

TAXIWAY LIGHTING:
 CONTRACTOR SHALL DISABLE TAXIWAY LIGHTING IN CLOSED SECTIONS OF TAXIWAY BY INSTALLING TEMPORARY JUMPER CABLE OR COVERING LIGHTS. ALL GUIDANCE SIGNS WITHIN AND LEADING TO CLOSED AREAS SHALL BE COVERED.

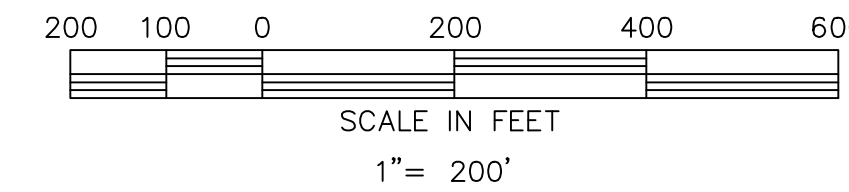
CONSTRUCTION PHASING NOTES:

PHASE I
 PHASE I CONSTRUCTION CONSISTS OF PREPARATORY ITEMS REQUIRED TO BE COMPLETED PRIOR TO CLOSING RUNWAY 5-23. THESE ITEMS SHALL INCLUDE INSTALLATION OF TEMPORARY RELOCATED THRESHOLD, REMARKING RUNWAY 11-29, PAVEMENT REPAIRS, AND SWEEPING/VACUUMING RUNWAY 11-29 PRIOR TO REOPENING. SEE SHEETS A1.4 AND A1.5 FOR DETAILS.

PHASE II
 PHASE II CONSTRUCTION CONSISTS OF REHABILITATION OF RUNWAY 5-23 FROM THE RUNWAY 5 END TO STATION 13+80 AND SHALL BE THE INITIAL PHASE OF CONSTRUCTION FOR THE RUNWAY. FOR THIS PHASE, ACCESS TO RUNWAY 11-29 SHALL BE THROUGH THE TAXIWAY 'B' CROSSING (WORK AREA C). SEE SHEET A1.7 FOR DETAILS.

PHASE III
 PHASE III CONSTRUCTION CONSISTS OF REHABILITATION OF RUNWAY 5-23 FROM STATION 13+80.00 TO THE RUNWAY 23 END. FOR THIS PHASE, ACCESS TO RUNWAY 11-29 SHALL BE THROUGH THE TAXIWAY 'B' CROSSING (WORK AREA C) OR TAXIWAY 'C' CROSSING (WORK AREA A1). SEE SHEET A1.8 FOR DETAILS.

- LEGEND**
- APPROXIMATE AIRPORT PROPERTY LINE
 - RSA --- RUNWAY SAFETY AREA
 - ROFA --- RUNWAY OBJECT FREE AREA
 - ROFZ --- RUNWAY OBSTACLE FREE ZONE
 - [Pattern] WORK AREA A (PHASE II)
 - [Pattern] WORK AREA A1 (PHASE II)
 - [Pattern] WORK AREA B (PHASE III)
 - [Pattern] WORK AREA C (PHASE III)
 - [Dotted] CONTRACTOR'S ACCESS ROUTE
 - [Shaded] CONTRACTORS STAGING/STOCKPILE AREA
 - LIGHTED BARRICADES



NOTE D - CONTRACTOR'S ACCESS ROUTE:
 DUE TO CONCURRENT CONSTRUCTION ON-SITE ADJACENT TO CLOSED RUNWAY, THE ACCESS AVAILABLE FOR THE REHABILITATION PROJECT SHALL BE 60' IN WIDTH. THE ACCESS ROUTE MAY ALSO BE UTILIZED AS A TAXIWAY FOR A LIMITED NUMBER OF AIRCRAFT OPERATIONS ON A WEEKLY BASIS. AIRCRAFT OPERATION SHALL HAVE THE RIGHT OF WAY AT ALL TIMES. CLOSE COORDINATION WILL BE REQUIRED BETWEEN THE AIRCRAFT OPERATOR, CONTRACTOR, AND AIRPORT MANAGEMENT. THE CONTRACTOR SHALL ALSO COORDINATE WORK WITH THE CONTRACTOR WORKING ON THE ADJACENT SITE.

ACTIVE AIR OPERATIONS AREA
 ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED WITH FLAGGER CONTROL USING RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTE A - MAINTENANCE OF AIRCRAFT TRAFFIC (RUNWAY 11-29):
 CONTRACTOR SHALL MAINTAIN WORK AREA A1 OR WORK AREA C OPEN TO PROVIDE AIRCRAFT ACCESS TO RUNWAY 11-29 AT ALL TIMES DURING CONSTRUCTION ACTIVITIES ON RUNWAY 5-23. CONTRACTOR WILL BE REQUIRED TO UTILIZE FLAGMEN AT TAXIWAY 'B' CROSSING (WORK AREA C) AND TAXIWAY 'C' (WORK AREA A1) DURING CONSTRUCTION WHEN OPEN TO AIRCRAFT TRAFFIC. FLAGMEN SHALL HAVE ALL TRUCKS STOP PRIOR TO CROSSING RUNWAY APPROACH/TAXIWAY, AS SHOWN ON THE PLANS. CONTRACTOR SHALL MONITOR AIR TRAFFIC ON TWO-WAY RADIO PRIOR TO VEHICLES CROSSING ACTIVE RUNWAY APPROACH/TAXIWAYS.

