

ROADWAY CONSTRUCTION PLANS

US-54 SIDEWALK PROJECT

CITY OF CAMDENTON, MISSOURI PHASE 1 TAP-9900(563) & PHASE 2 TAP-9900(529)

DESIGN STANDARDS

SIDEWALK CONSTRUCTION: PROWAG/MODOT
ROADWAY CONSTRUCTION: MODOT
SIGNAL/LIGHTING/PAVEMENT MARKING/SIGNING: MODOT

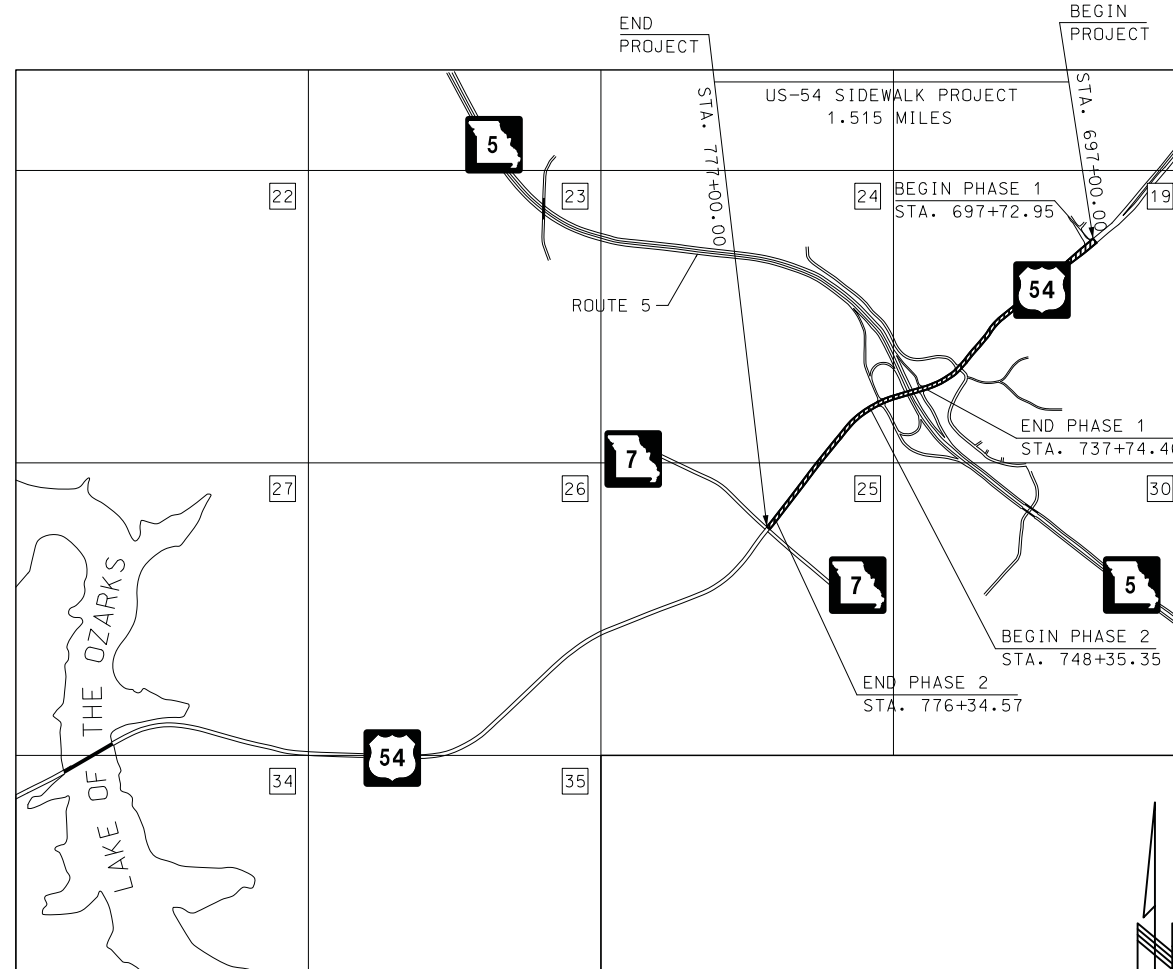
TOWNSHIP 38N RANGE 16W SECTION 19
TOWNSHIP 38N RANGE 17W SECTIONS 24 & 25

INDEX OF SHEETS

TITLE SHEET	1
TYPICAL SECTIONS (4 SHEETS)	2
SUMMARY OF QUANTITIES (10 SHEETS)	3
PLAN SHEETS	4-18
REFERENCE POINTS	19
COORDINATE POINTS	20-21
TRAFFIC CONTROL SHEETS	22-29
SWPPP SHEETS	30-36
SIGNALS	37-48
SIGNING & PAVEMENT MARKING	49-57
CROSS SECTIONS	58-122

LEGEND

EXISTING ROADWAY EASEMENT	-----	EXISTING POWER POLE	□ PP
EXISTING RIGHT-OF-WAY	-----	POWER POLE	○ PILE
SAWCUT LINE	-----	TELEPHONE POLE	○ TEL
SLOPE LIMIT LINE	-----SL-----	GUY WIRE	○ GUY
PROPOSED RIGHT-OF-WAY	-----	GUY WIRE	○ TEL
PROPERTY LINE	-----	POWER AND LIGHT	○ TEL
FENCE	-----	TELEPHONE PEDESTAL	○ TEL
EXISTING CABLE TV LINE	-----	MAILBOX	○ TEL
PROPOSED POWER/CABLE TV LINE	-----	PROPOSED E ROAD	○ TEL
TELEPHONE LINE	-----	SIGN	○ TEL
OVERHEAD POWER LINE	-----	TRAFFIC MOVEMENT	○ TEL
UNDERGROUND POWER LINE	-----	ADJUST TO GRADE	○ TEL
PROPOSED UNDERGROUND POWER LINE	-----	TO BE REMOVED	○ TEL
FIBER LINE	-----	TO BE REMOVED AND REPLACED	○ TEL
GAS LINE	-----	TO BE RELOCATED	○ TEL
WATERMAIN	-----	USE IN PLACE	○ TEL
EXIST. GUARDRAIL	-----	DO NOT DISTURB	○ TEL
GATE VALVE WITH VALVE BOX.	-----	TO BE ABANDONED	○ TEL
FIRE HYDRANT ASSEMBLY	-----	TO BE ABANDONED AND FILLED	○ TEL
MJ PLUG WITH THRUST BLOCK	-----	TO BE REMOVED BY OTHERS	○ TEL
EXIST. VEGETATION	-----	REMOVED BY OTHERS	○ TEL
EXIST. MANHOLE/PROPOSED	-----	REMOVED AND RELOCATED	○ TEL
STORM SEWER MANHOLE	-----	TYPICAL	○ TEL
ELECTRICAL MANHOLE	-----	CURB AND GUTTER	○ TEL
ELECTRICAL METER	-----	VARIAS	○ TEL
TELEPHONE MANHOLE	-----	EXISTING HOUSE	○ TEL
GAS DISCONNECT	-----	EXISTING BUILDING	○ TEL
TELEPHONE AND CABLE	-----	PERMANENT SIDEWALK AND UTILITY EASEMENT	○ TEL
FIRE HYDRANT	-----	PERMANENT UTILITY EASEMENT	○ TEL
SPRINKLER HEAD	-----	TEMPORARY CONSTRUCTION EASEMENT	○ TEL
WATER SPIGOT	-----	CONCRETE	○ TEL
IRRIGATION CONTROL VALVE	-----	PROFILE GRADE LINE	○ TEL
WATER VALVE	-----	CURB RAMP & DOME	○ TEL
WATER METER	-----	POWER POLE	○ TEL
GAS VALVE	-----		
GAS METER	-----		
POWER SUPPLY	-----		



NOT TO SCALE

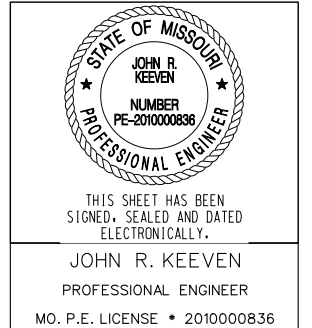
UTILITY OWNERS	CONTACT PERSON	
CITY OF CAMDENTON WATER & SEWER	437 WEST US HIGHWAY 54 CAMDENTON, MO 65020	BILL JEFFRIES (573) 346-7293
CENTURYLINK	P.O. BOX 1099 ROLLA, MO 65402	GARY SHOCKLEY (573) 341-0479
SUMMIT NATURAL GAS	14108 N. HWY 5 SUNRISE BEACH, MO 65079	DAVID MORGAN (573) 374-1244 *668
MODOT-SIGNALS AND LIGHTING	740 MODOT DRIVE JEFFERSON CITY, MO 65102	KEVIN EGGEMEYER (573) 526-3207
CHARTER COMMUNICATIONS	5151 OSAGE BEACH PKWY OSAGE BEACH, MO 65065	THOMAS STAACK (573) 216-6434
AT&T	4 SOUTH OAK STREET ELDON, MO 65026	BRIAN MALONEY (573) 392-9829
LACLEDE ELECTRIC	1400 E. ROUTE 66 LEBANON, MO 65536	KURT GREMLER (417) 532-3164 SHANE DUSTIN (417) 588-7793

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APPROVED BY:
Rebecca Kingston
CITY CLERK
LPA RESPONSIBLE PERSON IN CHARGE
DATE: 12/21/2015

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MISSOURI ONE CALL: 1-800-DIG-RITE (1-800-344-7483)

ENGINEERS AUTHENTICATION
THE RESPONSIBILITY FOR PROFESSIONAL ENGINEERING LIABILITY ON THIS PROJECT IS HEREBY LIMITED TO THIS SET OF PLANS AUTHENTICATED BY THE SEAL, SIGNATURE AND DATE HEREUNDER ATTACHED.
RESPONSIBILITY IS DISCLAIMED FOR ALL OTHER ENGINEERING PLANS INVOLVED IN THIS PROJECT AND SPECIFICALLY EXCLUDES REVISIONS AFTER THIS DATE UNLESS REAUTHENTICATED.
C.M.T.
John R. Keeven
SIGNATURE
DATE: 12/29/15



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SHEET NO. 1

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JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

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NUMBER	BY	DATE

TYPICAL SECTIONS
 SHEET 1 OF 4
 CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

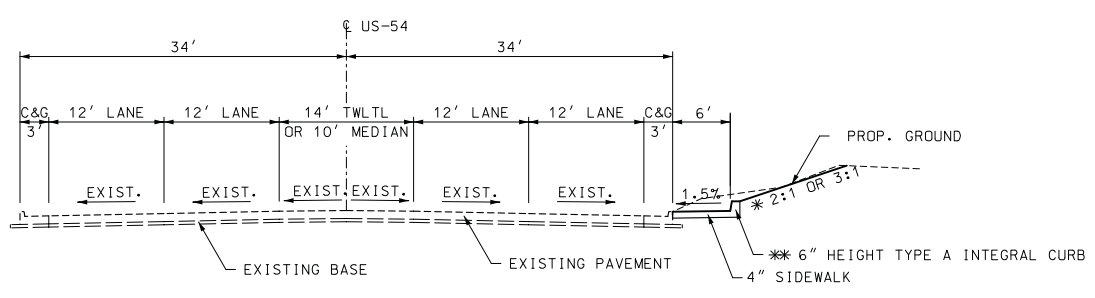
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DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

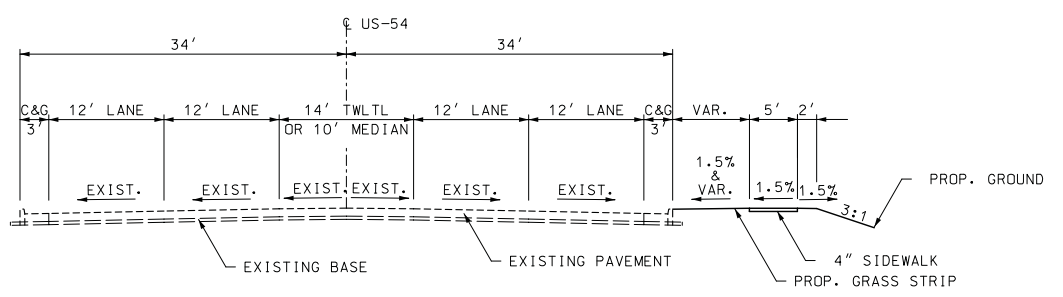
SHEET 2 OF 122

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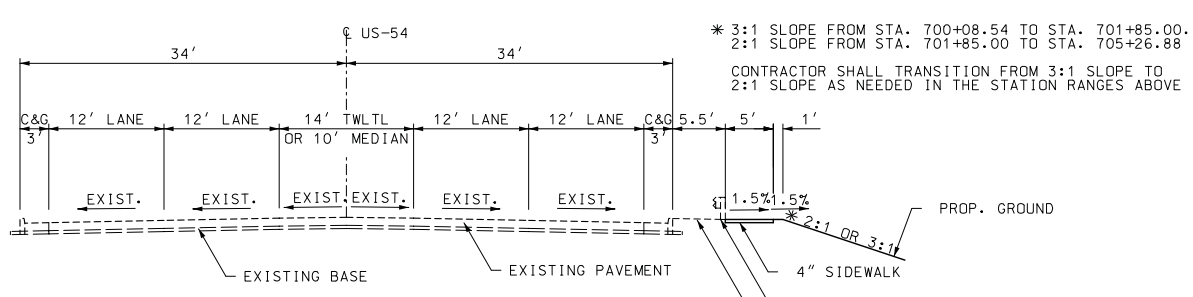


1 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 698+99.88 TO STA. 699+64.50

* 2:1 ROCK LINED SLOPE FROM STA. 698+99.88 TO STA. 699+38.15 WITH PERMANENT EROSION CONTROL GEOTEXTILE
 CONTRACTOR SHALL TRANSITION FROM 2:1 SLOPE TO 3:1 SLOPE AS NEEDED IN THE STATION RANGES ABOVE
 ** 6" HEIGHT TYPE A INTEGRAL CURB FROM STA. 698+99.88 TO STA. 399+36.86

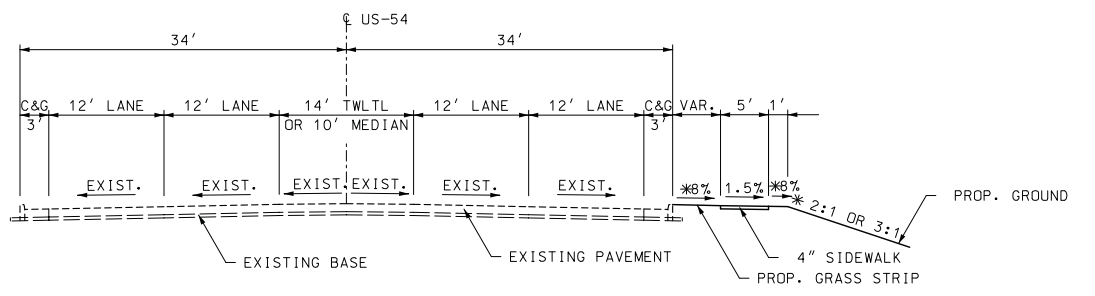


2 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 699+64.50 TO STA. 700+14.75



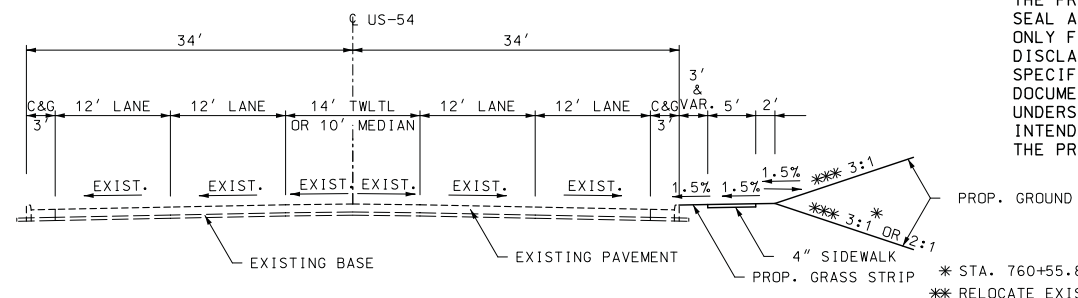
3 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 700+14.75 TO STA. 705+26.88

* 3:1 SLOPE FROM STA. 700+08.54 TO STA. 701+85.00
 2:1 SLOPE FROM STA. 701+85.00 TO STA. 705+26.88
 CONTRACTOR SHALL TRANSITION FROM 3:1 SLOPE TO 2:1 SLOPE AS NEEDED IN THE STATION RANGES ABOVE



4 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 705+54.16 TO STA. 706+37.88
 STA. 715+26.37 TO STA. 715+39.66
 ** STA. 716+59.20 TO STA. 716+97.10
 STA. 730+12.47 TO STA. 730+27.15

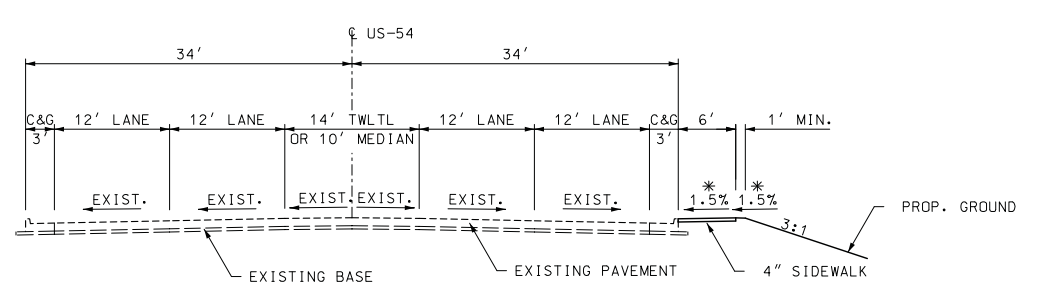
* STA. 705+85.00 TO STA. 706+00.00 TREELAWN TRANSITIONS FROM -8% TO +1.5% AND PROP. GROUND SLOPE TRANSITIONS FROM 2:1 TO 3:1
 ** 2:1 ROCK LINED SLOPE FROM STA. 716+59.20 TO STA. 716+97.10 WITH PERMANENT EROSION CONTROL GEOTEXTILE



5 SECTION ON TANGENT
 TYPICAL SECTION US-54

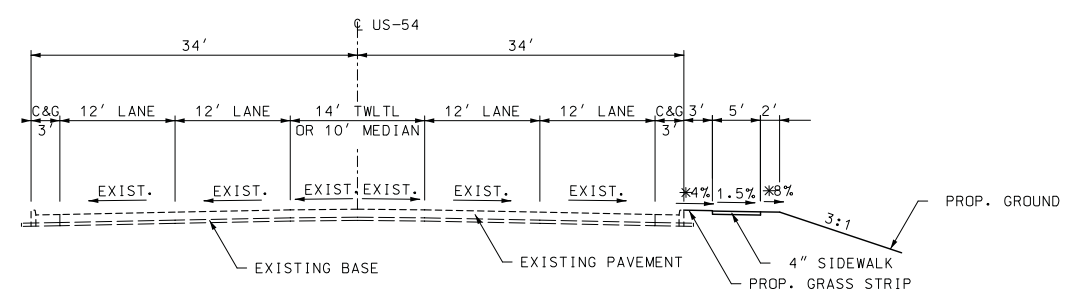
- STA. 706+76.27 TO STA. 707+68.54
- STA. 708+17.08 TO STA. 708+88.45
- STA. 709+48.38 TO STA. 710+72.37
- STA. 711+22.11 TO STA. 711+53.48
- STA. 711+83.33 TO STA. 712+45.27
- STA. 712+75.28 TO STA. 713+20.06
- STA. 713+40.11 TO STA. 713+73.84
- STA. 717+37.67 TO STA. 719+12.25
- STA. 719+44.75 TO STA. 721+49.65
- ** STA. 724+02.50 TO STA. 724+30.25
- ** STA. 724+50.10 TO STA. 724+84.20
- ** STA. 725+10.06 TO STA. 725+40.29
- STA. 725+64.25 TO STA. 727+61.30
- STA. 727+90.15 TO STA. 729+87.95
- STA. 749+63.96 TO STA. 750+12.56
- STA. 750+71.56 TO STA. 751+15.41
- STA. 752+12.34 TO STA. 752+55.20
- STA. 752+95.64 TO STA. 753+43.36
- STA. 753+72.83 TO STA. 753+98.95
- STA. 757+31.76 TO STA. 758+00.65
- ** STA. 760+55.29 TO STA. 760+92.18 (6' WIDE SIDEWALK)
- ** STA. 762+58.22 TO STA. 763+42.39
- ** STA. 763+64.55 TO STA. 764+44.40
- STA. 768+39.23 TO STA. 768+75.79
- STA. 773+18.10 TO STA. 773+59.56
- STA. 774+98.13 TO STA. 775+48.73

SEE CROSS SECTIONS FOR VARIABLE DIMENSIONS/SLOPES & MORE DETAIL



6 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 713+73.84 TO STA. 715+26.37
 STA. 730+27.15 TO STA. 731+63.45

* SIDEWALK SLOPES AT 1.5% AWAY FROM THE STREET AT STA. 715+24.29



7 SECTION ON TANGENT
 TYPICAL SECTION US-54
 ** STA. 721+81.54 TO STA. 723+77.43

* STA. 723+25.00 TO STA. 723+40.00 TREELAWN TRANSITIONS FROM 4% OR 8% TO 1.5%
 ** STA. 723+57.68 +/- TO STA. 723+77.10 RELOCATE EXISTING ROCK TO FINAL SURFACE WITH PERMANENT EROSION CONTROL GEOTEXTILE

APPLICATION RATE
 ASPHALTIC CONCRETE MIXTURE PG64-22 (SP125C MIX) 1.927 TON/CY

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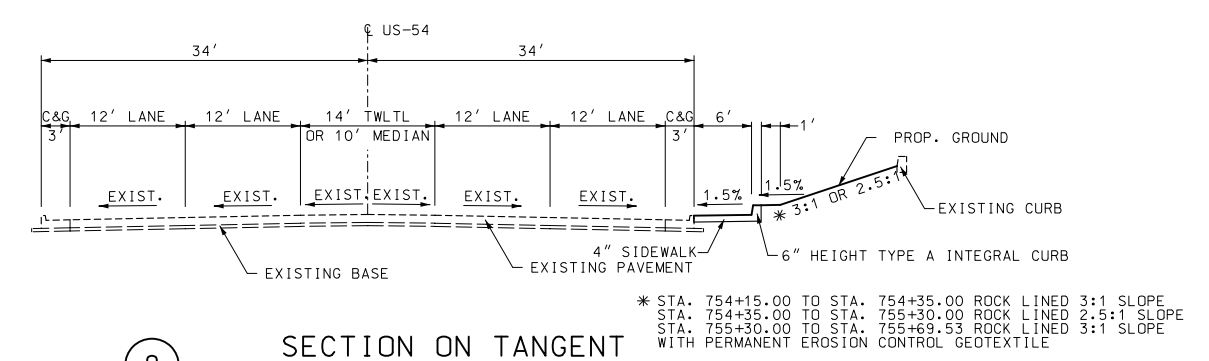
JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
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NUMBER	BY	DATE

TYPICAL SECTIONS
 SHEET 2 OF 4

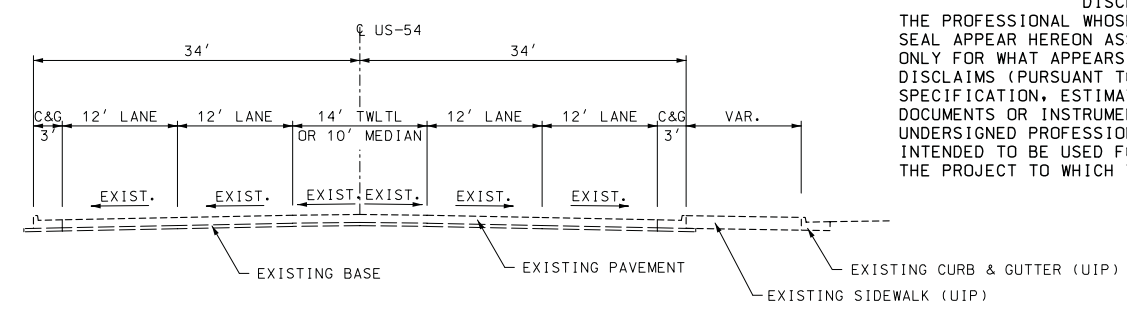
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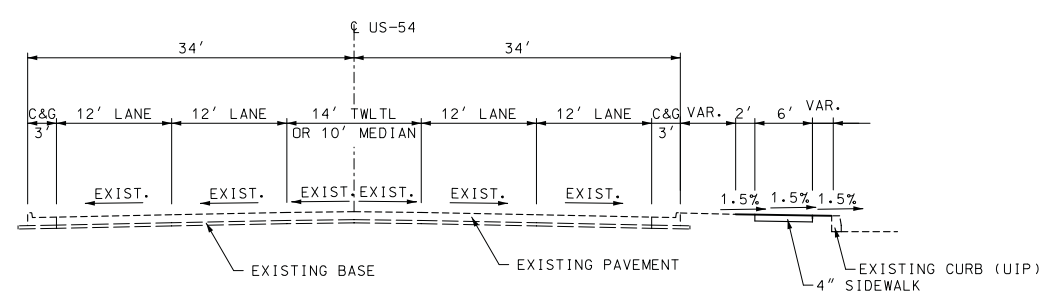


8 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 754+15.00 TO STA. 754+69.53
 STA. 755+08.98 TO STA. 755+30.00
 STA. 755+30.00 TO STA. 755+69.60

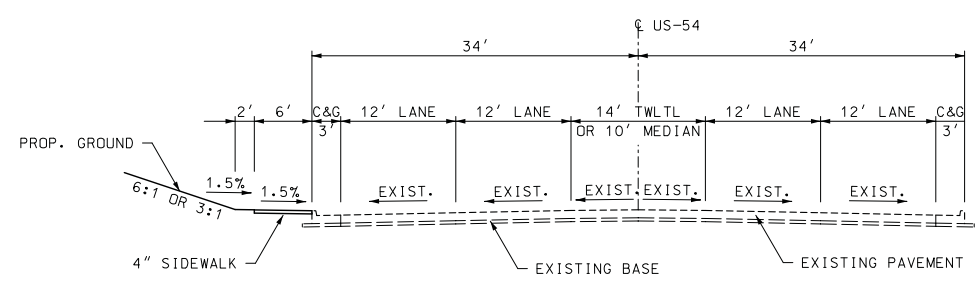
* STA. 754+15.00 TO STA. 754+35.00 ROCK LINED 3:1 SLOPE
 STA. 754+35.00 TO STA. 755+30.00 ROCK LINED 2.5:1 SLOPE
 STA. 755+30.00 TO STA. 755+69.53 ROCK LINED 3:1 SLOPE
 WITH PERMANENT EROSION CONTROL GEOTEXTILE



12 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 716+00.18 TO STA. 716+40.72

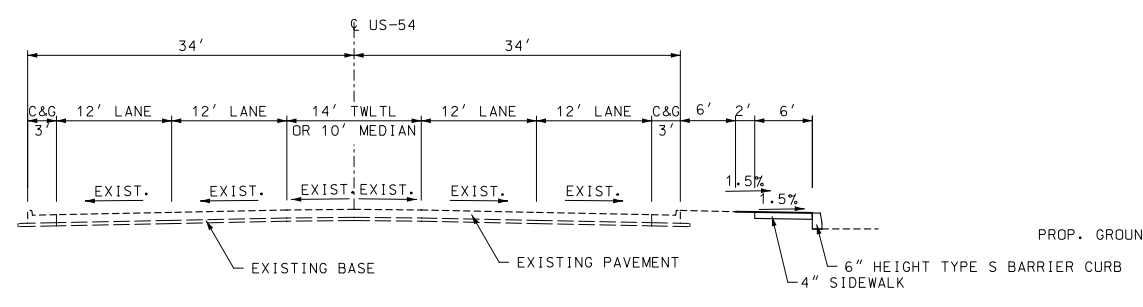


9 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 758+42.98 TO STA. 758+83.31

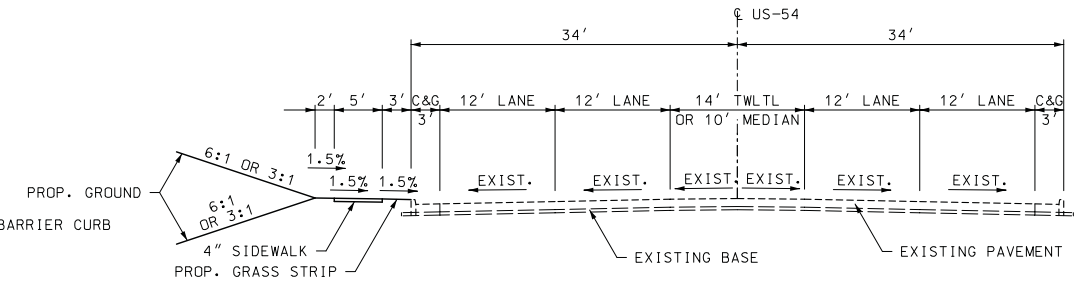


13 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 731+46.11 TO STA. 732+69.60
 STA. 737+12.94 TO STA. 737+26.17

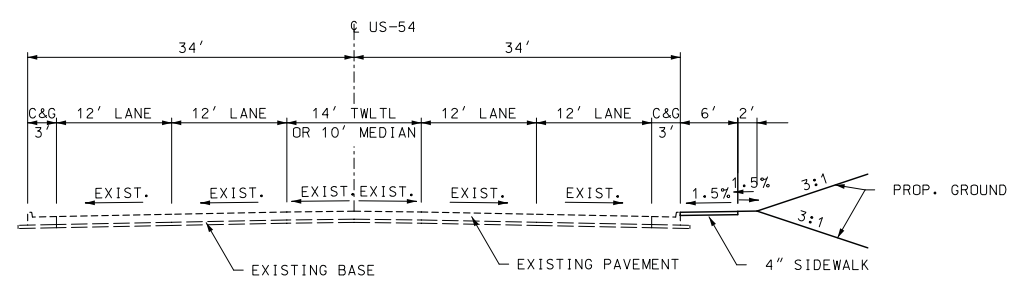
SEE CROSS SECTIONS FOR VARIABLE DIMENSIONS/SLOPES & MORE DETAIL



10 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 759+20.10 TO STA. 759+71.28
 STA. 760+11.13 TO STA. 760+35.45

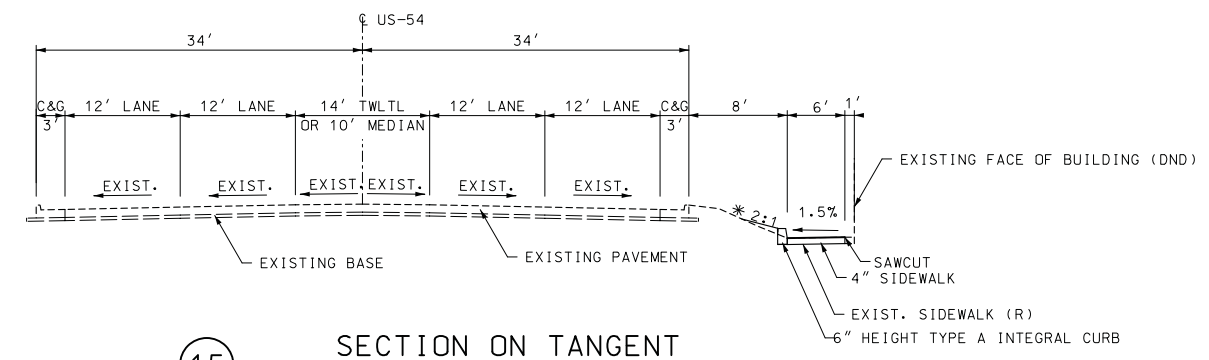


14 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 732+69.60 TO STA. 734+00.35
 STA. 734+52.88 TO STA. 735+48.94
 STA. 735+93.11 TO STA. 736+12.98
 STA. 736+42.62 TO STA. 737+12.94



11 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 753+98.95 TO STA. 754+15.00
 STA. 756+02.98 TO STA. 756+98.52
 * STA. 761+23.62 TO STA. 762+24.32
 STA. 767+79.37 TO STA. 768+14.43
 STA. 771+92.19 TO STA. 772+84.79

** RELOCATE EXISTING ROCK TO FINAL SURFACE WITH PERMANENT EROSION CONTROL GEOTEXTILE



15 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 764+65.37 TO STA. 765+22.98

* ROCK LINED 2:1 SLOPE WITH PERMANENT EROSION CONTROL GEOTEXTILE

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TYPICAL SECTIONS
 SHEET 3 OF 4

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 US-54 SIDEWALK PROJECT
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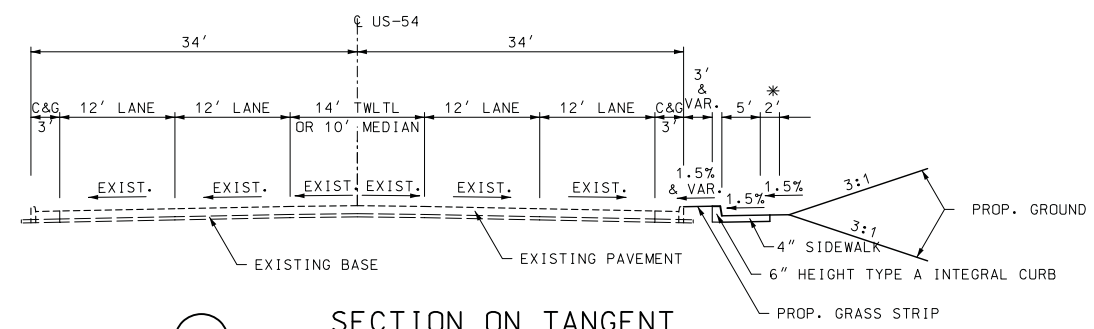
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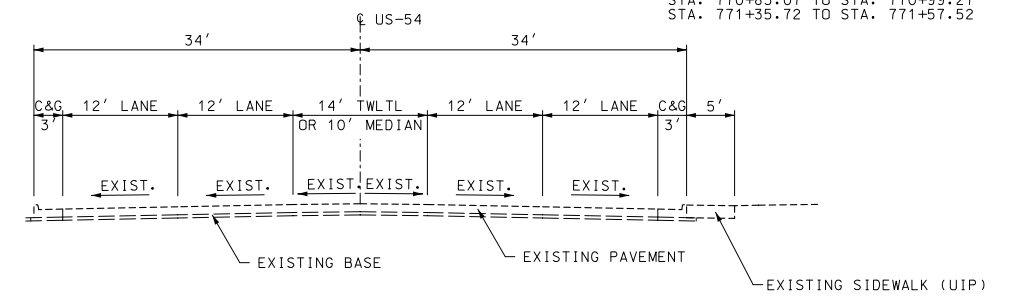
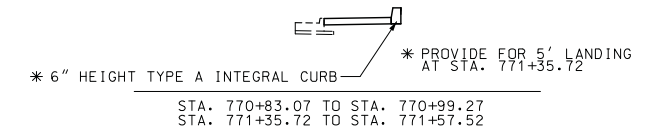
SHEET 2 OF 122

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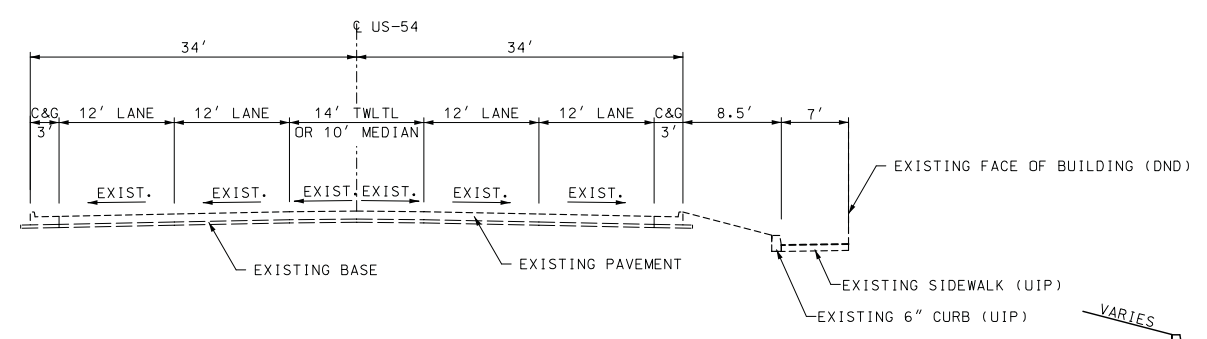


16 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 751+74.12 TO STA. 751+87.43
 STA. 769+00.00 TO STA. 769+27.75
 STA. 769+61.30 TO STA. 769+98.24
 STA. 770+32.82 TO STA. 770+58.54
 STA. 773+86.76 TO STA. 774+12.72
 STA. 774+39.97 TO STA. 774+64.84
 * FROM STA. 699+64.50 TO STA. 699+84.90
 USE A 1' MIN BUFFER

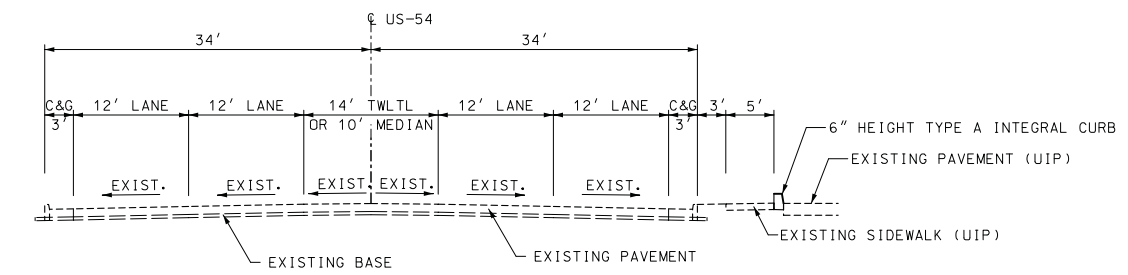
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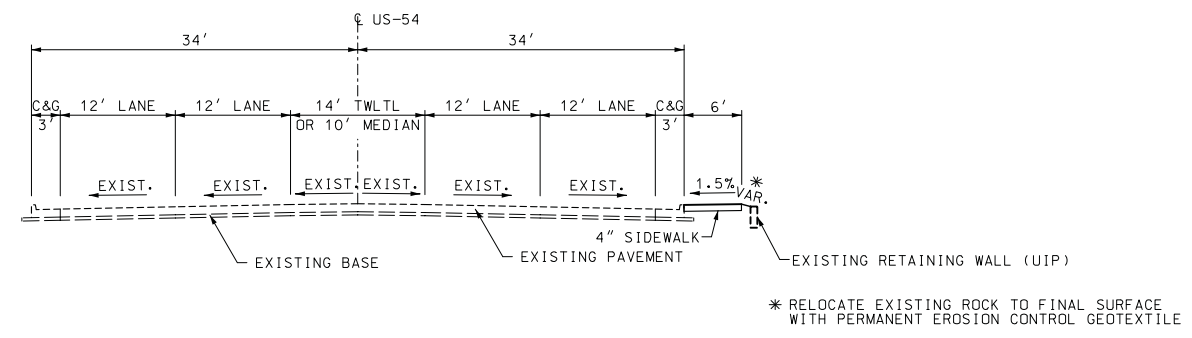
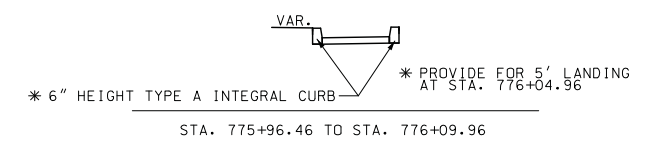
19 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 770+83.07 TO STA. 771+52.52



17 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 765+52.78 TO STA. 766+44.88
 * 6" HEIGHT TYPE A INTEGRAL CURB
 * PROVIDE FOR 5' LANDING AT STA. 765+63.17
 STA. 765+52.78 TO STA. 765+68.17
 STA. 766+30.96 TO STA. 766+44.88



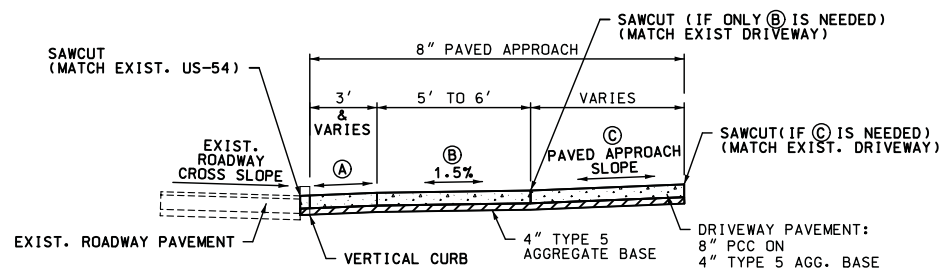
20 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 775+96.46 TO STA. 776+34.57



18 SECTION ON TANGENT
 TYPICAL SECTION US-54
 STA. 766+77.21 TO STA. 767+49.93
 * RELOCATE EXISTING ROCK TO FINAL SURFACE WITH PERMANENT EROSION CONTROL GEOTEXTILE

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SEE CROSS SECTIONS FOR SECTION OF EACH DRIVEWAY



21 PAVED APPROACH

STATION	PAVED APPROACH SLOPES		
	A	B	C
705+40.54 RT	-3.3%	-1.5%	
706+57.00 RT	+7.6%	+1.5%	+4.0%
707+91.49 RT	+8.0%	+1.5%	+8.0%
* 709+18.68 RT (TURNER PKWY PVT)	+8.0%	+1.5%	
710+97.32 RT	+2.1%	+1.5%	
711+68.39 RT	+4.0%	-1.5%	
712+60.23 RT	+4.7%	-1.5%	
713+30.10 RT	+2.4%	-1.5%	
715+70.17 RT	+3.6%	+1.5%	
716+51.46 RT	+1.0%	+1.0%	
717+20.66 RT	+6.9%	+1.5%	
719+30.26 RT	+3.6%	+1.5%	
721+67.07 RT	+3.0%	-1.5%	
723+86.37 RT	+3.4%	+1.5%	
724+37.19 RT	+8.0%	+1.5%	+4.0%
724+97.42 RT	+5.3%	+1.5%	
725+52.22 RT	+5.3%	+1.5%	
727+75.22 RT	+5.7%	-1.5%	
730+00.00 RT	+4.9%	-1.5%	
736+29.33 LT	+7.8%	+1.75%	+2.25%
737+46.26 LT	+11.75%	+9.75%	+1.75%

STATION	PAVED APPROACH SLOPES		
	A	B	C
750+42.15 RT	+3.5%	-1.5%	
751+45.04 RT	+3.2%	+1.5%	
752+00.00 RT	-3.3%	+1.5%	
751+75.91 RT	+5.3%	-1.5%	
753+57.91 RT	+7.0%	+1.5%	
* 755+06.79 RT (FIFTH ST.)	+4.0%	+1.5%	+15.0%
755+86.23 RT	+8.0%	+1.5%	+12.0%
757+15.04 RT	+3.0%	+1.5%	
* 758+50.41 RT (THIRD ST.)	-2.7%	-1.5%	
759+01.45 RT	-0.30%	-1.5%	
759+89.59 RT	-6.3%	-1.5%	
760+45.62 RT	-12.0%	-1.5%	-28.0%
761+07.94 RT	-10.0%	-1.5%	-15.0%
* 762+41.39 RT (FIRST ST.)	-4.0%	-1.5%	-15.0%
763+53.48 RT	-7.2%	+1.5%	
764+54.94 RT	-15.0%	-1.5%	
* 765+38.07 RT (GRANT AVE.)	-8.0%	-1.5%	-15.0%
766+61.17 RT	-8.0%	-1.5%	-14.8%
768+27.19 RT	+1.3%	+1.3%	

STATION	PAVED APPROACH SLOPES		
	A	B	C
768+87.67 RT	+1.3%	+1.5%	
770+15.49 RT	+3.9%	+1.5%	
770+70.28 RT	+4.7%	-1.5%	
773+01.75 RT	+4.9%	+1.5%	
773+73.86 RT	+1.2%	+1.2%	
774+26.24 RT	+1.5%	+1.5%	
774+81.49 RT	+3.5%	+1.5%	
* 775+70.36 RT (CIRCLE COURT)	+4.8%	-1.5%	

* = CURB RAMPS REQUIRED AT STREETS, SEE PLAN SHEETS FOR CURB RAMP DETAILS

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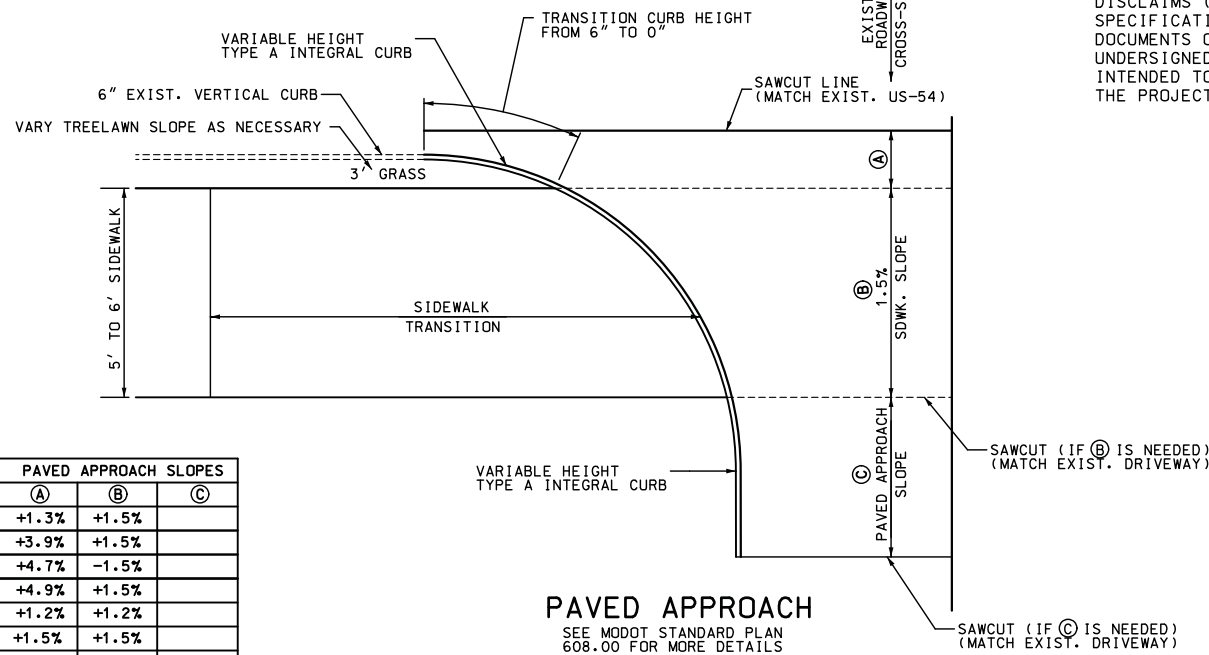
REVISIONS		
NUMBER	BY	DATE

TYPICAL SECTIONS
SHEET 4 OF 4

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

DATE: 5/19/2016
JOB No: 15480-02-00
15480-03-00

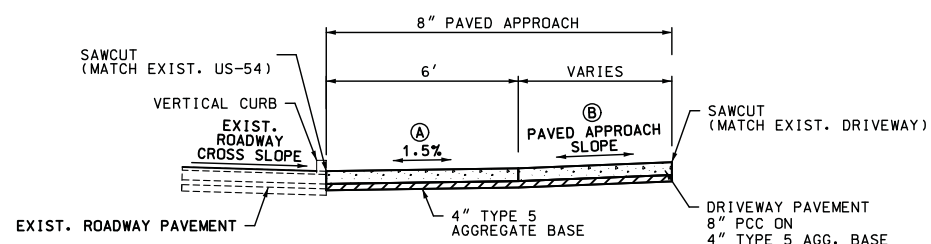


PAVED APPROACH

SEE MDDOT STANDARD PLAN 608.00 FOR MORE DETAILS

NOTES:

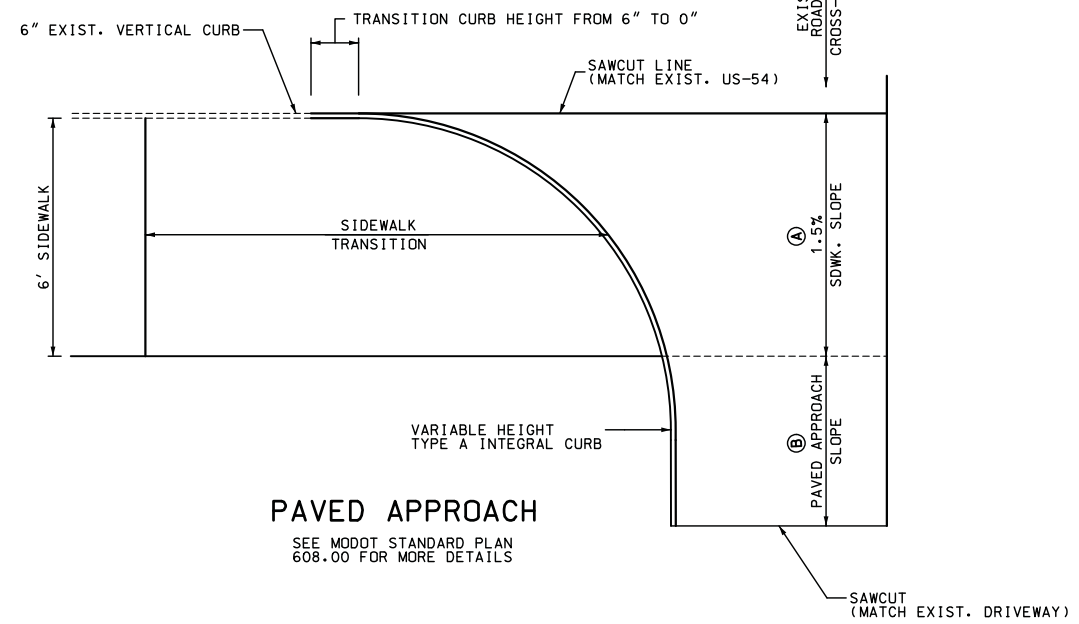
- (1) SIDEWALKS SHALL TRANSITION AS GRADUAL AS POSSIBLE AT ALL PAVED APPROACHES. IDEAL TRANSITION SLOPES SHALL BE IN THE 2% TO 3% OR LESS RANGE. MAXIMUM TRANSITION SLOPES SHALL BE 5%.
- (2) PAVED APPROACH SLOPES SHOWN ARE AT THE CENTERLINE. PAVED APPROACH SLOPES SHALL BE WARPED AT THE ENDS.
- (3) MATCH EXIST. RADIUS RETURNS AT PAVED APPROACHES.



