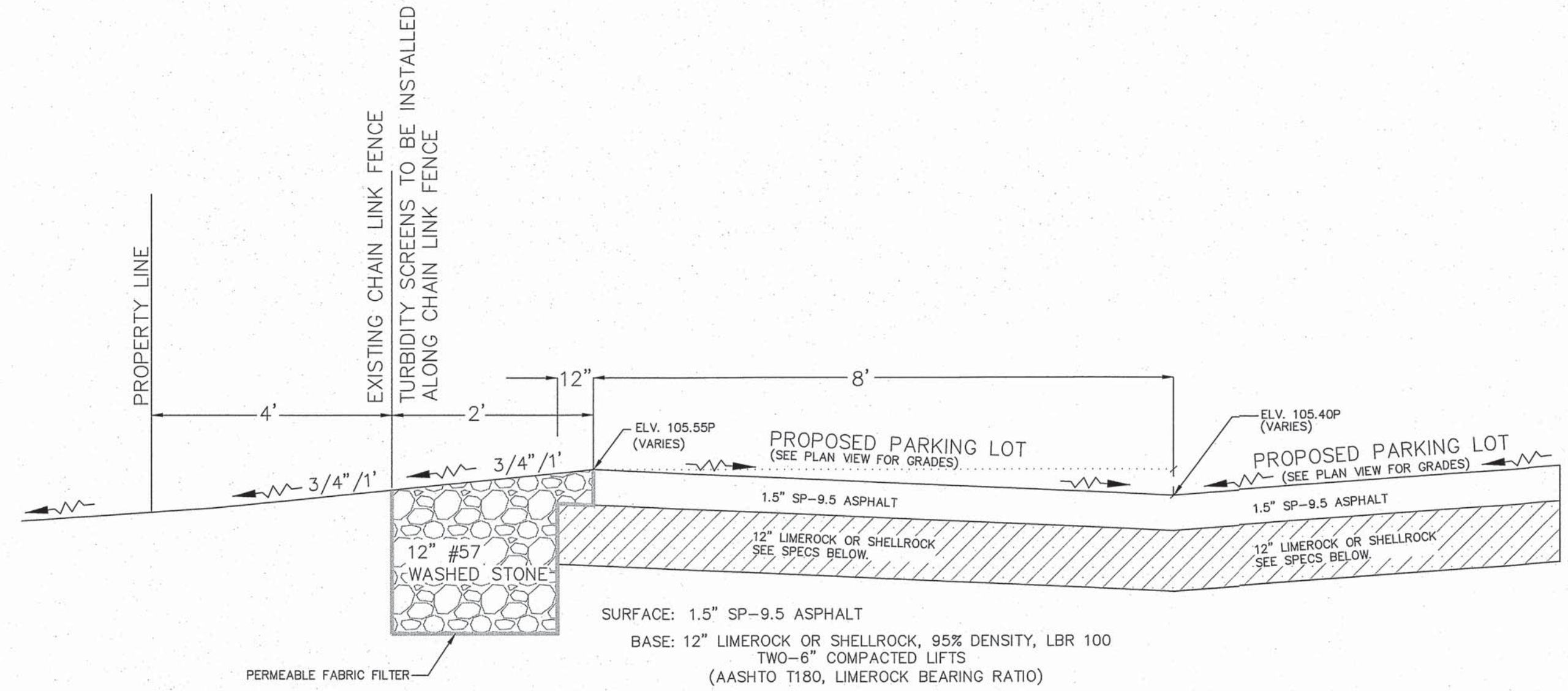
TYPES OF PERMANENT LONGITUDINAL LINES
 ALL PAVEMENT MARKINGS WILL BE MANUFACTURED BY THE SHERWIN-WILLIAMS COMPANY
 PRODUCT NAME: HOTLINE® FAST DRY WATERBORNE TRAFFIC MARKING PAINT (TT-P-1952D/E TYPE I & II)
 PRODUCT CODE: TM2152
 (OR EQUAL)
 WHITE LINES SEPARATE FLOWS IN THE SAME DIRECTION.
 YELLOW LINES SEPARATE FLOWS IN THE OPPOSITE DIRECTION.
 YELLOW DOTTED LINES MAY BE USED IN SPECIAL CASES.