NOTE C - ENGINEER'S TRAILER:
 THE FINAL LOCATION FOR THE ENGINEER'S TRAILER SHALL BE COORDINATED WITH AIRPORT MANAGEMENT. THE INSTALLATION AND SETUP FOR THE TRAILER SHALL BE AS SPECIFIED IN SECTION C-105-4 OF THE PROJECT SPECIFICATIONS.

NOTAMS:
 AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS.

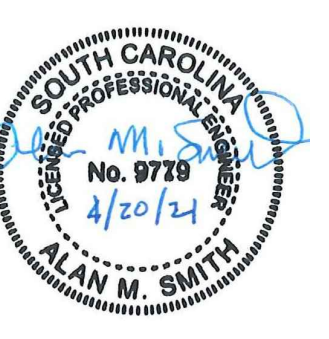
NOTE B - ACCESS GATE:
 THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS.

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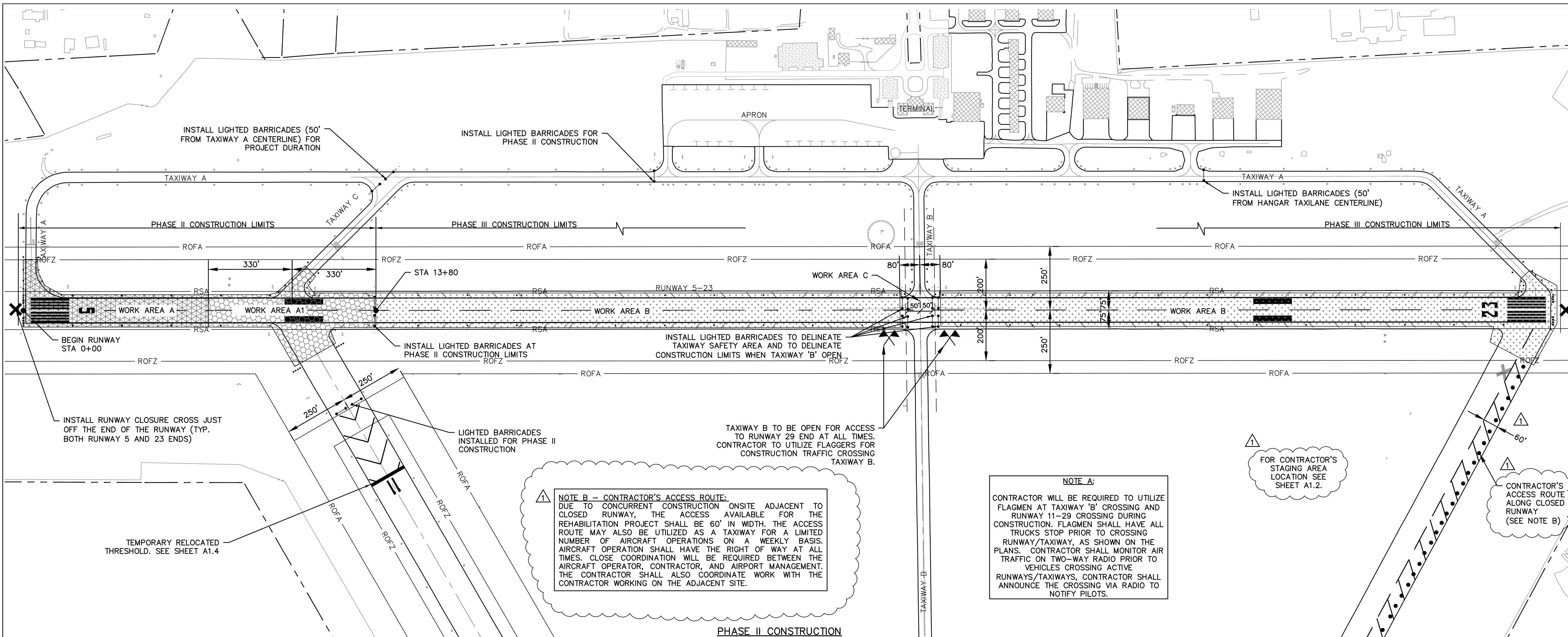
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| 1 | REVISED ACCESS ROUTE AND STAGING AREA | 4/20/21 |

GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
RUNWAY 5-23 REHABILITATION
PROJECT LAYOUT AND SAFETY PLAN



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A1.2



NOTE B - CONTRACTOR'S ACCESS ROUTE:
 DUE TO CONCURRENT CONSTRUCTION ONSITE ADJACENT TO CLOSED RUNWAY, THE ACCESS AVAILABLE FOR THE REHABILITATION PROJECT SHALL BE 60' IN WIDTH. THE ACCESS ROUTE MAY ALSO BE UTILIZED AS A TAXIWAY FOR A LIMITED NUMBER OF AIRCRAFT OPERATIONS ON A WEEKLY BASIS. AIRCRAFT OPERATION SHALL HAVE THE RIGHT OF WAY AT ALL TIMES. CLOSE COORDINATION WILL BE REQUIRED BETWEEN THE AIRCRAFT OPERATOR, CONTRACTOR, AND AIRPORT MANAGEMENT. THE CONTRACTOR SHALL ALSO COORDINATE WORK WITH THE CONTRACTOR WORKING ON THE ADJACENT SITE.