22 PAVED APPROACH

STATION	PAVED APPROACH SLOPES	
	A	B
735+72.08 LT	+1.75%	-8.7%
767+65.87 RT	-1.5%	-10.4%
* 771+74.77 RT (ILLINOIS ST.)	-1.5%	-15.0%

* = CURB RAMPS REQUIRED AT STREETS, SEE PLAN SHEETS FOR CURB RAMP DETAILS



PAVED APPROACH

SEE MDDOT STANDARD PLAN 608.00 FOR MORE DETAILS

REMOVAL OF IMPROVEMENTS (1 OF 2)				
SHEET	LOCATION	FROM STATION	TO STATION	DESCRIPTION
PLAN 1	US 54 RT	697+77	697+82	6.5' OF CURB
PLAN 1	US 54 RT	697+95	698+00	6.9' OF CURB
PLAN 1	US 54 RT	698+11	698+11	7.0' OF CURB
PLAN 1	US 54 RT	698+66	698+66	7.0' OF CURB
PLAN 1	US 54 RT	698+80	698+86	9.7' OF CURB
PLAN 1	US 54 RT	699+00	699+04	6.6' OF CURB
PLAN 6	US 54 RT	727+42	727+60	19.3' OF CURB
PLAN 7	US 54 RT	731+47	731+63	16.1' OF CURB
PLAN 7	US 54 LT	731+46	731+65	19.5' OF CURB
PLAN 7	US 54 LT	732+40	732+62	25.9' OF CURB
PLAN 1	US 54 RT	697+95	698+11	8.8 SY CONC. ISLAND
PLAN 1	US 54 RT	698+66	698+86	9.8 SY CONC. ISLAND
PLAN 7	US 54 LT	732+74		FIBER OPTIC PULLBOX SEE FIBER OPTIC CONNECTION JSP FOR MORE DETAILS
PLAN 7	US 54 LT	733+91		FIBER OPTIC PULLBOX SEE FIBER OPTIC CONNECTION JSP FOR MORE DETAILS
PLAN 7	US 54 LT	734+71		FIBER OPTIC PULLBOX SEE FIBER OPTIC CONNECTION JSP FOR MORE DETAILS
PLAN 8	US 54 LT	736+00		FIBER OPTIC PULLBOX SEE FIBER OPTIC CONNECTION JSP FOR MORE DETAILS
PLAN 2	US 54 RT	705+22	705+60	21.9 SY OF PAVED APPROACH
PLAN 2	US 54 RT	706+22	906+91	51.0 SY OF PAVED APPROACH
PLAN 2	US 54 RT	707+53	708+32	69.2 SY OF PAVED APPROACH
PLAN 3	US 54 RT	708+81	709+56	61.6 SY OF PAVED APPROACH
PLAN 3	US 54 RT	710+66	711+29	50.6 SY OF PAVED APPROACH
PLAN 3	US 54 RT	711+46	711+91	31.4 SY OF PAVED APPROACH
PLAN 3	US 54 RT	712+38	712+82	31.8 SY OF PAVED APPROACH
PLAN 3	US 54 RT	713+13	713+47	22.6 SY OF PAVED APPROACH
PLAN 4	US 54 RT	715+33	716+07	57.6 SY OF PAVED APPROACH
PLAN 4	US 54 RT	716+34	716+70	22.0 SY OF PAVED APPROACH
PLAN 4	US 54 RT	716+91	717+45	46.0 SY OF PAVED APPROACH
PLAN 5	US 54 RT	719+09	719+51	31.7 SY OF PAVED APPROACH
PLAN 5	US 54 RT	721+35	721+89	33.0 SY OF PAVED APPROACH
PLAN 5	US 54 RT	723+70	724+11	29.3 SY OF PAVED APPROACH
PLAN 5	US 54 RT	724+23	724+58	26.5 SY OF PAVED APPROACH
PLAN 6	US 54 RT	724+76	725+16	28.4 SY OF PAVED APPROACH
PLAN 6	US 54 RT	725+32	725+72	29.1 SY OF PAVED APPROACH
PLAN 6	US 54 RT	727+53	727+97	32.6 SY OF PAVED APPROACH
PLAN 6	US 54 RT	729+81	730+19	26.0 SY OF PAVED APPROACH
PLAN 8	US 54 LT	735+48	735+93	59.6 SY OF PAVED APPROACH
PLAN 8	US 54 LT	736+02	736+50	62.8 SY OF PAVED APPROACH
PLAN 8	US 54 LT	737+16	737+75	78.4 SY OF PAVED APPROACH
SIGNING 1	US 54 RT	697+83		1 - PIPE POST, SALVAGE SIGN FOR RELOCATION
SIGNING 1	US 54 RT	699+13		1 - PIPE POST, SALVAGE SIGN FOR RELOCATION
SIGNING 1	US 54 RT	705+72		1 - WOOD POST, SALVAGE SIGN FOR RELOCATION
SIGNING 1	US 54 RT	705+85		1 - PSST POST, SALVAGE SIGN FOR RELOCATION
SIGNING 2	US 54 RT	708+86		1 - PSST POST, SALVAGE SIGN FOR RELOCATION
SIGNING 3	US 54 RT	730+47		1 - PIPE POST, SALVAGE SIGN FOR RELOCATION
SIGNING 3	US 54 LT	733+73		1 - WOOD POST, SALVAGE SIGN FOR RELOCATION
SIGNING 4	US 54 LT	736+77		1 - PSST POST, SALVAGE SIGN FOR RELOCATION
CONTINUED				

- NOTES:
1. REMOVAL OF IMPROVEMENTS INCLUDES SAWCUTS FOR REMOVAL
2. REMOVAL OF EXISTING PAVED APPROACH INCLUDES REMOVAL OF EXISTING CURB
3. ANY CLEARING AND GRUBBING REQUIRED SHALL BE INCIDENTAL TO THE PAY ITEM FOR REMOVAL OF IMPROVEMENTS

UTILITY ADJUSTMENTS BY OTHERS - FOR INFORMATION ONLY				
PLAN SHEET XX OF 15	LOCATION	STATION	DESCRIPTION	REMARKS
1	US 54 RT	699+29.19	MANHOLE	ATG TO BE COMPLETED BY AT&T, TOP OF VAULT (DND); SEE PLANS
4	US 54 RT	713+98.47	LIGHT POLE	TO BE RELOCATED BY LACLEDE ELECTRIC
4	US 54 RT	715+09.39	PULL BOX	ATG TO BE COMPLETED BY LACLEDE ELECTRIC

NOTE: ADDITIONAL QUANTITIES LOCATED IN THE SIGNAL & SIGNING SECTIONS

REMOVAL OF IMPROVEMENTS CONT. (2 OF 2)					
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	SAWCUT L.F.	REMARKS
1	US 54 RT	697+77	697+82	6	SAWCUT FOR CURB REMOVAL
1	US 54 RT	697+95	698+11	48	SAWCUT FOR ISLAND CUT OUT
1	US 54 RT	698+66	698+84	56	SAWCUT FOR ISLAND CUT OUT
1	US 54 RT	699+00	699+04	7	SAWCUT FOR CURB REMOVAL
2	US 54 RT	705+22	705+60	38	SAWCUT FOR PAVED APPROACH REMOVAL
2	US 54 RT	706+22	706+91	107	SAWCUT FOR PAVED APPROACH REMOVAL
2-3	US 54 RT	707+53	708+32	124	SAWCUT FOR PAVED APPROACH REMOVAL
3	US 54 RT	708+81	709+56	134	SAWCUT FOR TURNER PKWY REMOVAL
3	US 54 RT	710+66	711+29	112	SAWCUT FOR PAVED APPROACH REMOVAL
3	US 54 RT	711+46	711+91	69	SAWCUT FOR PAVED APPROACH REMOVAL
3	US 54 RT	712+38	712+82	73	SAWCUT FOR PAVED APPROACH REMOVAL
3	US 54 RT	713+13	713+47	53	SAWCUT FOR PAVED APPROACH REMOVAL
4	US 54 RT	715+33	716+07	134	SAWCUT FOR PAVED APPROACH REMOVAL
4	US 54 RT	716+34	716+70	54	SAWCUT FOR PAVED APPROACH REMOVAL
4	US 54 RT	716+91	171+45	95	SAWCUT FOR PAVED APPROACH REMOVAL
5	US 54 RT	718+97	719+51	84	SAWCUT FOR PAVED APPROACH REMOVAL
5	US 54 RT	721+35	721+89	83	SAWCUT FOR PAVED APPROACH REMOVAL
5	US 54 RT	723+70	724+11	66	SAWCUT FOR PAVED APPROACH REMOVAL
5-6	US 54 RT	724+23	724+58	54	SAWCUT FOR PAVED APPROACH REMOVAL
6	US 54 RT	724+76	725+16	65	SAWCUT FOR PAVED APPROACH REMOVAL
6	US 54 RT	725+32	725+72	63	SAWCUT FOR PAVED APPROACH REMOVAL
6	US 54 RT	727+53	727+97	72	SAWCUT FOR PAVED APPROACH REMOVAL
6-7	US 54 RT	729+81	730+19	61	SAWCUT FOR PAVED APPROACH REMOVAL
7	US 54 RT	731+17	731+63	16	SAWCUT FOR CURB REMOVAL
7	US 54 LT	731+46	731+64	21	SAWCUT FOR CURB REMOVAL
7	US 54 LT	732+40	732+63	26	SAWCUT FOR CURB REMOVAL
8	US 54 LT	735+48	735+93	91	SAWCUT FOR PAVED APPROACH REMOVAL
8	US 54 LT	736+03	736+50	78	SAWCUT FOR PAVED APPROACH REMOVAL
8	US 54 LT	737+17	737+75	102	SAWCUT FOR PAVED APPROACH REMOVAL
				TOTAL	1 LUMP SUM

NOTE: THIS MAY NOT BE AN EXACT TOTAL OF SAWCUTS REQUIRED. ALL REQUIRED SAWCUTS, REGARDLESS IF LISTED ABOVE, SHALL BE INCLUDED IN LUMP SUM COST FOR REMOVAL OF IMPROVEMENTS.

ADJUST TO GRADE						
PLAN SHEET XX OF 15	LOCATION	STATION	ADJUSTING MANHOLE EACH	ADJUSTING PULL BOX EACH	ADJUSTING WATER METERS / WATER VALVES EACH	REMARKS
2	US 54 RT	707+31.22			1	ATG EXISTING WATER VALVE
2	US 54 RT	707+41.15			1	ATG EXISTING WATER VALVE
3	US 54 RT	708+10.37	1			ATG EXISTING MANHOLE
7	US 54 LT	731+48.88		1		RELOCATE SIGNAL BOX
7	US 54 LT	732+59.79		1		ATG LIGHTING BOX
7	US 54 LT	732+65.27		1		ATG SIGNAL BOX
8	US 54 LT	735+47.68			1	ATG EXISTING WATER METER
8	US 54 LT	737+06.82			1	ATG EXISTING WATER METER
			TOTAL	1	3	4

MOBILIZATION
TOTAL = 1 LUMP SUM

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PHASE 1 TAP-9900 (563)

SHEET 1 OF 10

SUMMARY OF QUANTITIES



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 3 OF 122

003-QUANTITIES.dgn

FINAL SLOPE SURFACE

PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	FURNISHING TYPE 2 ROCK DITCH LINER C.Y.	PLACING TYPE 2 ROCK DITCH LINER C.Y.	REMOVAL & PLACEMENT OF EXISTING ROCK S.Y.	PERMANENT EROSION CONTROL GEOTEXTILE S.Y.	TURF TYPE TALL FESCUE SODDING S.Y.	SEEDING COOL SEASON MIXTURES ACRE	REMARKS
1	US 54 RT	698+99.98	699+37.08	7.6	7.6		22.8			FOR BACKSLOPE PROTECTION
1-2	US 54 RT	699+37.08	705+26.88						0.097	TO BE USED IF GRADING IS REQUIRED ALONG EXISTING GUARDRAIL
1-2	US 54 RT	699+63.92	705+27.43						0.067	
2	US 54 RT	705+53.71	706+38.65						0.007	
2	US 54 RT	705+54.16	706+32.41						0.007	
2	US 54 RT	706+75.58	707+69.98					23.3		
2	US 54 RT	706+81.45	707+63.37					25.2		
3	US 54 RT	708+15.69	708+88.96					18.4		
3	US 54 RT	708+22.12	708+86.95					19.9		
3	US 54 RT	709+47.88	710+72.86					33.7		
3	US 54 RT	709+49.59	710+71.19					46.2		
3	US 54 RT	711+21.61	711+53.87					7.8		
3	US 54 RT	711+23.32	711+52.24					8.5		
3	US 54 RT	711+82.90	712+45.76					20.0		
3	US 54 RT	711+84.61	712+44.06					18.7		
3	US 54 RT	712+74.84	713+20.46					36.1		
3	US 54 RT	712+76.47	713+18.74					11.9		
3-4	US 54 RT	713+39.63	715+40.16					179.6		
3-4	US 54 RT	713+41.15	713+73.84					8.5		
4	US 54 RT	714+26.38	715+38.21					2.7		
4	US 54 RT	716+59.20	716+97.10	6.4	6.4		19.1			
4	US 54 RT	716+62.06	716+96.17					10.4		
4-5	US 54 RT	717+37.19	719+15.22					128.9		
4-5	US 54 RT	717+38.09	719+07.26					64.2		
5	US 54 RT	719+44.05	721+49.65					297.4		
5	US 54 RT	719+45.59	721+44.93					66.4		
5	US 54 RT	721+80.97	723+61.53					195.2		
5	US 54 RT	721+82.98	723+60.90					58.9		
5	US 54 RT	723+57.68	723+75.93			4.4	4.4			TO BE USED ON LT SIDE OF SIDEWALK
5	US 54 RT	723+61.53	723+77.94			4.0	4.0			TO BE USED ON RT SIDE OF SIDEWALK
5	US 54 RT	724+02.50	724+30.26			10.3	10.3			TO BE USED ON RT SIDE OF SIDEWALK
5	US 54 RT	724+04.33	724+29.07			8.0	8.0			TO BE USED ON LT SIDE OF SIDEWALK
6	US 54 RT	724+50.10	724+84.72			12.0	12.0			TO BE USED ON RT SIDE OF SIDEWALK
6	US 54 RT	724+51.57	724+82.04			9.0	9.0			TO BE USED ON LT SIDE OF SIDEWALK
6	US 54 RT	725+10.03	725+40.79			8.0	8.0			TO BE USED ON RT SIDE OF SIDEWALK
6	US 54 RT	725+10.74	725+39.16			9.9	9.9			TO BE USED ON LT SIDE OF SIDEWALK
6	US 54 RT	725+63.75	727+61.30					83.4		
6	US 54 RT	725+65.11	727+59.69					64.7		
6	US 54 RT	727+89.65	729+88.46						0.020	
6	US 54 RT	727+91.10	729+86.96						0.018	
7	US 54 RT	730+11.97	731+63.55						0.028	
7	US 54 RT	730+13.38	730+27.16						0.001	
7	US 54 LT	731+44.29	731+54.72					5.1		
7	US 54 LT	731+57.08	731+64.96					1.6		
7	US 54 LT	732+31.10	734+00.08					76.5		
7	US 54 LT	732+69.60	733+99.74					45.2		
7-8	US 54 LT	734+52.40	7335+49.04					45.7		
7-8	US 54 LT	734+52.93	735+47.66					34.3		
8	US 54 LT	735+93.10	736+12.98					7.7		
8	US 54 LT	735+93.95	736+05.30					2.3		
8	US 54 LT	736+42.62	737+18.42					36.5		
8	US 54 LT	736+48.54	737+15.32					21.1		
TOTAL				14	14	66	108	1706	0.3	

NOTE: TYPE 2 ROCK DITCH LINER SHALL BE 1' THICK.

EARTHWORK					
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	LINEAR GRADING CLASS 2 STA.	REMARKS
1-7	US 54 RT	697+72.44	731+63.45	33.9	
7-9	US 54 LT	731+46.10	737+74.46	6.3	
TOTAL				40.2	CONTRACTOR SHALL HAUL AWAY EXCESS EXCAVATION

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PHASE 1 TAP-9900 (563)

SHEET 2 OF 10

SUMMARY OF QUANTITIES



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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 ENGINEERING CORPORATION - 000631

DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

SHEET 3 OF 122

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

PAVED APPROACH						
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	PAVED APPROACH 8 IN. S.Y.	TYPE 5 AGGREGATE FOR BASE (4 IN.THICK) S.Y.	REMARKS
2	US 54 RT	705+21.77	705+59.54	35.8	35.8	
2	US 54 RT	706+21.75	706+91.30	50.5	50.5	
2-3	US 54 RT	707+53.31	708+31.99	68.9	68.9	
3	US 54 RT	708+80.94	709+55.69	61.6	61.6	PAVED APPROACH AT TURNER PKWY
3	US 54 RT	710+65.65	711+28.89	50.5	50.5	
3	US 54 RT	711+45.91	711+91.13	31.6	31.6	
3	US 54 RT	712+38.27	712+82.34	31.9	31.9	
3	US 54 RT	713+12.96	713+47.12	22.5	22.5	
4	US 54 RT	715+33.20	716+07.23	58.1	58.1	
4	US 54 RT	716+34.32	716+70.29	21.9	21.9	
4	US 54 RT	716+90.80	717+45.38	46.2	46.2	
4-5	US 54 RT	718+97.27	719+51.42	39.5	39.5	
5	US 54 RT	721+34.83	721+88.89	40.1	40.1	
5	US 54 RT	723+69.85	724+11.27	28.8	28.8	
5-6	US 54 RT	724+23.27	724+57.65	25.9	25.9	
6	US 54 RT	724+76.42	725+16.18	27.9	27.9	
6	US 54 RT	725+32.10	725+71.78	29.2	29.2	
6	US 54 RT	727+52.78	727+96.59	31.8	31.8	
6-7	US 54 RT	729+81.46	730+18.88	26.2	26.2	
8	US 54 LT	735+46.70	735+93.99	60.4	60.4	
8	US 54 LT	736+02.56	736+52.22	63.0	63.0	
8	US 54 LT	737+12.97	737+74.46	77.5	77.5	
TOTAL				929.8	930	

SIDEWALK & CURB RAMP							
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	CONCRETE SIDEWALK, 4 IN. S.Y.	CONCRETE CURB RAMP S.Y.	TRUNCATED DOMES S.F.	REMARKS
1	US 54 RT	697+72.44	697+81.80		4.4	10	
1	US 54 RT	697+97.81				10	IN CECIL STREET ISLAND
1	US 54 RT	698+11.17				10	IN CECIL STREET ISLAND
1	US 54 RT	698+66.07				10	IN CECIL STREET ISLAND
1	US 54 RT	698+83.08				10	IN CECIL STREET ISLAND
1	US 54 RT	699+99.88	699+18.88		19.0	10	
1-2	US 54 RT	699+18.88	705+27.38	345.1			5' TO 6' WIDE SIDEWALK SEE PLANS
2	US 54 RT	705+53.66	706+33.31	45.4			5' WIDE SIDEWALK
2	US 54 RT	706+80.59	707+64.29	48.8			5' WIDE SIDEWALK
3	US 54 RT	708+21.23	708+81.95	34.8			5' WIDE SIDEWALK
3	US 54 RT	708+81.95	708+88.95		4.4	10	
3	US 54 RT	709+47.88	709+54.59		4.2	10	
3	US 54 RT	709+54.59	710+71.78	65.4			5' WIDE SIDEWALK
3	US 54 RT	711+22.72	711+52.84	17.1			5' WIDE SIDEWALK
3	US 54 RT	711+84.01	712+44.66	34.0			5' WIDE SIDEWALK
3	US 54 RT	712+75.88	713+19.35	24.5			5' WIDE SIDEWALK
3-4	US 54 RT	713+40.57	715+38.84	127.8			5' TO 6' WIDE SIDEWALK SEE PLANS
4	US 54 RT	716+61.41	716+96.73	20.2			5' WIDE SIDEWALK
4-5	US 54 RT	717+37.55	719+08.13	97.0			5' WIDE SIDEWALK
5	US 54 RT	719+45.02	721+45.75	114.8			5' WIDE SIDEWALK
5	US 54 RT	721+82.37	723+76.53	110.2			5' WIDE SIDEWALK
5	US 54 RT	724+03.70	724+29.63	14.9			5' WIDE SIDEWALK
6	US 54 RT	724+50.95	724+82.64	18.2			5' WIDE SIDEWALK
6	US 54 RT	725+10.18	725+39.81	16.7			5' WIDE SIDEWALK
6	US 54 RT	725+64.55	727+60.29	109.1			5' WIDE SIDEWALK
6	US 54 RT	727+90.52	729+87.54	109.9			5' WIDE SIDEWALK
7	US 54 RT	730+12.80	731+46.66	87.2			5' TO 6' WIDE SIDEWALK SEE PLANS
7	US 54 RT	731+46.66	731+63.45		13.7	19	
7	US 54 LT	731+46.10	731+64.94		11.1	23	
7	US 54 LT	732+40.11	732+62.16		19.4	15	
7	US 54 LT	732+62.16	734+00.33	79.5			5' TO 6' WIDE SIDEWALK SEE PLANS
7-8	US 54 LT	734+52.39	735+49.43	58.8			5' WIDE SIDEWALK
8	US 54 LT	735+92.62	736+10.29	10.0			5' WIDE SIDEWALK
8	US 54 LT	736+44.86	737+26.17	48.6			5' TO 6' WIDE SIDEWALK SEE PLANS
TOTAL				1638.0	76.2	137	

PAVEMENT EDGE TREATMENT					
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	PAVEMENT EDGE TREATMENT L.F.	REMARKS
1-8	US 54	697+72.44	737+74.46	1108	TO BE USED AS DIRECTED BY ENGINEER
TOTAL				1108	

CONCRETE CURB						
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	INTEGRAL CURB (6 IN. HEIGHT & UNDER) TYPE A L.F.	CONCRETE CURB (6 IN. HEIGHT & UNDER) TYPE S L.F.	REMARKS
1	US 54 RT	697+72.44	697+76.90	6.6		PLACE ALONG CURB RAMP
1	US 54 RT	698+99.88	399+37.08	46.5		PLACE ALONG CURB RAMP
3	US 54 RT	708+81.94	708+88.95	7.0		PLACE ALONG CURB RAMP
3	US 54 RT	708+81.95	708+86.54	4.6		PLACE ALONG CURB RAMP
3	US 54 RT	709+47.88	709+54.59	6.7		PLACE ALONG CURB RAMP
3	US 54 RT	709+49.94	709+54.59	4.6		PLACE ALONG CURB RAMP
4	US 54 RT	716.58.83	716+59.20	3.1		
5	US 54 RT	724+01.52	724+02.50	5.2		
6	US 54 RT	725+10.03	725+10.47	1.3		
6	US 54 RT	727+42.43	727+61.30		23.3	
7	US 54 RT	731+46.77	751+53.64	6.9		PLACE ALONG CURB RAMP
7	US 54 RT	731+45.01	731+62.93	25.7		PLACE ALONG CURB RAMP
7	US 54 LT	731+45.91	731+53.19	19.6		PLACE ALONG CURB RAMP
7	US 54 LT	731+46.11	731+46.96	7.7		PLACE ALONG CURB RAMP
7	US 54 LT	732+40.11	732+56.98	17.4		PLACE ALONG CURB RAMP
7	US 54 LT	732+47.63	732+62.50	16.2		PLACE ALONG CURB RAMP
7	US 54 LT	733+99.70	733+99.74	1.3		
7	US 54 LT	734+53.12	734+52.88	1.0		
8	US 54 LT	735+46.70	735+47.66	1.5		
8	US 54 LT	735+92.63	736+10.29	18.2		PLACE ALONG SIDEWALK
8	US 54 LT	735+93.13	736+06.06	13.5		PLACE ALONG SIDEWALK
8	US 54 LT	737+12.97	737+22.64	13.3		PLACE ALONG CURB RAMP
8	US 54 LT	737+12.83	737+26.17	18.1		PLACE ALONG CURB RAMP
TOTAL				246	23	



JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 3 OF 122

PHASE 1 TAP-9900 (563)

SHEET 3 OF 10

SUMMARY OF QUANTITIES

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003-QUANTITIES.dgn

PAVEMENT MARKING

PAVEMENT MARKING SHEET XX OF 7	LOCATION	FROM STATION	TO STATION	TYPE 2 PREFORMED MARKING TAPE (GROOVED) 6 IN., WHITE L.F.	TYPE 2 PREFORMED MARKING TAPE (GROOVED) 24 IN., WHITE L.F.	PAINTED ARROWS EACH	TYPE 2 PREFORMED MARKING TAPE (GROOVED), 16 IN. WHITE YIELD LINE TRIANGLES EACH	4 IN. WHITE HIGH BUILD ACRYLIC WATERBORNE PAVEMENT MARKING PAINT L.F.	PAVEMENT MARKING REMOVAL L.F.	REMARKS
1	US 54 RT	697+75.38	697+92.19				11			CECIL ST. INTERSECTION
1	US 54 RT	697+77.35	697+95.49	26.6						CECIL ST. INTERSECTION
1	US 54 RT	697+87.80	697+99.91	26.6						CECIL ST. INTERSECTION
1	US 54 RT	698+11.15	698+66.03	54.9						CECIL ST. INTERSECTION
1	US 54 RT	698+11.17	698+66.03	54.9						CECIL ST. INTERSECTION
1	US 54 RT	698+51.91	698+63.90		12.0					CECIL ST. INTERSECTION
1	US 54 RT	698+51.92	698+64.12						12.2	CECIL ST. INTERSECTION
1	US 54 RT	698+76.68	698+98.00				11			CECIL ST. INTERSECTION
1	US 54 RT	698+79.09	699+00.14	24.5						CECIL ST. INTERSECTION
1	US 54 RT	699+83.08	699+04.22	24.6						CECIL ST. INTERSECTION
2	US 54 RT	707+78.33				1				STRAIGHT WHITE ARROW
2	US 54 RT	707+85.67	707+85.67					1.8		
2	US 54 RT	707+91.77				1				LEFT TURN WHITE ARROW
2	US 54 RT	707+99.67	707+99.67					1.8		
2	US 54 RT	708+07.21				1				RIGHT TURN WHITE ARROW
2	US 54 RT	708+87.20	709+49.32	62.1						TURNER PKWY INTERSECTION
2	US 54 RT	708+88.95	709+47.84	58.9						TURNER PKWY INTERSECTION
2	US 54 RT	709+23.85	709+47.84		24.0					TURNER PKWY INTERSECTION
3	US 54 RT	731+49.33	731+49.35		24.0					LAKER PRIDE RD INTERSECTION
3	US 54 RT	731+49.33	731+63.08						37.1	LAKER PRIDE RD INTERSECTION
3	US 54 RT	731+53.19	731+53.64	77.4						LAKER PRIDE RD INTERSECTION
3	US 54 RT	731+61.97	731+62.93	79.6						LAKER PRIDE RD INTERSECTION
3	US 54 LT	731+61.97	732+47.63	87.9						LAKER PRIDE RD INTERSECTION
3	US 54 LT	731+64.40	732+40.68	78.6						LAKER PRIDE RD INTERSECTION
3	US 54 LT	731+75.03	731+97.71		25.5					LAKER PRIDE RD INTERSECTION
3	US 54 LT	731+75.24	731+98.00						24.1	LAKER PRIDE RD INTERSECTION
TOTAL				657	86	3	22	4	73	

TEMPORARY EROSION CONTROL

EROSION SHEET XX OF 7	LOCATION	FROM STATION	TO STATION	SILT FENCE L.F.	SEDIMENT REMOVAL C.Y.	CURB INLET CHECK EACH	REMARKS
1	US 54 RT	698+99.88	699+65.06	76	0.8		
1	US 54 RT	699+65.06	705+24.94	563	5.6		
1	US 54 RT	705+54.74	705+91.30	40	0.4		
1	US 54 RT	705+91.30	706+37.89	47	0.5		
2	US 54 RT	706+76.28	707+69.21	93	0.9		
2	US 54 RT	708+16.48	708+88.95	73	0.7		
2	US 54 RT	709+47.88	710+73.72	125	1.3		
2	US 54 RT	711+22.54	711+52.88	31	0.3		
2	US 54 RT	711+83.57	712+44.96	64	0.6		
2	US 54 RT	712+75.73	713+20.02	49	0.5		
2	US 54 RT	713+40.32	713+74.23	39	0.4		
2	US 54 RT	713+74.23	714+23.00	49	0.5		
2	US 54 RT	714+23.00	715+39.44	129	1.3		
2	US 54 RT	716+59.18	716+96.40	39	0.4		
2	US 54 RT	717+37.72	719+14.52	182	1.8		
2-3	US 54 RT	719+45.41	721+52.06	216	2.2		
3	US 54 RT	721+81.26	723+27.34	164	1.6		
3	US 54 RT	723+27.34	723+77.14	50	0.5		
3	US 54 RT	724+02.03	724+30.19	28	0.3		
3	US 54 RT	724+50.21	724+83.82	34	0.3		
3	US 54 RT	725+10.61	725+39.99	29	0.3		
3	US 54 RT	725+64.12	727+60.22	198	2.0		
3	US 54 RT	727+90.58	729+87.44	202	2.0		
3	US 54 RT	730+12.46	731+62.51	155	1.6		
3	US 54 LT	731+45.42	731+64.96	28	0.3		
3	US 54 LT	732+40.48	734+00.36	169	1.7		
4	US 54 LT	734+53.84	735+46.38	96	1.0		
4	US 54 LT	735+93.44	736+12.24	23	0.2		
4	US 54 LT	736+46.02	737+25.20	86	0.9		
1-4	US 54 RT/LT	697+72.44	737+74.46		21.0	21	TO BE USED AT CURB INLETS & GRATED INLETS AS DIRECTED BY THE ENGINEER
TOTAL				3077	52	21	

CONTRACTOR FURNISHED SURVEYING & STAKING
TOTAL = 1 LUMP SUM

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PHASE 1 TAP-9900 (563)

SHEET 4 OF 10

SUMMARY OF QUANTITIES



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 201000836

REVISIONS		
NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 3 OF 122

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

FINAL SLOPE SURFACE							
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	REMOVAL & PLACEMENT OF EXISTING ROCK S.Y.	PERMANENT EROSION CONTROL GEOTEXTILE S.Y.	TURF TYPE TALL FESCUE SODDING S.Y.	REMARKS
10	US 54 RT	749+63.49	750+13.05			13.0	
10	US 54 RT	749+64.84	750+11.35			14.7	
10	US 54 RT	750+71.08	751+15.90			13.0	
10	US 54 RT	750+72.73	751+14.38			13.1	
11	US 54 RT	751+73.62	751+87.42			4.0	
11	US 54 RT	751+75.86	751+85.92			2.0	
11	US 54 RT	752+11.73	752+55.19			13.8	
11	US 54 RT	752+13.69	752+55.67			17.7	
11	US 54 RT	752+95.16	753+43.85			18.2	
11	US 54 RT	752+97.07	753+42.37			14.3	
11	US 54 RT	753+72.28	754+71.94			33.0	
11	US 54 RT	753+74.50	753+98.95			4.3	
11	US 54 RT	754+96.92	755+70.68			17.4	
11	US 54 RT	756+02.79	756+99.15			39.1	
12	US 54 RT	757+31.18	758+01.03			20.0	
12	US 54 RT	757+36.12	757+93.89			16.8	
12	US 54 RT	758+40.50	758+80.55			7.3	
12	US 54 RT	758+48.04	758+83.31			7.0	
12	US 54 RT	759+20.80	759+70.60			21.2	
12	US 54 RT	760+11.79	760+32.26			9.2	
12	US 54 RT	760+56.29	760+92.18	17.8	17.8		TO BE USED ON RT SIDE OF SIDEWALK
12	US 54 RT	760+60.29	760+89.74	11.2	11.2		TO BE USED ON LT SIDE OF SIDEWALK
12	US 54 RT	761+23.62	762+24.51	64.6	64.6		TO BE USED ON RT SIDE OF SIDEWALK
13	US 54 RT	762+58.09	763+42.89	40.9	40.9		TO BE USED ON RT SIDE OF SIDEWALK
13	US 54 RT	762+61.28	763+42.33	41.7	41.7		TO BE USED ON LT SIDE OF SIDEWALK
13	US 54 RT	763+64.05	764+44.90	33.9	33.9		TO BE USED ON RT SIDE OF SIDEWALK
13	US 54 RT	763+64.42	764+44.56	56.3	56.3		TO BE USED ON LT SIDE OF SIDEWALK
13	US 54 RT	764+65.50	765+21.33			18.1	
13	US 54 RT	765+53.27	765+68.17			4.8	
13	US 54 RT	765+54.60	765+68.08			6.2	
13	US 54 RT	766+30.92	766+46.04			5.1	
13	US 54 RT	766+76.71	767+50.36	8.8	8.8		TO BE USED ON RT SIDE OF SIDEWALK
13-14	US 54 RT	767+79.37	768+06.45			10.9	
14	US 54 RT	768+10.32	768+14.43			1.7	
14	US 54 RT	768+39.23	768+76.35			13.8	
14	US 54 RT	768+40.08	768+71.53			8.7	
14	US 54 RT	768+99.46	769+28.34			7.1	
14	US 54 RT	769+04.90	769+23.18			3.5	
14	US 54 RT	769+60.72	769+98.81			11.0	
14	US 54 RT	769+66.21	769+93.09			6.0	
14	US 54 RT	770+32.15	770+59.11			7.6	
14	US 54 RT	770+37.98	770+51.37			2.4	
14	US 54 RT	770+82.78	770+99.27			3.2	
14	US 54 RT	771+35.72	771+59.23			4.8	
14	US 54 RT	771+88.30	772+85.42			44.2	
14-15	US 54 RT	773+18.09	773+59.56			10.1	
15	US 54 RT	773+22.36	773+56.02			8.5	
15	US 54 RT	773+86.76	774+12.72			6.4	
15	US 54 RT	773+93.18	774+09.02			3.1	
15	US 54 RT	774+39.39	774+64.84			4.7	
15	US 54 RT	774+44.78	774+59.69			3.1	
15	US 54 RT	774+98.13	775+49.51			19.5	
15	US 54 RT	775+01.99	775+44.01			11.9	
15	US 54 RT	775+97.67	776+09.96			2.4	
		TOTAL		275	275	528	

SIDEWALK & CURB RAMP							
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	CONCRETE SIDEWALK 4 IN. S.Y.	CONCRETE CURB RAMP S.Y.	TRUNCATED DOMES S.F.	REMARKS
10	US 54 RT	749+64.28	750+11.94	27.3			5' WIDE SIDEWALK
10	US 54 RT	750+72.15	751+14.95	24.5			5' WIDE SIDEWALK
11	US 54 RT	751+75.25	751+86.53	7.6			5' WIDE SIDEWALK
11	US 54 RT	752+13.11	752+56.19	24.4			5' WIDE SIDEWALK
11	US 54 RT	752+96.47	753+42.94	26.7			5' WIDE SIDEWALK
11	US 54 RT	753+73.84	754+54.52	54.6			*5' TO 6' WIDE SIDEWALK
11	US 54 RT	754+54.52	754+68.97		9.0	10	
11	US 54 RT	755+09.73	755+27.72		10.4	20	
11	US 54 RT	755+27.72	755+69.83	30.0			6' WIDE SIDEWALK
11	US 54 RT	756+02.79	756+98.52	62.8			6' WIDE SIDEWALK
12	US 54 RT	757+31.76	757+82.76	27.3			5' WIDE SIDEWALK
12	US 54 RT	757+82.76	758+00.36		9.9	16	
12	US 54 RT	758+42.98	758+56.73		7.7	10	
12	US 54 RT	758+56.73	758+83.31	14.4			5' WIDE SIDEWALK
12	US 54 RT	759+20.13	759+70.85	36.6			5' WIDE SIDEWALK
12	US 54 RT	760+11.63	760+32.26	16.7			5' WIDE SIDEWALK
12	US 54 RT	760+60.29	760+89.72	22.2			5' WIDE SIDEWALK
12	US 54 RT	761+23.62	762+09.85	57.8			6' WIDE SIDEWALK
12	US 54 RT	762+09.85	762+24.51		10.5	10	
13	US 54 RT	762+58.09	762+80.83		14.3	10	
13	US 54 RT	762+80.83	763+42.39	34.2			5' WIDE SIDEWALK
13	US 54 RT	763+64.44	764+44.29	44.5			5' WIDE SIDEWALK
13	US 54 RT	764+65.50	765+09.14	31.5			5' WIDE SIDEWALK
13	US 54 RT	765+09.14	765+21.33		10.4	10	
13	US 54 RT	765+54.60	765+63.08		7.3	10	
13	US 54 RT	765+63.08	765+68.09	3.3			5' WIDE SIDEWALK
13	US 54 RT	766+30.96	766+44.38	10.4			5' WIDE SIDEWALK
13	US 54 RT	766+77.21	767+47.52	48.3			6' WIDE SIDEWALK
13-14	US 54 RT	767+79.37	768+10.47	24.4			6' WIDE SIDEWALK
14	US 54 RT	768+40.08	768+71.53	19.2			5' WIDE SIDEWALK
14	US 54 RT	769+04.90	769+24.12	14.9			5' WIDE SIDEWALK
14	US 54 RT	769+65.29	769+93.09	20.4			5' WIDE SIDEWALK
14	US 54 RT	770+37.72	770+51.37	12.6			5' WIDE SIDEWALK
14	US 54 RT	770+83.35	770+99.26	7.7			5' WIDE SIDEWALK
14	US 54 RT	771+35.70	771+40.70	2.8			5' WIDE SIDEWALK
14	US 54 RT	771+40.70	771+57.23		9.0	20	
14	US 54 RT	771+91.85	772+10.03		10.7	20	
14	US 54 RT	772+10.03	772+84.79	48.2			6' WIDE SIDEWALK
14-15	US 54 RT	773+18.10	773+56.02	21.3			5' WIDE SIDEWALK
15	US 54 RT	773+86.76	774+09.02	13.3			5' WIDE SIDEWALK
15	US 54 RT	774+40.01	774+59.73	12.5			5' WIDE SIDEWALK
15	US 54 RT	774+98.13	775+37.69	21.3			5' WIDE SIDEWALK
15	US 54 RT	775+37.69	775+48.96		6.2	10	
15	US 54 RT	775+96.95	776+04.96		6.2	10	
15	US 54 RT	776+04.96	776+09.96	3.6			6' WIDE SIDEWALK
			TOTAL	827.3	111.6	156	

* SEE PLANS FOR SIDEWALK TRANSITION FROM 5' WIDE TO 6' WIDE.