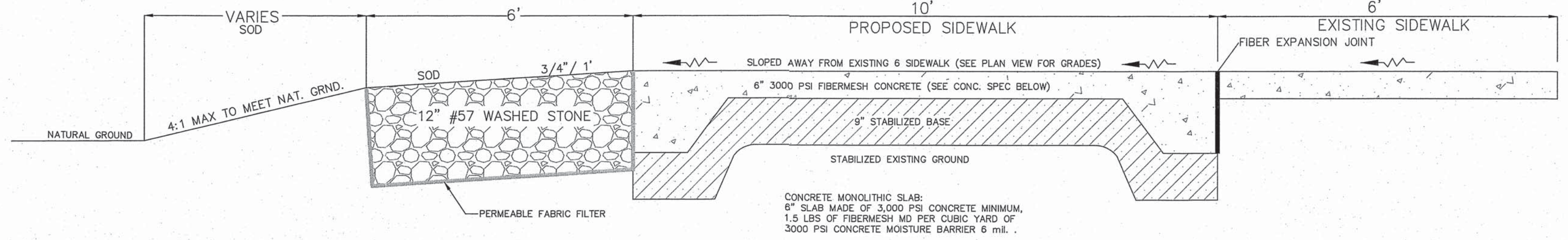
ASPHALT PAVING
 PROPOSED PARKING LOT DETAIL
 SECTION B-B



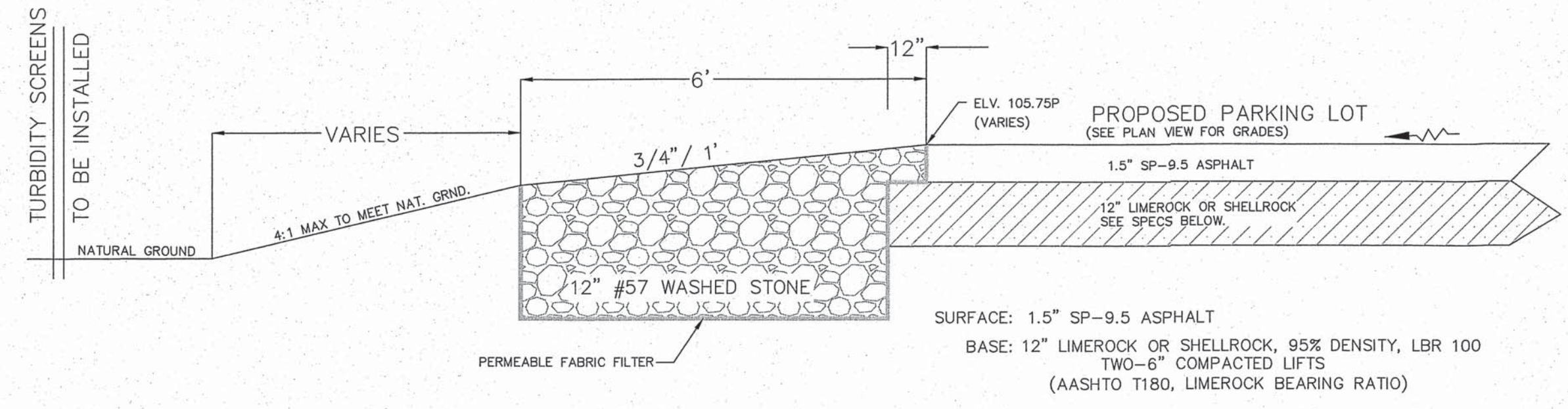
ASPHALT PAVING
 PROPOSED PARKING LOT DETAIL
 SECTION E-E



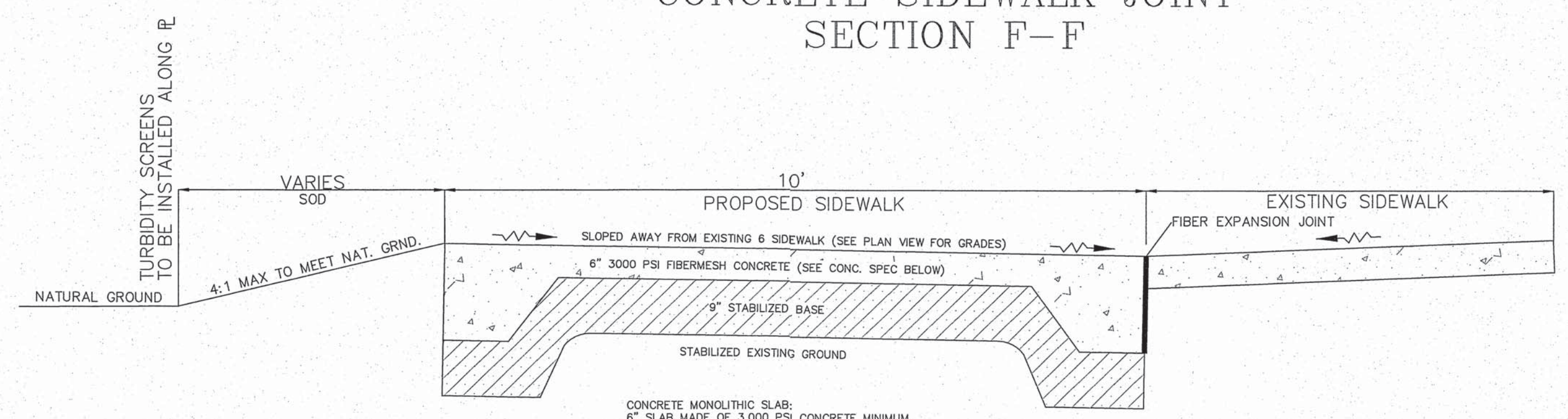
ASPHALT PAVING
 PROPOSED PARKING LOT DETAIL
 SECTION C-C



CONCRETE SIDEWALK JOINT
 SECTION F-F



ASPHALT PAVING
 PROPOSED PARKING LOT DETAIL
 SECTION D-D



CONCRETE SIDEWALK JOINT
 SECTION G-G

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CADDY SHACK PARKING
 CITY OF SEBRING
 SITE DEVELOPMENT PLANS
 SECTIONAL VIEWS

ENGINEER JOB # **17019**

DRAWING SCALE
NTS
 SHEET
5 OF 6