NOTE A:
 CONTRACTOR WILL BE REQUIRED TO UTILIZE FLAGMEN AT TAXIWAY 'B' CROSSING AND RUNWAY 11-29 CROSSING DURING CONSTRUCTION. FLAGMEN SHALL HAVE ALL TRUCKS STOP PRIOR TO CROSSING RUNWAY/TAXIWAY, AS SHOWN ON THE PLANS. CONTRACTOR SHALL MONITOR AIR TRAFFIC ON TWO-WAY RADIO PRIOR TO VEHICLES CROSSING ACTIVE RUNWAYS/TAXIWAYS, CONTRACTOR SHALL ANNOUNCE THE CROSSING VIA RADIO TO NOTIFY PILOTS.

FOR CONTRACTOR'S STAGING AREA LOCATION SEE SHEET A1.2.

CONTRACTOR'S ACCESS ROUTE ALONG CLOSED RUNWAY (SEE NOTE B)

PHASE II CONSTRUCTION

SEQUENCE OF CONSTRUCTION - PHASE II:
 PHASE II CONSISTS OF REHABILITATION OF RUNWAY 5-23 (WORK AREA A) AND RUNWAY 11-29/TAXIWAY C CROSSING OF RUNWAY 11-29 (WORK AREA A1) TO APPROXIMATELY STATION 13+80. TAXIWAY 'B' AND RUNWAY 11-29 (WITH TEMPORARY RELOCATED THRESHOLD) SHALL REMAIN OPEN. THE CONTRACTOR SHALL HAVE SEVEN CALENDAR DAYS TO COMPLETE WORK SPECIFIED WITHIN PHASE I AREA.

1. CLOSE RUNWAY 5-23 BY PLACING LIGHTED CLOSURE CROSSES ON EACH END.
2. INSTALL TEMPORARY SEDIMENTATION EROSION CONTROL ITEMS AS REQUIRED.
3. INSTALL LIGHTED BARRICADES ACROSS TAXIWAYS, APRON CONNECTOR TAXIWAY AND ACROSS RUNWAY 5-23 AS SHOWN.
4. COMPLETE PROFILE MILLING OF RUNWAY 5-23 IN PHASE I AREA. CONTRACTOR MAY COMPLETE MILLING OPERATIONS WITHIN AREA B CONCURRENTLY WITH WORK IN PHASE I AREA. ACCESS TO RUNWAY 11-29 USING CROSSING AT TAXIWAY 'B' SHALL REMAIN OPEN AT ALL TIMES. SEE NOTE A.
5. COMPLETE CRACK CLEANING AND SEALING AS REQUIRED PER SPECIFICATION P-101. CONTRACTOR MAY COMPLETE CRACK CLEANING AND SEALING WITHIN AREA B CONCURRENTLY WITH WORK IN PHASE I AREA. ACCESS TO RUNWAY 11-29 USING CROSSING AT TAXIWAY 'B' SHALL REMAIN OPEN AT ALL TIMES. SEE NOTE A.
6. APPLY TACK COAT AND PAVE FIRST 2" LIFT OF P-401. CONTRACTOR MAY COMPLETE PAVING OF FIRST 2" LIFT OF P-401 WITHIN AREA B CONCURRENTLY WITH WORK IN PHASE I AREA. ACCESS TO RUNWAY 11-29 USING CROSSING AT TAXIWAY 'B' SHALL REMAIN OPEN AT ALL TIMES. SEE NOTE A.

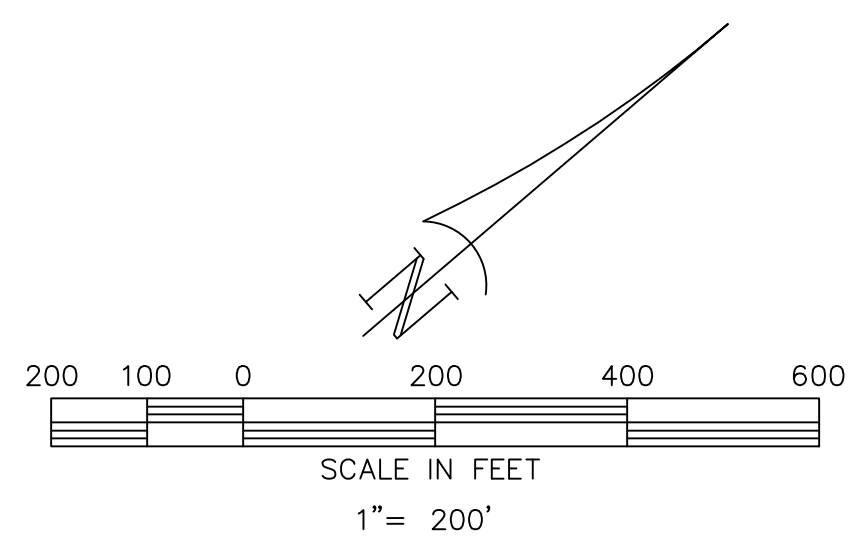
SEQUENCE OF CONSTRUCTION - PHASE III (SEE SHEET A1.8)
 PHASE III CONSISTS OF REHABILITATION OF RUNWAY 5-23 FROM APPROXIMATELY STATION 13+80 TO THE RUNWAY 23 END.