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PHASE 2 TAP-9900 (529)

SUMMARY OF QUANTITIES



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 2010000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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ONE MEMORIAL DRIVE, SUITE 500,
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

PAVED APPROACH						
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	PAVED APPROACH 8 IN. S.Y.	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK) S.Y.	REMARKS
10	US 54 RT	750+05.78	750+78.47	61.2	61.2	
10-11	US 54 RT	751+09.00	751+81.05	60.4	60.4	
11	US 54 RT	751+81.05	752+19.33	30.5	30.5	
11	US 54 RT	752+49.90	753+02.88	44.0	44.0	
11	US 54 RT	753+37.02	753+79.07	30.1	30.1	
11	US 54 RT	754+54.67	755+27.21	89.6	89.6	PAVED APPROACH AT FIFTH STREET
11	US 54 RT	755+56.62	756+14.70	58.5	58.5	
11-12	US 54 RT	756+85.87	757+45.38	39.3	39.3	
12	US 54 RT	757+84.70	758+61.27	63.3	63.3	PAVED APPROACH AT THIRD STREET
12	US 54 RT	758+76.15	759+24.18	57.6	57.6	
12	US 54 RT	759+65.39	760+18.71	68.5	68.5	
12	US 54 RT	760+18.71	760+78.07	48.5	48.5	
12	US 54 RT	760+83.96	761+33.84	61.4	61.4	
12-13	US 54 RT	762+16.45	762+71.76	83.6	83.6	PAVED APPROACH AT FIRST STREET
13	US 54 RT	763+36.89	763+69.48	27.9	27.9	
13	US 54 RT	764+41.16	764+70.85	34.8	34.8	
13	US 54 RT	765+07.45	765+67.58	92.5	92.5	PAVED APPROACH AT GRANT AVENUE
13	US 54 RT	766+39.38	766+83.69	54.5	54.5	
13	US 54 RT	767+47.52	767+82.95	38.3	38.3	
14	US 54 RT	768+10.56	768+45.90	28.9	28.9	
14	US 54 RT	768.62.46	769+15.04	31.4	31.4	
14	US 54 RT	769+84.81	770+40.74	39.1	39.1	
14	US 54 RT	770+46.59	770+95.73	28.1	28.1	
14	US 54 RT	771+45.03	772+03.93	77.0	77.0	PAVED APPROACH AT ILLINOIS STREET
14	US 54 RT	772+72.04	773+32.42	38.8	38.8	
14-15	US 54 RT	773+46.47	774+01.13	35.0	35.0	
15	US 54 RT	774+01.13	774+52.80	33.7	33.7	
15	US 54 RT	774+52.80	775+10.56	39.5	39.5	
15	US 54 RT	775+35.64	776+05.70	54.3	54.3	PAVED APPROACH AT CIRCLE COURT
TOTAL				1450.3	1450	

UTILITY ADJUSTMENTS BY OTHERS - FOR INFORMATION ONLY				
PLAN SHEET XX OF 15	LOCATION	STATION	DESCRIPTION	REMARKS
11	US 54 RT	756+82.36	MANHOLE	ATG TO BE COMPLETED BY AT&T
12	US 54 RT	761+52.60	POWER POLE	TO BE RELOCATED BY LACLEDE ELECTRIC
13	US 54 RT	764+41.85	MANHOLE	ATG TO BE COMPLETED BY AT&T
14	US 54 RT	772+25.66	MANHOLE	ATG TO BE COMPLETED BY AT&T
15	US 54 RT	775+78.95	MANHOLE	ATG TO BE COMPLETED BY AT&T

CONCRETE CURB						
PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	INTEGRAL CURB (6 IN. HEIGHT & UNDER) TYPE A L.F.	CONCRETE CURB (6 IN. HEIGHT & UNDER) TYPE S L.F.	REMARKS
11	US 54 RT	751+75.86	751+85.92	10.2		
11	US 54 RT	754+15.00	754+69.27	56.0		PLACE ALONG SIDEWALK AND CURB RAMP
11	US 54 RT	755+09.14	755+69.85	62.8		PLACE ALONG SIDEWALK AND CURB RAMP
12	US 54 RT	757+82.75	758+01.03	18.3		PLACE ALONG CURB RAMP
12	US 54 RT	757+82.77	757+94.86	12.1		PLACE ALONG CURB RAMP
12	US 54 RT	758+42.04	758+56.36	14.4		PLACE ALONG CURB RAMP
12	US 54 RT	758+47.30	758+56.73	9.4		PLACE ALONG CURB RAMP
12	US 54 RT	759+19.60	759.71.27		51.7	
12	US 54 RT	760+11.12	760+35.52		25.1	
12	US 54 RT	762+14.14	762+25.04	13.2		PLACE ALONG CURB RAMP
13	US 54 RT	762+57.58	762+80.82	23.2		PLACE ALONG CURB RAMP
13	US 54 RT	762+60.62	762+80.83	20.2		PLACE ALONG CURB RAMP
13	US 54 RT	764+65.50	765+21.89	56.4		PLACE ALONG SIDEWALK AND CURB RAMP
13	US 54 RT	765+09.00	765+23.49	14.5		PLACE ALONG CURB RAMP
13	US 54 RT	765+52.77	765+63.18	10.4		PLACE ALONG CURB RAMP
13	US 54 RT	765+54.06	765+63.08	9.0		PLACE ALONG CURB RAMP
13	US 54 RT	766+30.96	766+44.69	13.7		
13	US 54 RT	766+30.96	766+44.88	13.9		
14	US 54 RT	769+04.90	769+24.06	19.2		
14	US 54 RT	769+65.32	769+93.09	27.8		
14	US 54 RT	770+37.72	770+51.37	13.6		
14	US 54 RT	770+82.78	770+99.27	16.5		
14	US 54 RT	770+40.70	771+49.30	8.6		
14	US 54 RT	771+40.72	771+57.87	13.8		PLACE ALONG CURB RAMP
14	US 54 RT	771+91.28	772+10.10	18.8		PLACE ALONG CURB RAMP
15	US 54 RT	773+93.18	774+09.02	15.8		
15	US 54 RT	774+44.78	774+59.69	14.9		
15	US 54 RT	775+37.68	775+49.51	11.8		PLACE ALONG CURB RAMP
15	US 54 RT	775+37.69	775+44.69	7.0		PLACE ALONG CURB RAMP
15	US 54 RT	775+96.46	776+34.57	38.1		PLACE ALONG SIDEWALK AND CURB RAMP
15	US 54 RT	775+96.75	776+04.96	8.2		PLACE ALONG CURB RAMP
TOTAL				572	77	



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE • 201000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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PHASE 2 TAP-9900 (529)
SHEET 8 OF 10

SUMMARY OF QUANTITIES

DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 3 OF 122

TEMPORARY EROSION CONTROL

EROSION SHEET XX OF 7	LOCATION	FROM STATION	TO STATION	SILT FENCE L.F.	SEDIMENT REMOVAL C.Y.	CURB INLET CHECK EACH	REMARKS
5	US 54 RT	749+64.22	750+12.19	51	0.5		
5	US 54 RT	750+71.86	751+15.12	46	0.5		
5	US 54 RT	751+74.15	751+86.79	13	0.1		
5	US 54 RT	752+12.78	752+54.00	44	0.4		
5	US 54 RT	752+96.05	753+42.88	48	0.5		
5	US 54 RT	753+73.03	754+69.28	99	1.0		
5	US 54 RT	755+03.37	755+69.88	71	0.7		
5	US 54 RT	756+02.97	756+97.82	98	1.0		
5	US 54 RT	757+32.54	757+98.23	67	0.7		
5	US 54 RT	758+49.07	758+83.31	34	0.3		
5	US 54 RT	759+20.74	759+70.69	50	0.5		
6	US 54 RT	760+12.36	760+30.92	19	0.2		
6	US 54 RT	760+56.13	760+91.86	35	0.4		
6	US 54 RT	761+25.19	762+26.58	105	1.1		
6	US 54 RT	762+56.94	763+42.05	93	0.9		
6	US 54 RT	763+65.11	764+42.95	80	0.8		
6	US 54 RT	764+65.51	765+20.26	55	0.6		
6	US 54 RT	767+78.95	768+13.54	37	0.4		
6	US 54 RT	768+39.71	768+74.97	35	0.4		
6	US 54 RT	769+02.00	769+27.55	26	0.3		
6	US 54 RT	769+61.97	769+95.97	35	0.4		
6	US 54 RT	770+34.36	770+58.12	24	0.2		
6	US 54 RT	770+84.27	770+98.90	15	0.2		
6	US 54 RT	771+41.95	771+56.91	15	0.2		
6-7	US 54 RT	771+89.14	772+52.62	68	0.7		
7	US 54 RT	772+52.62	772+82.22	30	0.3		
7	US 54 RT	773+19.18	773+58.82	40	0.4		
7	US 54 RT	773+88.36	774+12.48	24	0.2		
7	US 54 RT	774+40.85	774+63.18	22	0.2		
7	US 54 RT	774+99.53	775+48.59	50	0.5		
5-7	US 54 RT	748+34.64	776+34.57		8.0	8	TO BE USED AT CURB INLETS & GRATED INLETS AS DIRECTED BY THE ENGINEER
TOTAL				1429	22	8	

CONTRACTOR FURNISHED SURVEYING & STAKING
TOTAL = 1 LUMP SUM

PAVEMENT MARKING

PAVEMENT MARKING SHEET XX OF 7	LOCATION	FROM STATION	TO STATION	TYPE 2 PREFORMED MARKING TAPE (GROOVED) 6 IN., WHITE L.F.	TYPE 2 PREFORMED MARKING TAPE (GROOVED) 24 IN., WHITE L.F.	PAINTED ARROWS EACH	4 IN. YELLOW HIGH BUILD ACRYLIC WATERBORNE PAVEMENT MARKING PAINT L.F.	REMARKS
5	US 54 RT	754+64.78	755+17.25	53.5				FIFTH ST. INTERSECTION
5	US 54 RT	754+69.84	755+08.32	39.3				FIFTH ST. INTERSECTION
5	US 54 RT	754+85.91	754+98.94		14.0			FIFTH ST. INTERSECTION
5	US 54 RT	757+94.86	758+47.30	52.6				THIRD ST. INTERSECTION
5	US 54 RT	758+01.03	758+42.04	41.1				THIRD ST. INTERSECTION
5	US 54 RT	758+23.80	758+36.91		14.0			THIRD ST. INTERSECTION
6	US 54 RT	761+00.44				1	15.6	STRAIGHT YELLOW ARROW
6	US 54 RT	761+08.08	761+08.08					
6	US 54 RT	761+16.62				1		STRAIGHT YELLOW ARROW
6	US 54 RT	762+21.96	762+60.60	38.6				FIRST ST. INTERSECTION
6	US 54 RT	762+25.04	762+57.57	32.5				FIRST ST. INTERSECTION
6	US 54 RT	762+42.97	762+56.97		14.0			FIRST ST. INTERSECTION
6	US 54 RT	765+22.12	765+53.86	31.7				GRANT AVE. INTERSECTION
6	US 54 RT	765+23.48	765+52.78	29.3				GRANT AVE. INTERSECTION
6	US 54 RT	765+38.08	765+52.08		14.0			GRANT AVE. INTERSECTION
6	US 54 RT	771+49.30	771+99.72	50.4				ILLINOIS ST. INTERSECTION
6	US 54 RT	771+57.59	771+91.59	34.0				ILLINOIS ST. INTERSECTION
6	US 54 RT	771+74.77	771+88.77		14.0			ILLINOIS ST. INTERSECTION
7	US 54 RT	775+45.48	775+96.45	51.0				CIRCLE CT. INTERSECTION
7	US 54 RT	775+49.28	775+96.46	47.2				CIRCLE CT. INTERSECTION
7	US 54 RT	775+72.38	775+96.38		24.0			CIRCLE CT. INTERSECTION
TOTAL				501	94	2	16	

PAVEMENT EDGE TREATMENT

PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	PAVEMENT EDGE TREATMENT L.F.	REMARKS
9-15	US 54	749+63.47	776+34.57	1571	TO BE USED AS DIRECTED BY ENGINEER
TOTAL				1571	

EARTHWORK

PLAN SHEET XX OF 15	LOCATION	FROM STATION	TO STATION	LINEAR GRADING CLASS 2 STA.	REMARKS
11-15	US 54 RT	749+63.47	776+34.57	27.0	
TOTAL				27.0	CONTRACTOR SHALL HAUL AWAY EXCESS EXCAVATION

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PHASE 2 TAP-9900 (529)

SHEET 9 OF 10

SUMMARY OF QUANTITIES



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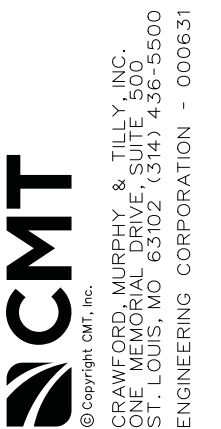
JOHN R. KEEVEN
PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE

SUMMARY OF QUANTITIES

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 3 OF 122

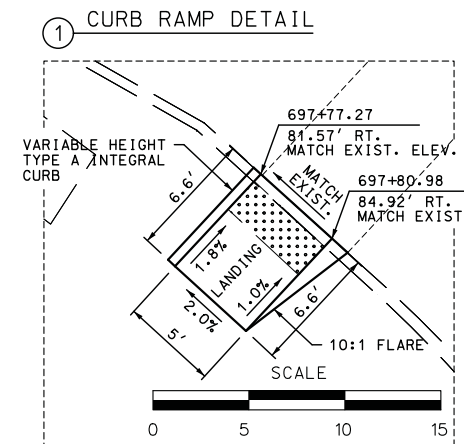
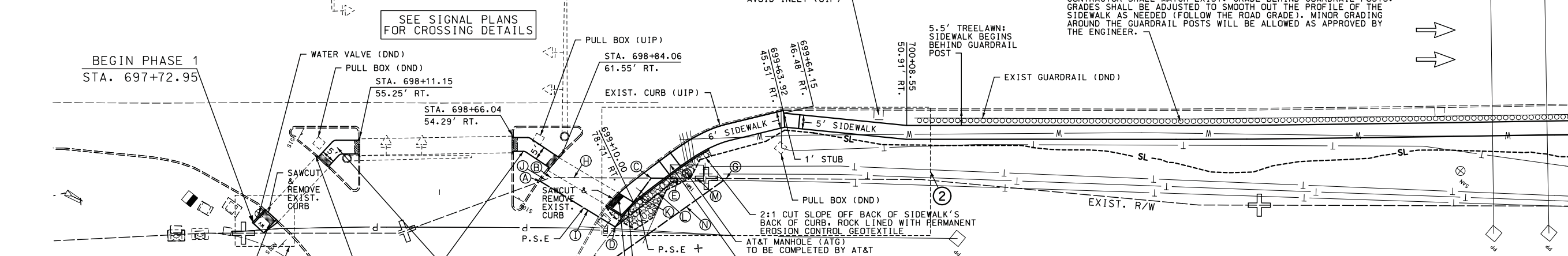
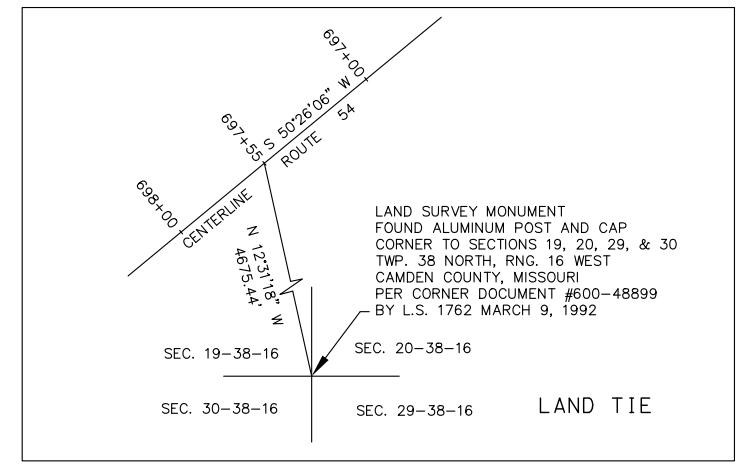
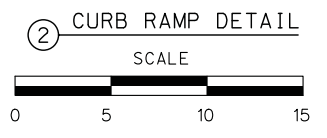
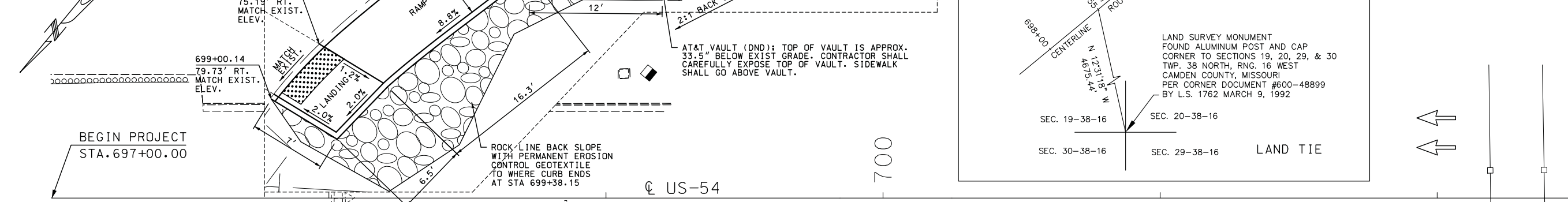
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CURB RAMP LEGEND

- TRUNCATED DOME (2" DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMPS SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.



N/F WAL MART
 REAL EST.
 BUSINESS TRUST
 0.00 SF NEW R/W
 371.00 SF PERM. SIDEWALK ESM'T
 0.00 SF TEMP. CONST. ESM'T
 26.61 AC REMAINING

- (A) 698+76.32 73.13' RT
- (B) 698+80.08 69.75' RT
- (C) 699+16.32 69.94' RT
- (D) 699+03.35 87.60' RT
- (E) 699+25.90 70.00' RT
- (F) 689+86.27 110.84' RT
- (G) 699+43.42 70.09' RT
- (H) S 50° 44' 24\"/>

N/F HOPKINS TR
 0.00 SF NEW R/W
 0.00 SF PERM. ESM'T
 85.00 SF PERM. SIDEWALK ESM'T
 472.00 SF TEMP. CONST. ESM'T
 18.70 AC REMAINING

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GENERAL NOTES

1. ANY WORK INDICATED ON THE PLANS THAT EXTENDS BEYOND THE PROJECT LIMITS IS CONSIDERED INCIDENTAL TO AND PART OF THE CONSTRUCTION OF THIS PROJECT.
2. BEARINGS SHOWN ARE STATE PLANE BEARINGS, CENTRAL ZONE.
3. NO DIRECT PAYMENT FOR OVERHAUL ON THIS PROJECT.
4. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEY AND RECORDS. THE CITY DOES NOT WARRANT THE LOCATIONS OF THESE FACILITIES AS PRECISE. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND PRECISE LOCATION OF ALL FACILITIES AND TO AVOID DAMAGE. SEE THE JOB SPECIAL PROVISIONS FOR A LIST OF UTILITY COMPANIES ON OR WITHIN THE VICINITY OF THE PROJECT LIMITS.
5. FOR SIGNING REMOVALS & RELOCATES, SEE SIGNING SHEETS.
6. CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS, LANDINGS, SIDEWALK, PAVED APPROACH SIDEWALK CONNECTIONS, PEDESTRIAN SIGNALS, AND ANY OTHER PEDESTRIAN ITEMS ON THIS PROJECT.
7. ALL SLOPE LIMITS AND PAVED APPROACH LIMITS SHALL STAY WITHIN THE EXISTING RIGHT OF WAY.

MATCHLINE STA. 702+50.00, US-54



JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS		
NUMBER	BY	DATE

PLAN SHEETS
 SHEET 1 OF 15
 CITY OF CAMDEN, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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 ST. LOUIS, MO 63102 (314) 436-5500
 ENGINEERING CORPORATION - 000631

DATE: 5/19/2016
 JOB No: 15480-02-00
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004_PLAN-1_120.dgn

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JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE • 201000836

REVISIONS

NUMBER	BY	DATE

PLAN SHEETS
 SHEET 2 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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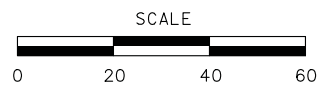
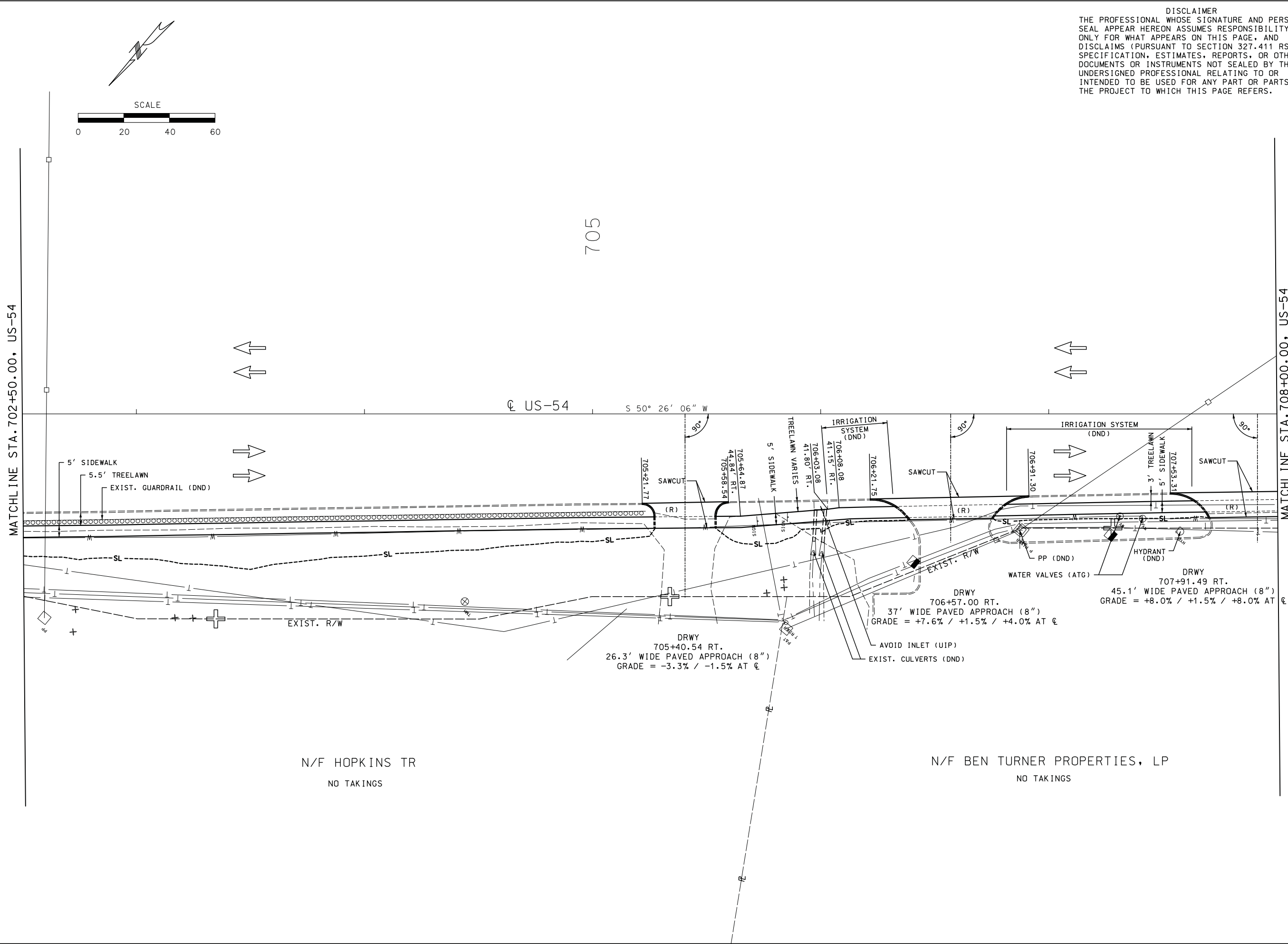
DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

SHEET 5 OF 122

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005_PLAN-2-120.dgn



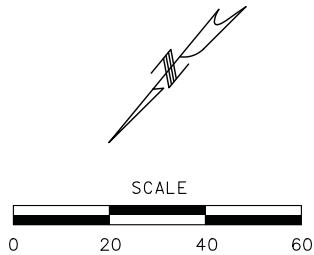
MATCHLINE STA. 702+50.00, US-54

MATCHLINE STA. 708+00.00, US-54

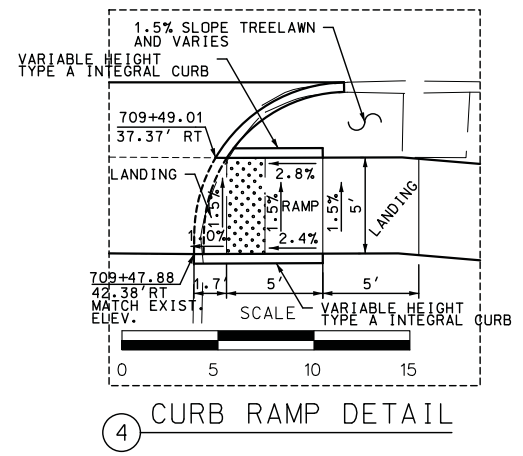
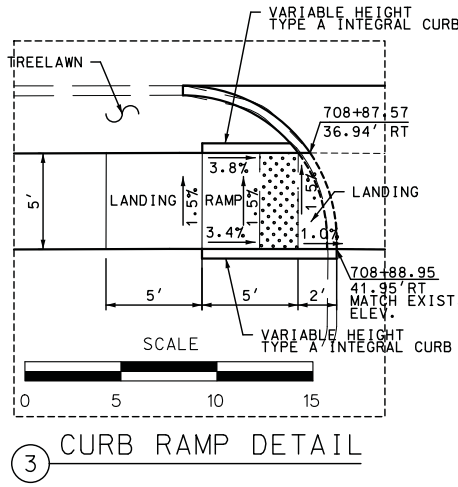
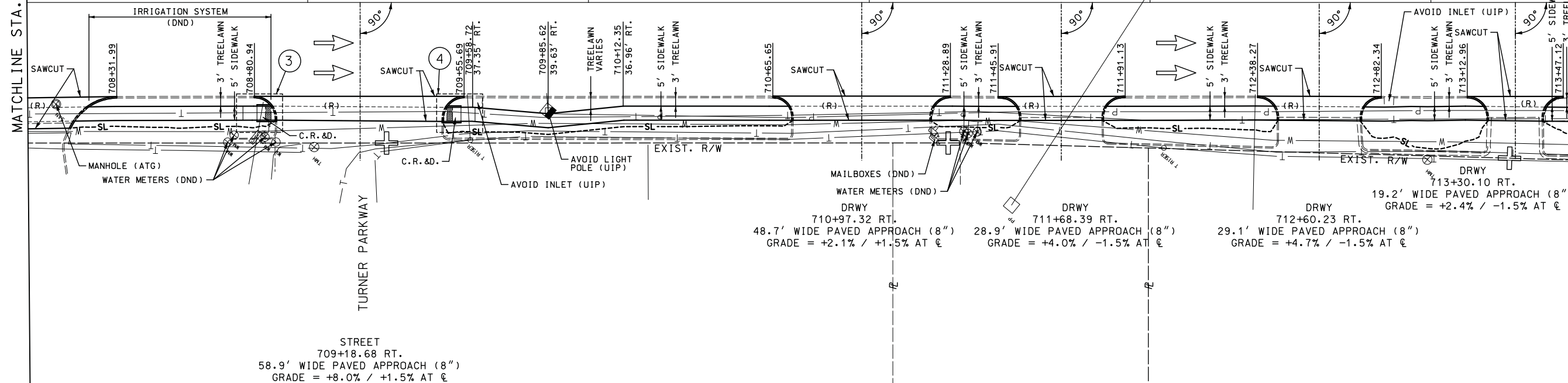
705

N/F HOPKINS TR
 NO TAKINGS

N/F BEN TURNER PROPERTIES, LP
 NO TAKINGS



MATCHLINE STA. 708+00.00, US-54



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JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS		
NUMBER	BY	DATE

PLAN SHEETS
 SHEET 3 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

CURB RAMP LEGEND

- TRUNCATED DOME (2' DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMP SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.

STREET
 709+18.68 RT.
 58.9' WIDE PAVED APPROACH (8")
 GRADE = +8.0% / +1.5% AT C

N/F BEN TURNER PROPERTIES, LP
 NO TAKINGS

N/F ROUSE
 NO TAKINGS

N/F CEKAY CORP.
 NO TAKINGS

CMT
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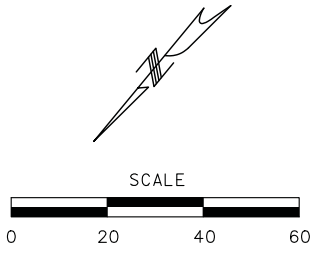
DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

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007_PLAN-4_120.dgn



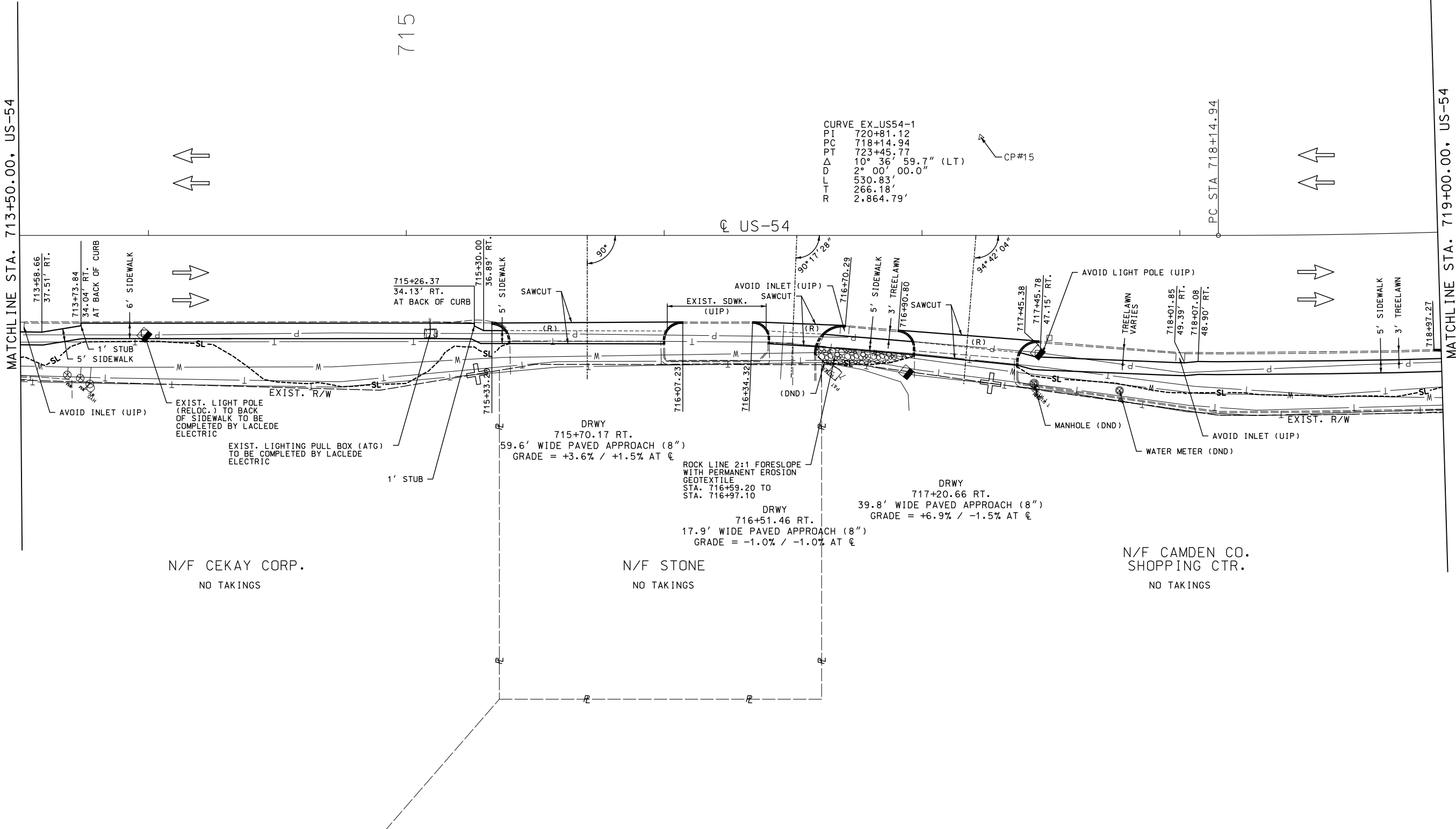
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NUMBER	BY	DATE



CURVE EX_US54-1
 PI 720+81.12
 PC 718+14.94
 PT 723+45.77
 Δ 10° 36' 59.7" (LT) CP#15
 2° 00' 00.0"
 530.83'
 266.18'
 2,864.79'

N/F CEKAY CORP.
 NO TAKINGS

N/F STONE
 NO TAKINGS

N/F CAMDEN CO.
 SHOPPING CTR.
 NO TAKINGS

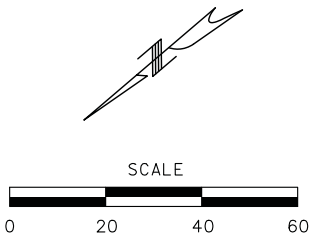
PLAN SHEETS
 SHEET 4 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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DATE: 5/19/2016
 JOB No: 15480-02-00
 15480-03-00

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CURVE EX_US54-1
 PI 720+81.12
 PC 718+14.94
 PT 723+45.77
 Δ 10° 36' 59.7" (LT)
 D 2° 00' 00.0"
 L 530.83'
 T 266.18'
 R 2,864.79'

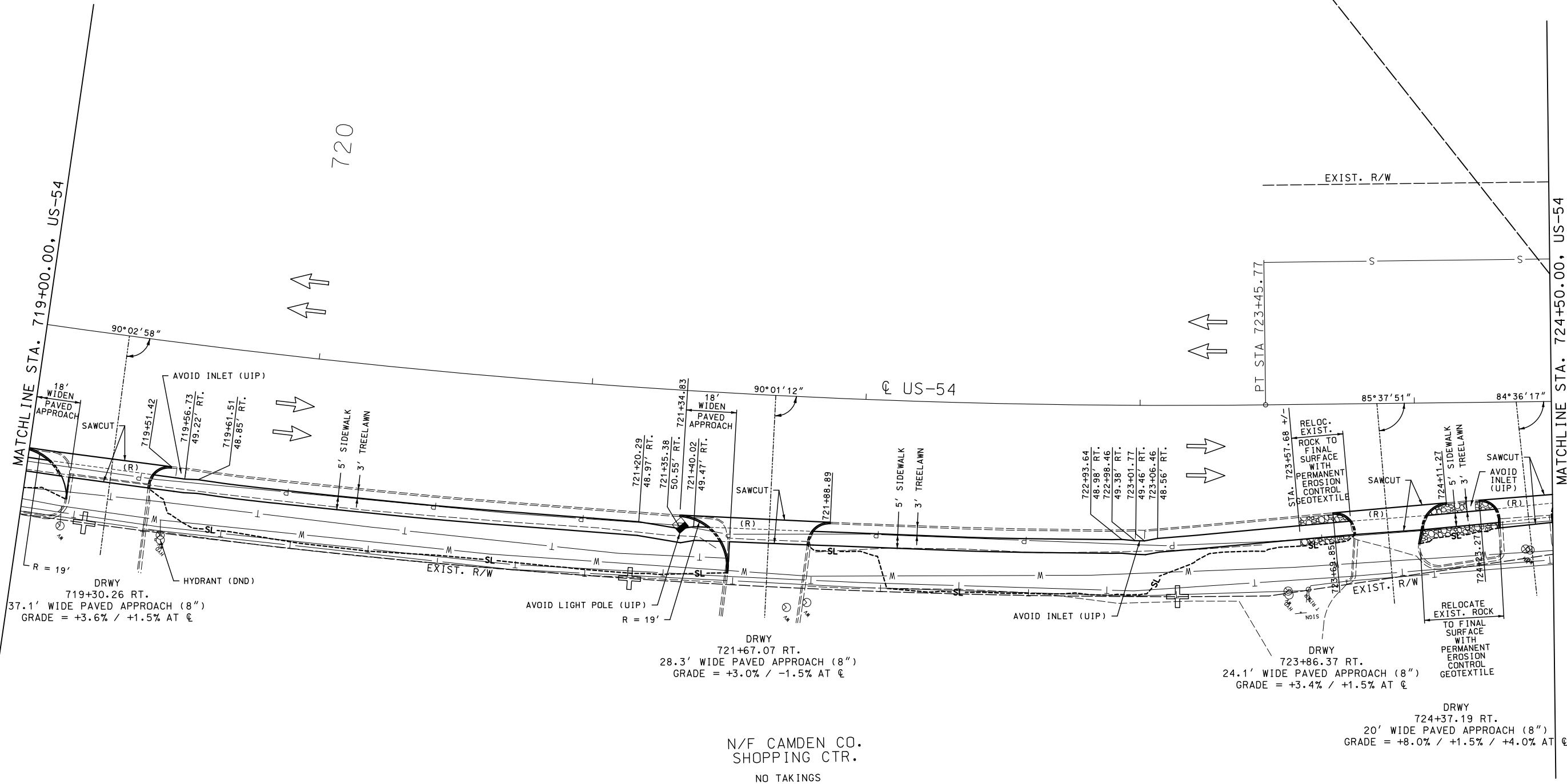
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NUMBER	BY	DATE



N/F CAMDEN CO.
 SHOPPING CTR.
 NO TAKINGS

PLAN SHEETS
 SHEET 5 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

SHEET 8 OF 122

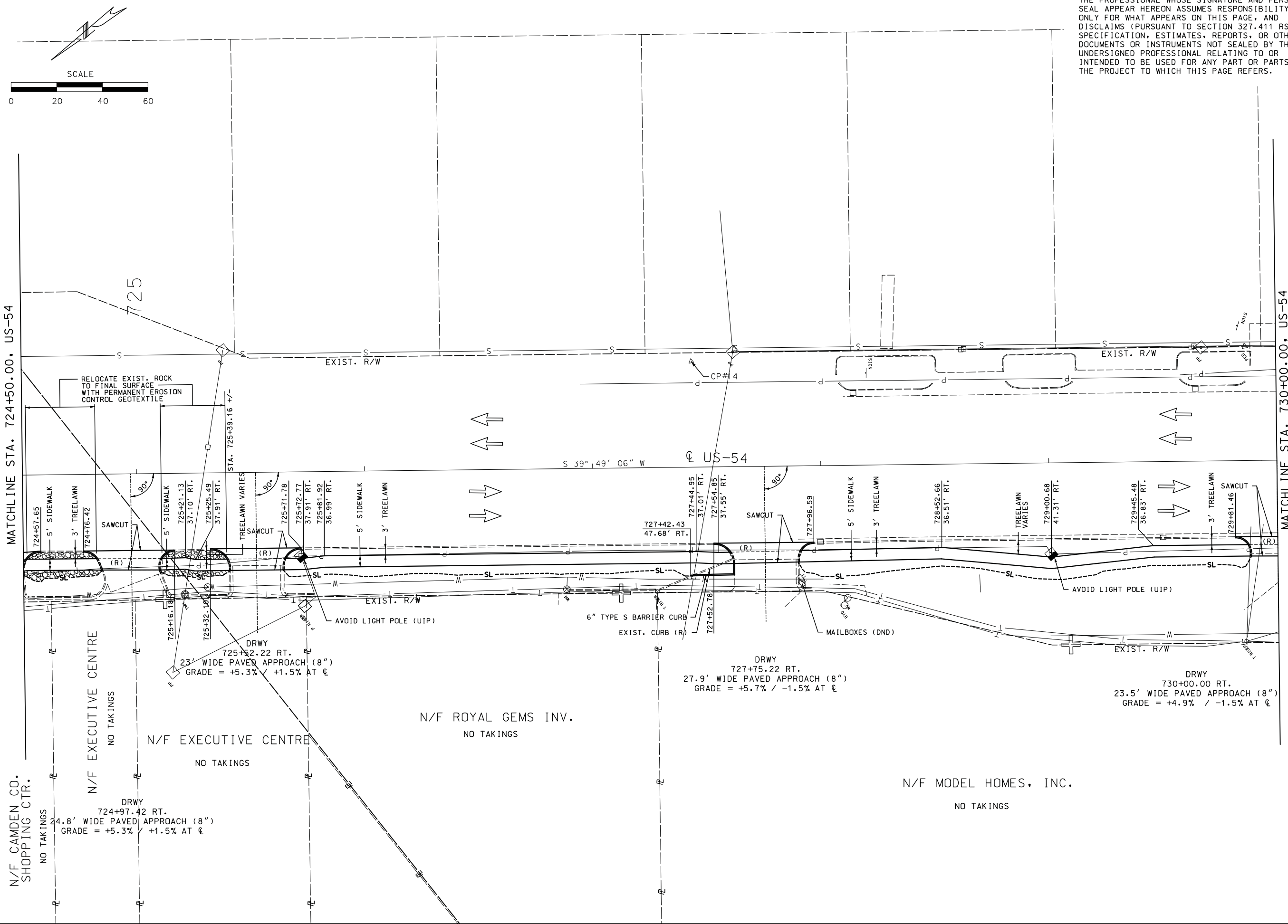
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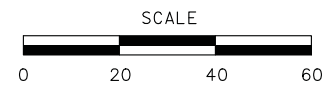
009_PLAN-6_120.dgn

MATCHLINE STA. 724+50.00, US-54

MATCHLINE STA. 730+00.00, US-54



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PLAN SHEETS
 SHEET 6 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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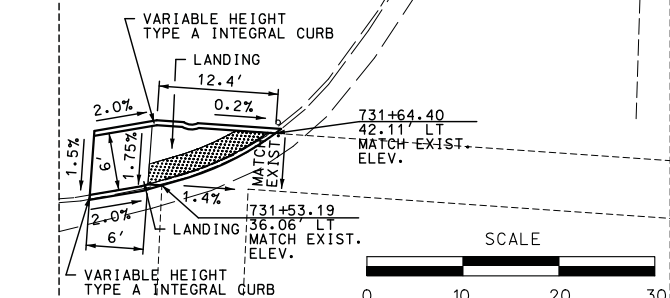
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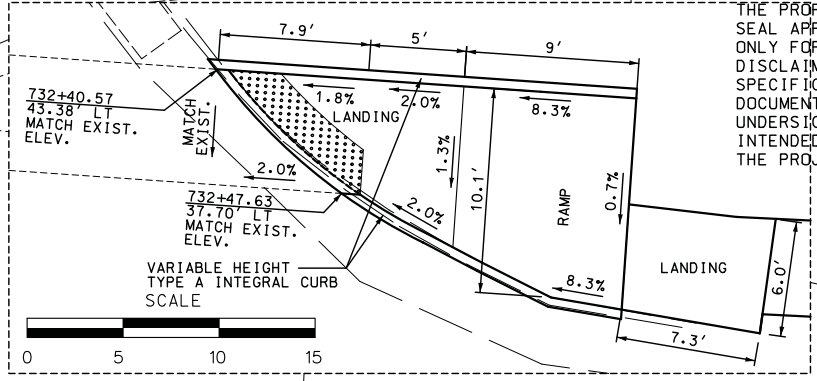
SHEET 9 OF 122

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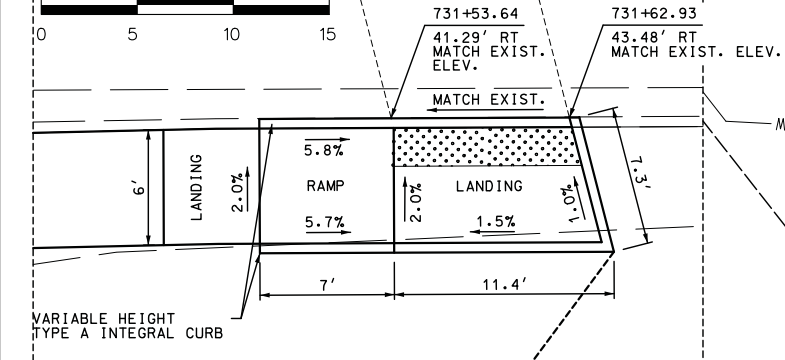
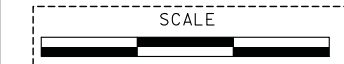
010_Plan-7_120.dgn



6 CURB RAMP DETAIL



7 CURB RAMP DETAIL



5 CURB RAMP DETAIL

CURB RAMP LEGEND

- TRUNCATED DOME (2" DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMPS SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.