1. EITHER WORK AREA C OR WORK AREA A1 MUST BE OPEN AT ALL TIMES TO ALLOW AIRCRAFT ACCESS TO RUNWAY 11-29. AT HIS DISCRETION, CONTRACTOR MAY ELECT TO MAINTAIN TAXIWAY 'B' AND CONTINUE WORK OPERATIONS WITHIN PHASE I WORK AREA. ONCE THE CROSSING AT TAXIWAY 'B' (WORK AREA C) IS CLOSED THE CROSSING AT TAXIWAY 'C' SHALL BE OPEN TO AIRCRAFT TRAFFIC. CONTRACTOR SHALL BE REQUIRED TO COMPLETE TEMPORARY PAVEMENT TRANSITIONS TO MAINTAIN TRAFFIC, CONTRACTOR SHALL BE REQUIRED TO COMPLETE SAFETY INSPECTION AND PLACE BARRICADES AS SHOWN PRIOR TO OPENING TAXIWAY 'C' CROSSING.
2. ONCE FIRST LIFT OF PAVING IS COMPLETE IN PHASE II, CONTRACTOR SHALL WORK TO COMPLETE PAVING OF FIRST 2" LIFT OF PAVEMENT IN PHASE III AREA.
3. COMPLETE PROFILE MILLING AND CRACK CLEANING AND SEALING IN WORK AREA B.
4. APPLY TACK COAT AND PAVE FIRST 2" LIFT OF P-401 WITHIN WORK AREA B.
5. BY MAINTAINING THE TAXIWAY 'B' CROSSING OPEN, THE CONTRACTOR MAY CONCURRENTLY BEGIN OPERATIONS TO PAVE SECOND 2" LIFT OF P-401 STARTING AT THE RUNWAY 5 END. PAVING OPERATIONS SHALL BE PLANNED TO MINIMIZE THE NUMBER OF TRANSVERSE JOINTS IN THE FINAL LIFT OF PAVEMENT.
6. ONCE THE TAXIWAY 'B' CROSSING IS CLOSED, AIRCRAFT TRAFFIC TO RUNWAY 11-29 SHALL UTILIZE THE TAXIWAY 'C' CROSSING. TRANSITIONS SHALL BE COMPLETED AS REQUIRED AND BARRICADES INSTALLED IN ACCORDANCE WITH DETAILS SHOWN.
7. CONTRACTOR SHALL COMPLETE PROFILE MILLING OPERATIONS, CLEANING AND CRACK FILLING AND PLACEMENT OF FIRST 2" LIFT OF P-401 WITHIN WORK AREA C.
8. COMPLETE PAVING OF FINAL 2" LIFT OF P-401 AND ALL PAVEMENT TRANSITIONS. CONTRACTOR SHALL BEGIN FINAL LIFT OF PAVEMENT ON THE RUNWAY 5 END AND PROCEED TO THE RUNWAY 23 END MINIMIZING THE NUMBER OF JOINTS IN THE FINAL LIFT OF P-401, MAINTAINING ACCESS TO RUNWAY 11-29 AT ALL TIMES.
9. COMPLETE SHOULDER GRADING OPERATIONS.

10. MODIFY TAXIWAY AND RUNWAY LIGHTS TO PROVIDE 14" HEIGHT.
11. COMPLETE SODDING AND SEEDING AND MULCHING OPERATIONS FOR ALL DISTURBED AREAS.
12. COMPLETE FIRST APPLICATION OF PAVEMENT MARKING FOR RUNWAY 5-23 AND ALL CONNECTING TAXIWAYS. CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS TO RUNWAY 11-29 DURING MARKING OPERATIONS.
13. AFTER CONSTRUCTION AND FINAL PUNCH LIST ITEMS ARE COMPLETED, A FINAL SAFETY INSPECTION FOR RUNWAY 5-23 AND TAXIWAYS SHALL BE COMPLETED PRIOR TO REOPENING. REMOVE BARRICADES AND CLOSURE CROSSES AND REOPEN RUNWAY 5-23 AND TAXIWAYS TO AIR OPERATIONS.
14. AFTER RUNWAY 5-23 IS REOPENED, RUNWAY 11-29 SHALL BE CLOSED PLACING BARRICADES AND CLOSURE CROSSES AS REQUIRED. THE TEMPORARY RELOCATED THRESHOLD SHALL BE REMOVED AND NEW MARKING WITH BEADS SHALL BE COMPLETED ON ORIGINAL RUNWAY 11 THRESHOLD MARKINGS. REMOVE BARRICADES AND CLOSURE CROSSES AND REOPEN RUNWAY 11-29 TO AIR OPERATIONS.
15. REMOVE ANY TEMPORARY SILT FENCE AND SEDIMENTATION CONTROL MEASURES WITHIN THE PROJECT AREA AS DIRECTED BY THE ENGINEER. RESTORE DISTURBED AREAS AND SEED AND MULCH.
16. CONTRACTOR SHALL REMOVE ALL EQUIPMENT, MATERIALS, AND DEBRIS. REMOVE STAGING AREA. REGRADE AND RESTORE DISTURBED AREAS AND SEED AND MULCH.
17. AFTER SPECIFIED CURING PERIOD, CONTRACTOR SHALL COMPLETE GROOVING OPERATIONS AND APPLY FINAL APPLICATION OF MARKING WITHIN PROJECT WITH REFLECTIVE BEADS AS REQUIRED. SEE PHASE IV PHASING PLAN ON SHEET A1.9.

LEGEND

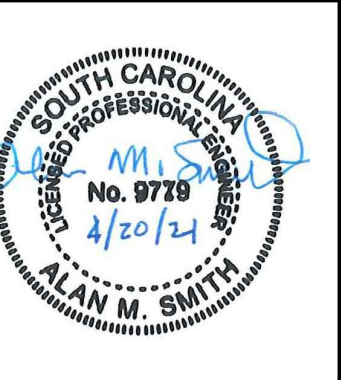
- APPROXIMATE AIRPORT PROPERTY LINE
- RSA --- RUNWAY SAFETY AREA
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- ROFZ --- RUNWAY OBSTACLE FREE ZONE
- [Pattern] WORK AREA A (PHASE II)
- [Pattern] WORK AREA A1 (PHASE II)
- [Pattern] WORK AREA B (PHASE III)



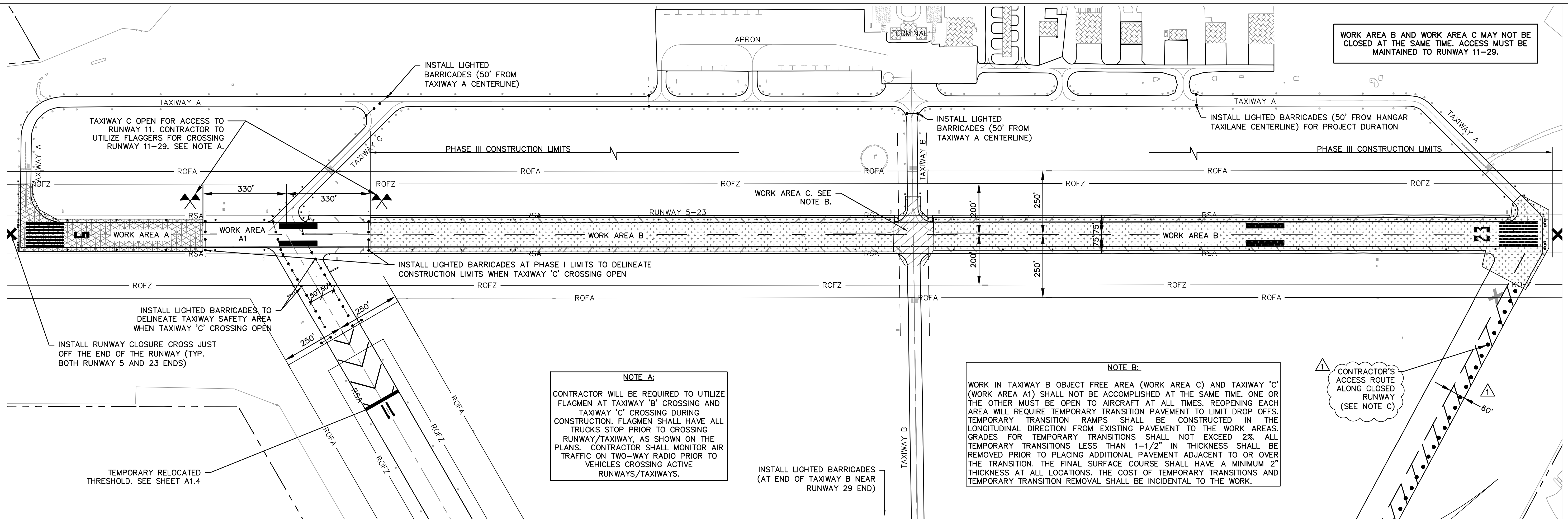
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GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
RUNWAY 5-23 REHABILITATION
PHASING PLAN - PHASE II