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CURVE EX_US54-2
 PI 734+99.44
 PC 730+63.34
 PT 739+10.00
 Δ 33° 51' 59.9" (RT)
 D 4° 00' 00.1"
 T 846.66'
 R 436.10'
 L 1,432.39'



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PLAN SHEETS
 SHEET 7 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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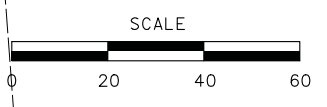
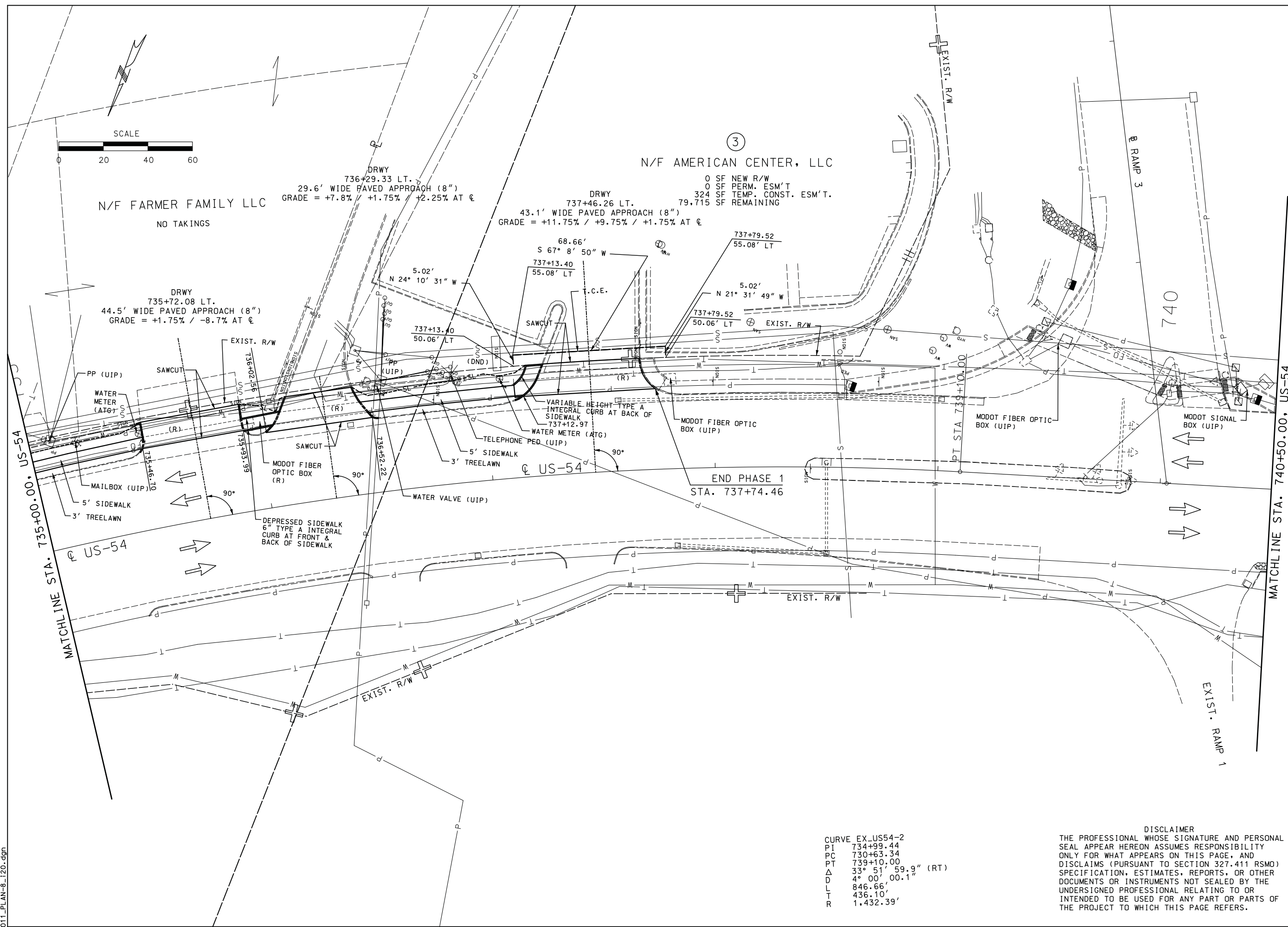
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011_PLAN-8_120.dgn



N/F FARMER FAMILY LLC
NO TAKINGS

③
N/F AMERICAN CENTER, LLC

0 SF NEW R/W
0 SF PERM. ESM'T
324 SF TEMP. CONST. ESM'T.
79,715 SF REMAINING

DRWY
735+72.08 LT.
44.5' WIDE PAVED APPROACH (8")
GRADE = +1.75% / -8.7% AT €

DRWY
736+29.33 LT.
29.6' WIDE PAVED APPROACH (8")
GRADE = +7.8% / +1.75% / +2.25% AT €

DRWY
737+46.26 LT.
43.1' WIDE PAVED APPROACH (8")
GRADE = +11.75% / +9.75% / +1.75% AT €

END PHASE 1
STA. 737+74.46

CURVE EX_US54-2
PI 734+99.44
PC 730+63.34
PT 739+10.00
Δ 33° 51' 59.9" (RT)
D 4° 00' 00.1"
L 846.66'
T 436.10'
R 1,432.39'

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PLAN SHEETS
SHEET 8 OF 15

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

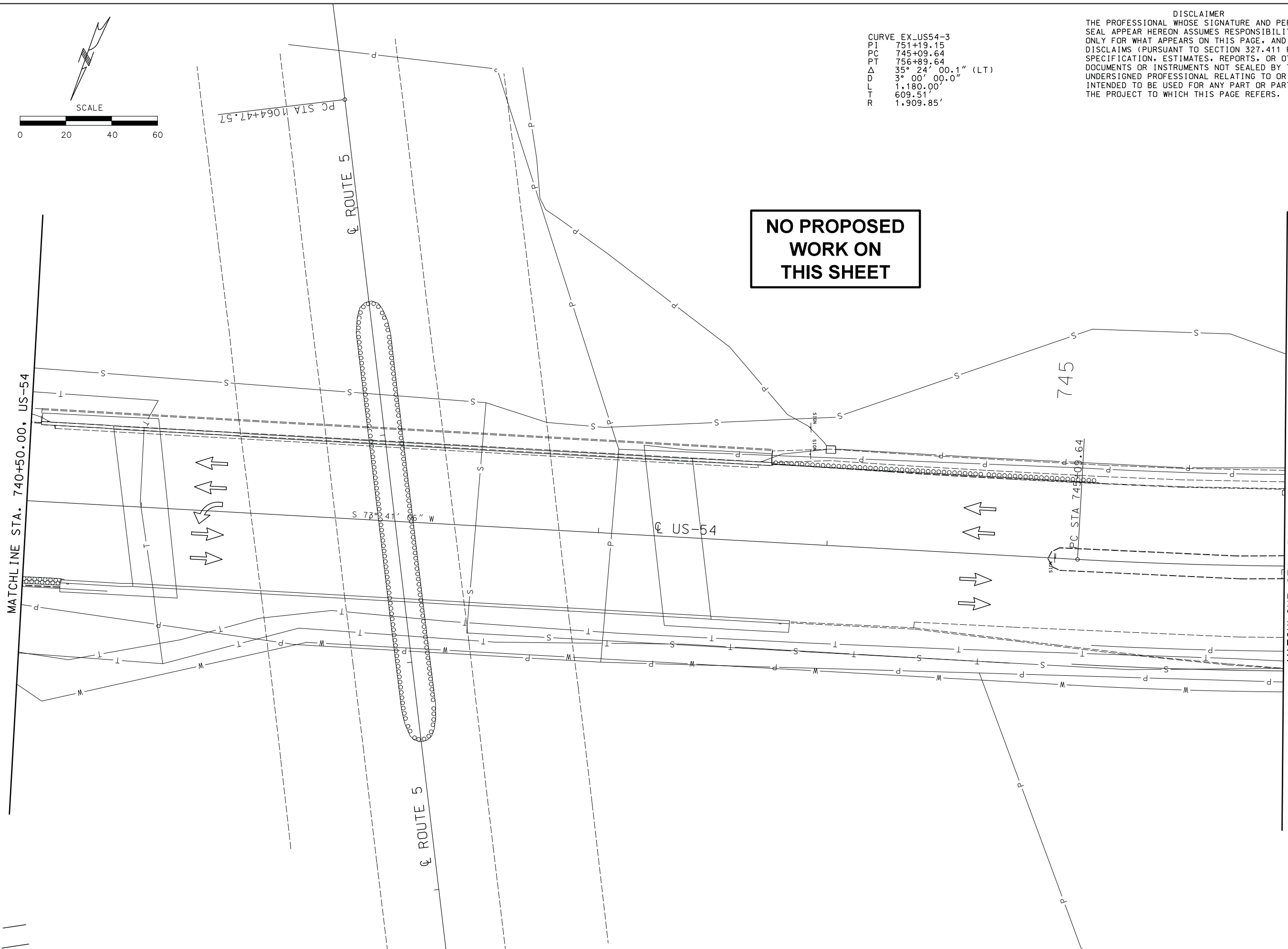
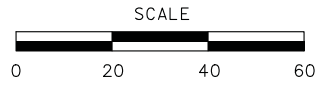


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JOB No: 15480-02-00
15480-03-00

SHEET 11 OF 122

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CURVE EX_US54-3
 PI 751+19.15
 PC 745+09.64
 PT 756+89.64
 Δ 35° 24' 00.1" (LT)
 D 3° 00' 00.0"
 L 1,180.00'
 T 609.51'
 R 1,909.85'

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PLAN SHEETS
 SHEET 9 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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SHEET 12 OF 122



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PLAN SHEETS

SHEET 10 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

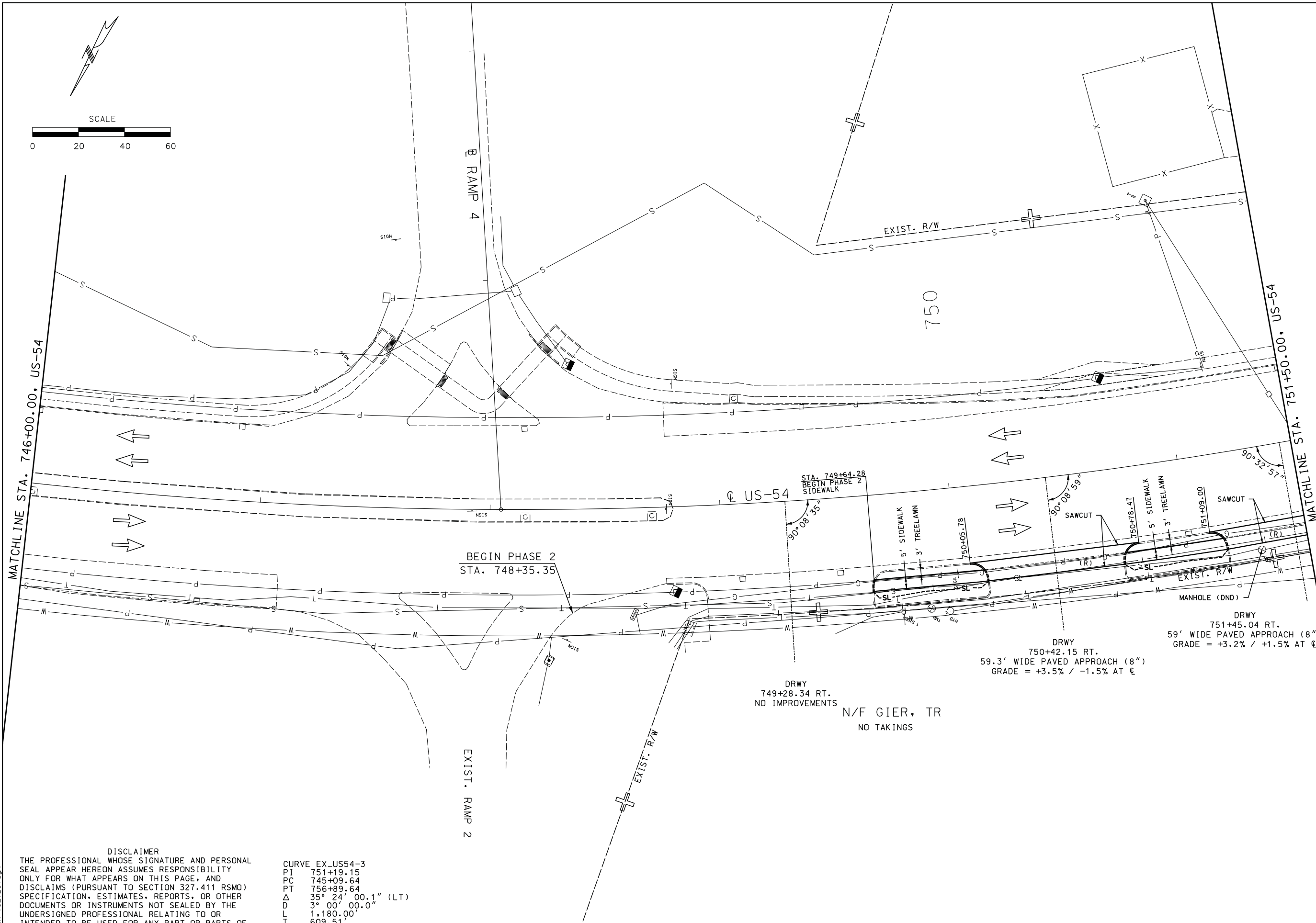
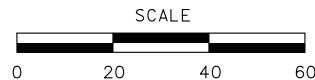


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SHEET 13 OF 122



BEGIN PHASE 2
 STA. 748+35.35

STA. 749+64.28
 BEGIN PHASE 2
 SIDEWALK

DRWY
 749+28.34 RT.
 NO IMPROVEMENTS

N/F GIER, TR
 NO TAKINGS

DRWY
 750+42.15 RT.
 59.3' WIDE PAVED APPROACH (8")
 GRADE = +3.5% / -1.5% AT C

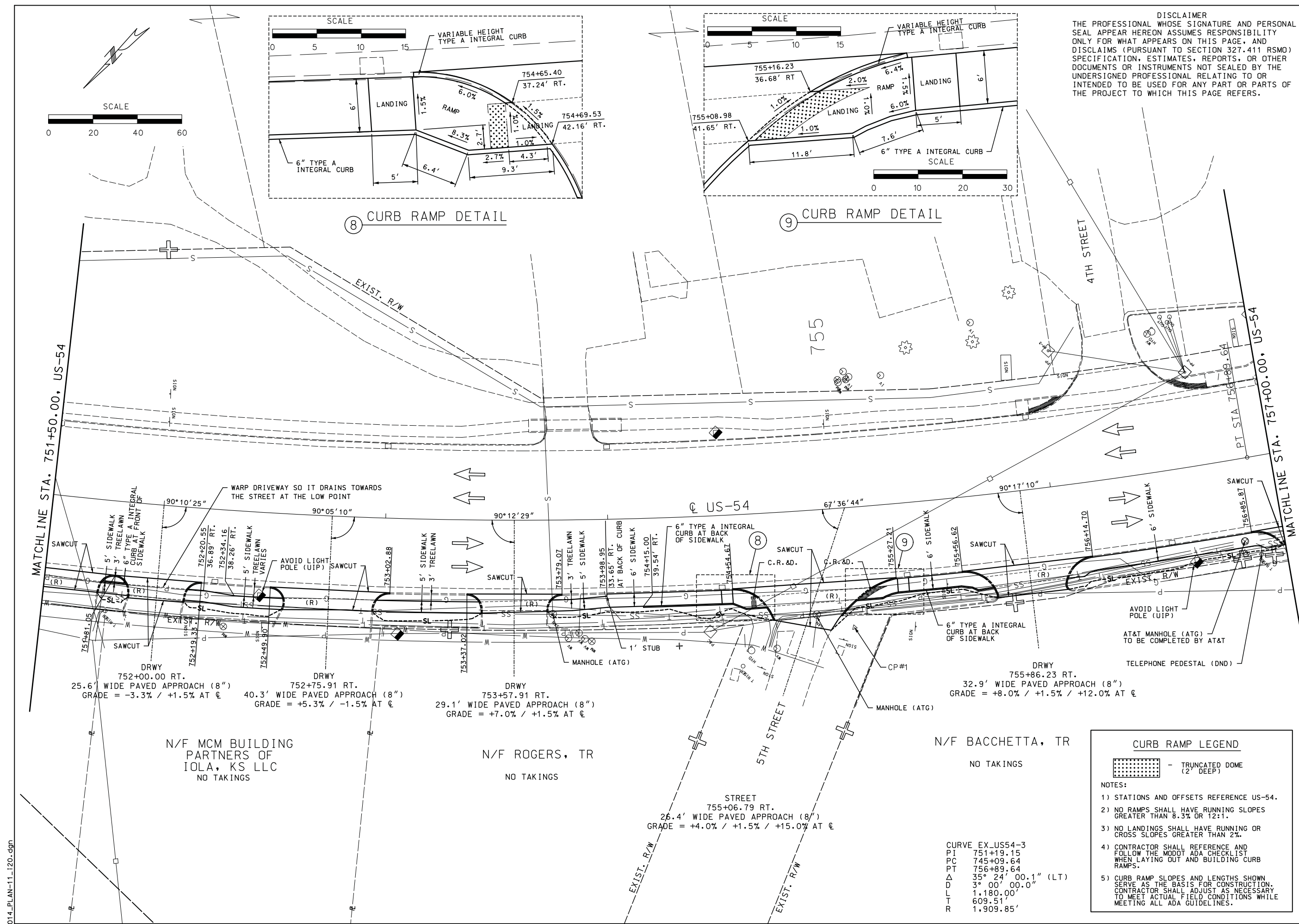
DRWY
 751+45.04 RT.
 59' WIDE PAVED APPROACH (8")
 GRADE = +3.2% / +1.5% AT C

CURVE EX_US54-3
 PI 751+19.15
 PC 745+09.64
 PT 756+89.64
 Δ 35° 24' 00.1" (LT)
 D 3° 00' 00.0"
 L 1,180.00'
 T 609.51'
 R 1,909.85'

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013-PLAN-10-120.dgn

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 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

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NUMBER	BY	DATE

PLAN SHEETS
 SHEET 11 OF 15

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

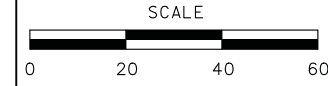
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JOB No: 15480-02-00
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014_PLAN-11_120.dgn

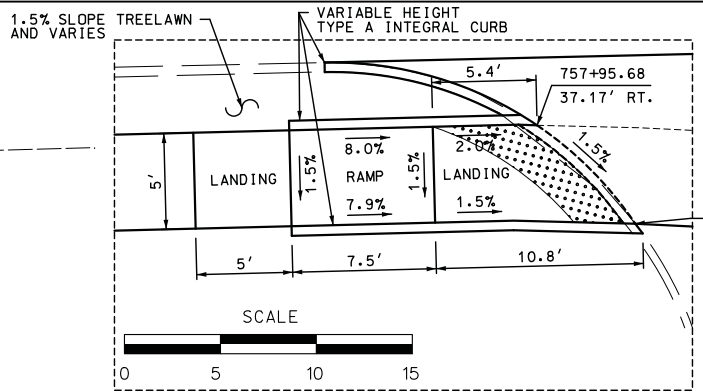


CURB RAMP LEGEND

- TRUNCATED DOME (2" DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMP SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMP.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.



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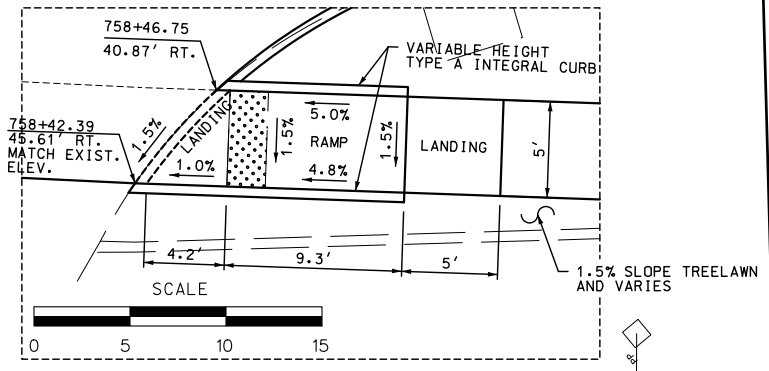
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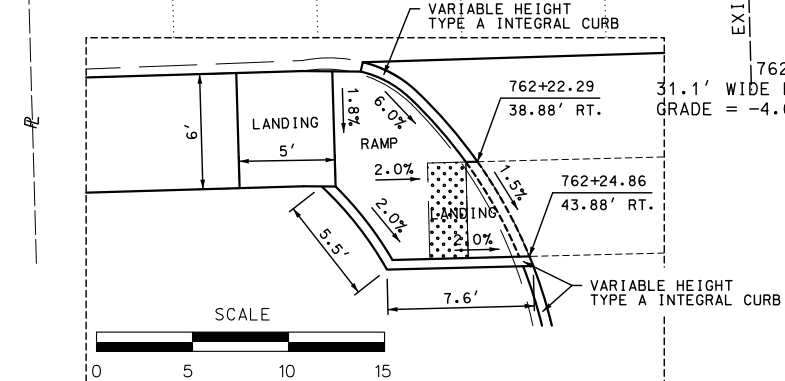
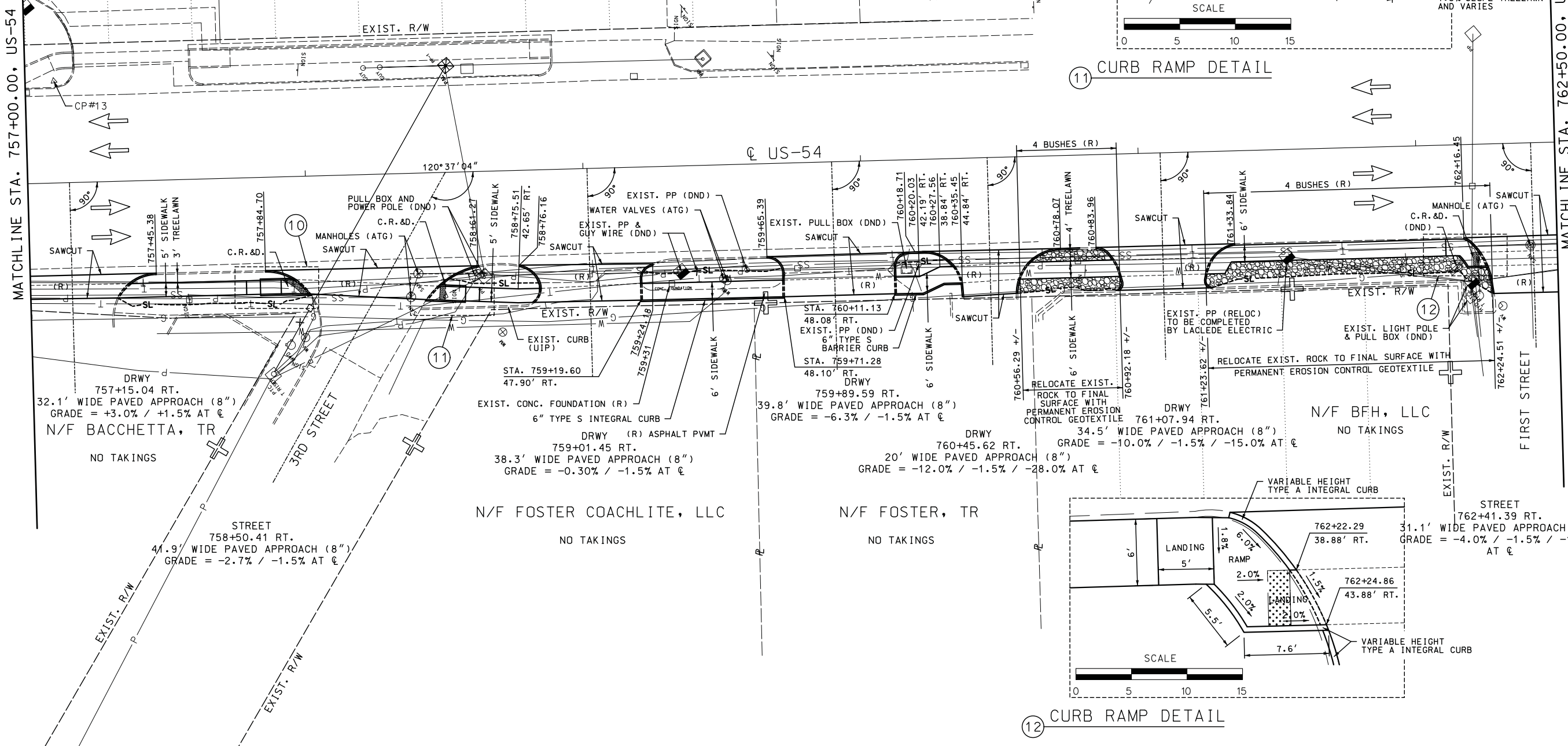
REVISIONS

NUMBER	BY	DATE

10 CURB RAMP DETAIL



11 CURB RAMP DETAIL



12 CURB RAMP DETAIL

MATCHLINE STA. 762+50.00, US-54

MATCHLINE STA. 757+00.00, US-54

PLAN SHEETS

SHEET 12 OF 15

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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JOB No: 15480-02-00
15480-03-00

SHEET 15 OF 122

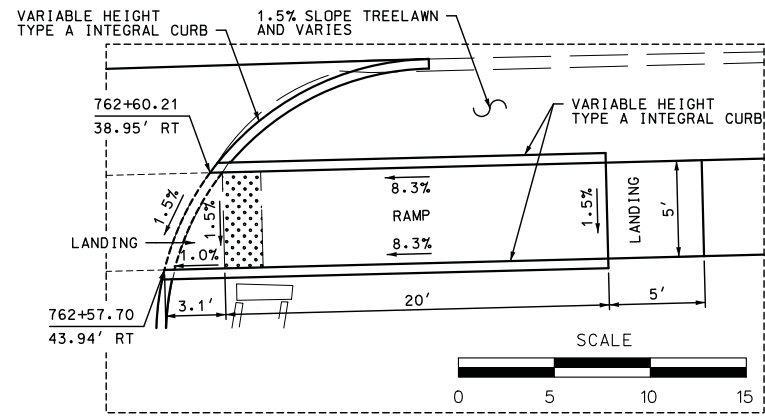
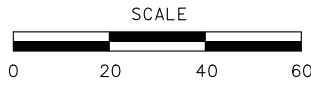
015-PLAN-12-120.dgn

016-PLAN-13-120.dgn

MATCHLINE STA. 762+50.00, US-54

MATCHLINE STA. 768+00.00, US-54

765



13 CURB RAMP DETAIL

CURB RAMP LEGEND

- TRUNCATED DOME (2" DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMP SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MDDOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMP.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.

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PROFESSIONAL ENGINEER
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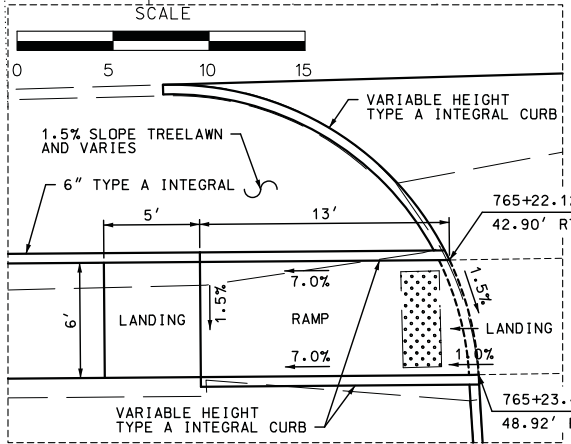
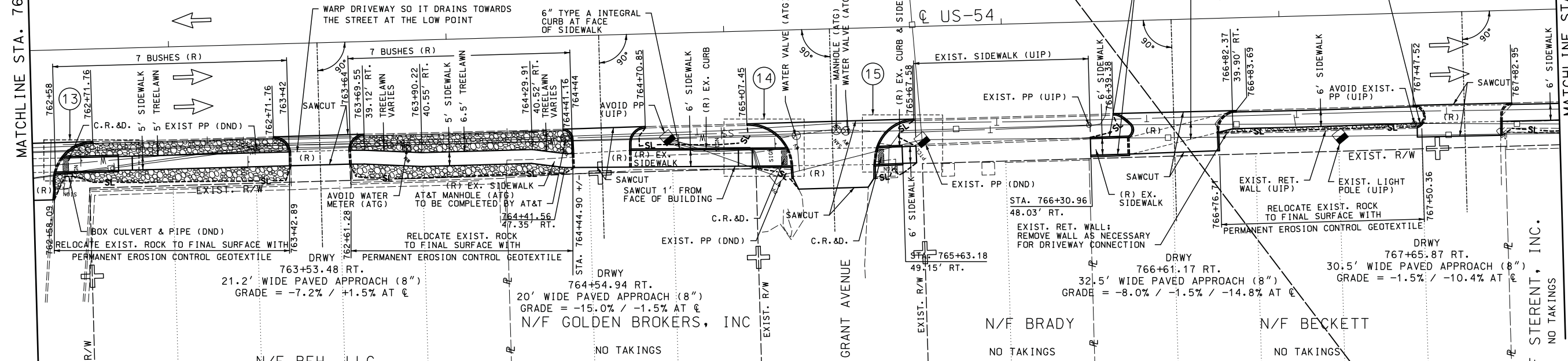
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NUMBER	BY	DATE

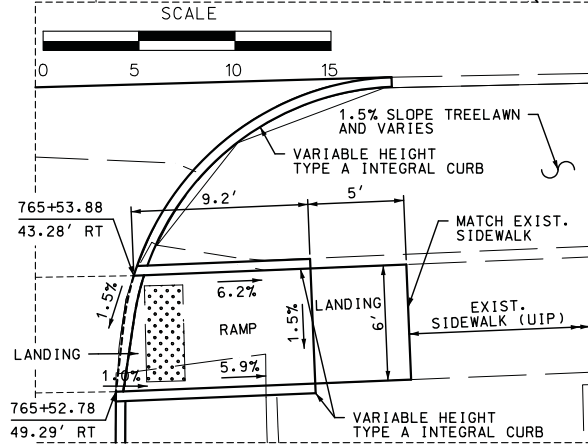
PLAN SHEETS
SHEET 13 OF 15

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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14 CURB RAMP DETAIL



15 CURB RAMP DETAIL

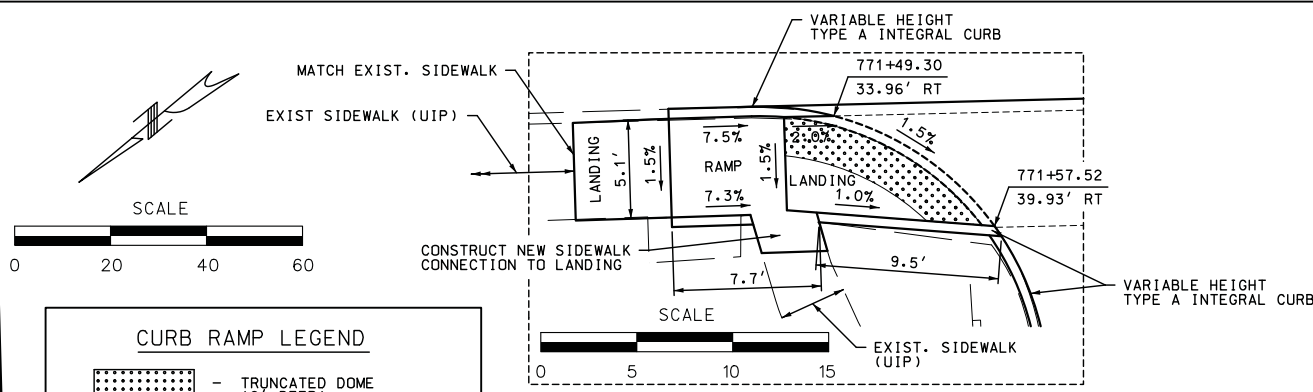


DATE: 5/19/2016

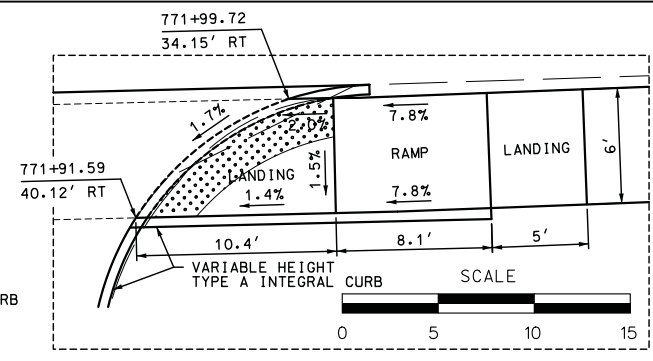
JOB No: 15480-02-00
15480-03-00

MATCHLINE STA. 768+00.00, US-54

MATCHLINE STA. 773+50.00, US-54



16 CURB RAMP DETAIL



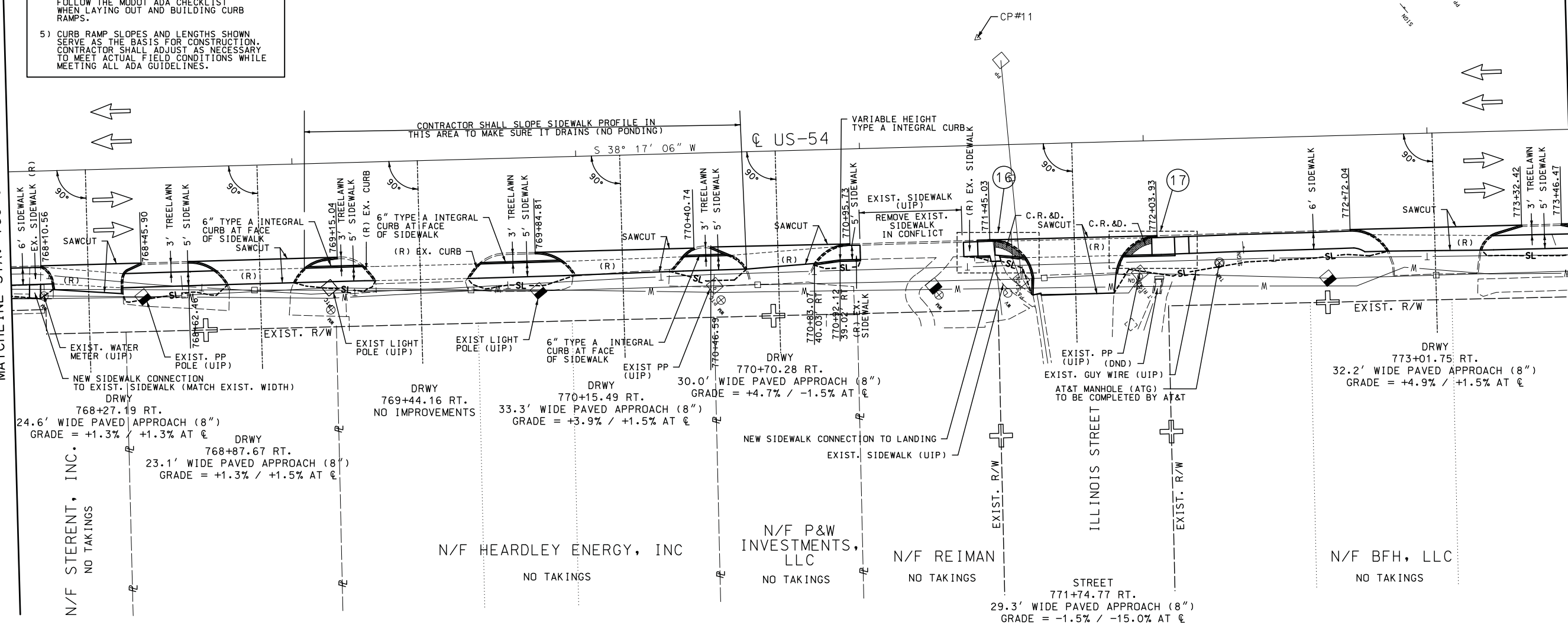
17 CURB RAMP DETAIL

CURB RAMP LEGEND

- TRUNCATED DOME (2" DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMPS SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MODOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.



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JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS		
NUMBER	BY	DATE

PLAN SHEETS
 SHEET 14 OF 15

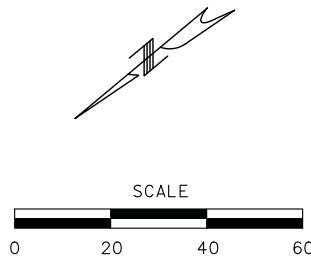
CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



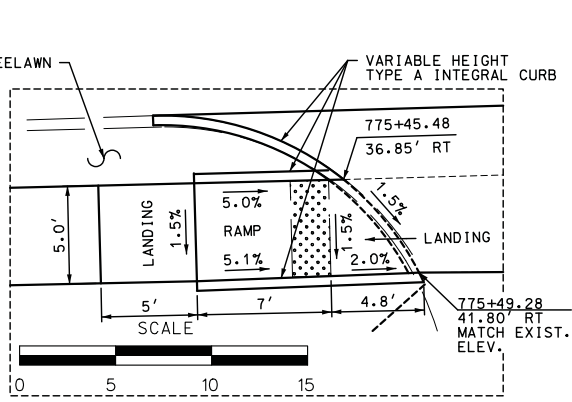
DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

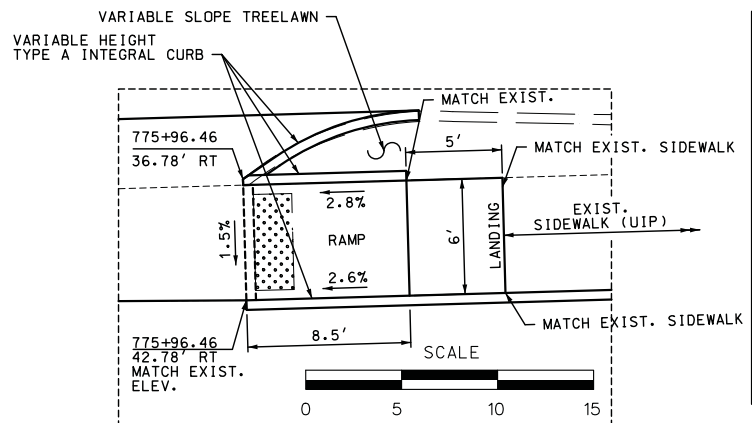
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775



18 CURB RAMP DETAIL



19 CURB RAMP DETAIL

CURB RAMP LEGEND

- TRUNCATED DOME (2' DEEP)

NOTES:

- 1) STATIONS AND OFFSETS REFERENCE US-54.
- 2) NO RAMPS SHALL HAVE RUNNING SLOPES GREATER THAN 8.3% OR 12:1.
- 3) NO LANDINGS SHALL HAVE RUNNING OR CROSS SLOPES GREATER THAN 2%.
- 4) CONTRACTOR SHALL REFERENCE AND FOLLOW THE MDDOT ADA CHECKLIST WHEN LAYING OUT AND BUILDING CURB RAMPS.
- 5) CURB RAMP SLOPES AND LENGTHS SHOWN SERVE AS THE BASIS FOR CONSTRUCTION. CONTRACTOR SHALL ADJUST AS NECESSARY TO MEET ACTUAL FIELD CONDITIONS WHILE MEETING ALL ADA GUIDELINES.

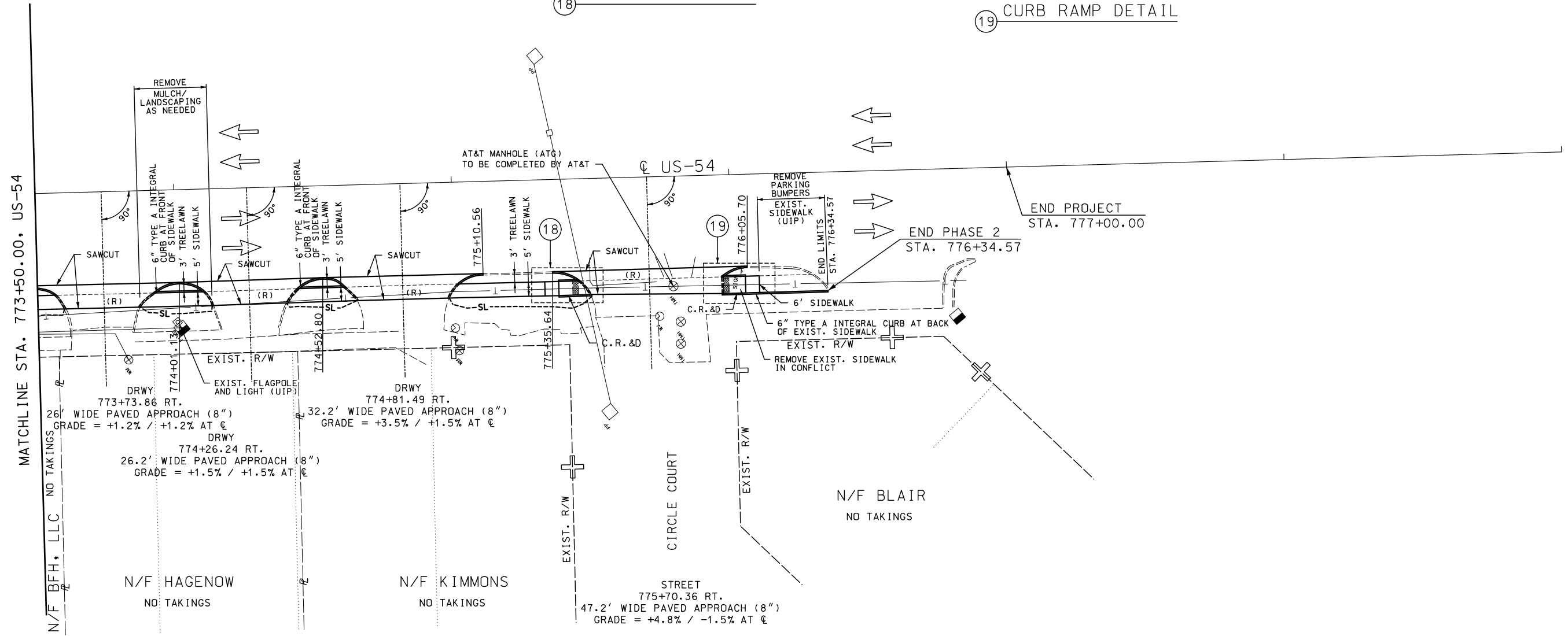
STATE OF MISSOURI
 JOHN R. KEEVEN
 NUMBER PE-201000836
 PROFESSIONAL ENGINEER

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 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE



PLAN SHEETS
 SHEET 15 OF 15

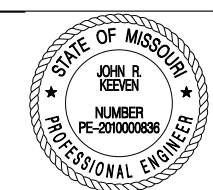
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 JOB No: 15480-02-00
 15480-03-00

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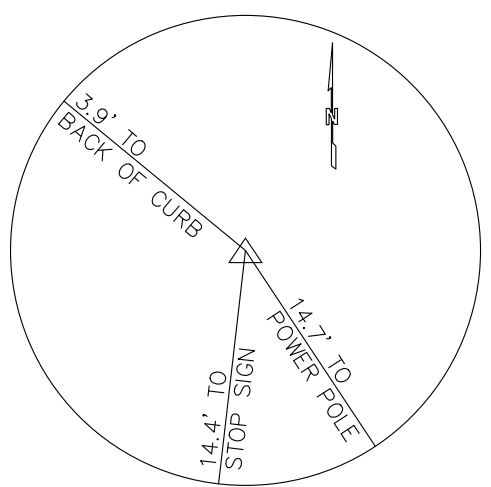
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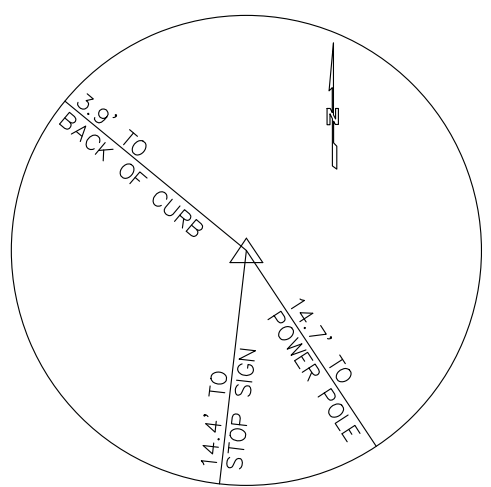
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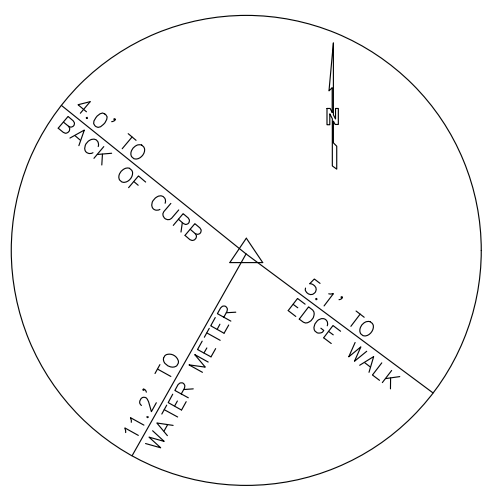
REVISIONS		
NUMBER	BY	DATE



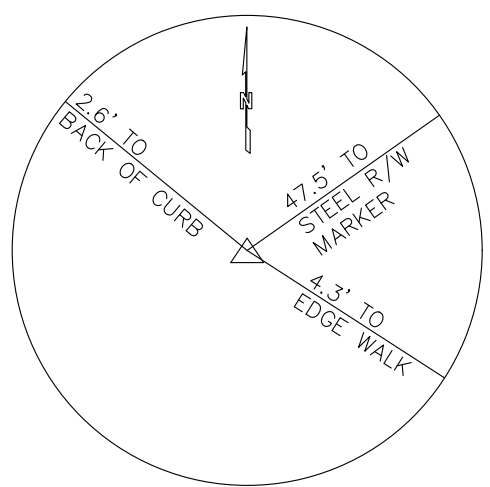
CP #1 (J5P2228 #4036)
 N: 793910.7400
 E: 1571577.5200
 STA: 755+07.19
 54.06' RT
 ELEV.=1019.89
 1,296.13' WEST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 FOUND NAIL FROM PROJECT J5P2228



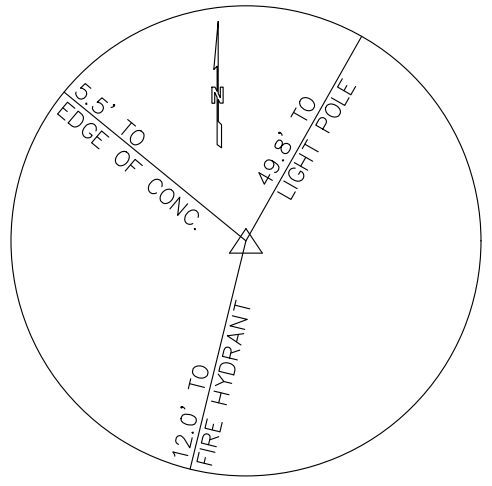
CP #11
 N: 792571.7400
 E: 1570626.4050
 STA: 771+42.63
 37.94' LT
 ELEV.=1040.19
 2,933.69' WEST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET 5/8" IRON PIN W/CONTROL CAP