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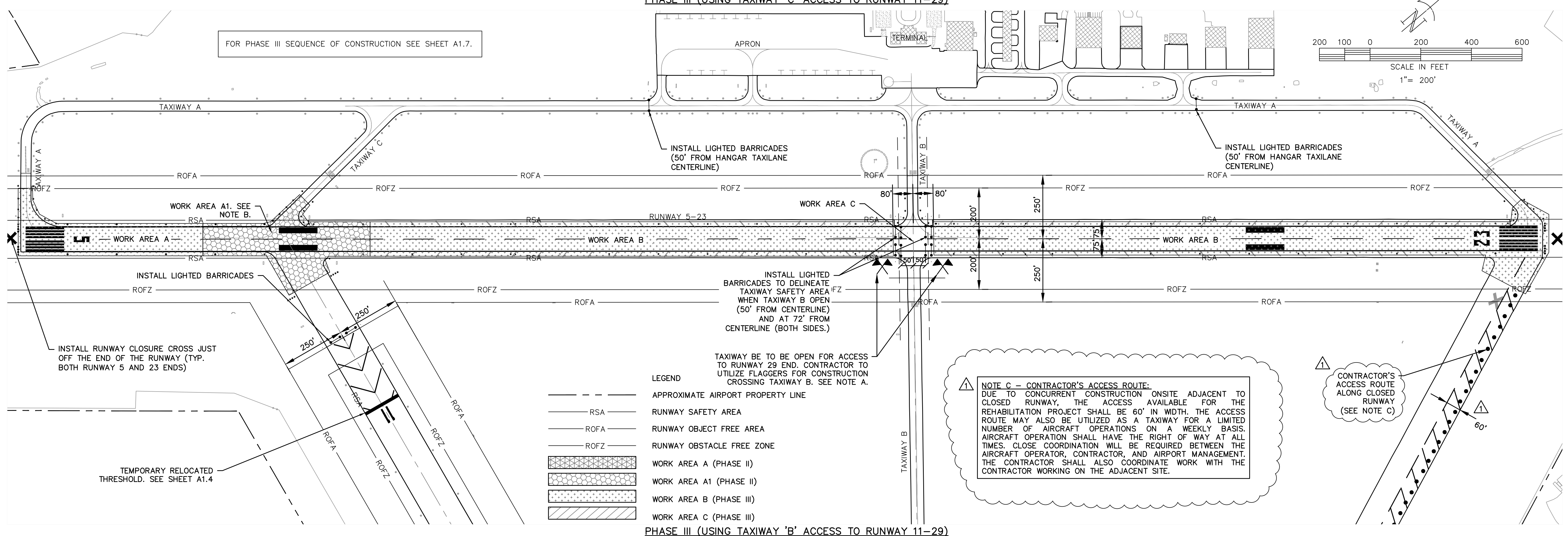
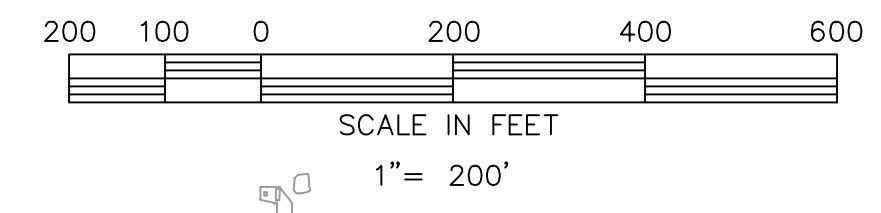
WORK AREA B AND WORK AREA C MAY NOT BE CLOSED AT THE SAME TIME. ACCESS MUST BE MAINTAINED TO RUNWAY 11-29.

NOTE A:
CONTRACTOR WILL BE REQUIRED TO UTILIZE FLAGMEN AT TAXIWAY 'B' CROSSING AND TAXIWAY 'C' CROSSING DURING CONSTRUCTION. FLAGMEN SHALL HAVE ALL TRUCKS STOP PRIOR TO CROSSING RUNWAY/TAXIWAY, AS SHOWN ON THE PLANS. CONTRACTOR SHALL MONITOR AIR TRAFFIC ON TWO-WAY RADIO PRIOR TO VEHICLES CROSSING ACTIVE RUNWAYS/TAXIWAYS.

NOTE B:
WORK IN TAXIWAY B OBJECT FREE AREA (WORK AREA C) AND TAXIWAY 'C' (WORK AREA A1) SHALL NOT BE ACCOMPLISHED AT THE SAME TIME. ONE OR THE OTHER MUST BE OPEN TO AIRCRAFT AT ALL TIMES. REOPENING EACH AREA WILL REQUIRE TEMPORARY TRANSITION PAVEMENT TO LIMIT DROP OFFS. TEMPORARY TRANSITION RAMPS SHALL BE CONSTRUCTED IN THE LONGITUDINAL DIRECTION FROM EXISTING PAVEMENT TO THE WORK AREAS. GRADES FOR TEMPORARY TRANSITIONS SHALL NOT EXCEED 2%. ALL TEMPORARY TRANSITIONS LESS THAN 1-1/2" IN THICKNESS SHALL BE REMOVED PRIOR TO PLACING ADDITIONAL PAVEMENT ADJACENT TO OR OVER THE TRANSITION. THE FINAL SURFACE COURSE SHALL HAVE A MINIMUM 2" THICKNESS AT ALL LOCATIONS. THE COST OF TEMPORARY TRANSITIONS AND TEMPORARY TRANSITION REMOVAL SHALL BE INCIDENTAL TO THE WORK.

CONTRACTOR'S ACCESS ROUTE ALONG CLOSED RUNWAY (SEE NOTE C)

FOR PHASE III SEQUENCE OF CONSTRUCTION SEE SHEET A1.7.



NOTE C - CONTRACTOR'S ACCESS ROUTE:
DUE TO CONCURRENT CONSTRUCTION ON SITE ADJACENT TO CLOSED RUNWAY, THE ACCESS AVAILABLE FOR THE REHABILITATION PROJECT SHALL BE 60' IN WIDTH. THE ACCESS ROUTE MAY ALSO BE UTILIZED AS A TAXIWAY FOR A LIMITED NUMBER OF AIRCRAFT OPERATIONS ON A WEEKLY BASIS. AIRCRAFT OPERATION SHALL HAVE THE RIGHT OF WAY AT ALL TIMES. CLOSE COORDINATION WILL BE REQUIRED BETWEEN THE AIRCRAFT OPERATOR, CONTRACTOR, AND AIRPORT MANAGEMENT. THE CONTRACTOR SHALL ALSO COORDINATE WORK WITH THE CONTRACTOR WORKING ON THE ADJACENT SITE.

CONTRACTOR'S ACCESS ROUTE ALONG CLOSED RUNWAY (SEE NOTE C)

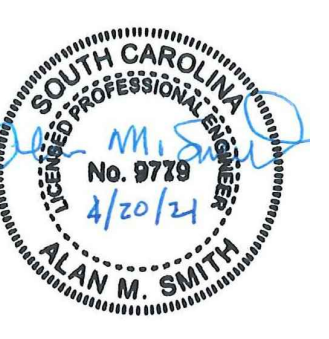
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 - [Pattern] WORK AREA C (PHASE III)

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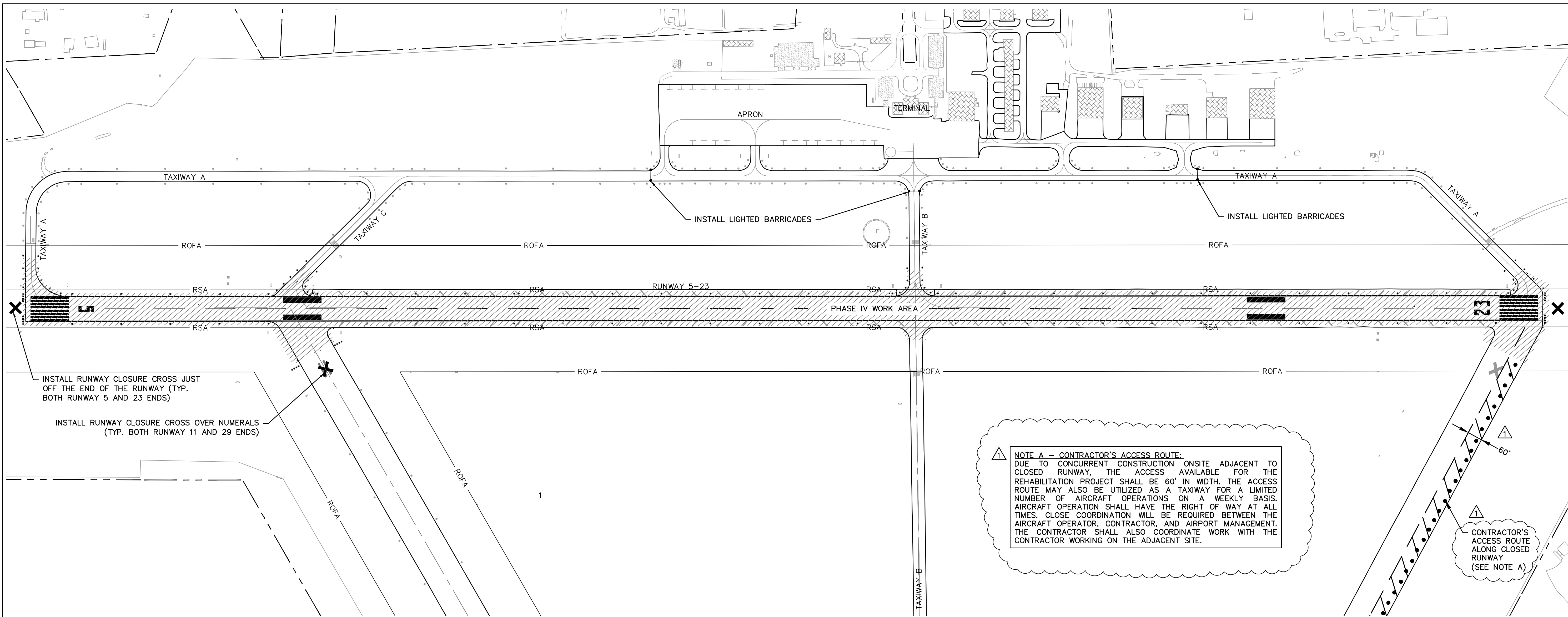
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
RUNWAY 5-23 REHABILITATION
PHASING PLAN - PHASE III



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A1.8



PHASE IV

LEGEND

- APPROXIMATE AIRPORT PROPERTY LINE
- RSA — RUNWAY SAFETY AREA
- ROFA — RUNWAY OBJECT FREE AREA
- ▨ PHASE IV WORK AREA

SEQUENCE OF CONSTRUCTION - PHASE IV:
 PHASE IV CONSISTS OF GROOVING RUNWAY 5-23 AND FINAL MARKING OF RUNWAY 5-23 WHICH SHALL TAKE PLACE AFTER SPECIFIED CURING PERIOD. THE GEORGETOWN COUNTY AIRPORT WILL BE CLOSED FOR WORK IN PHASE IV AS DESCRIBED BELOW.

GROOVING:
 THE CONTRACTOR SHALL HAVE 10 CALENDAR NIGHTS BETWEEN THE HOURS OF 9:00 PM AND 6:00 AM TO COMPLETE THE GROOVING ON RUNWAY 5-23. EACH NIGHT THE CONTRACTOR SHALL CLOSE THE RUNWAY AND TAXIWAYS BY PLACING BARRICADES AND CLOSURE CROSSES AS SHOWN ON THIS SHEET. EACH MORNING ALL RUNWAYS AND TAXIWAYS SHALL BE REOPENED FOR USE BY AIRCRAFT BY 6:00 AM.