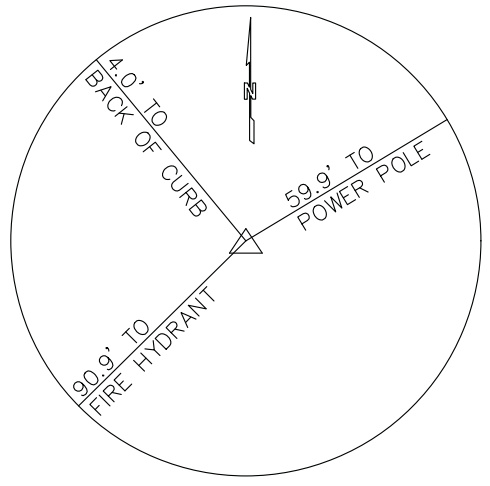
CP #12
 N: 793125.9413
 E: 1571062.8508
 STA: 764+37.20
 37.15' LT
 ELEV.=1038.95
 2,228.26' WEST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET 5/8" IRON PIN W/CONTROL CAP



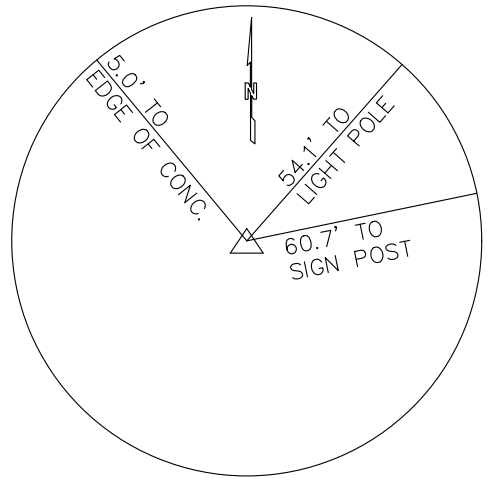
CP #13
 N: 793696.7248
 E: 1571512.8420
 STA: 757+10.37
 36.72' LT
 ELEV.=1024.72
 1,501.43' WEST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET 5/8" IRON PIN W/CONTROL CAP



CP #14
 N: 795227.1021
 E: 1573973.0473
 STA: 727+43.74
 46.38' LT
 ELEV.=972.41
 1,465.20' EAST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET PK NAIL



CP #15
 N: 795965.7432
 E: 1574663.2012
 STA: 717+22.83
 38.12' LT
 ELEV.=955.03
 2,486.09' EAST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET 5/8" IRON PIN W/CONTROL CAP



CP #16
 N: 796514.4535
 E: 1575339.8327
 STA: 708+51.71
 45.10' LT
 ELEV.=946.87
 3,357.21' EAST OF HWY 5 AND 54
 CENTERLINE INTERSECTION
 SET PK NAIL

REFERENCE POINTS
 SHEET 1 OF 1

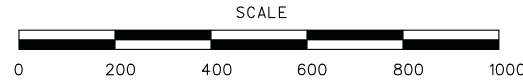
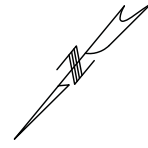
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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

DATE: 5/19/2016

JOB No: 15480-02-00
 15480-03-00

019-REFERENCE_POINTS.dgn

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CURVE EX_US54-1	
PI	720+81.12
PC	718+14.94
PT	723+45.77
Δ	10° 36' 59.7" (LT)
D	2° 00' 00" (LT)
L	530.83'
T	266.18'
R	2.864.79'

CURVE EX_US54-2	
PI	734+99.44
PC	730+63.34
PT	739+10.00
Δ	33° 51' 59.9" (RT)
D	4° 00' 00.1" (RT)
L	846.66'
T	436.10'
R	1.432.39'

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 PROFESSIONAL ENGINEER
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REVISIONS

NUMBER	BY	DATE

COORDINATE POINTS
 SHEET 1 OF 2

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

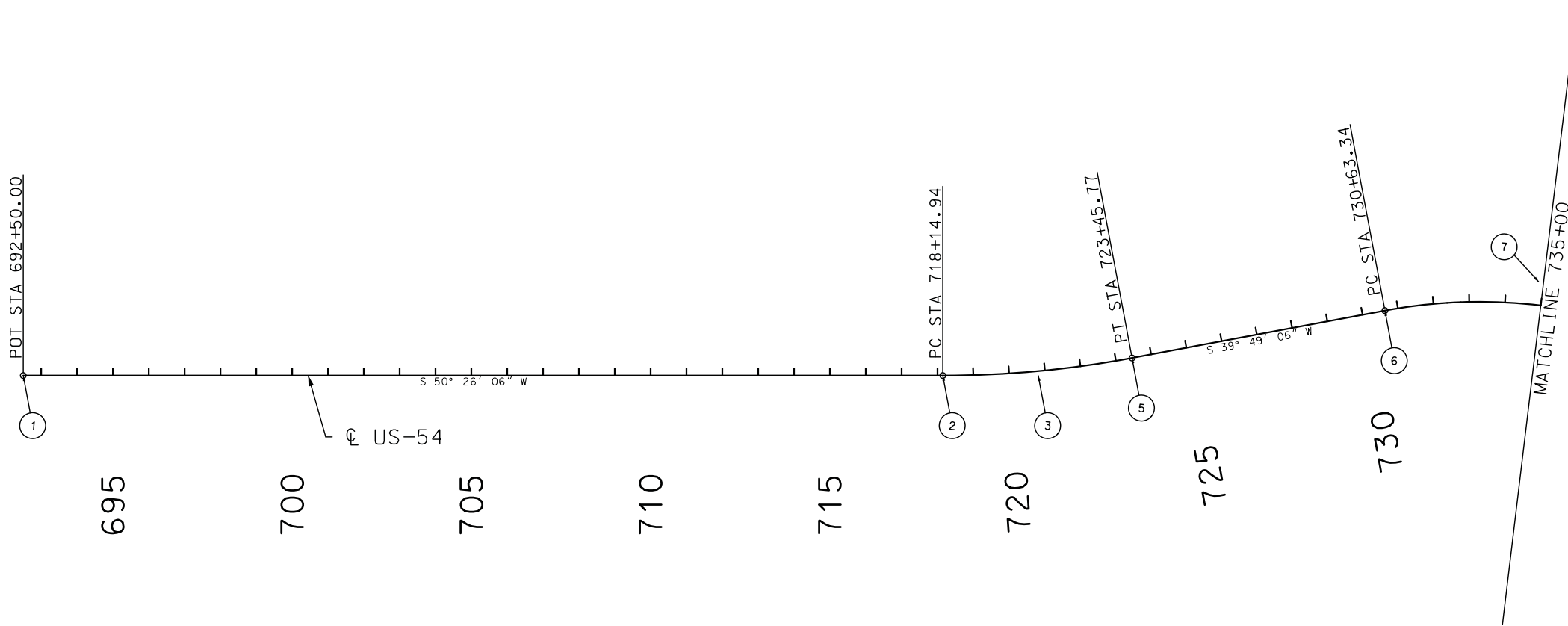


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JOB No: 15480-02-00
 15480-03-00

SHEET 20 OF 122



PROJECT COORDINATE SYSTEM IS BASED ON GROUND DISTANCES, AND IS TIED TO MODIFIED MISSOURI STATE PLANE COORDINATE SYSTEM PER MoDOT PLANS J5P2228 DATED 11/9/2011 AT CONTROL POINT #4036
 SCALE FACTOR = 0.99991316
 PROJECTION FACTOR = 1.0000868475
 VERTICAL DATUM: NAVD 1988

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NUMBER	BY	DATE

COORDINATE POINTS
 SHEET 2 OF 2

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



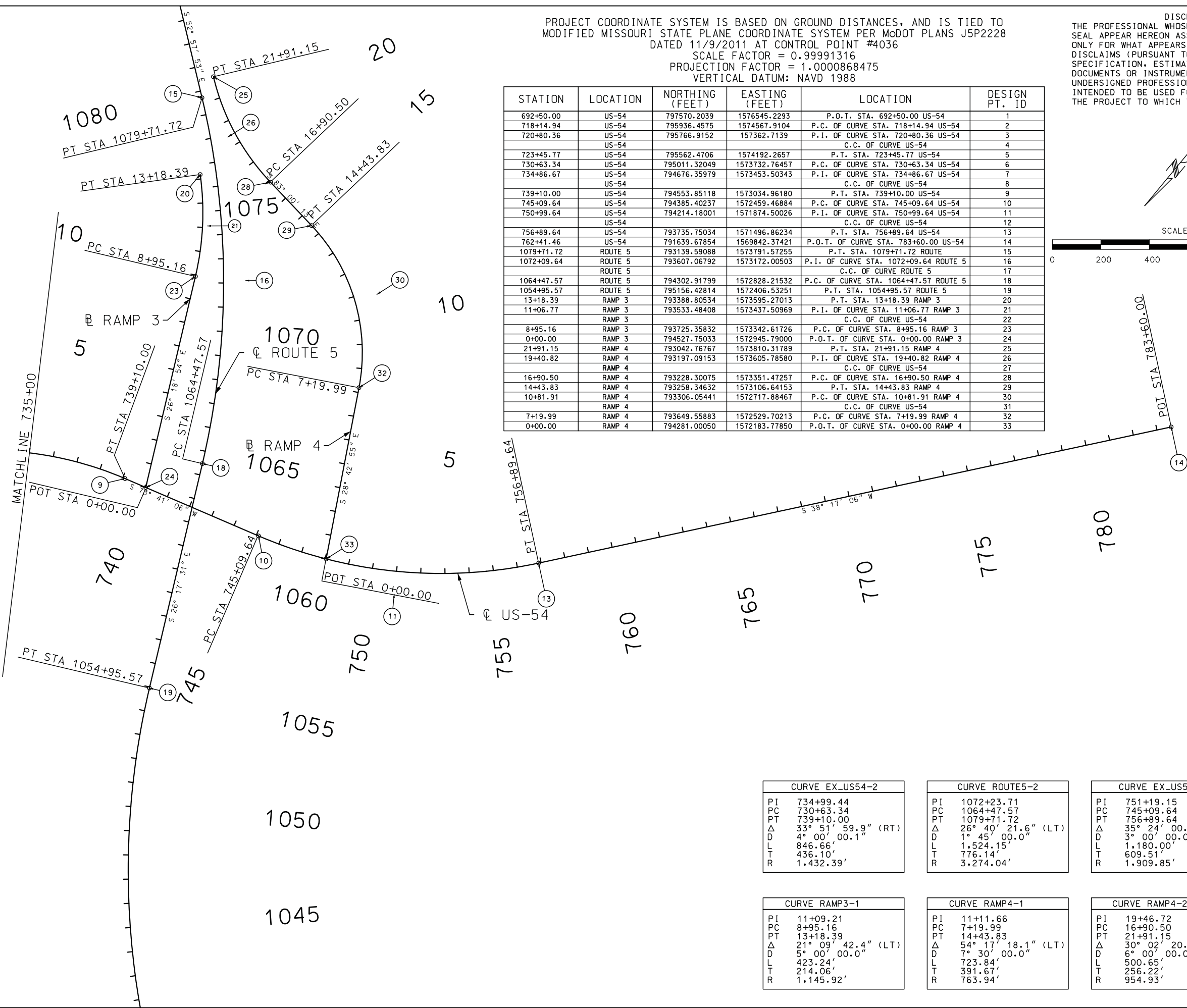
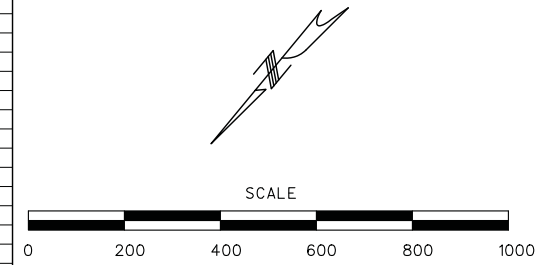
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DATE: 5/19/2016

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 15480-03-00

SHEET 21 OF 122

STATION	LOCATION	NORTHING (FEET)	EASTING (FEET)	LOCATION	DESIGN PT. ID
692+50.00	US-54	797570.2039	1576545.2293	P.O.T. STA. 692+50.00 US-54	1
718+14.94	US-54	795936.4575	1574567.9104	P.C. OF CURVE STA. 718+14.94 US-54	2
720+80.36	US-54	795766.9152	157362.7139	P.I. OF CURVE STA. 720+80.36 US-54	3
	US-54			C.C. OF CURVE US-54	4
723+45.77	US-54	795562.4706	1574192.2657	P.T. STA. 723+45.77 US-54	5
730+63.34	US-54	795011.32049	1573732.76457	P.C. OF CURVE STA. 730+63.34 US-54	6
734+86.67	US-54	794676.35979	1573453.50343	P.I. OF CURVE STA. 734+86.67 US-54	7
	US-54			C.C. OF CURVE US-54	8
739+10.00	US-54	794553.85118	1573034.96180	P.T. STA. 739+10.00 US-54	9
745+09.64	US-54	794385.40237	1572459.46884	P.C. OF CURVE STA. 745+09.64 US-54	10
750+99.64	US-54	794214.18001	1571874.50026	P.I. OF CURVE STA. 750+99.64 US-54	11
	US-54			C.C. OF CURVE US-54	12
756+89.64	US-54	793735.75034	1571496.86234	P.T. STA. 756+89.64 US-54	13
762+41.46	US-54	791639.67854	1569842.37421	P.O.T. OF CURVE STA. 783+60.00 US-54	14
1079+71.72	ROUTE 5	793139.59088	1573791.57255	P.T. STA. 1079+71.72 ROUTE 5	15
1072+09.64	ROUTE 5	793607.06792	1573172.00503	P.I. OF CURVE STA. 1072+09.64 ROUTE 5	16
	ROUTE 5			C.C. OF CURVE ROUTE 5	17
1064+47.57	ROUTE 5	794302.91799	1572828.21532	P.C. OF CURVE STA. 1064+47.57 ROUTE 5	18
1054+95.57	ROUTE 5	795156.42814	1572406.53251	P.T. STA. 1054+95.57 ROUTE 5	19
13+18.39	RAMP 3	793388.80534	1573595.27013	P.T. STA. 13+18.39 RAMP 3	20
11+06.77	RAMP 3	793533.48408	1573437.50969	P.I. OF CURVE STA. 11+06.77 RAMP 3	21
	RAMP 3			C.C. OF CURVE US-54	22
8+95.16	RAMP 3	793725.35832	1573342.61726	P.C. OF CURVE STA. 8+95.16 RAMP 3	23
0+00.00	RAMP 3	794527.75033	1572945.79000	P.O.T. OF CURVE STA. 0+00.00 RAMP 3	24
21+91.15	RAMP 4	793042.76767	1573810.31789	P.T. STA. 21+91.15 RAMP 4	25
19+40.82	RAMP 4	793197.09153	1573605.78580	P.I. OF CURVE STA. 19+40.82 RAMP 4	26
	RAMP 4			C.C. OF CURVE US-54	27
16+90.50	RAMP 4	793228.30075	1573351.47257	P.C. OF CURVE STA. 16+90.50 RAMP 4	28
14+43.83	RAMP 4	793258.34632	1573106.64153	P.T. STA. 14+43.83 RAMP 4	29
10+81.91	RAMP 4	793306.05441	1572717.88467	P.C. OF CURVE STA. 10+81.91 RAMP 4	30
	RAMP 4			C.C. OF CURVE US-54	31
7+19.99	RAMP 4	793649.55883	1572529.70213	P.C. OF CURVE STA. 7+19.99 RAMP 4	32
0+00.00	RAMP 4	794281.00050	1572183.77850	P.O.T. OF CURVE STA. 0+00.00 RAMP 4	33



CURVE EX_US54-2	
PI	734+99.44
PC	730+63.34
PT	739+10.00
Δ	33° 51' 59.9" (RT)
D	4° 00' 00.1"
L	846.66'
T	436.10'
R	1,432.39'

CURVE ROUTE5-2	
PI	1072+23.71
PC	1064+47.57
PT	1079+71.72
Δ	26° 40' 21.6" (LT)
D	1° 45' 00.0"
L	1,524.15'
T	776.14'
R	3,274.04'

CURVE EX_US54-3	
PI	751+19.15
PC	745+09.64
PT	756+89.64
Δ	35° 24' 00.1" (LT)
D	3° 00' 00.0"
L	1,180.00'
T	609.51'
R	1,909.85'

CURVE RAMP3-1	
PI	11+09.21
PC	8+95.16
PT	13+18.39
Δ	21° 09' 42.4" (LT)
D	5° 00' 00.0"
L	423.24'
T	214.06'
R	1,145.92'

CURVE RAMP4-1	
PI	11+11.66
PC	7+19.99
PT	14+43.83
Δ	54° 17' 18.1" (LT)
D	7° 30' 00.0"
L	723.84'
T	391.67'
R	763.94'

CURVE RAMP4-2	
PI	19+46.72
PC	16+90.50
PT	21+91.15
Δ	30° 02' 20.4" (RT)
D	6° 00' 00.0"
L	500.65'
T	256.22'
R	954.93'

021_COORDINATE_POINTS_1200.dgn

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PROFESSIONAL ENGINEER
MO. P.E. LICENSE # 201000836

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NUMBER	BY	DATE

TRAFFIC CONTROL LEGEND AND DETAILS
SHEET 1 OF 8

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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DATE: 5/19/2016

JOB No: 15480-02-00
15480-03-00

SHEET 22 OF 122

LEGEND

SYMBOLS USED ON TRAFFIC CONTROL PLANS

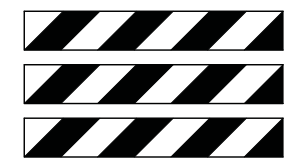
- WORK AREA
- COMPLETED WORK AREA
- TEMPORARY TRAFFIC BARRIER (TTB)
- LANE USAGE
- CHANNELIZERS
- SIGN
- TYPE III MOVEABLE BARRICADE
- SAND FILLED IMPACT ATTENUATOR
- DIRECTIONAL INDICATOR BARRICADE
- CMS CHANGEABLE MESSAGE SIGN
- SW/T SOLID WHITE TAPE (4")
- SY/T SOLID YELLOW TAPE (4")
- SW/P SOLID WHITE PAINT (4")
- SY/P SOLID YELLOW PAINT (4")
- DSY/P DOUBLE SOLID YELLOW PAINT (4")
- AWRS ADVANCED WARNING RAIL SYSTEM
- FLAGS FLAG ASSEMBLY (2 PER SIGN)



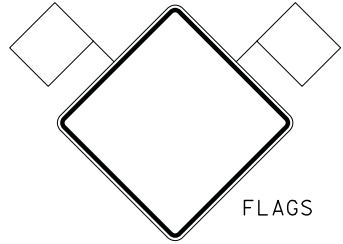
W020-1

(2)

BRIDGE OR RAMP



AWRS (L)



FLAGS



R2-1

(4)



G020-5aP

(54)



W020-5

(5)



W05-5

(34)

LEFT LANE



W020-6a

(6)

LEFT



W021-5b

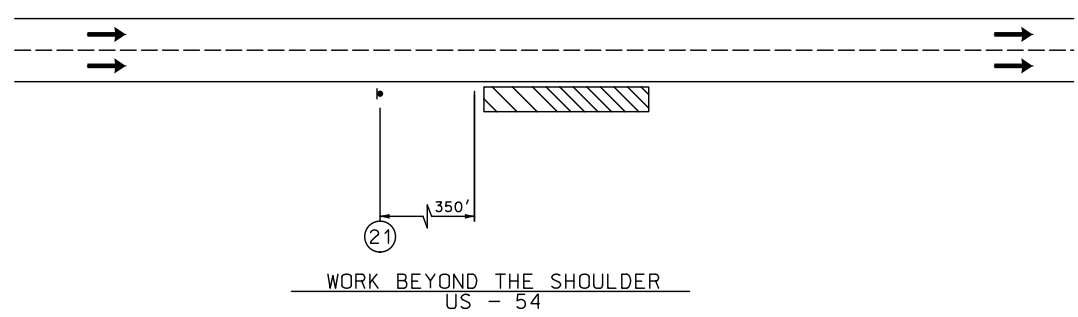
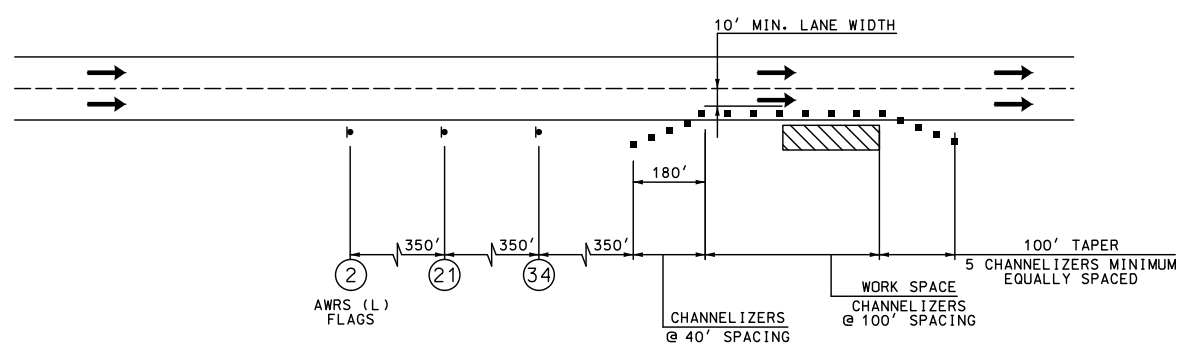
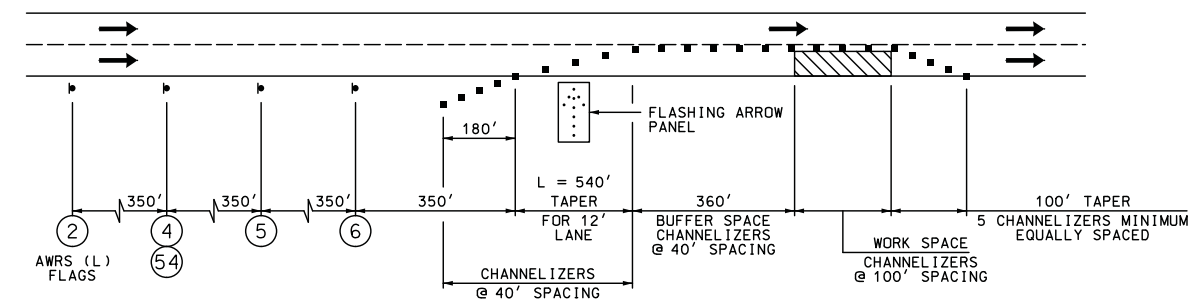
(21)



R9-9

(P1)

USE SIGN P1 AS DIRECTED BY THE ENGINEER



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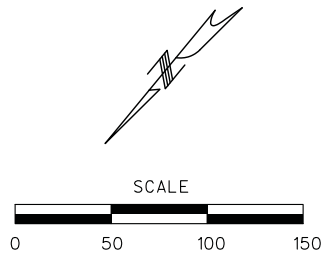
GENERAL NOTES

1. SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS, AS DIRECTED BY THE ENGINEER
2. CHANNELIZERS SHALL BE PLACED OFF THE PAVEMENT WHEN WORK IS NOT IN PROGRESS UNLESS LANE CLOSURE IS IN EFFECT.
3. CHANNELIZERS SHALL BE USED FOR LANE CLOSING AND CHANNELIZING TAPERS. CONES MAY BE USED THROUGH THE WORK AREA UNLESS LANE CLOSURE IS REQUIRED OVERNIGHT.
4. WORK THIS SHEET WITH THE TRAFFIC CONTROL PLAN (T.C.P.).
5. ALL EXISTING SIGNS AND MARKERS WITHIN THE LIMITS OF OPERATIONS SHALL BE REMOVED OR RELOCATED IN ACCORDANCE WITH SECTION 104.10.2 OF THE STANDARD SPECIFICATIONS.
6. ALL EXISTING CONFLICTING SIGNS SHALL BE COVERED.
7. ALL SIGNS SHALL BE PORTABLE UNLESS OTHERWISE NOTED.
8. LANE CLOSURES SHALL ONLY BE USED AS APPROVED BY THE ENGINEER.
9. NO DIRECT PAYMENT WILL BE MADE FOR RELOCATING OF PORTABLE MOUNTED SIGNS.
10. REMOVE TEMPORARY PAVEMENT MARKING FROM PERMANENT PAVEMENT WHEN IT IS NO LONGER NEEDED.
11. THE CONTRACTOR SHALL FOLLOW ALL GUIDELINES SET FORTH IN ARTICLE 616, TEMPORARY TRAFFIC CONTROL, IN THE MODOT ENGINEERING POLICY GUIDE.
12. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS & SIDE STREETS AT ALL TIMES.

022-TCP-LEGEND.dgn

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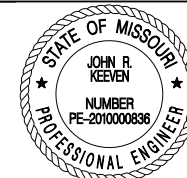


LEGEND

SYMBOLS USED ON TRAFFIC CONTROL PLANS

	WORK AREA	CMS	CHANGEABLE MESSAGE SIGN
	COMPLETED WORK AREA	SW/T	SOLID WHITE TAPE (4")
	TEMPORARY TRAFFIC BARRIER (TTB)	SY/T	SOLID YELLOW TAPE (4")
	LANE USAGE	SW/P	SOLID WHITE PAINT (4")
	CHANNELIZERS	SY/P	SOLID YELLOW PAINT (4")
	SIGN	DSY/P	DOUBLE SOLID YELLOW PAINT (4")
	TYPE III MOVEABLE BARRICADE	AWRS	ADVANCED WARNING RAIL SYSTEM
	SAND FILLED IMPACT ATTENUATOR	FLAGS	FLAG ASSEMBLY (2 PER SIGN)
	DIRECTIONAL INDICATOR BARRICADE		

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NUMBER	BY	DATE

TRAFFIC CONTROL

SHEET 2 OF 8

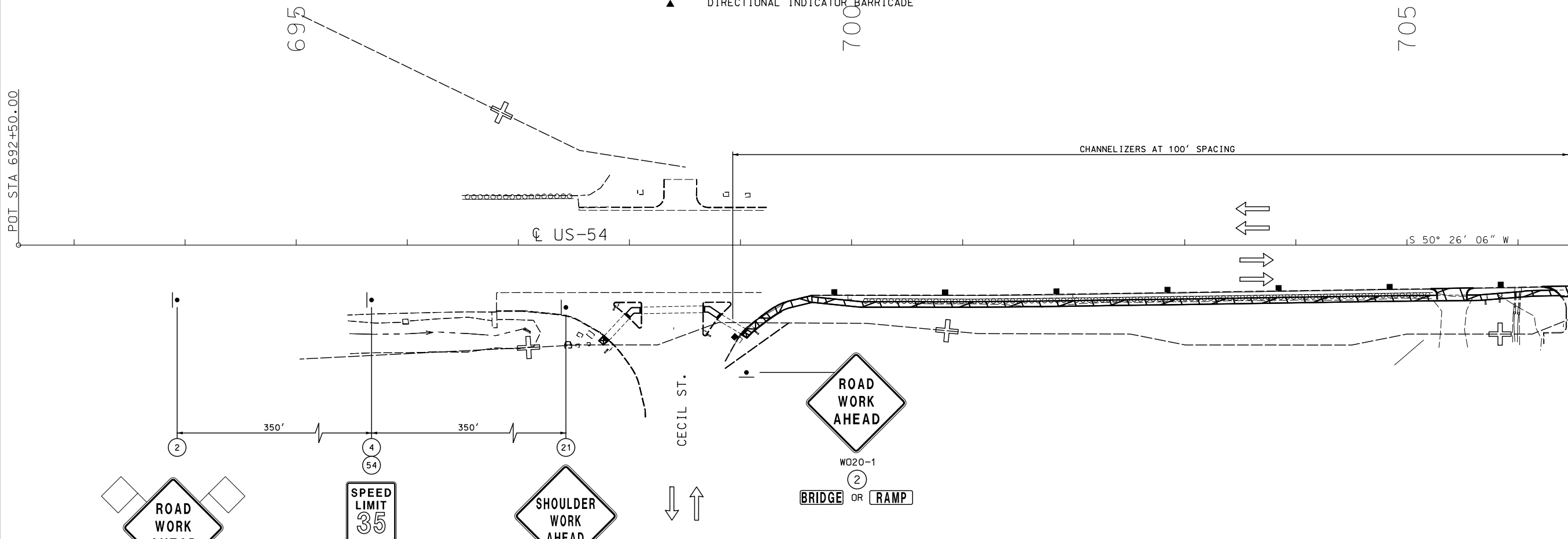
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 US-54 SIDEWALK PROJECT
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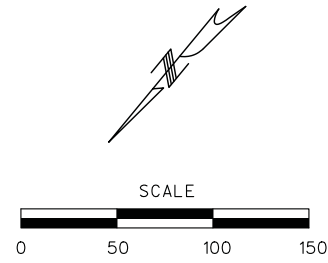
SHEET 23 OF 122












NOTES:

1. PLACE ALL CONSTRUCTION DEVICES AS SHOWN ON PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. SEE JOB SPECIAL PROVISIONS FOR OTHER DETAILS.
2. CONSTRUCT ALL IMPROVEMENTS ALONG US-54. USE TYPICAL LANE CLOSURE DETAIL, MINOR TRAVELWAY ENCROACHMENT DETAIL, AND WORK BEYOND THE SHOULDER DETAIL AS NEEDED DURING WORKING HOURS.
3. MAINTAIN ACCESS TO ENTRANCES & SIDEROADS BEING USED ALONG US-54 AT ALL TIMES.
4. USE PAVEMENT EDGE TREATMENT AT PAVED APPROACH CONSTRUCTION AREAS AS DIRECTED BY THE ENGINEER.
5. DEPENDING ON THE CONTRACTOR'S OPERATIONS, SIGN LOCATIONS MAY BE ADJUSTED CLOSER TO THE ACTUAL CONSTRUCTION LIMITS OF EACH PHASE AS DIRECTED BY THE ENGINEER.

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LEGEND
SYMBOLS USED ON TRAFFIC CONTROL PLANS

- | | | | |
|---|---------------------------------|-------|--------------------------------|
|  | WORK AREA | CMS | CHANGEABLE MESSAGE SIGN |
|  | COMPLETED WORK AREA | SW/T | SOLID WHITE TAPE (4") |
|  | TEMPORARY TRAFFIC BARRIER (TTB) | SY/T | SOLID YELLOW TAPE (4") |
|  | LANE USAGE | SW/P | SOLID WHITE PAINT (4") |
|  | CHANNELIZERS | SY/P | SOLID YELLOW PAINT (4") |
|  | SIGN | DSY/P | DOUBLE SOLID YELLOW PAINT (4") |
|  | TYPE III MOVEABLE BARRICADE | AWRS | ADVANCED WARNING RAIL SYSTEM |
|  | SAND FILLED IMPACT ATTENUATOR | FLAGS | FLAG ASSEMBLY (2 PER SIGN) |
|  | DIRECTIONAL INDICATOR BARRICADE | | |

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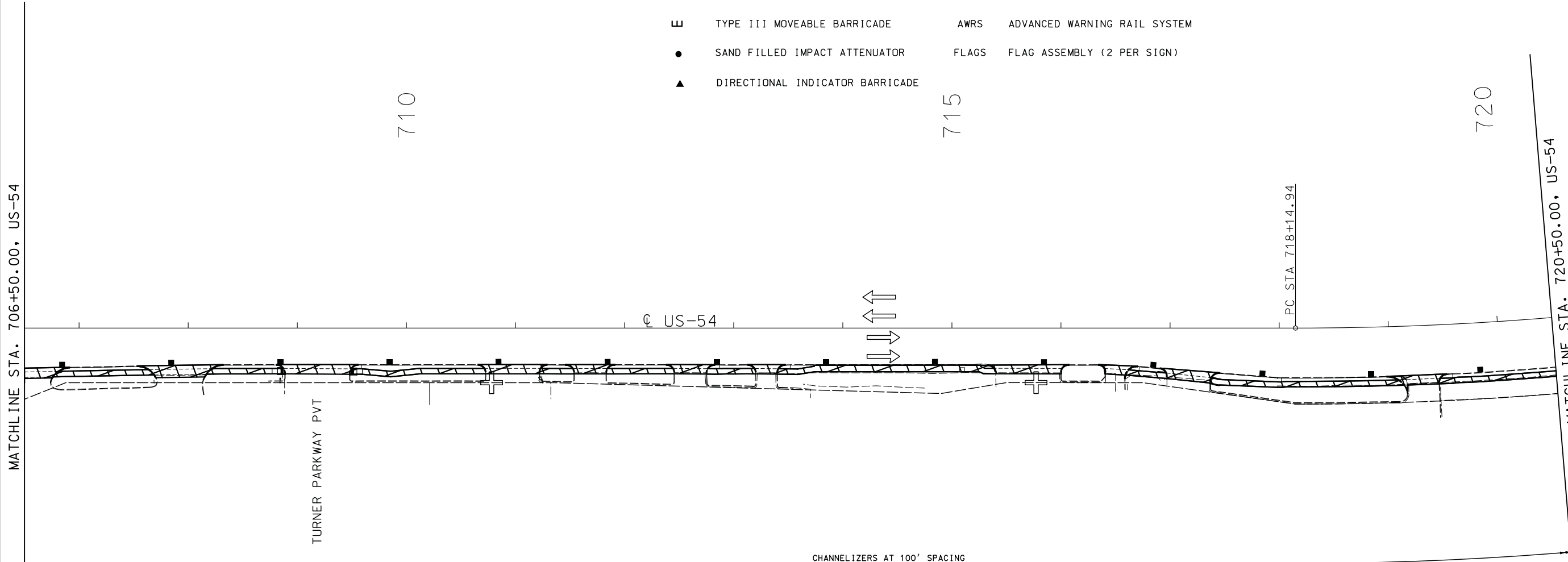


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TRAFFIC CONTROL

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







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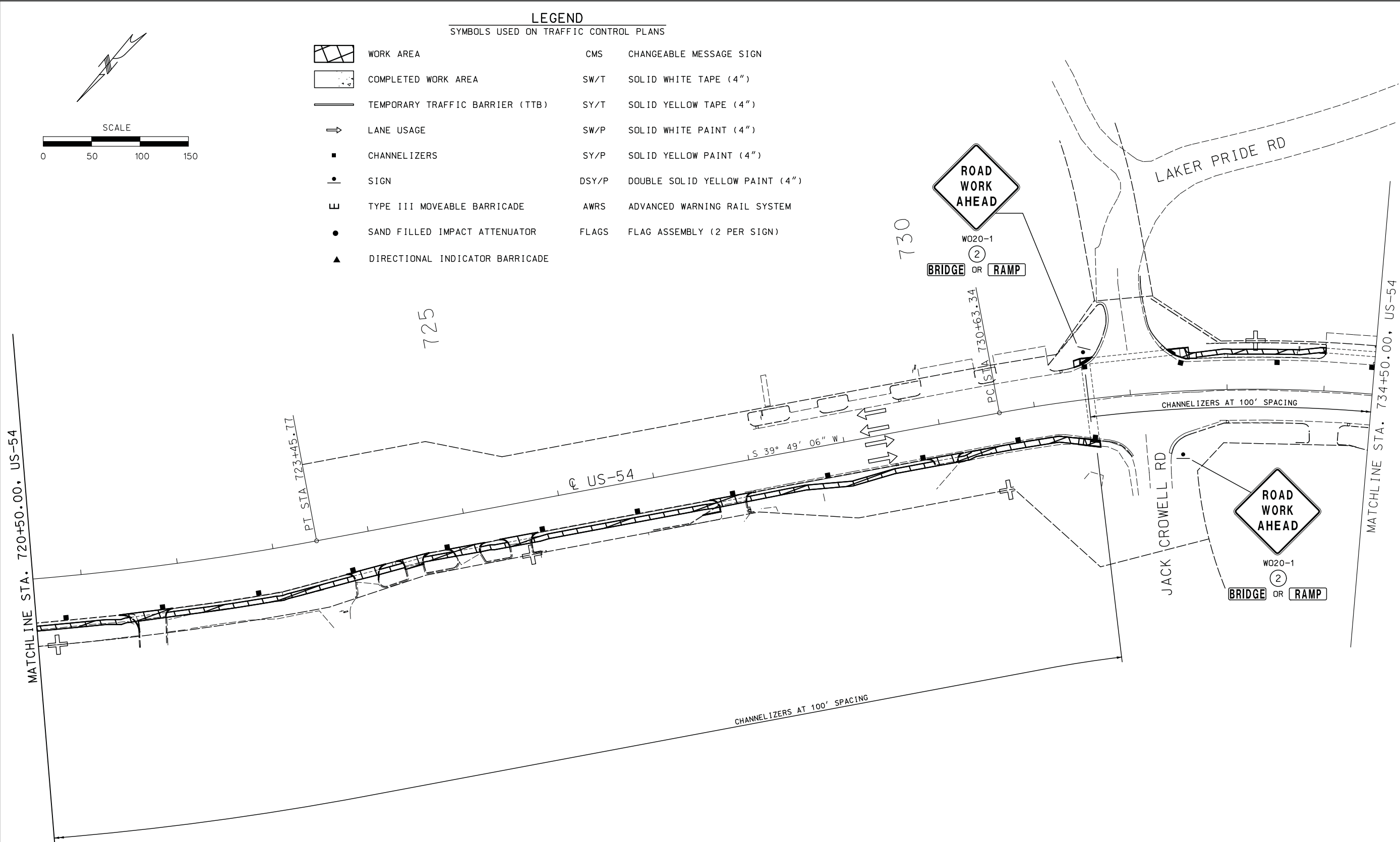
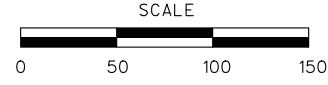
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LEGEND
SYMBOLS USED ON TRAFFIC CONTROL PLANS

- | | | | |
|---|---------------------------------|-------|--------------------------------|
|  | WORK AREA | CMS | CHANGEABLE MESSAGE SIGN |
|  | COMPLETED WORK AREA | SW/T | SOLID WHITE TAPE (4") |
|  | TEMPORARY TRAFFIC BARRIER (TTB) | SY/T | SOLID YELLOW TAPE (4") |
|  | LANE USAGE | SW/P | SOLID WHITE PAINT (4") |
|  | CHANNELIZERS | SY/P | SOLID YELLOW PAINT (4") |
|  | SIGN | DSY/P | DOUBLE SOLID YELLOW PAINT (4") |
|  | TYPE III MOVEABLE BARRICADE | AWRS | ADVANCED WARNING RAIL SYSTEM |
|  | SAND FILLED IMPACT ATTENUATOR | FLAGS | FLAG ASSEMBLY (2 PER SIGN) |
|  | DIRECTIONAL INDICATOR BARRICADE | | |



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SHEET 4 OF 8

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US-54 SIDEWALK PROJECT
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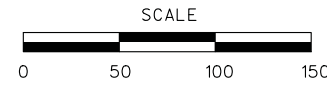
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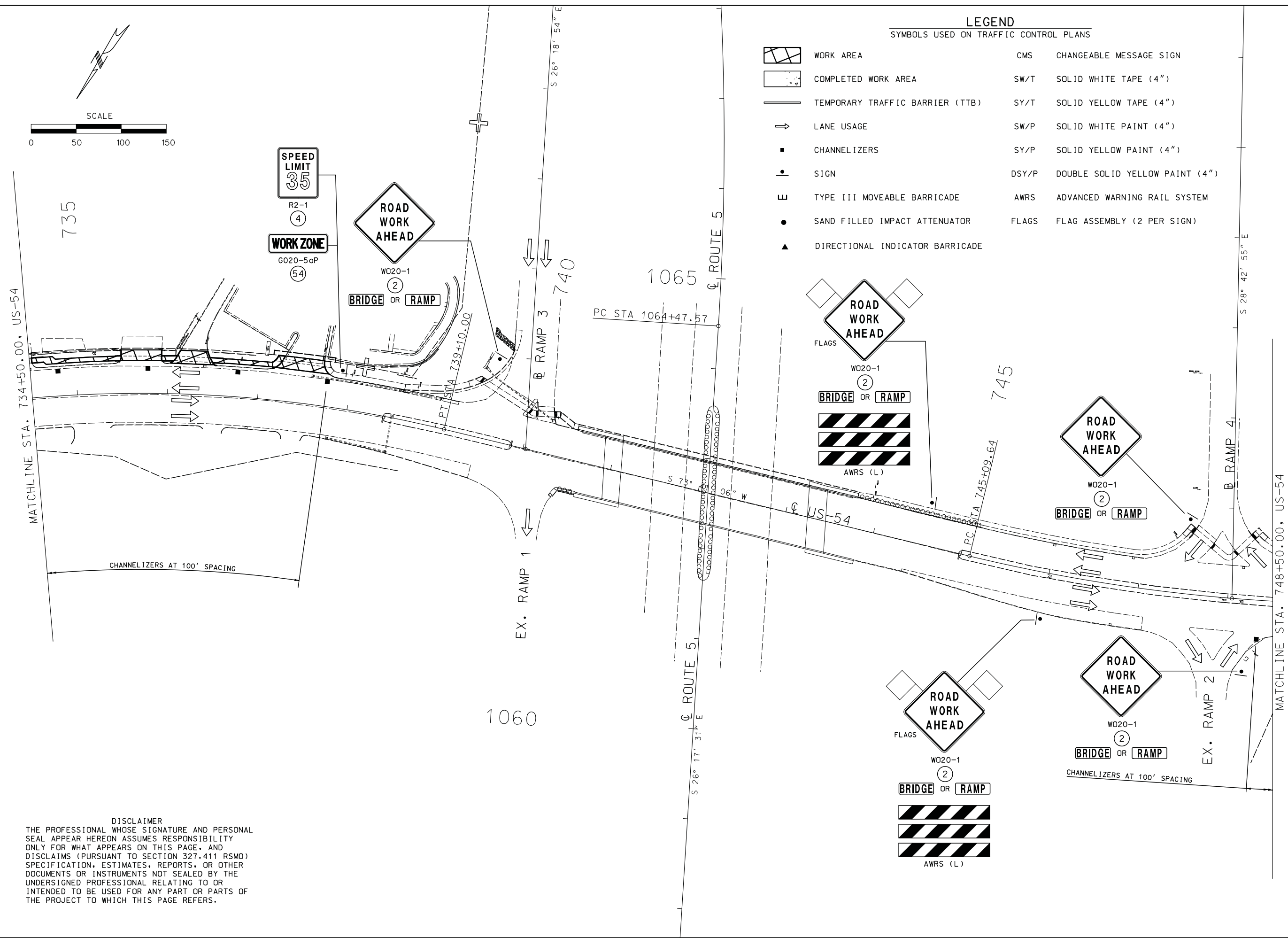
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026_TRAFFIC_CONTROL-4_150.dgn



SCALE



LEGEND

SYMBOLS USED ON TRAFFIC CONTROL PLANS

- WORK AREA
- COMPLETED WORK AREA
- TEMPORARY TRAFFIC BARRIER (TTB)
- LANE USAGE
- CHANNELIZERS
- SIGN
- TYPE III MOVEABLE BARRICADE
- SAND FILLED IMPACT ATTENUATOR
- DIRECTIONAL INDICATOR BARRICADE
- CMS CHANGEABLE MESSAGE SIGN
- SW/T SOLID WHITE TAPE (4")
- SY/T SOLID YELLOW TAPE (4")
- SW/P SOLID WHITE PAINT (4")
- SY/P SOLID YELLOW PAINT (4")
- DSY/P DOUBLE SOLID YELLOW PAINT (4")
- AWRS ADVANCED WARNING RAIL SYSTEM
- FLAGS FLAG ASSEMBLY (2 PER SIGN)



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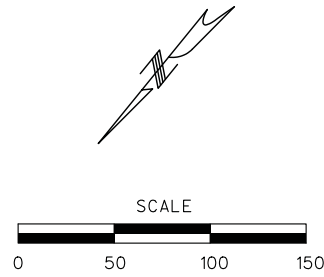
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LEGEND
SYMBOLS USED ON TRAFFIC CONTROL PLANS

- | | | | |
|--|---------------------------------|-------|--------------------------------|
| | WORK AREA | CMS | CHANGEABLE MESSAGE SIGN |
| | COMPLETED WORK AREA | SW/T | SOLID WHITE TAPE (4") |
| | TEMPORARY TRAFFIC BARRIER (TTB) | SY/T | SOLID YELLOW TAPE (4") |
| | LANE USAGE | SW/P | SOLID WHITE PAINT (4") |
| | CHANNELIZERS | SY/P | SOLID YELLOW PAINT (4") |
| | SIGN | DSY/P | DOUBLE SOLID YELLOW PAINT (4") |
| | TYPE III MOVEABLE BARRICADE | AWRS | ADVANCED WARNING RAIL SYSTEM |
| | SAND FILLED IMPACT ATTENUATOR | FLAGS | FLAG ASSEMBLY (2 PER SIGN) |
| | DIRECTIONAL INDICATOR BARRICADE | | |