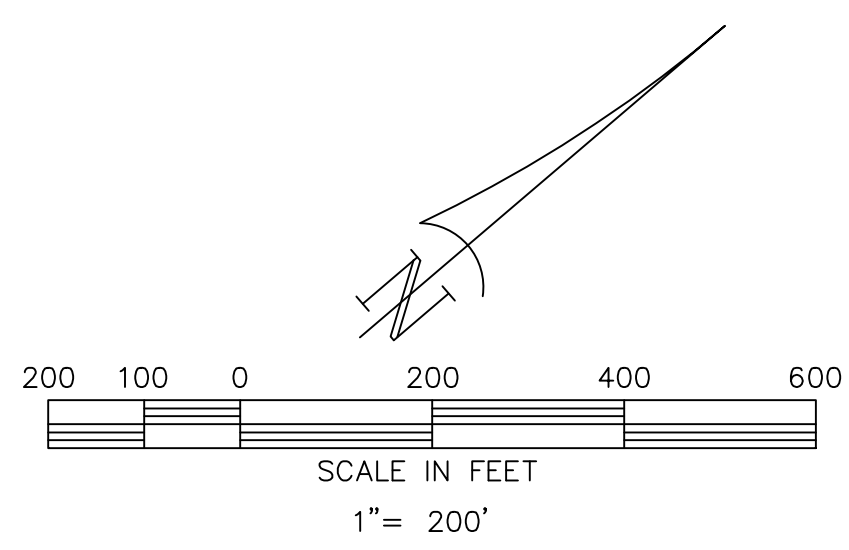
1. CLOSE RUNWAY 5-23 AND RUNWAY 11-29 BY PLACING LIGHTED CLOSURE CROSSES ON EACH END. INSTALL LIGHTED BARRICADES AS SHOWN TO CLOSE TAXIWAYS AND KEEP AIRCRAFT FROM TAXIING ON TO CLOSED RUNWAYS.
2. COMPLETE GROOVING OPERATIONS.
3. AFTER GROOVING IS COMPLETE, CONTRACTOR TO THOROUGHLY CLEAN (INCLUDING PRESSURE WASHING AS NEEDED) RUNWAY 5-23 PRIOR TO MARKING TO BE SURE ALL SEDIMENT AND FROM GROOVING OPERATIONS IS REMOVED AND PAVEMENT IS READY FOR MARKING.
4. EACH MORNING COMPLETE SAFETY INSPECTION, REMOVE CLOSURE CROSSES AND BARRICADES, AND REOPEN RUNWAY 5-23 TO AIR OPERATIONS.

MARKING:
 AFTER GROOVING AND THOROUGH CLEANING, THE CONTRACTOR SHALL HAVE ONE CALENDAR DAY FOR A TWELVE HOUR PERIOD TO COMPLETE THE FINAL MARKING INCLUDING REFLECTIVE BEADS AS REQUIRED ON RUNWAY 5-23. THE ONE DAY CLOSURE SHALL OCCUR ON EITHER A TUESDAY OR WEDNESDAY AS COORDINATED WITH THE AIRPORT MANAGEMENT. CONTRACTOR SHALL CLOSE THE RUNWAY AND TAXIWAYS BY PLACING BARRICADES AND CLOSURE CROSSES AS SHOWN ON THIS SHEET.

1. CLOSE RUNWAY 5-23 AND RUNWAY 11-29 BY PLACING LIGHTED CLOSURE CROSSES ON EACH END. INSTALL LIGHTED BARRICADES AS SHOWN TO CLOSE TAXIWAYS AND KEEP AIRCRAFT FROM TAXIING ON TO CLOSED RUNWAYS.
2. COMPLETE FINAL MARKING AND BEAD APPLICATION AS REQUIRED.
3. COMPLETE SAFETY INSPECTION, REMOVE CLOSURE CROSSES AND BARRICADES, AND REOPEN RUNWAY 5-23 TO AIR OPERATIONS.

NOTE A - CONTRACTOR'S ACCESS ROUTE:
 DUE TO CONCURRENT CONSTRUCTION ONSITE ADJACENT TO CLOSED RUNWAY, THE ACCESS AVAILABLE FOR THE REHABILITATION PROJECT SHALL BE 60' IN WIDTH. THE ACCESS ROUTE MAY ALSO BE UTILIZED AS A TAXIWAY FOR A LIMITED NUMBER OF AIRCRAFT OPERATIONS ON A WEEKLY BASIS. AIRCRAFT OPERATION SHALL HAVE THE RIGHT OF WAY AT ALL TIMES. CLOSE COORDINATION WILL BE REQUIRED BETWEEN THE AIRCRAFT OPERATOR, CONTRACTOR, AND AIRPORT MANAGEMENT. THE CONTRACTOR SHALL ALSO COORDINATE WORK WITH THE CONTRACTOR WORKING ON THE ADJACENT SITE.

CONTRACTOR'S ACCESS ROUTE ALONG CLOSED RUNWAY (SEE NOTE A)

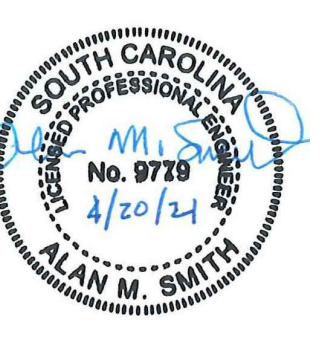


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RUNWAY 5-23 REHABILITATION
PHASING PLAN - PHASE IV



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