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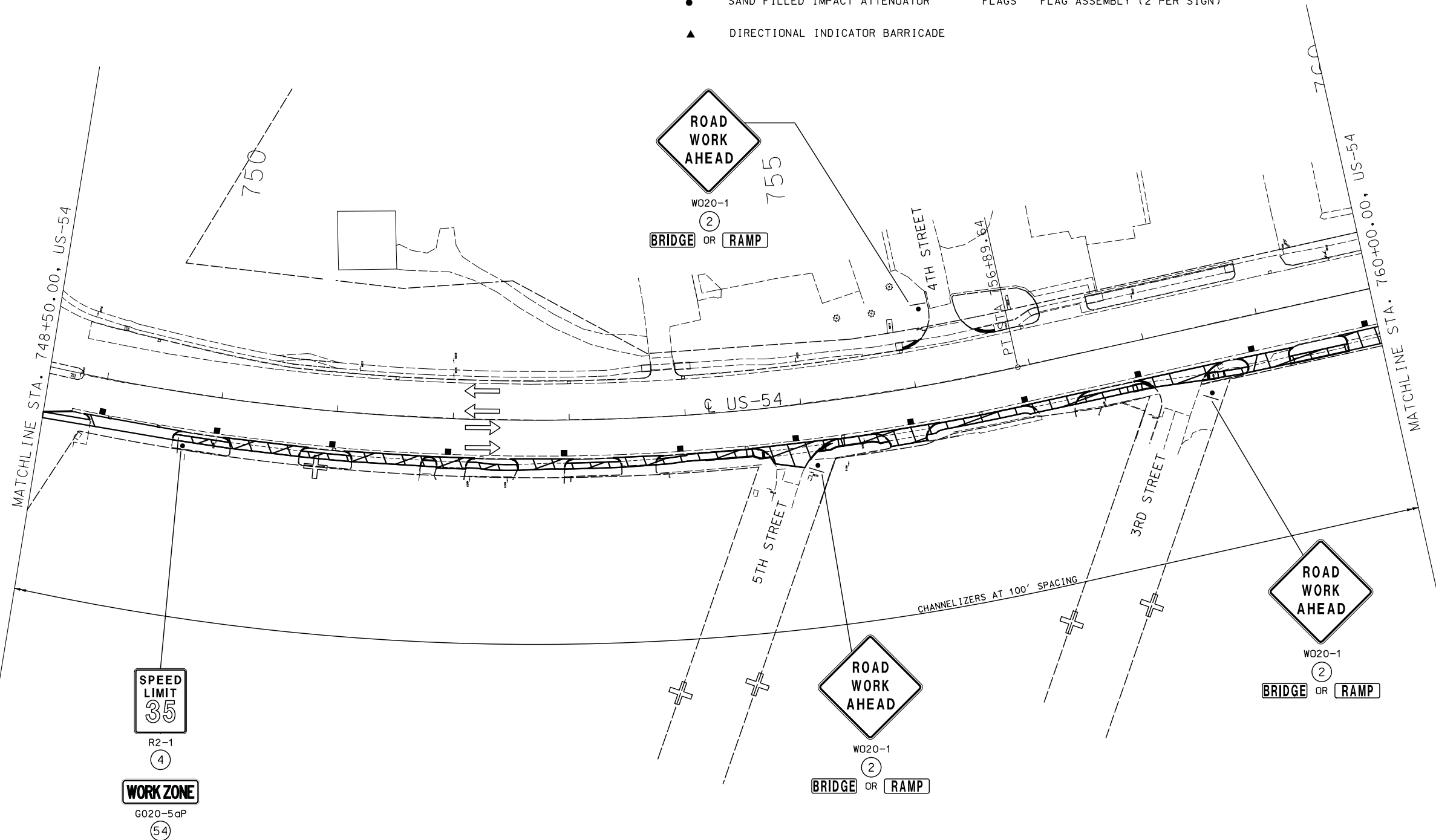
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TRAFFIC CONTROL
SHEET 6 OF 8

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US-54 SIDEWALK PROJECT
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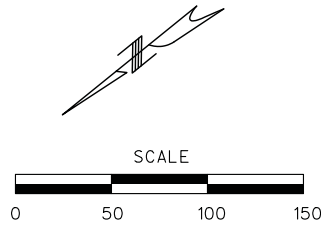
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LEGEND
SYMBOLS USED ON TRAFFIC CONTROL PLANS

	WORK AREA	CMS	CHANGEABLE MESSAGE SIGN
	COMPLETED WORK AREA	SW/T	SOLID WHITE TAPE (4")
	TEMPORARY TRAFFIC BARRIER (TTB)	SY/T	SOLID YELLOW TAPE (4")
	LANE USAGE	SW/P	SOLID WHITE PAINT (4")
	CHANNELIZERS	SY/P	SOLID YELLOW PAINT (4")
	SIGN	DSY/P	DOUBLE SOLID YELLOW PAINT (4")
	TYPE III MOVEABLE BARRICADE	AWRS	ADVANCED WARNING RAIL SYSTEM
	SAND FILLED IMPACT ATTENUATOR	FLAGS	FLAG ASSEMBLY (2 PER SIGN)
	DIRECTIONAL INDICATOR BARRICADE		

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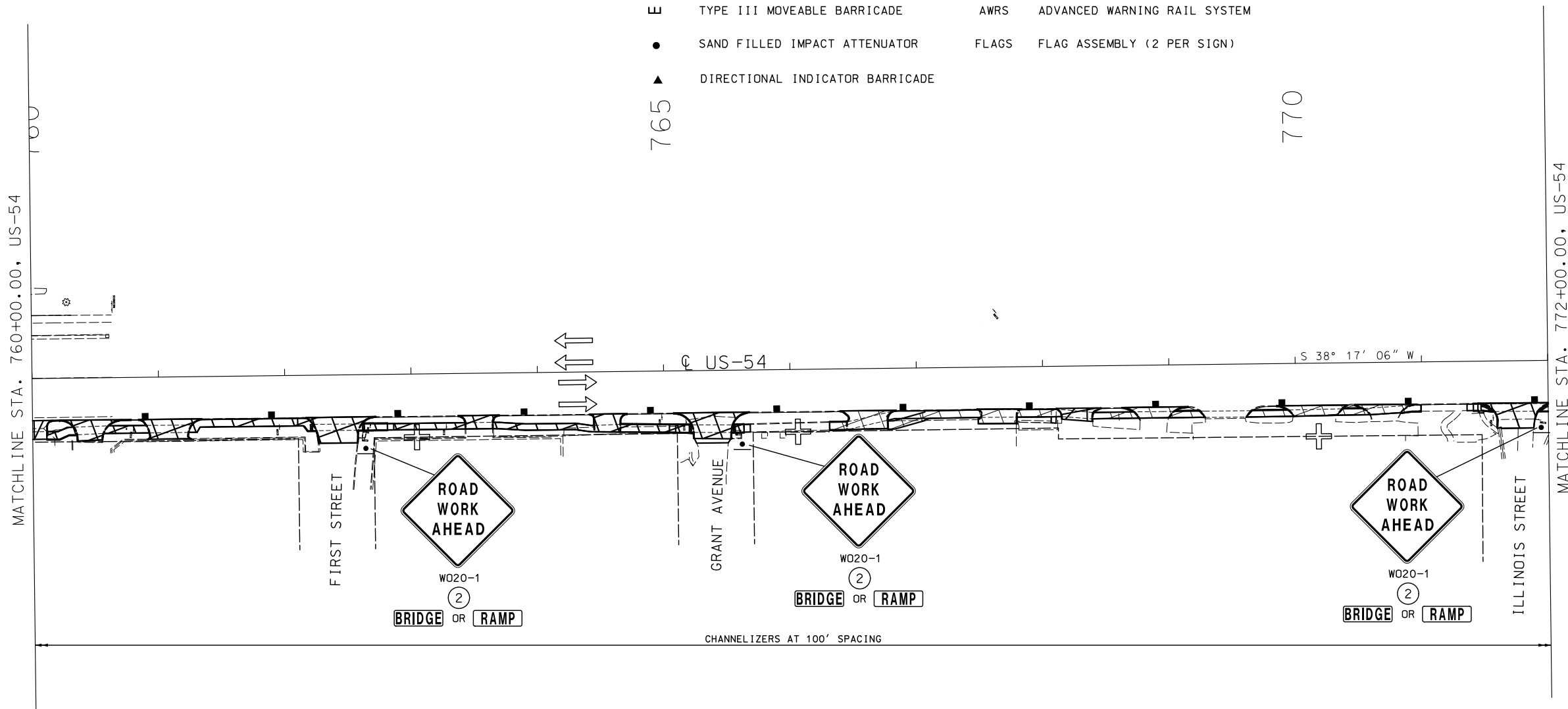
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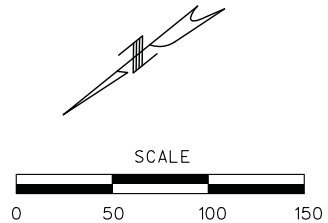
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LEGEND
SYMBOLS USED ON TRAFFIC CONTROL PLANS

- | | | | |
|--|---------------------------------|-------|--------------------------------|
| | WORK AREA | CMS | CHANGEABLE MESSAGE SIGN |
| | COMPLETED WORK AREA | SW/T | SOLID WHITE TAPE (4") |
| | TEMPORARY TRAFFIC BARRIER (TTB) | SY/T | SOLID YELLOW TAPE (4") |
| | LANE USAGE | SW/P | SOLID WHITE PAINT (4") |
| | CHANNELIZERS | SY/P | SOLID YELLOW PAINT (4") |
| | SIGN | DSY/P | DOUBLE SOLID YELLOW PAINT (4") |
| | TYPE III MOVEABLE BARRICADE | AWRS | ADVANCED WARNING RAIL SYSTEM |
| | SAND FILLED IMPACT ATTENUATOR | FLAGS | FLAG ASSEMBLY (2 PER SIGN) |
| | DIRECTIONAL INDICATOR BARRICADE | | |

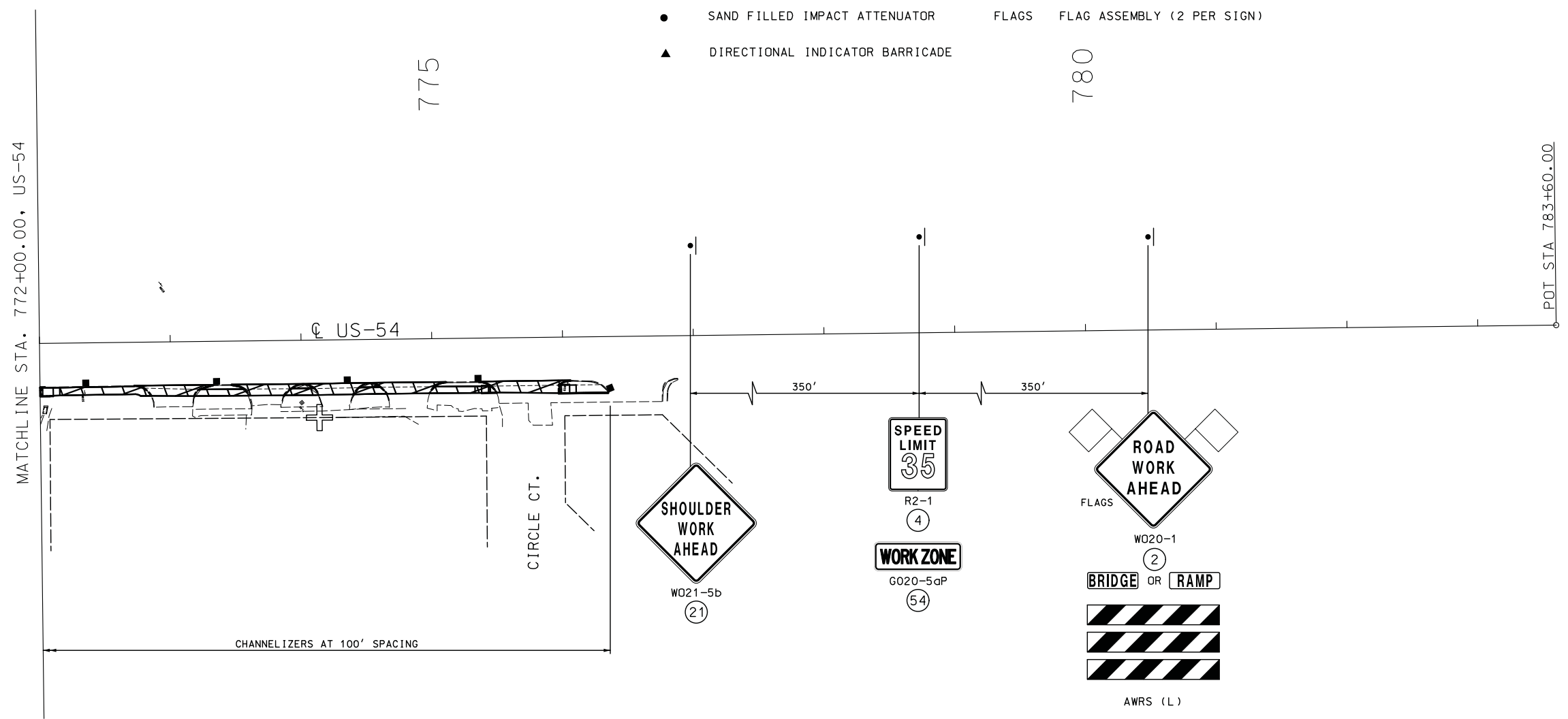
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TRAFFIC CONTROL
SHEET 8 OF 8

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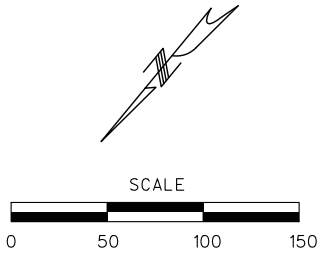
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SHEET 29 OF 122

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TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

NOTE: INSTALL CURB INLET CHECKS AT CURB INLETS AND GRATED INLETS AS DIRECTED BY THE ENGINEER.

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STORMWATER POLLUTION PREVENTION PLAN

SHEET 1 OF 7

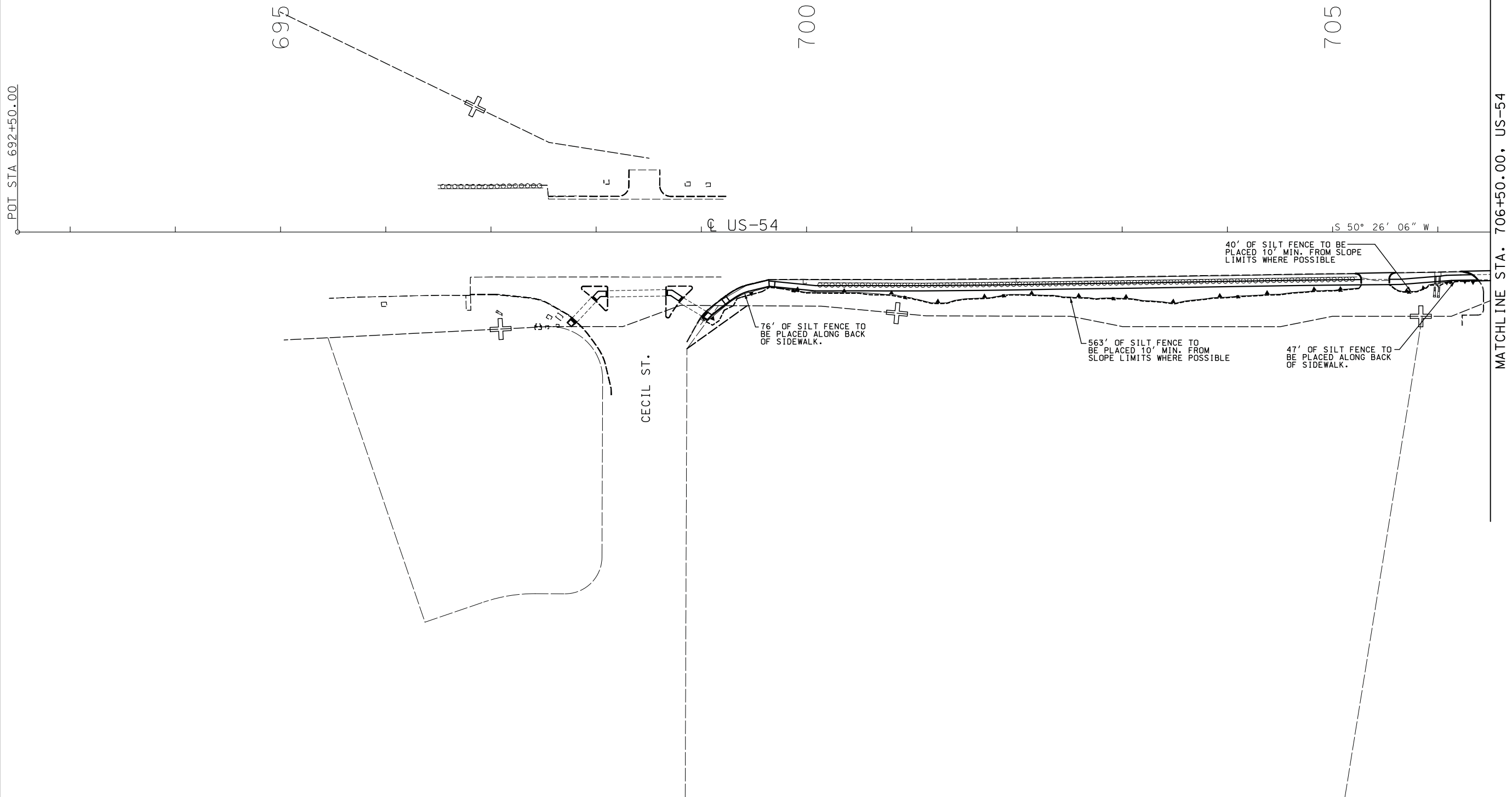
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STORMWATER POLLUTION PREVENTION PLAN

SHEET 2 OF 7

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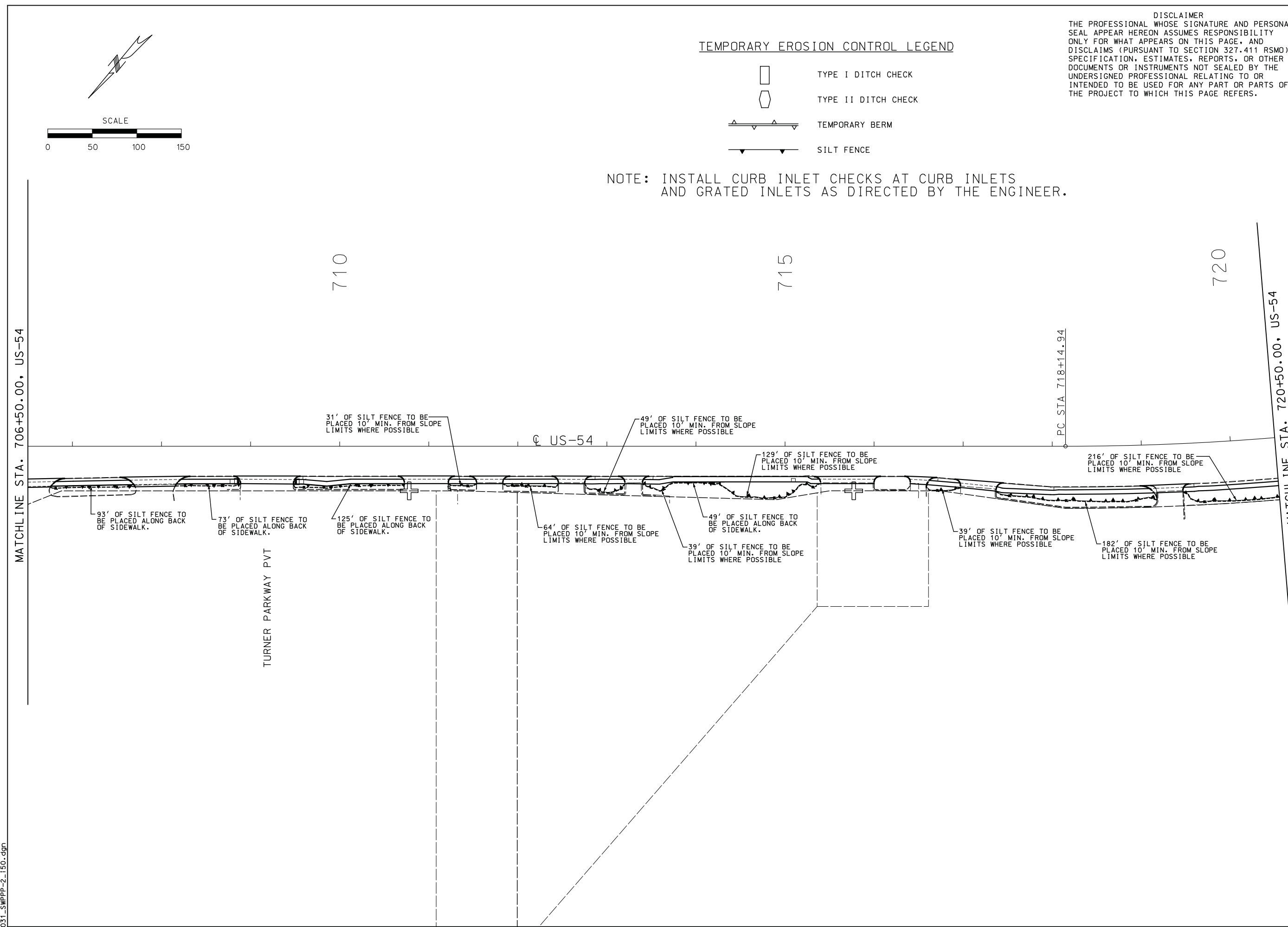
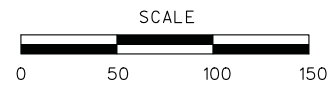
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TEMPORARY EROSION CONTROL LEGEND

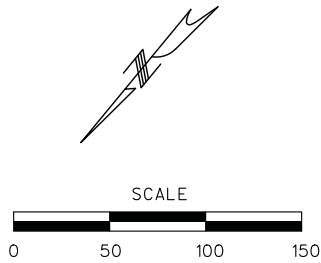
-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

NOTE: INSTALL CURB INLET CHECKS AT CURB INLETS AND GRATED INLETS AS DIRECTED BY THE ENGINEER.







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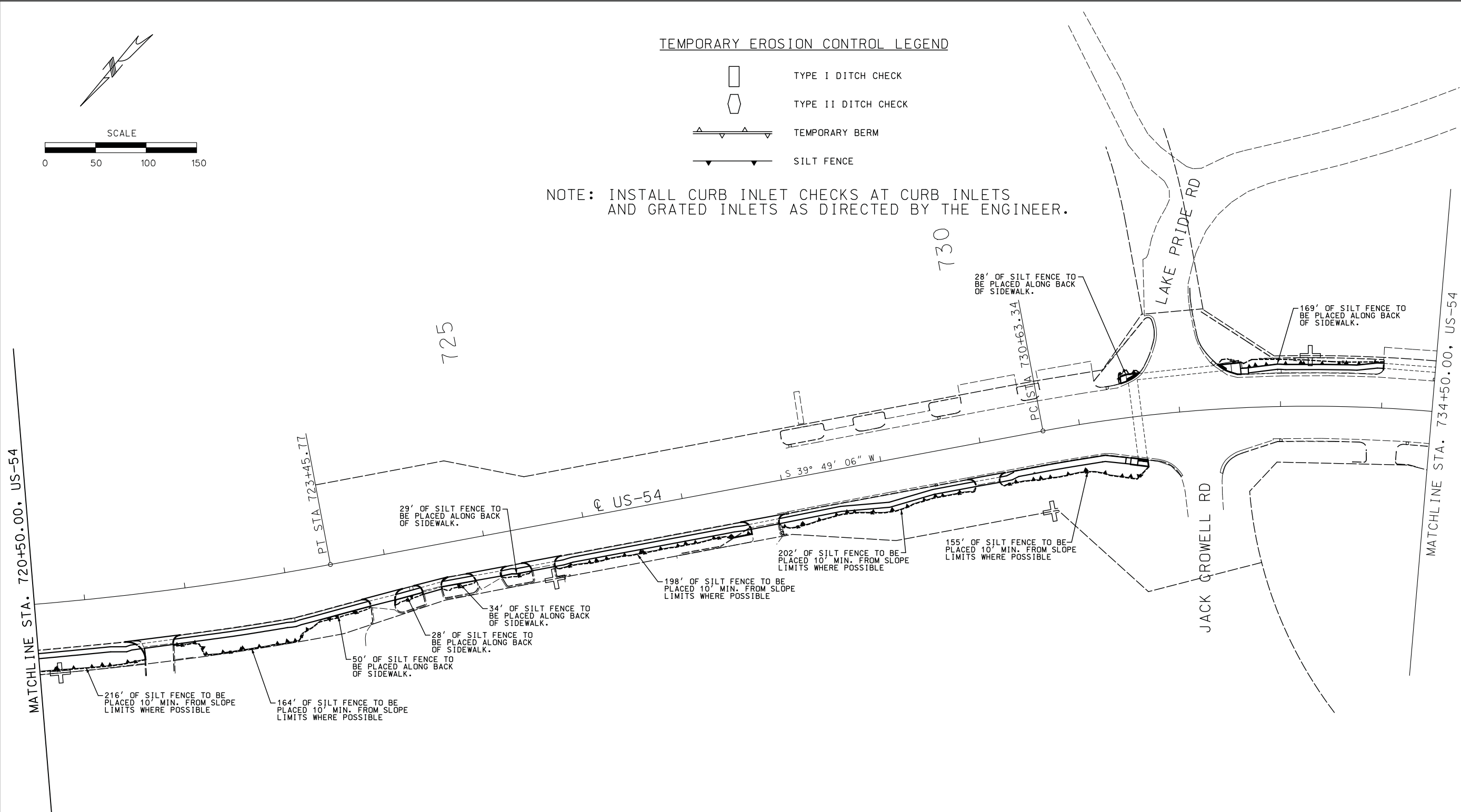
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TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

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STORMWATER POLLUTION PREVENTION PLAN
SHEET 3 OF 7

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PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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SHEET 32 OF 122

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NUMBER	BY	DATE

STORMWATER POLLUTION PREVENTION PLAN

SHEET 4 OF 7

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)







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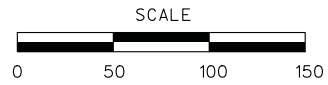
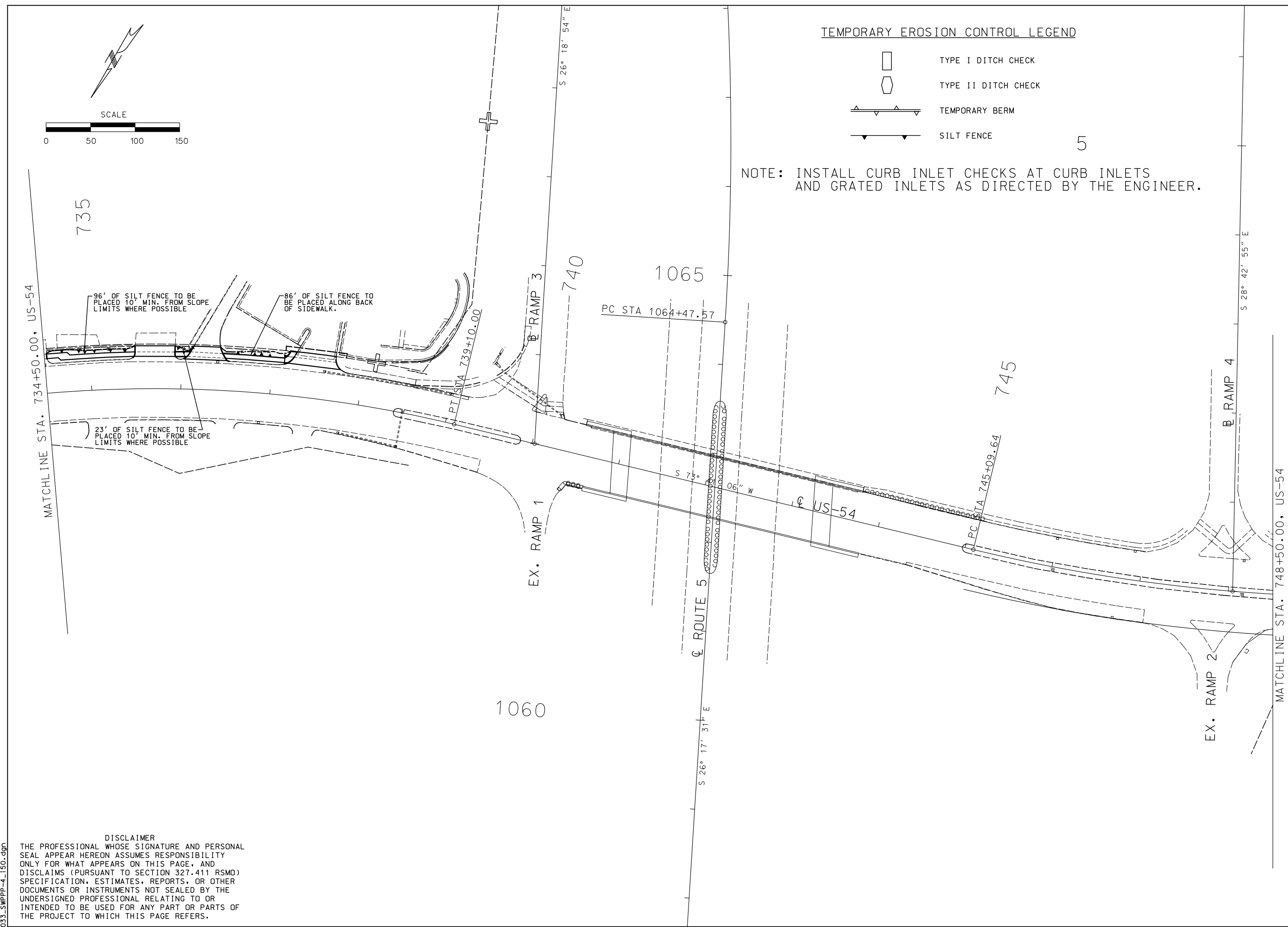
SHEET 33 OF 122

TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

5

NOTE: INSTALL CURB INLET CHECKS AT CURB INLETS AND GRATED INLETS AS DIRECTED BY THE ENGINEER.




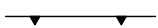


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TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
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-  TEMPORARY BERM
-  SILT FENCE

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STORMWATER POLLUTION PREVENTION PLAN

SHEET 5 OF 7

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

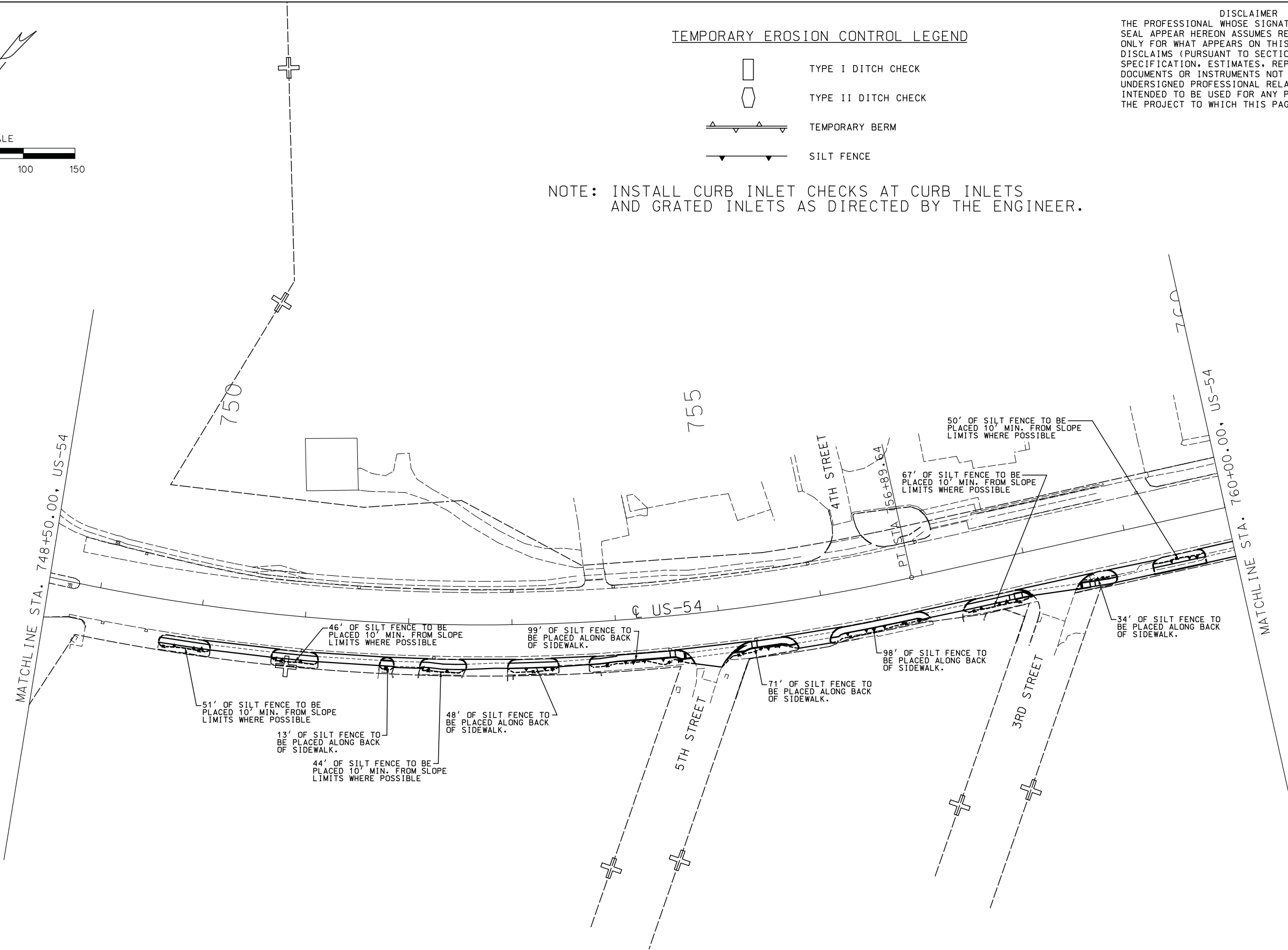
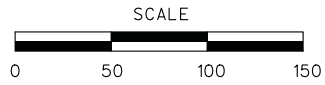


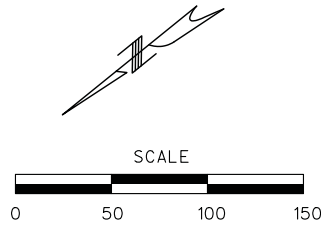
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SHEET 34 OF 122





TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

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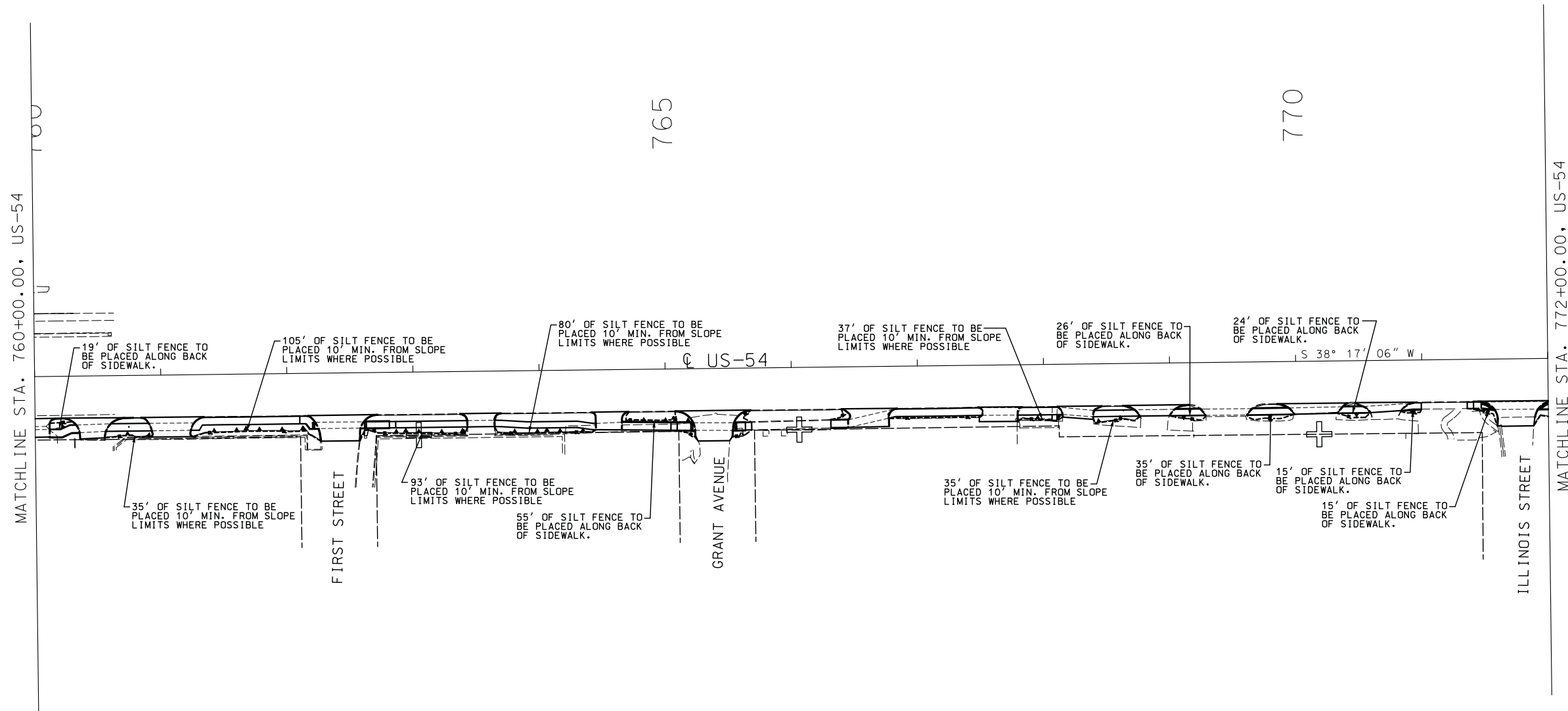
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STORMWATER POLLUTION PREVENTION PLAN

SHEET 6 OF 7

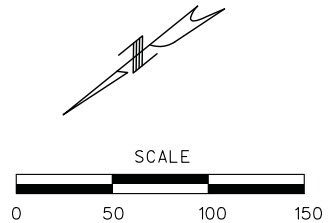
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SHEET 35 OF 122



TEMPORARY EROSION CONTROL LEGEND

-  TYPE I DITCH CHECK
-  TYPE II DITCH CHECK
-  TEMPORARY BERM
-  SILT FENCE

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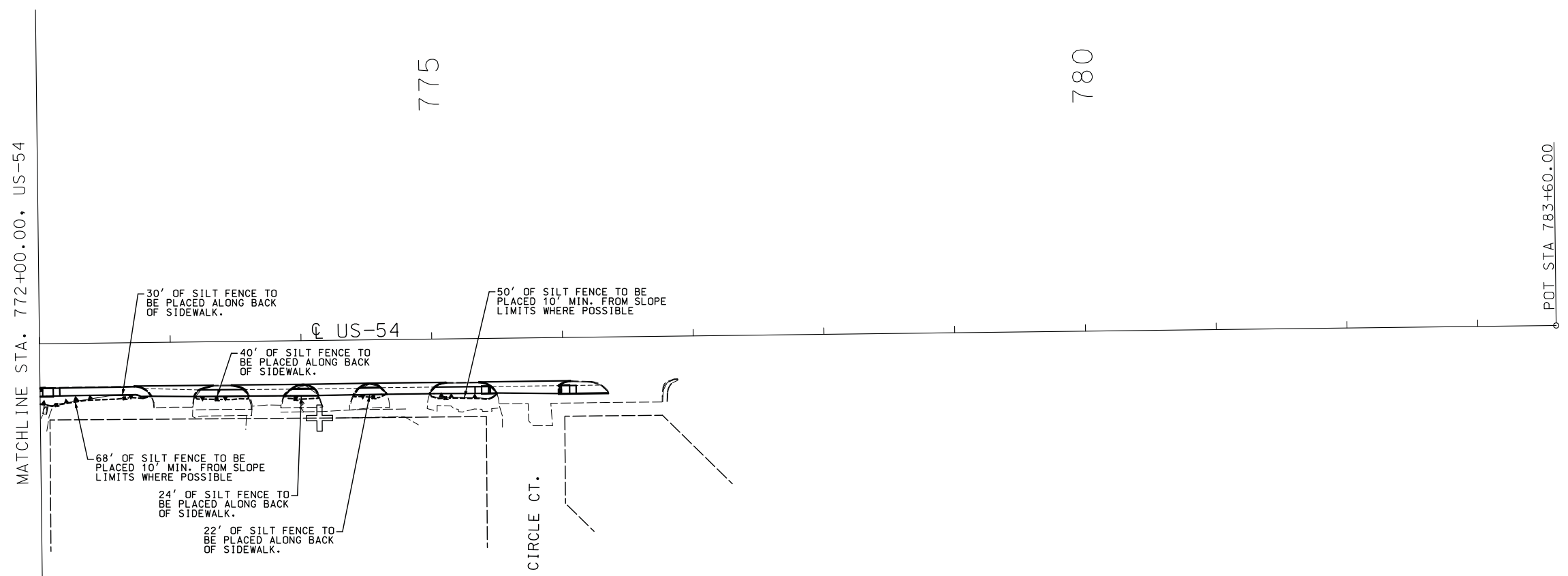


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STORMWATER POLLUTION PREVENTION PLAN

SHEET 7 OF 7

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
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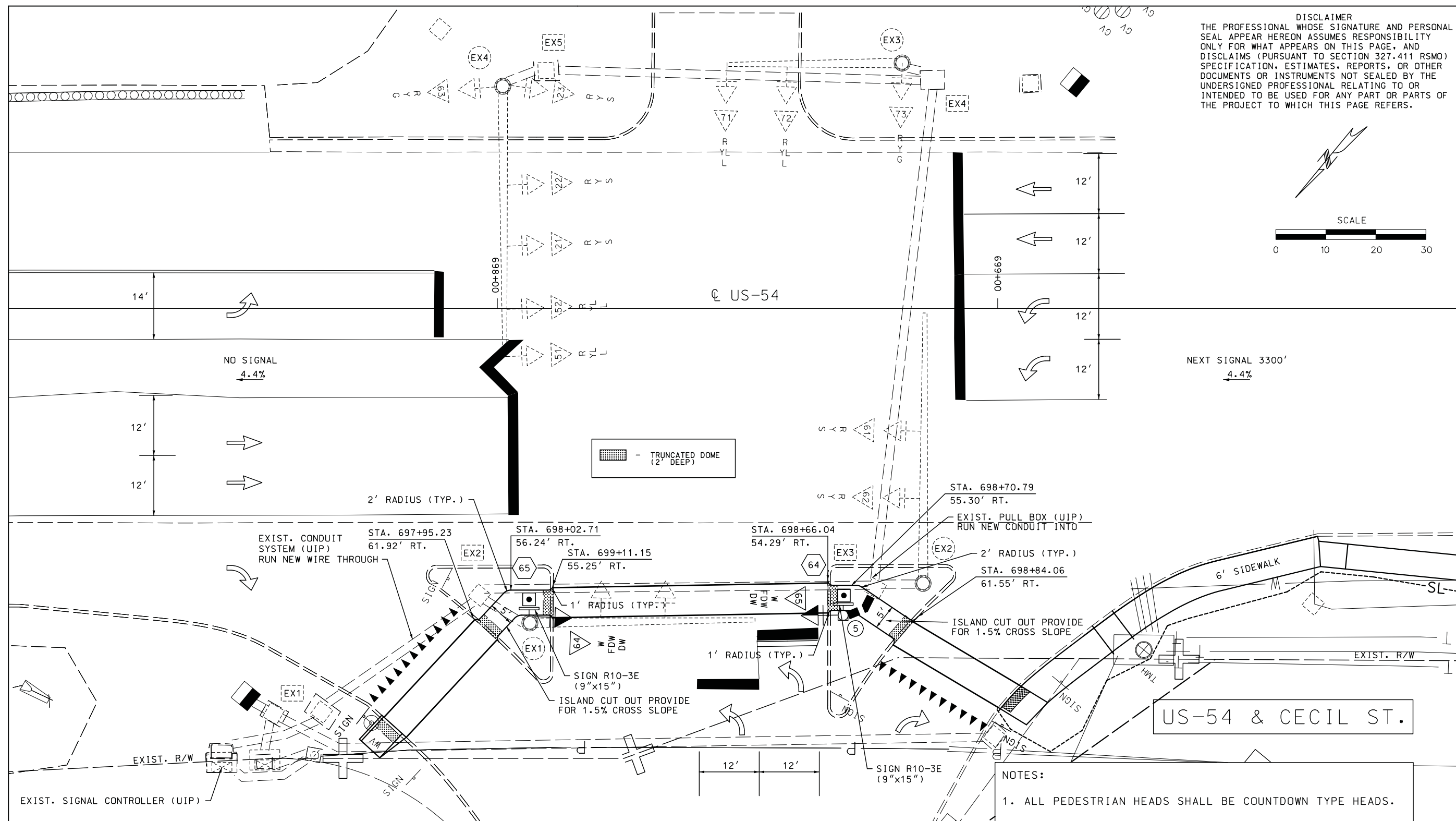
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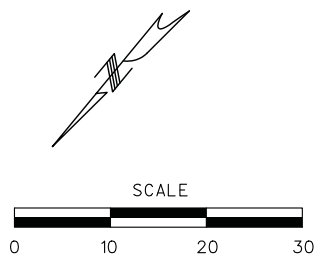
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REVISIONS		
NUMBER	BY	DATE

TRAFFIC SIGNALS
 SHEET 1 OF 12

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

TRAFFIC SIGNALS LEGEND			
	CONTROLLER (EXISTING)		VIDEO DETECTOR
	SIGNAL HEAD (OPT LIM CONV)		PUSH BUTTON DETECTOR
	SIGNAL HEAD (OPT LIM)		PREFORMED PULL BOX TYPE 1
	SIGNAL HEAD (PEDESTRIAN)		PREFORMED PULL BOX TYPE 2
	SIGNAL HEAD (STANDARD)		PULL BOX TYPE 1
	SIGNAL HEAD (EXISTING)		PULL BOX TYPE 2
	PULL BOX (EXISTING)		CONDUIT MEDIAN
	CONDUIT PUSHED		CONDUIT BRIDGE
	CONDUIT EXISTING		CONDUIT TRENCHED
	TYPE C MAST ARM A BASE		TYPE C MAST ARM B BASE
	SIGNAL POST WITH TYPE C BASE		

- NOTES:
- ALL PEDESTRIAN HEADS SHALL BE COUNTDOWN TYPE HEADS.
 - 1-7C-#16 WIRE TO BE USED FOR ONE PEDESTRIAN SIGNAL AND ONE PUSH BUTTON DETECTOR.
 - CONNECTION/HOOK UP OF WIRES AND CONFIGURATION OF PEDESTRIAN HEADS AND PUSH BUTTON DETECTORS IN EXISTING CONTROLLER INCLUDED IN UNIT PRICE FOR 1-7C-#16 WIRE.
 - COORDINATE CURB RAMP & LANDING CONSTRUCTION WITH SIGNAL PLANS. SEE CURB RAMP DETAILS SHEETS.
 - ALL PUSH BUTTONS REQUIRE SIGN R10-3E (9"X15") TO BE INSTALLED ABOVE THEM.
 - DRILLING EXIST. PULL BOX & CONNECTING CONDUIT IN PULL BOX SHALL BE INCLUDED IN UNIT PRICE FOR CONDUIT.

CECIL ST. 2.4%

NO SIGNAL

US-54 POSTED SPEED LIMIT = 45 MPH
 CECIL ST. SPEED LIMIT = 25 MPH

SIGN R10-3E (L) OR (R)
 INSTALLED ABOVE PUSH BUTTONS:
 65 & 64

037-SIGNAL-CECIL-110.dgn

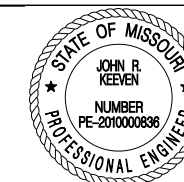


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NUMBER	BY	DATE

CONDUIT															CABLE											REMARKS										
FROM	TO	CENTER TO CENTER DISTANCE	LOOP DET.				TRENCH			PUSHED			MEDIAN			ON STRUCTURE		FROM	TO	CENTER TO CENTER DISTANCE	POWER				CONTROL				LUMINAIRE		(1) VIDEO CABLE	DETECTOR		FIBER		
			1"	2"	3"	4"	2"	3"	4"	2"	3"	4"	2"	3"	2"	3"	2c #16				5c #16	7c #16	3c #16	CONTROL	2c #12		1c #14 IN DUCT	2c #14 LEAD-IN CABLE	SINGLE-MODE	MULTI-MODE						
	(5)	7'			8'													(EX)	77'																* PEDESTRIAN HEAD 64	
																			(EX)	77'														* PUSH BUTTON 65		
																			(5)	154'														* PEDESTRIAN HEAD 65		
																			(5)	154'														* PUSH BUTTON 64		
SUBTOTALS					8'																															
TOTALS					8'																															

TRAFFIC SIGNALS
SHEET 3 OF 12

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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15480-03-00
SHEET 39 OF 122

039_SIGNAL_CECIL_D37-B.dgn

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* USE 1-7C#16 CABLE FOR BOTH THE PEDESTRIAN HEAD AND PUSH BUTTON

US-54 & CECIL ST
INTERSECTION

PHASE 1 TAP-9900 (563)

D-37B

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POWER SUPPLY													
LOCATION			POWER SUPPLY ASSEMBLY		CIRCUIT BREAKER TRIP RATING*				LIGHTING CONTROL * (ON POWER SUPPLY)		SERVICE POLE		
APPROACH	STATION	OFFSET	DRAWING 902.15	DRAWING	CONTROLLER		POWER SUPPLY DISCONNECT		120 VOLT CONTROL CABINET	MAIN BREAKER	CONTRACT FURNISH	UTILITY COMPANY	
					AUXILIARY BREAKER	CONT & SIGNAL LAMPS	MAIN BREAKER	SIGNALS LIGHTING					
USE EXISTING			Type		Amps	Amps	Amps	Amps		Amps	Cl.	Ft.	
			Type		15 Amps	Amps	Amps	Amps		Amps	Cl.	Ft.	

CONTROLLER ASSEMBLY AND AUXILIARY EQUIPMENT																											
LOCATION			SYSTEM MASTER (CLOSED LOOP)		ACTUATED		ON-OFF* SWITCH		COORDINATION INTERFACE *				TIME * CLOCK	NEMA CABINET TYPE *			170 CABINET TYPE *		170 SOFTWARE *								
APPROACH	STATION	OFFSET	NEMA		170		TYPE		12C/7C HARDWARE (1)		TIME BASE	CLOSED LOOP		FIBER	E			332		336S		BITRAN		WAPITI			
			USE EXISTING																								

DETECTOR SCHEDULE						
DETECTOR NUMBER	APPROACH	TYPE				VIDEO
		PUSH BUTTON (ADA APPROVED)	INDUCTION LOOP (2)			
			STANDARD	DELAY/EXTEND *		CALL UNIT *
64	NW QUAD EB PED	1				
65	NE QUAD WB PED	1				
TOTAL		2				

* ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.

- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWARE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.

TRAFFIC SIGNALS
SHEET 4 OF 12

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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NEMA DETECTOR ASSIGNMENT									
CARD POSITION									
CHANNEL	1	2	3	4	5	6	7	8	
	1								
2									

TOTAL NUMBER OF DETECTOR CARDS(2-CHANNEL) = (2)

NEMA LOAD SWITCH ASSIGNMENTS											
1	2	3	4	5	6	7	8	9	10	11	12
	Ø 2			Ø 5 LT	Ø 6	Ø 7 LT				PED Ø 6	

USE EXISTING

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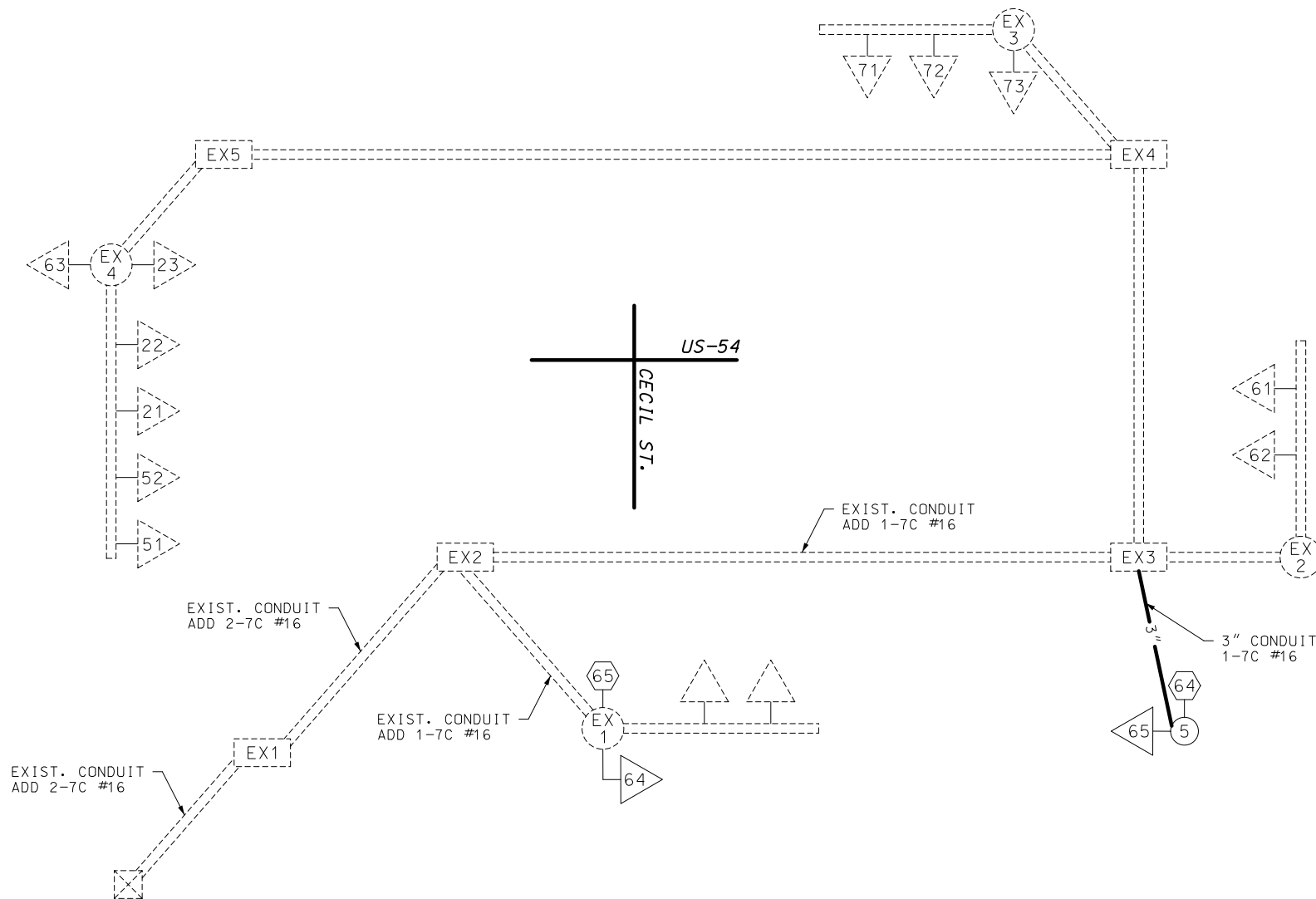
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US-54 & CECIL ST
INTERSECTION

D-37C

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WIRING DIAGRAM



EFFECTIVE 04-01-99

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LEGEND

- 3" CONDUIT CONTAINING CABLE WITH SIZE
- CABLE
- X-XC MULTI-CONDUCTOR CABLE #16 AWG (UNLESS OTHERWISE INDICATED)
- LIC #14 AWG DETECTOR LEAD-IN CABLE (2C TWISTED SHIELDED)
- PULL BOX WITH NUMBER
- POST WITH NUMBER
- POST WITH MAST ARM
- POST WITH MAST ARM AND LUMINAIRE
- DETECTOR WITH NUMBER
- SIGNAL HEAD WITH NUMBER
- SIGNAL CONTROLLER
- POWER SUPPLY
- LIGHTING CONTROLLER
- EXIST. VIDEO DETECTION CAMERA
- VIDEO DETECTION CAMERA

REVISIONS

NUMBER	BY	DATE

TRAFFIC SIGNALS
 SHEET 5 OF 12
 CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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 INTERSECTION

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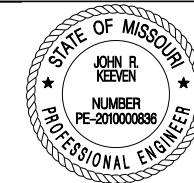
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SHEET 41 OF 122

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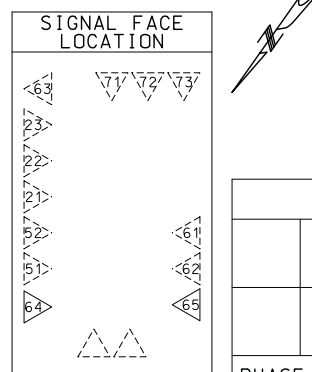
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TRAFFIC SIGNALS
 SHEET 6 OF 12

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
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NOTE:
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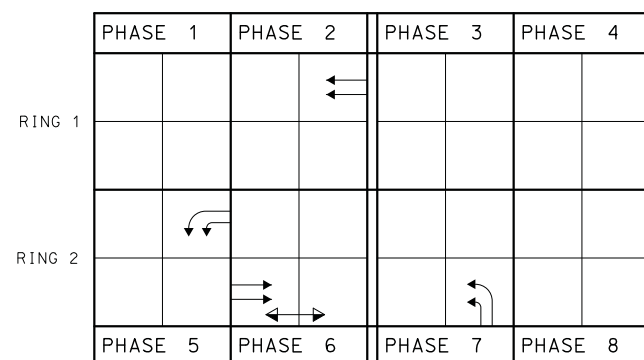


TRAFFIC PHASING AND COLOR SEQUENCE - ACTUATED CONTROLLERS

PHASE 1	PHASE 2	PHASE 3	PHASE 4	PHASE 5	PHASE 6	PHASE 7	PHASE 8	PED φ 2 OVERLAP	PED φ 4 OVERLAP	PED φ 6 OVERLAP	PED φ 8 OVERLAP				

FACE NUMBERS								FACE NUMBERS							

DETECTOR ASSIGNMENTS (PEDS. ONLY)								DETECTOR ASSIGNMENTS (PEDS. ONLY)							



RING STRUCTURE

IF CALLED, ALL NON-CONFLICTING PHASES SHALL COMBINE AND TIME CONCURRENTLY. TIME TO BE DETERMINED BY THE ENGINEER AT SIGNAL TURN ON AND SET IN THE CONTROLLER BY THE CONTRACTOR.

LEGEND

- ▶— ACTUATED VEHICULAR MOVEMENT
- - -▶- PARTIALLY RESTRICTED ACTUATED VEHICULAR MOVEMENT
- ▷— NON-ACTUATED VEHICULAR MOVEMENT
- - -▷- PARTIALLY RESTRICTED NON-ACTUATED VEHICULAR MOVEMENT
- ▶— ACTUATED PEDESTRIAN MOVEMENT
- ▷— NON-ACTUATED PEDESTRIAN MOVEMENT
- |— VEHICLE STOPPED
- FDW — FLASHING DON'T WALK
- R/W — RIGHT OF WAY INTERVAL
- DW — DON'T WALK
- YR+ — YELLOW RIGHT ARROW
- RT. — GREEN RIGHT ARROW
- WALK
- LY — YELLOW LEFT ARROW
- GREEN LEFT ARROW
- GREEN STRAIGHT AHEAD ARROW
- CIRCULAR GREEN
- CIRCULAR YELLOW
- CIRCULAR RED
- TRAFFIC PHASE
- ALL OTHERS
- AOR — FLASHING RED
- FR — FLASHING CIRCULAR YELLOW
- FYA — FLASHING YELLOW ARROW
- FYL — FLASHING YELLOW LEFT ARROW
- FYR — FLASHING YELLOW RIGHT ARROW

FLASHING OPERATION	
USE EXISTING	FR
USE EXISTING	FY
CONTROLLER TYPE	USE EXISTING

TRAFFIC SIGNAL CONTROL OPERATION	
INTERSECTION OF	
US-54 WITH CECIL ST	

US-54 & CECIL ST
 INTERSECTION

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D-38A

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JOHN R. KEEVEN
 PROFESSIONAL ENGINEER
 MO. P.E. LICENSE # 201000836

REVISIONS

NUMBER	BY	DATE

TRAFFIC SIGNALS
 SHEET 7 OF 12

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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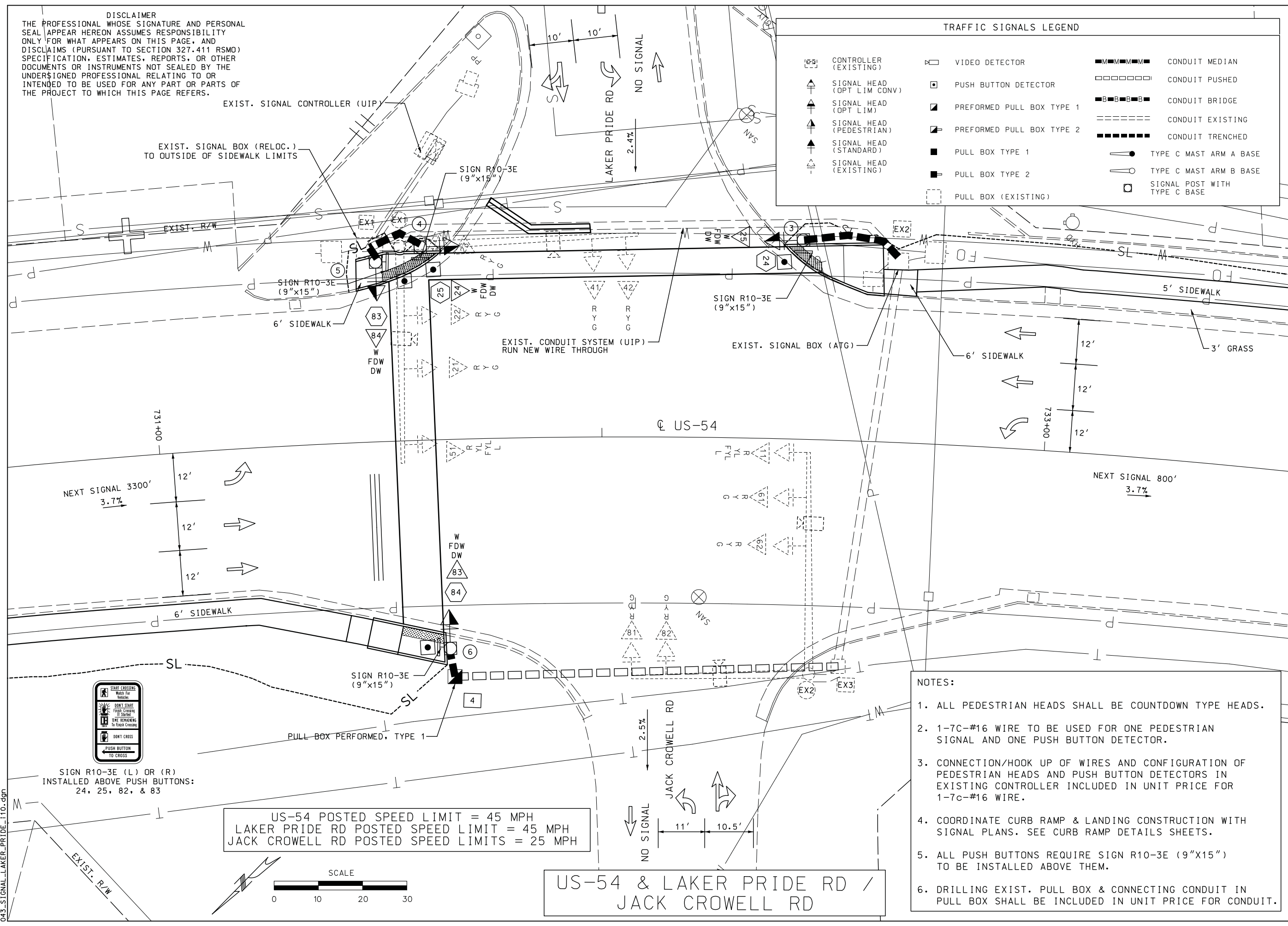
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JOB No: 15480-02-00
 15480-03-00

SHEET 43 OF 122

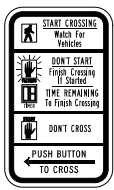
TRAFFIC SIGNALS LEGEND

	CONTROLLER (EXISTING)		VIDEO DETECTOR		CONDUIT MEDIAN
	SIGNAL HEAD (OPT LIM CONV)		PUSH BUTTON DETECTOR		CONDUIT PUSHED
	SIGNAL HEAD (PEDESTRIAN)		PREFORMED PULL BOX TYPE 1		CONDUIT BRIDGE
	SIGNAL HEAD (STANDARD)		PREFORMED PULL BOX TYPE 2		CONDUIT EXISTING
	SIGNAL HEAD (EXISTING)		PULL BOX TYPE 1		CONDUIT TRENCHED
	PULL BOX (EXISTING)		PULL BOX TYPE 2		TYPE C MAST ARM A BASE
			SIGNAL POST WITH TYPE C BASE		



- NOTES:
1. ALL PEDESTRIAN HEADS SHALL BE COUNTDOWN TYPE HEADS.
 2. 1-7C-#16 WIRE TO BE USED FOR ONE PEDESTRIAN SIGNAL AND ONE PUSH BUTTON DETECTOR.
 3. CONNECTION/HOOK UP OF WIRES AND CONFIGURATION OF PEDESTRIAN HEADS AND PUSH BUTTON DETECTORS IN EXISTING CONTROLLER INCLUDED IN UNIT PRICE FOR 1-7C-#16 WIRE.
 4. COORDINATE CURB RAMP & LANDING CONSTRUCTION WITH SIGNAL PLANS. SEE CURB RAMP DETAILS SHEETS.
 5. ALL PUSH BUTTONS REQUIRE SIGN R10-3E (9"x15") TO BE INSTALLED ABOVE THEM.
 6. DRILLING EXIST. PULL BOX & CONNECTING CONDUIT IN PULL BOX SHALL BE INCLUDED IN UNIT PRICE FOR CONDUIT.

US-54 POSTED SPEED LIMIT = 45 MPH
 LAKER PRIDE RD POSTED SPEED LIMIT = 45 MPH
 JACK CROWELL RD POSTED SPEED LIMITS = 25 MPH



SIGN R10-3E (L) OR (R)
 INSTALLED ABOVE PUSH BUTTONS:
 24, 25, 82, & 83

043-SIGNAL_LAKER_PRIDE_110.dgn

EFFECTIVE: 07/01/2013



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REVISIONS

Table with columns: NUMBER, BY, DATE. Contains one revision entry.

TRAFFIC SIGNALS
SHEET 8 OF 12

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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Main table with columns: LOCATION (Approach, Station, Offset), BASE (A, F, C), PULL BOX (Preformed, Concrete), POST (Type C & CL, Type B & BL), and LUMINAIRE BRACKET ARM. Includes subtotal and total rows.

SIGNAL HEADS table with columns: SPAN NUMBER, POST NUMBER, FACE NUMBER, LUMINAIRE, INDICATIONS (12" LENS CONVENTIONAL, 12" LENS OPTICAL LIMITING), VISORS, LOUVERS, BACKPLATE, BRACKET, ONE-FACE SECTION, TWO-FACE SECTIONS. Includes subtotal and total rows.

SIGNS table with columns: QUANTITY, MUTCD SIGN NUMBER, SIZE (INCHES), AREA (SQ. FT.), TOTAL AREA EACH SIGN (SQ. FT.), PAY TOTAL (EACH). Includes subtotal and total rows.

REMARKS
ALL LENGTHS AND SPACINGS ARE IN FEET UNLESS OTHERWISE INDICATED.
* ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.
** SEE STANDARD PLANS 902.10 AND 902.30 FOR CONCRETE REQUIREMENTS ON BASES.
*** USE D3-16 FOR ONE LINE. USE D3-18 FOR TWO LINE.
SIGNAL STRUCTURES WHICH WILL EXCEED THE DIMENSION LIMITS SHOWN ON STANDARD PLAN SHEETS IN SECTION 902 AND ANY OTHER INSTALLATION WHERE THE DETAILS OF CONSTRUCTION ARE NOT FURNISHED IN THE CONTRACT PLANS, SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER IN ACCORDANCE WITH THE 2001 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION, AND LATEST INTERIMS. THE STRUCTURE SHALL BE DESIGNED AS IMPORTANCE CATEGORY I FOR FATIGUE WITH A 50-YEAR DESIGN LIFE. THE CONTRACTOR SHALL SUBMIT A SET OF SHOP DETAIL DRAWINGS INCLUDING WELD PROCEDURE SPECIFICATIONS AND DESIGN COMPUTATIONS FOR MODOT RECORDS AND REFERENCE. THE SUBMITTED DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND SEALED IN ACCORDANCE WITH THE LAWS RELATING TO ARCHITECTS AND PROFESSIONAL ENGINEERS (CHAPTER 327, RSMO.), AND SHALL INCLUDE A TITLE BLOCK OR SUMMARY SHEET WHICH LISTS AND CERTIFIES THAT THE PRODUCT MEETS ALL OF THE SPECIFIED DESIGN CRITERIA.

PHASE 1 TAP-9900 (563)

US-54 & LAKER PRIDE RD / JACK CROWELL RD

INTERSECTION

LEGEND
T - TOP MOUNT
S - SIDE MOUNT
C - SPANWIRE MOUNT
B - MAST ARM MOUNT

044_SIGNAL_LAKER_PRIDE.D37-A.dgn

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NUMBER	BY	DATE

TRAFFIC SIGNALS
SHEET 9 OF 12

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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JOB No: 15480-02-00
15480-03-00

SHEET 45 OF 122

CONDUIT

CABLE

FROM	TO	CENTER TO CENTER DISTANCE	LOOP DET.				TRENCH			PUSHED			MEDIAN			ON STRUCTURE		REMARKS	FROM	TO	CENTER TO CENTER DISTANCE	POWER				CONTROL				LUMINAIRE	(1) VIDEO CABLE	DETECTOR		FIBER		REMARKS	
			1"	2"	3"	4"	2"	3"	4"	2"	3"	4"	2"	3"	2c #16	5c #16	7c #16					3c #16	2c #12	1c #14 IN DUCT	2c #14 LEAD-IN CABLE	SINGLE-MODE	MULTI-MODE										
[EX1]	(4)	15'			16'														[X]	(4)	42'															* PEDESTRIAN HEAD 24	
[EX1]	(5)	4'			5'														[X]	(4)	42'															* PUSH BUTTON 25	
[EX2]	(3)	25'			26'														[X]	(5)	31'															* PEDESTRIAN HEAD 84	
[EX3]	(4)	87'											85'						[X]	(5)	31'															* PUSH BUTTON 83	
[4]	(6)	7'			8'														[X]	(3)	176'															* PEDESTRIAN HEAD 25	
																			[X]	(3)	176'															* PUSH BUTTON 24	
																			[X]	(6)	337'															* PEDESTRIAN HEAD 83	
																			[X]	(6)	337'															* PUSH BUTTON 84	
SUBTOTALS					55'								85'						SUBTOTALS																		706'
TOTALS					55'								85'						TOTALS																		710'

045_SIGNAL_LAKER_PRIDE_D37-B.dgn

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* USE 1-7C#16 CABLE FOR BOTH THE PEDESTRIAN HEAD AND PUSH BUTTON

US-54 & LAKER PRIDE RD / JACK CROWELL RD
INTERSECTION

PHASE 1 TAP-9900 (563)

D-37B

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REVISIONS		
NUMBER	BY	DATE

POWER SUPPLY												
LOCATION			POWER SUPPLY ASSEMBLY		CIRCUIT BREAKER TRIP RATING*				LIGHTING CONTROL * (ON POWER SUPPLY)		SERVICE POLE	
APPROACH	STATION	OFFSET	DRAWING 902.15	DRAWING	CONTROLLER		POWER SUPPLY DISCONNECT		120 VOLT CONTROL CABINET	MAIN BREAKER	CONTRACT FURNISH	UTILITY COMPANY
					AUXILIARY BREAKER	CONT & SIGNAL LAMPS	MAIN BREAKER	SIGNALS LIGHTING				
USE EXISTING			Type			Amps	Amps	Amps	Amps		Amps	Cl. Ft.
			Type			15 Amps	Amps	Amps	Amps		Amps	Cl. Ft.

CONTROLLER ASSEMBLY AND AUXILIARY EQUIPMENT																					
LOCATION			SYSTEM MASTER (CLOSED LOOP)		ACTUATED		ON-OFF SWITCH		COORDINATION INTERFACE *			TIME * CLOCK	NEMA CABINET TYPE *			170 CABINET TYPE *		170 SOFTWARE *			
APPROACH	STATION	OFFSET	NEMA	170	NEMA	170	TYPE		12C/7C HARDWARE (1)		TIME BASE		CLOSED LOOP		FIBER	E EV DOUBLE			332 336S		BITRAN WAPITI
							I	II	MASTER	LOCAL		NEMA	170								
USE EXISTING																					

DETECTOR SCHEDULE						
DETECTOR NUMBER	APPROACH	TYPE				VIDEO
		PUSH BUTTON (ADA APPROVED)	INDUCTION LOOP (2)			
			STANDARD	DELAY/EXTEND *	CALL UNIT *	
24	SW QUAD EB PED	1				
25	SE QUAD WB PED	1				
83	SE QUAD NB PED	1				
84	NE QUAD SB PED	1				
TOTAL		4				

* ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.

- (1) ModDT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWARE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.

TRAFFIC SIGNALS
SHEET 10 OF 12

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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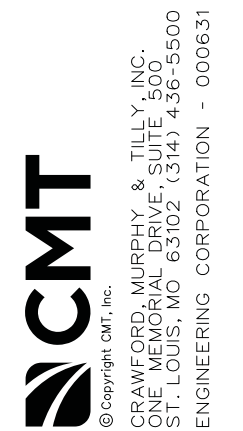
NEMA DETECTOR ASSIGNMENT									
CARD POSITION									
	1	2	3	4	5	6	7	8	
CHANNEL	1								
	2								

TOTAL NUMBER OF DETECTOR CARDS(2-CHANNEL) = (2)

NEMA LOAD SWITCH ASSIGNMENTS											
1	2	3	4	5	6	7	8	9	10	11	12
FYA Ø 1 OLA	Ø 2		Ø 4	FYA Ø 5 OLB	Ø 6		Ø 8	PED Ø 2 LT		Ø 5 LT	PED Ø 8

OLA = Ø 1 FLA
OLB = Ø 5 FLA

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PHASE 1 TAP-9900 (563)

US-54 & LAKER PRIDE RD / JACK CROWELL RD
INTERSECTION

D-37C

USE EXISTING

WIRING DIAGRAM

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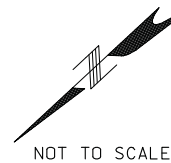
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TRAFFIC SIGNALS
 SHEET 11 OF 12
 CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

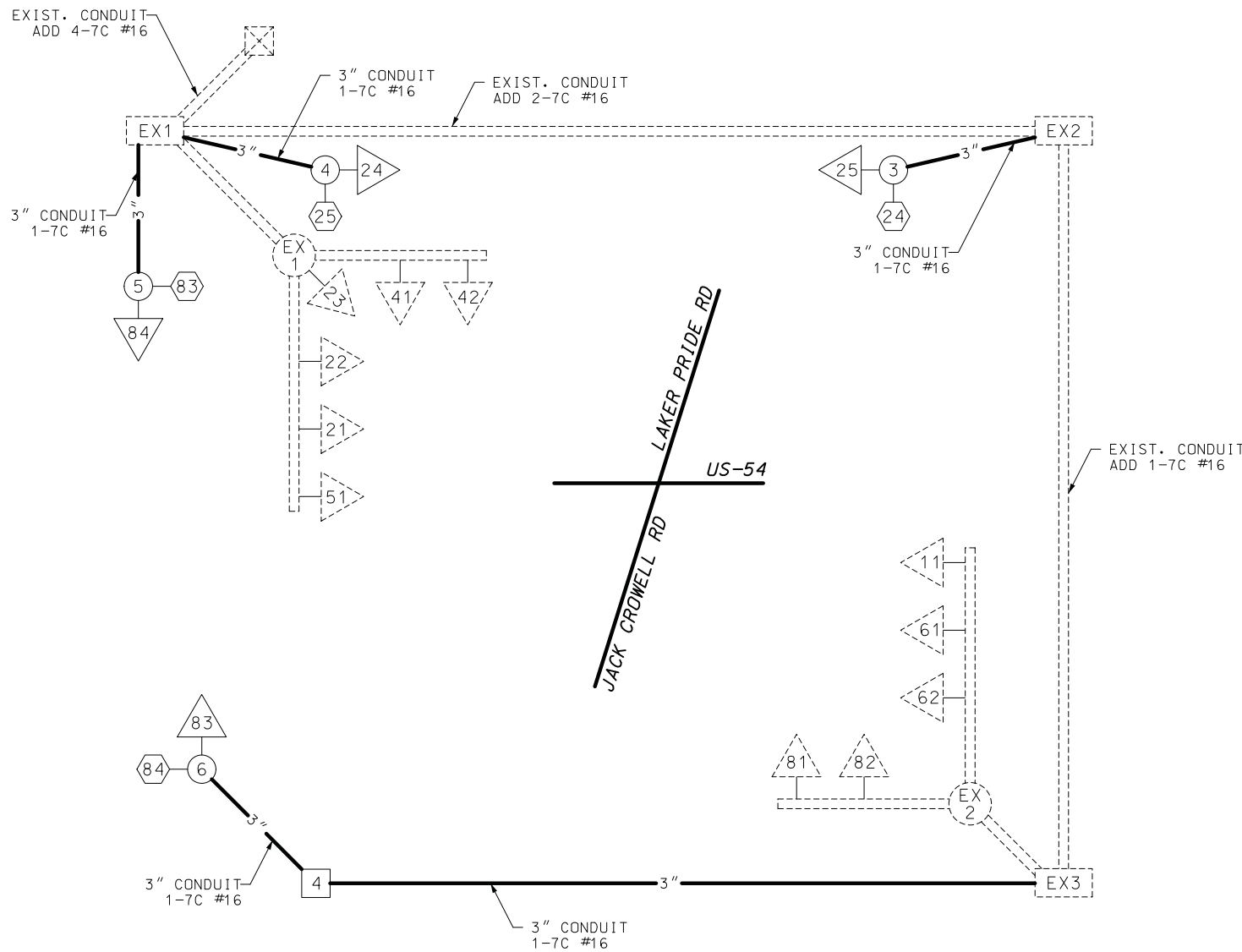
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DATE: 5/19/2016
 JOB No: 15480-02-00
 15480-03-00

SHEET 47 OF 122



NOT TO SCALE



LEGEND

- 3" CONDUIT CONTAINING CABLE WITH SIZE
- CABLE
- X-XC MULTI-CONDUCTOR CABLE #16 AWG (UNLESS OTHERWISE INDICATED)
- LIC #14 AWG DETECTOR LEAD-IN CABLE (2C TWISTED SHIELDED)
- [X] PULL BOX WITH NUMBER
- (X) POST WITH NUMBER
- (X) POST WITH MAST ARM
- (X) POST WITH MAST ARM AND LUMINAIRE
- (X) DETECTOR WITH NUMBER
- [X] SIGNAL HEAD WITH NUMBER
- [X] SIGNAL CONTROLLER
- [X] POWER SUPPLY
- [X] LIGHTING CONTROLLER
- [X] EXIST. VIDEO DETECTION CAMERA
- [X] VIDEO DETECTION CAMERA

US-54 & LAKER PRIDE RD / JACK CROWELL RD
 INTERSECTION

D-37D

047_SIGNAL_LAKER_PRIDE.D37-D.dgn

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048-SIGNAL_LAKER_PRIDE_D38-A.dgn

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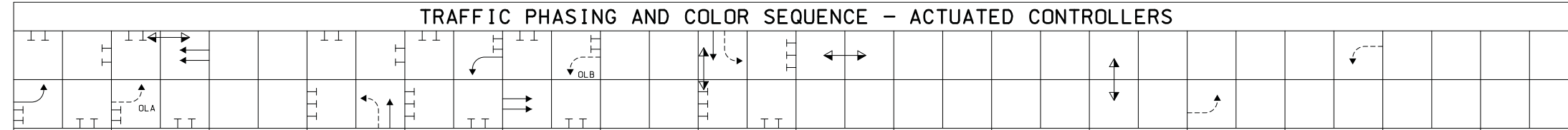
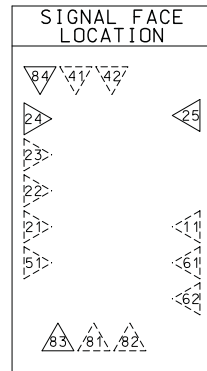
EXISTING SIGNAL PHASING PROVIDED FOR INFORMATION ONLY, CONTRACTOR SHALL VERIFY PHASING & ADJUST AS NECESSARY FOR PEDESTRIAN MOVEMENTS.

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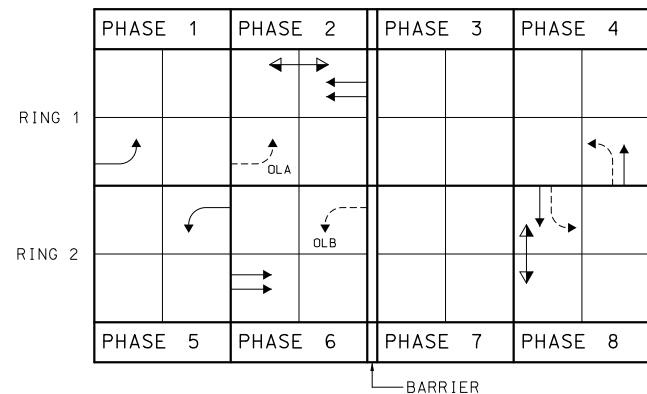


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EFFECTIVE 09/08/2009



FACE NUMBERS		PHASE 1	PHASE 2	PHASE 3	PHASE 4	PHASE 5	PHASE 6	PHASE 7	PHASE 8	PED + 2 OVERLAP	PED + 4 OVERLAP	PED + 6 OVERLAP	PED + 8 OVERLAP	OLA OVERLAP Ø 1	OLB OVERLAP Ø 5	FACE NUMBERS	
FACE NUMBERS		11	21 22 11		41 42	51	61 62 51		81 82	24 25			83 84	11	51	FACE NUMBERS	
SIGNAL INDICATIONS		R Y FYA G	R YL Y G	R R Y G	R Y Y G	R YL Y G	R R Y G	R R Y G	R Y Y G	DW FDW W	DW Y Y W	DW OVERLAP W	DW FDW W	R YL FYL	R YL FYL	SIGNAL INDICATIONS	
DETECTOR ASSIGNMENTS		11	21 22		41 42	51	61 62		81 82				83 84			DETECTOR ASSIGNMENTS	
DETECTOR ASSIGNMENTS (PEDS. ONLY)										24 25			83 84			DETECTOR ASSIGNMENTS (PEDS. ONLY)	
OVERLAP PARENT PHASES										Ø 2 Ø 6	Ø 2 Ø 6			Ø 2	Ø 6	OVERLAP PARENT PHASES	



RING STRUCTURE

IF CALLED, ALL NON-CONFLICTING PHASES SHALL COMBINE AND TIME CONCURRENTLY. TIME TO BE DETERMINED BY THE ENGINEER AT SIGNAL TURN ON AND SET IN THE CONTROLLER BY THE CONTRACTOR.

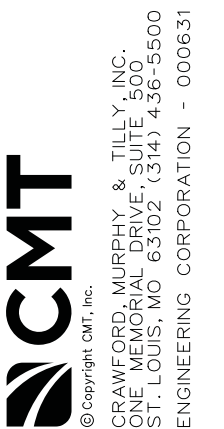
LEGEND	
→	ACTUATED VEHICULAR MOVEMENT
---→	PARTIALLY RESTRICTED ACTUATED VEHICULAR MOVEMENT
→	NON-ACTUATED VEHICULAR MOVEMENT
---→	PARTIALLY RESTRICTED NON-ACTUATED VEHICULAR MOVEMENT
→	ACTUATED PEDESTRIAN MOVEMENT
→	NON-ACTUATED PEDESTRIAN MOVEMENT
—	VEHICLE STOPPED
FDW	FLASHING DON'T WALK
R/W	RIGHT OF WAY INTERVAL
DW	DON'T WALK
YR+	YELLOW RIGHT ARROW
RT.	GREEN RIGHT ARROW
W	WALK
LY	YELLOW LEFT ARROW
YL	GREEN LEFT ARROW
LS	GREEN STRAIGHT AHEAD ARROW
CG	CIRCULAR GREEN
CR	CIRCULAR YELLOW
CR	CIRCULAR RED
Ø	TRAFFIC PHASE
AO	ALL OTHERS
FR	FLASHING RED
FY	FLASHING CIRCULAR YELLOW
FYA	FLASHING YELLOW ARROW
FYL	FLASHING YELLOW LEFT ARROW
FYR	FLASHING YELLOW RIGHT ARROW

FLASHING OPERATION	
USE EXISTING	FR
USE EXISTING	FY
CONTROLLER TYPE	USE EXISTING

TRAFFIC SIGNAL CONTROL OPERATION	
INTERSECTION OF	
US-54 WITH LAKE PRIDE RD / JACK CROWELL RD	

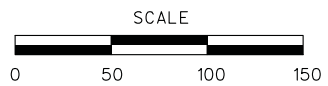
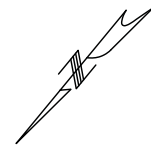
US-54 & LAKER PRIDE RD / JACK CROWELL RD
 INTERSECTION

TRAFFIC SIGNALS
 SHEET 12 OF 12
 CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



DATE: 5/19/2016
 JOB No: 15480-02-00
 15480-03-00

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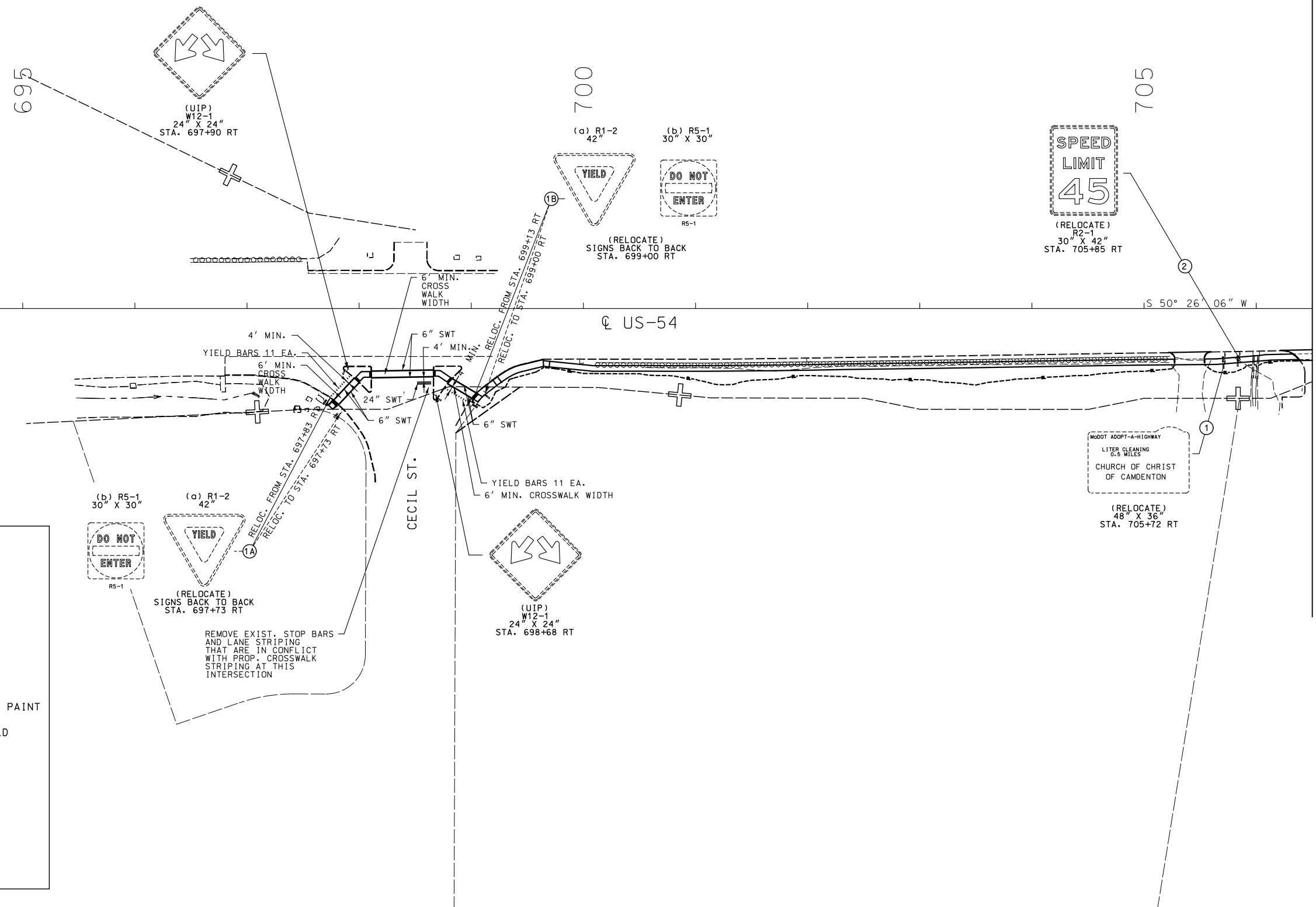
REVISIONS		
NUMBER	BY	DATE

SIGNING & PAVEMENT MARKING
 SHEET 1 OF 9
 CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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DATE: 5/19/2016
 JOB No: 15480-02-00
 15480-03-00

SHEET 49 OF 122



STRIPING KEY

S - SOLID
 I - INTERMITTENT
 D - DOUBLE
 W - WHITE
 Y - YELLOW
 T - TAPE
 P - PAINT
 I.E., 6" IWP = 6" INTERMITTENT WHITE PAINT

PAINT TO BE WET REFLECTIVE HIGH BUILD ACRYLIC WATERBORNE MARKING PAINT

TAPE TO BE TYPE 2 PREFORMED MARKING TAPE (GROOVED)

↖ LEFT TURN ARROW
 ↗ STRAIGHT/LEFT ARROW
 YIELD BAR
 ONLY WORD MARKING

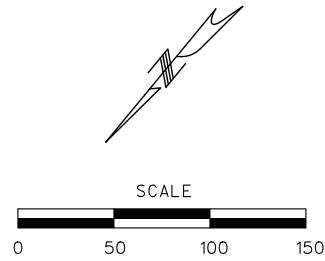
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MATCHLINE STA. 706+50.00, US-54

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REVISIONS

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SHEET 2 OF 9

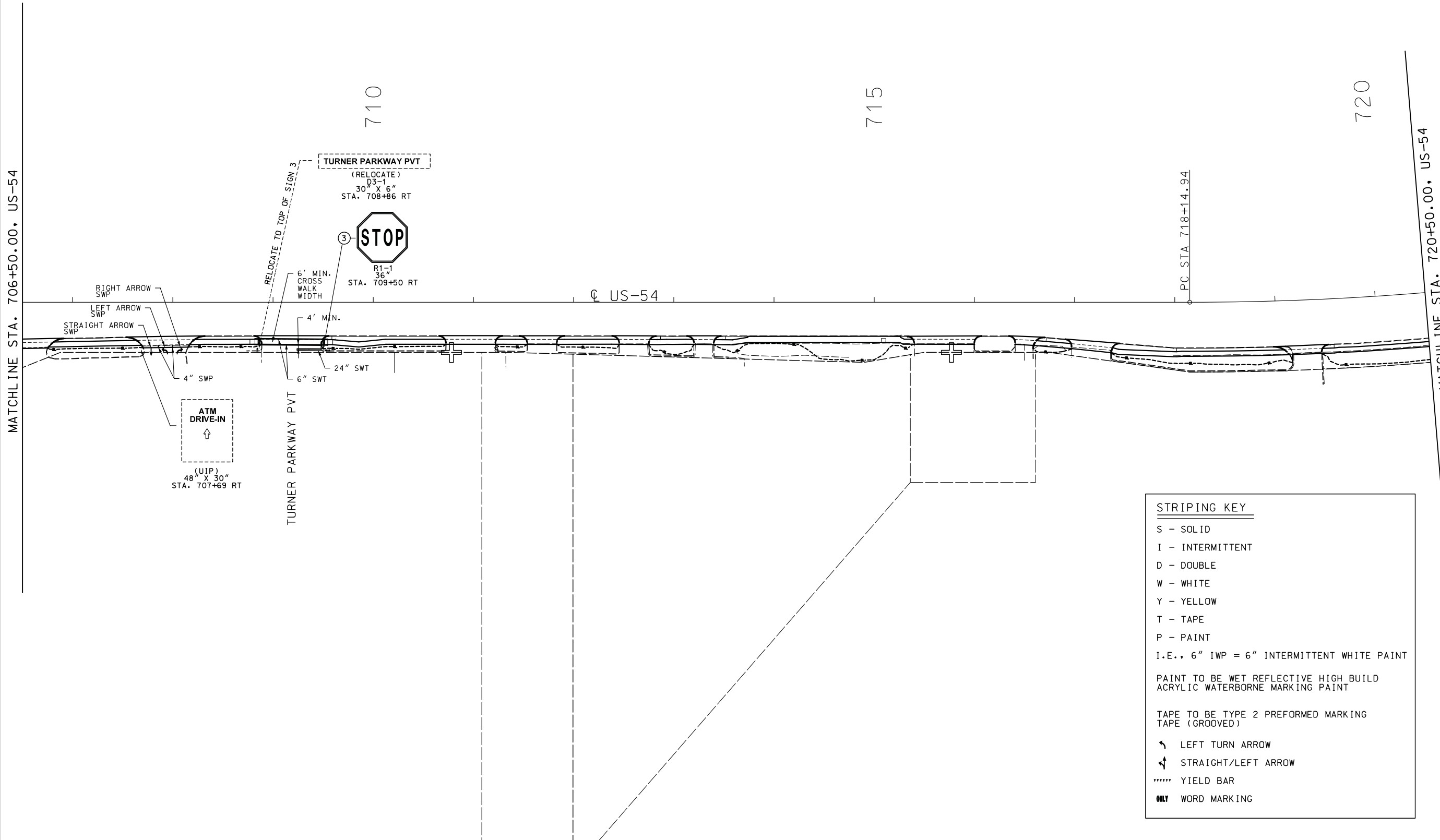
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SHEET 50 OF 122



STRIPING KEY

S - SOLID
 I - INTERMITTENT
 D - DOUBLE
 W - WHITE
 Y - YELLOW
 T - TAPE
 P - PAINT
 I.E., 6" IWP = 6" INTERMITTENT WHITE PAINT

PAINT TO BE WET REFLECTIVE HIGH BUILD ACRYLIC WATERBORNE MARKING PAINT

TAPE TO BE TYPE 2 PREFORMED MARKING TAPE (GROOVED)

↙ LEFT TURN ARROW
 ↕ STRAIGHT/LEFT ARROW
 ⋯ YIELD BAR
 ONLY WORD MARKING

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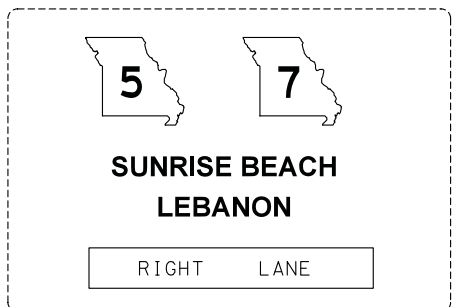
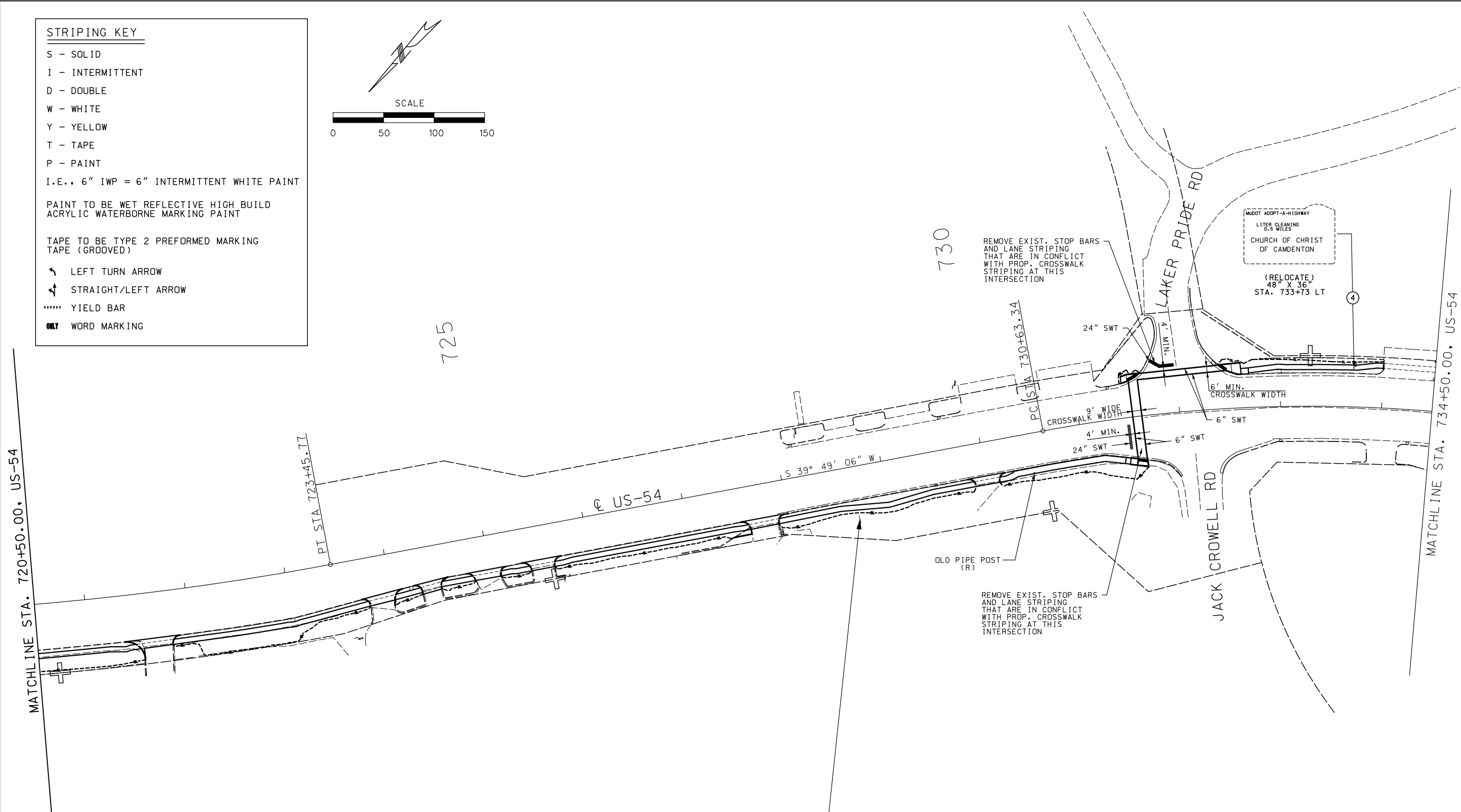
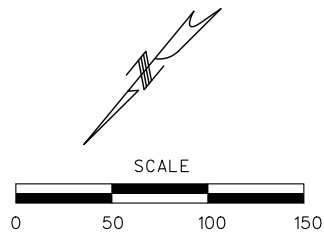
STRIPING KEY

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- I.E., 6" IWP = 6" INTERMITTENT WHITE PAINT

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TAPE TO BE TYPE 2 PREFORMED MARKING TAPE (GROOVED)

- ↙ LEFT TURN ARROW
- ↘ STRAIGHT/LEFT ARROW
- ⋯ YIELD BAR
- ONLY WORD MARKING



(UIP)
138" X 96"
STA. 728+69 RT

NOTES:

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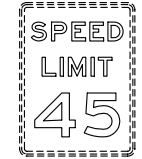
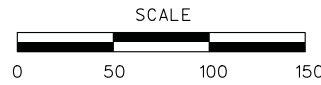
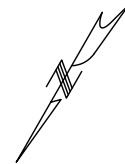
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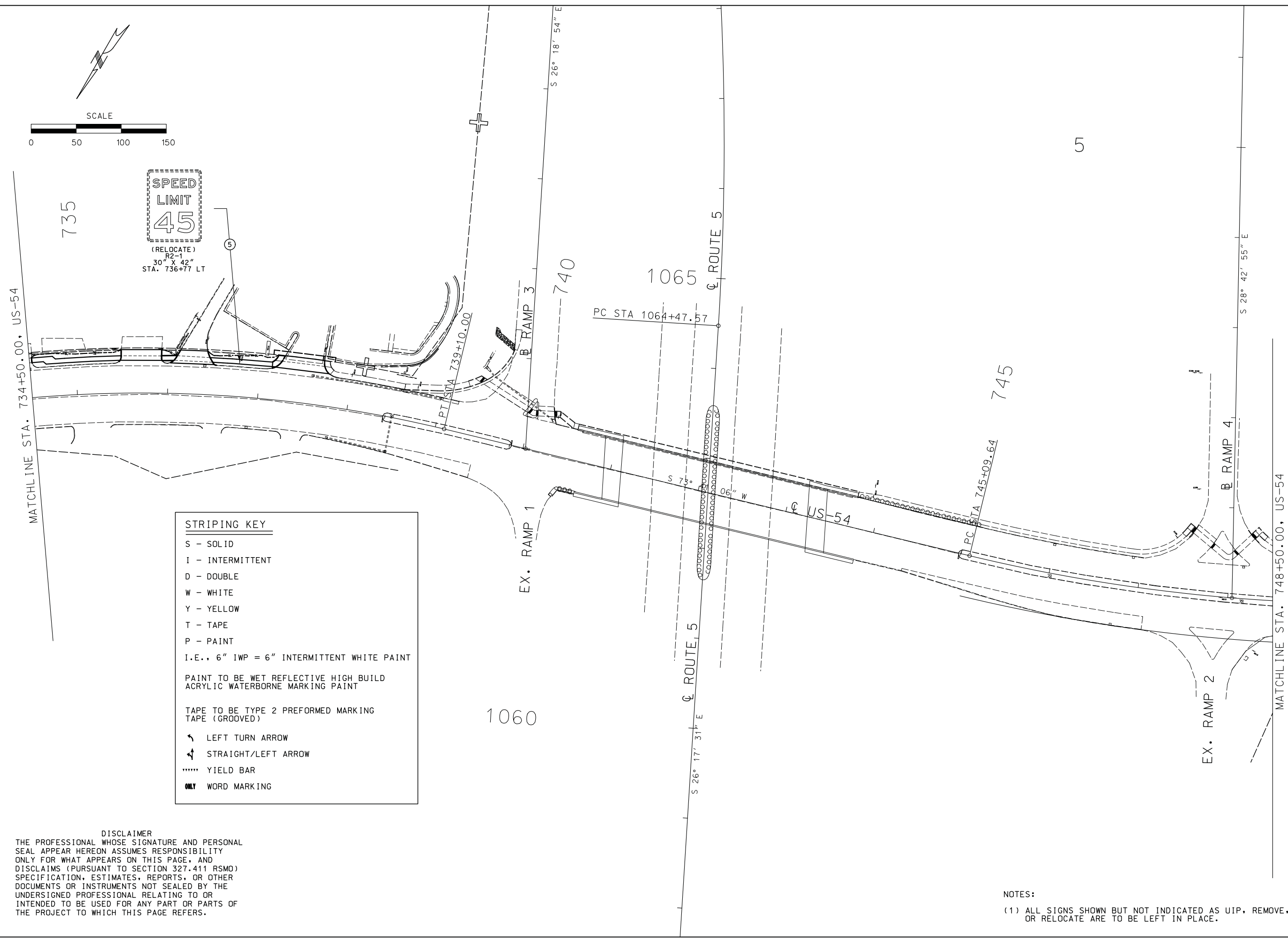
(RELOCATE)
R2-1
30" X 42"
STA. 736+77 LT

STRIPING KEY

- S - SOLID
- I - INTERMITTENT
- D - DOUBLE
- W - WHITE
- Y - YELLOW
- T - TAPE
- P - PAINT
- I.E.. 6" IWP = 6" INTERMITTENT WHITE PAINT
- PAINT TO BE WET REFLECTIVE HIGH BUILD ACRYLIC WATERBORNE MARKING PAINT
- TAPE TO BE TYPE 2 PREFORMED MARKING TAPE (GROOVED)
- ↖ LEFT TURN ARROW
- ↙ STRAIGHT/LEFT ARROW
- YIELD BAR
- WORD MARKING

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STRIPING KEY

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- I - INTERMITTENT
- D - DOUBLE
- W - WHITE
- Y - YELLOW
- T - TAPE
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I.E., 6" IWP = 6" INTERMITTENT WHITE PAINT

PAINT TO BE WET REFLECTIVE HIGH BUILD ACRYLIC WATERBORNE MARKING PAINT

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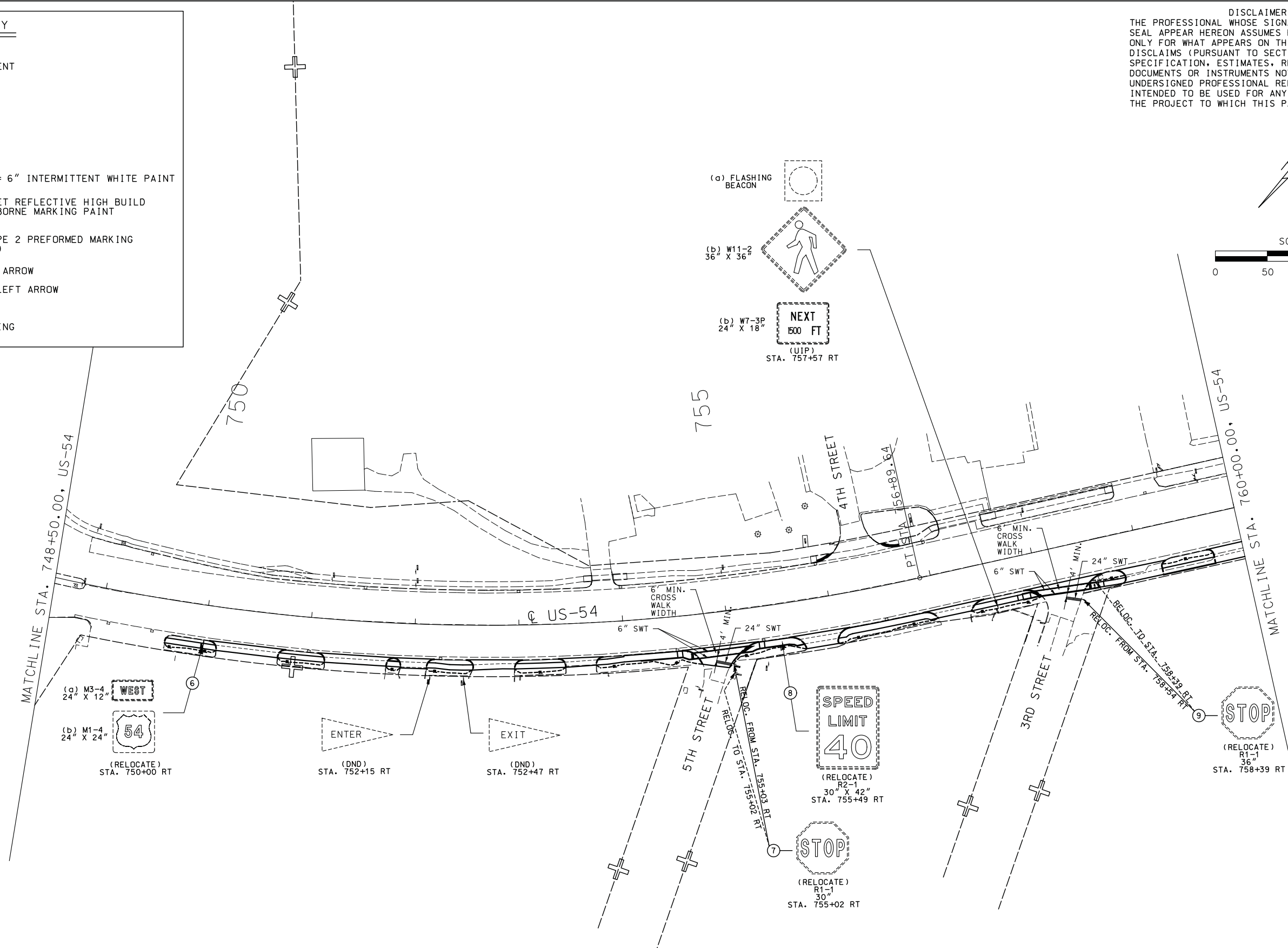
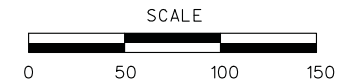


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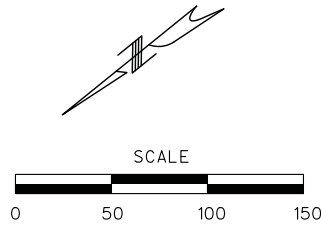
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 I - INTERMITTENT
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 SHEET 6 OF 9

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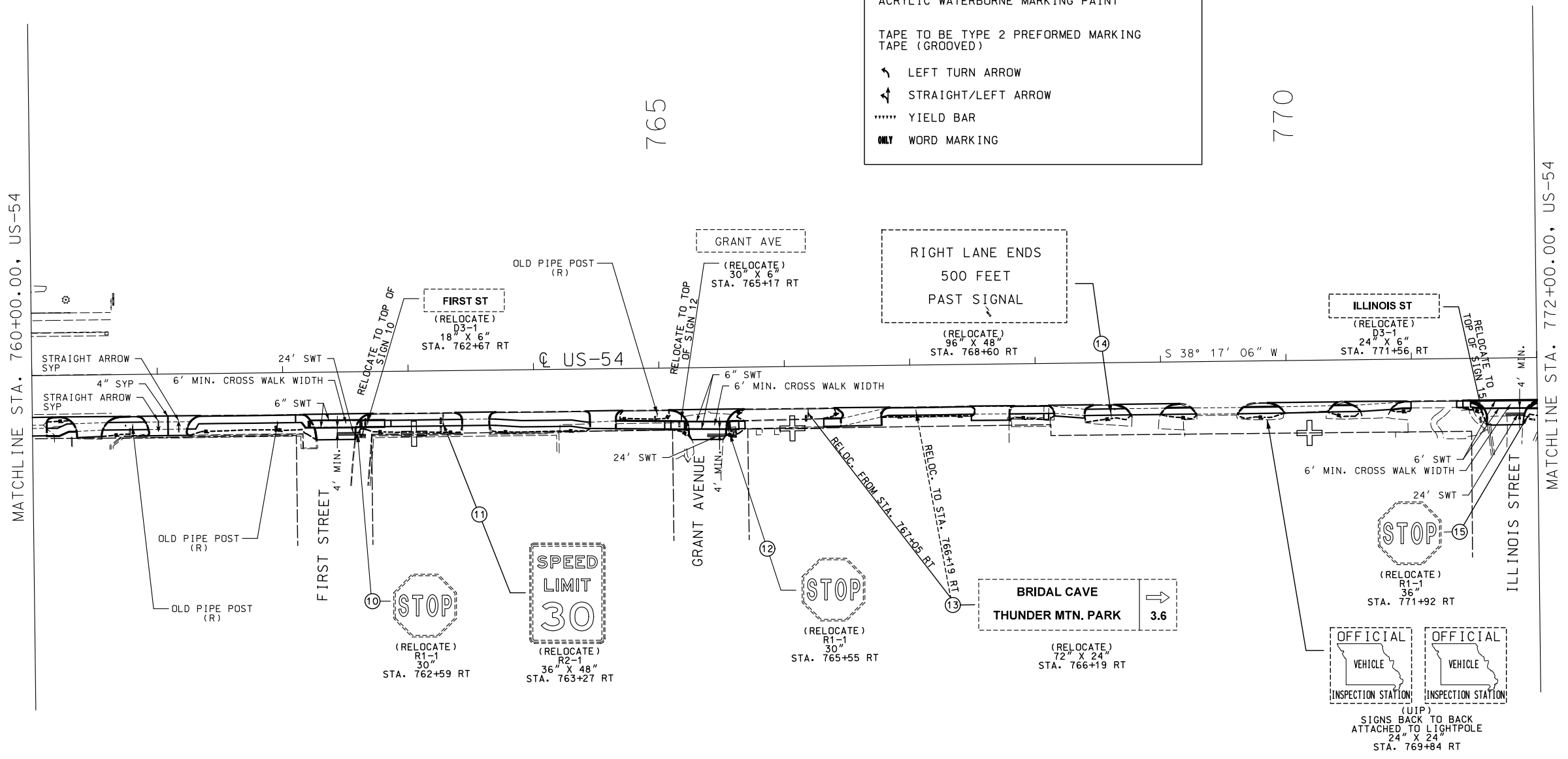
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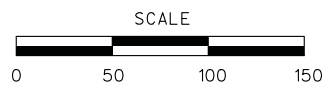
SHEET 54 OF 122

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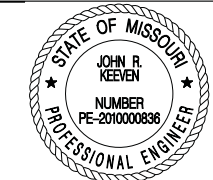


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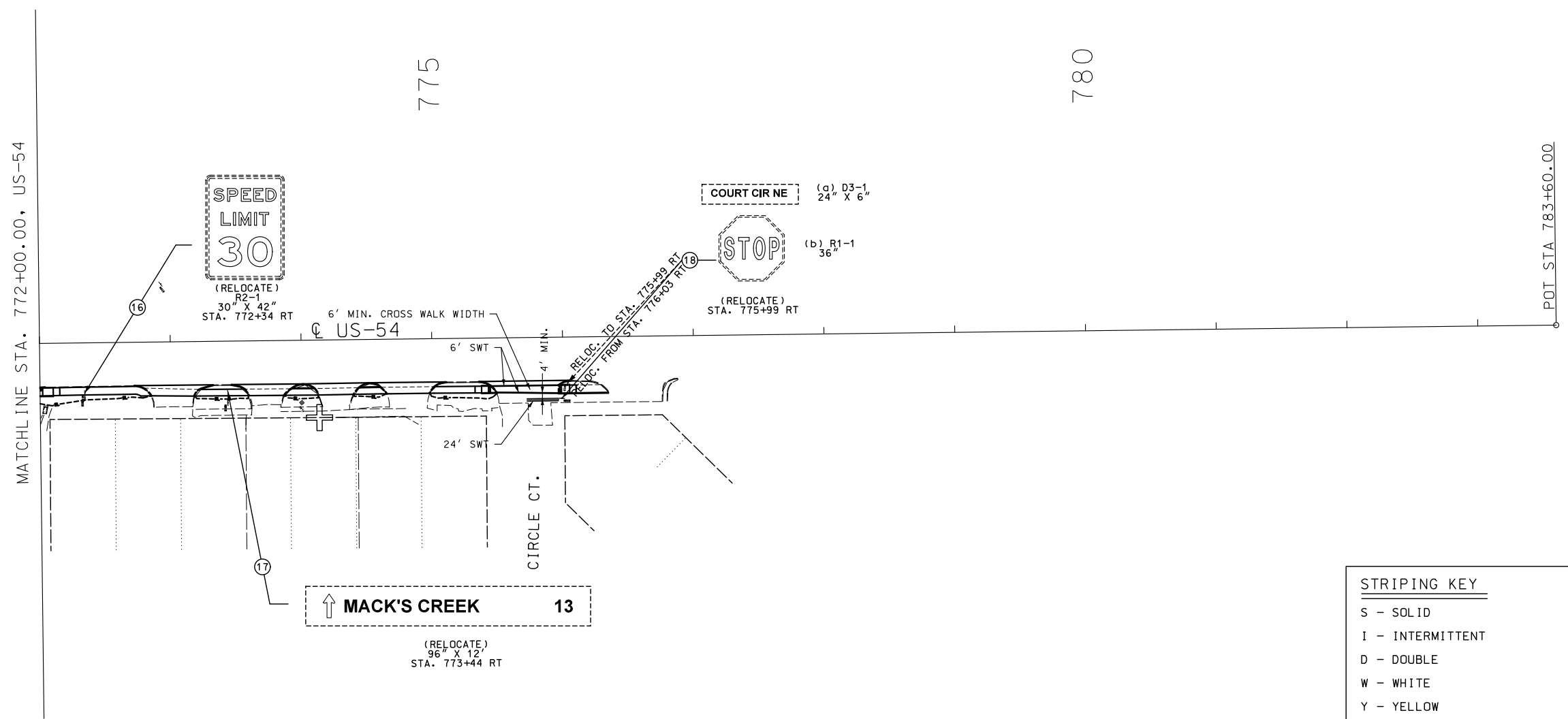
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	LEFT TURN ARROW
	STRAIGHT/LEFT ARROW
	YIELD BAR
	WORD MARKING

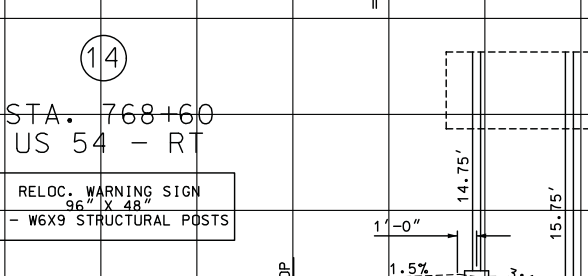
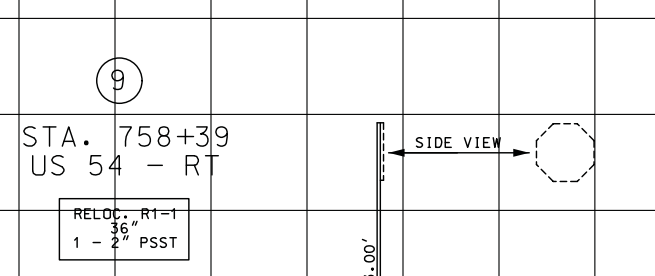
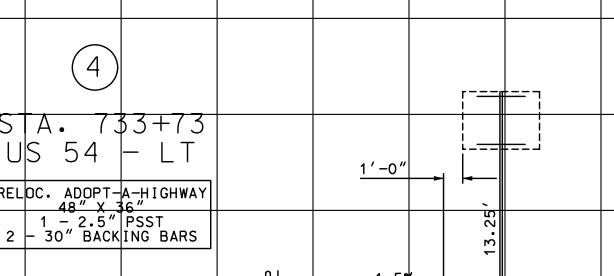
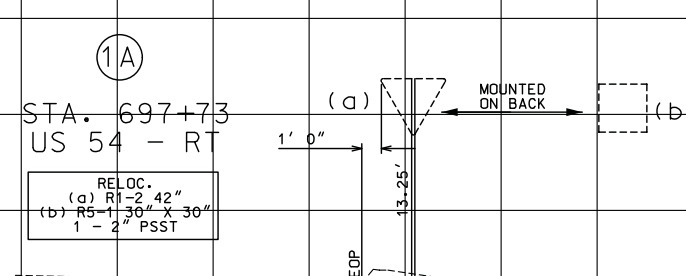
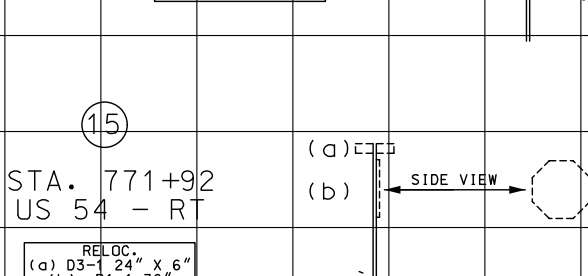
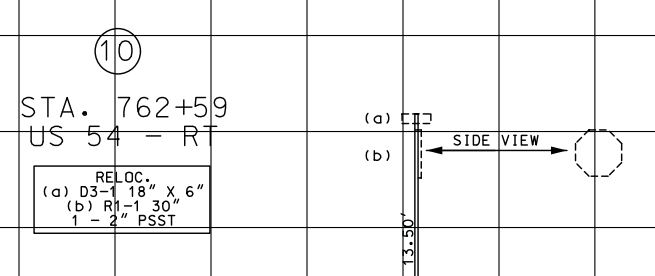
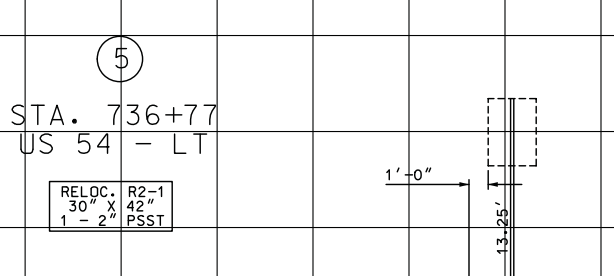
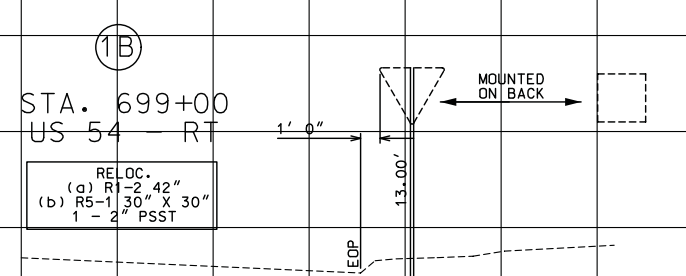
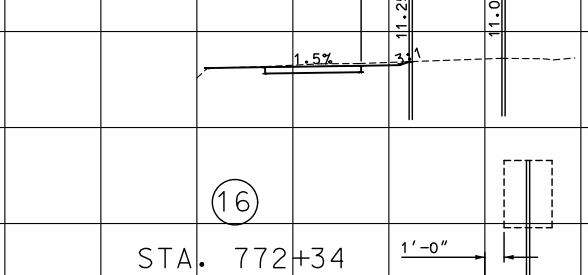
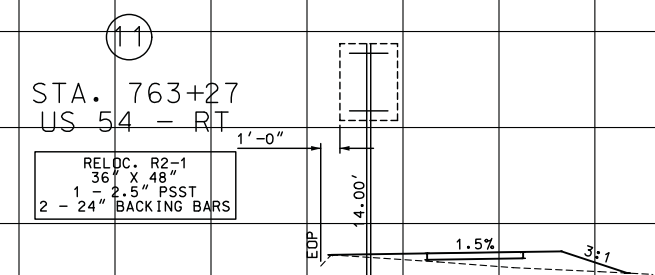
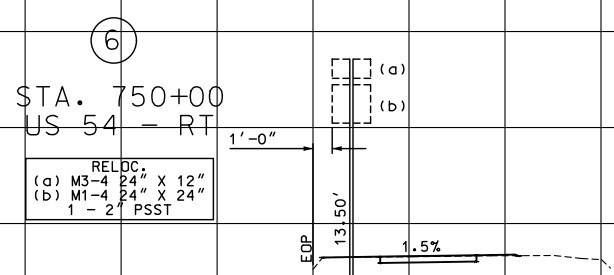
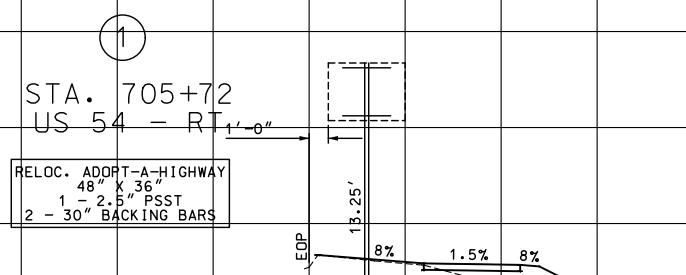
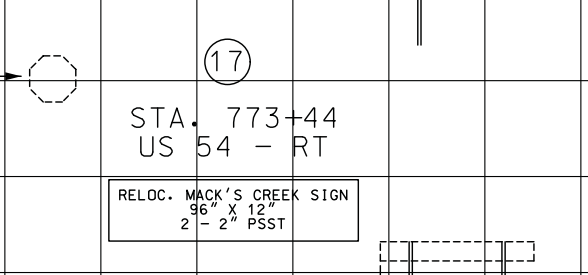
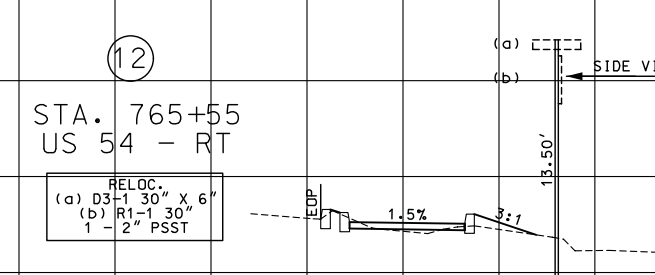
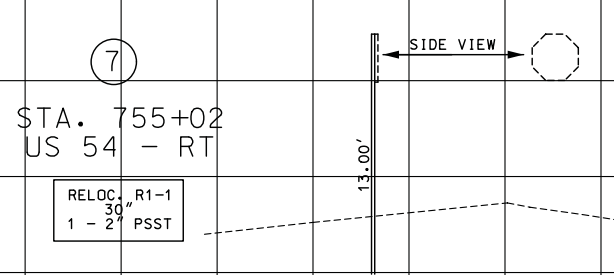
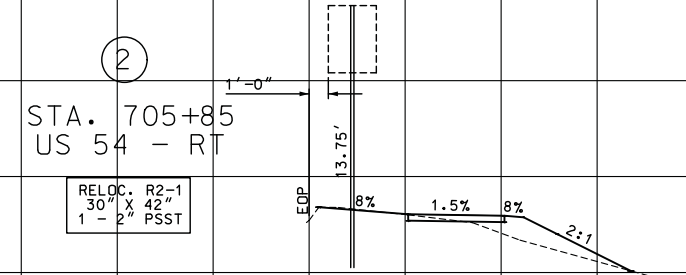
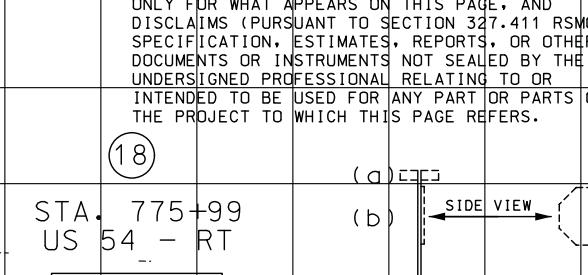
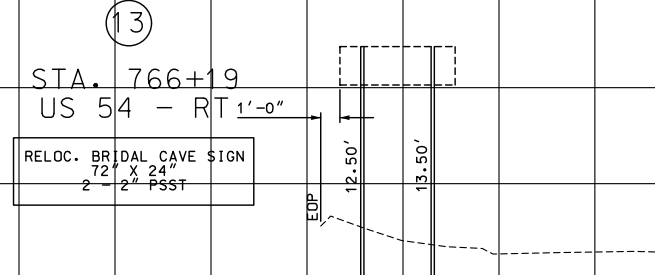
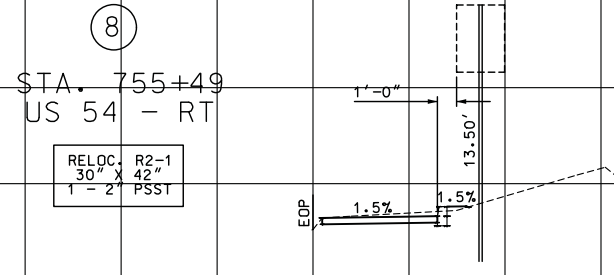
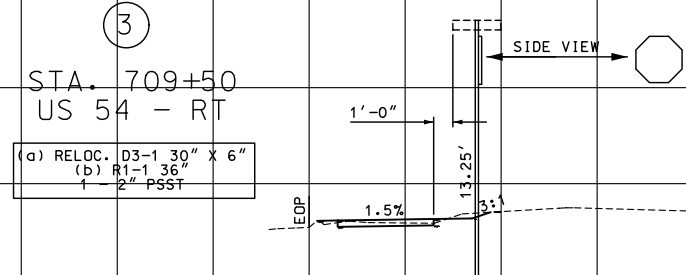
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SHEET 55 OF 122

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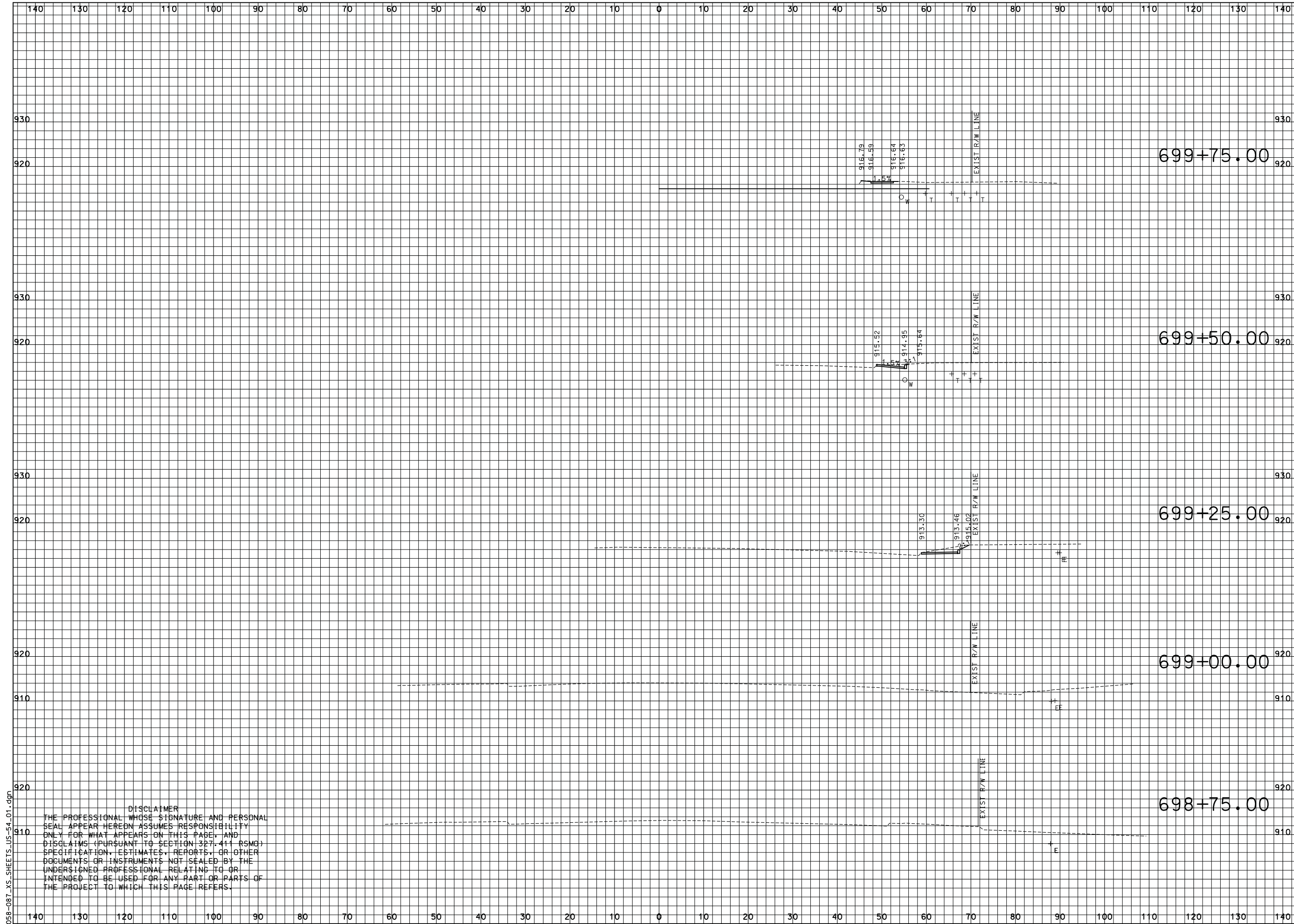
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CITY OF CAMDENTON, MISSOURI
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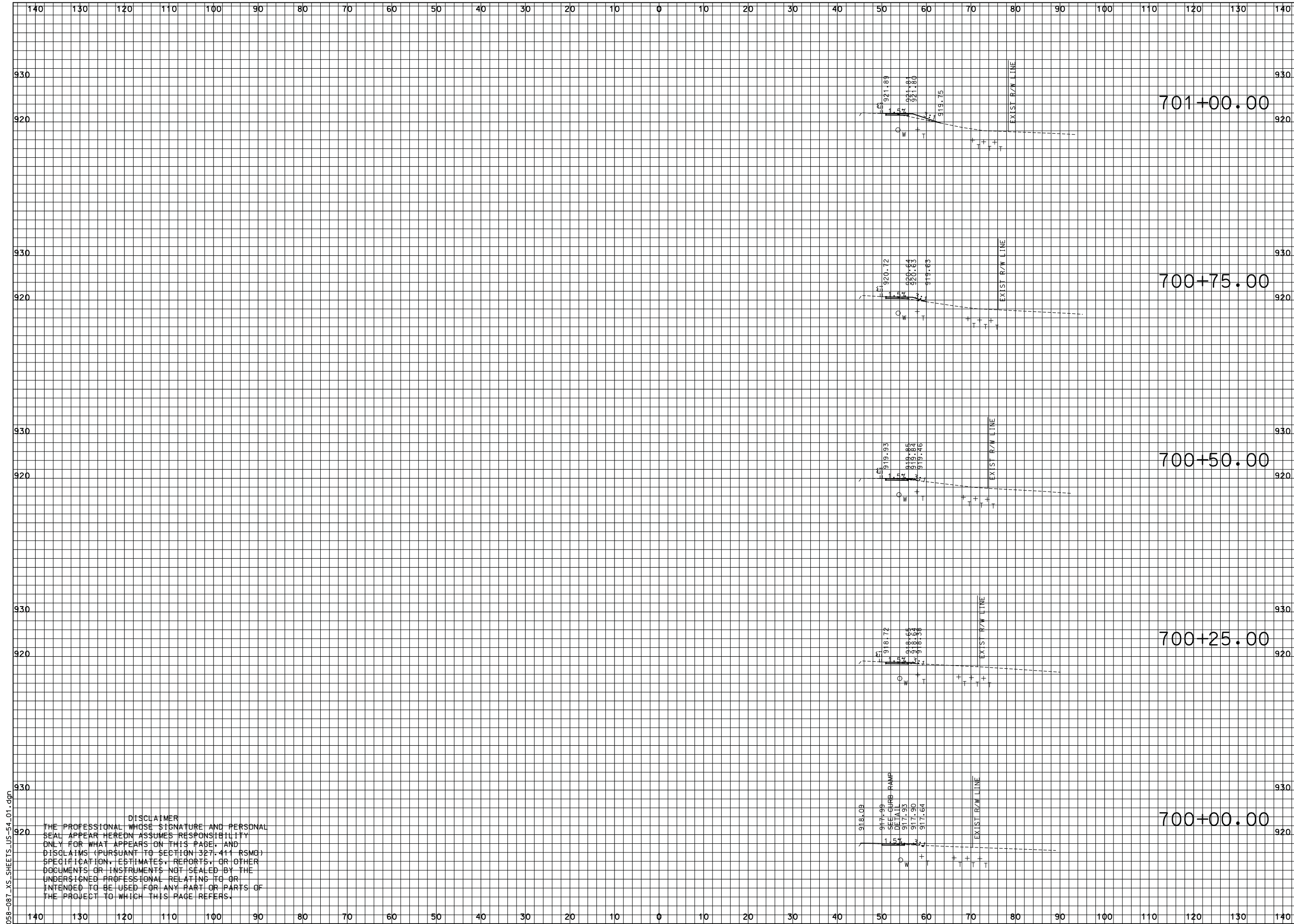
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 US-54 SIDEWALK PROJECT
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SHEET 2 OF 65

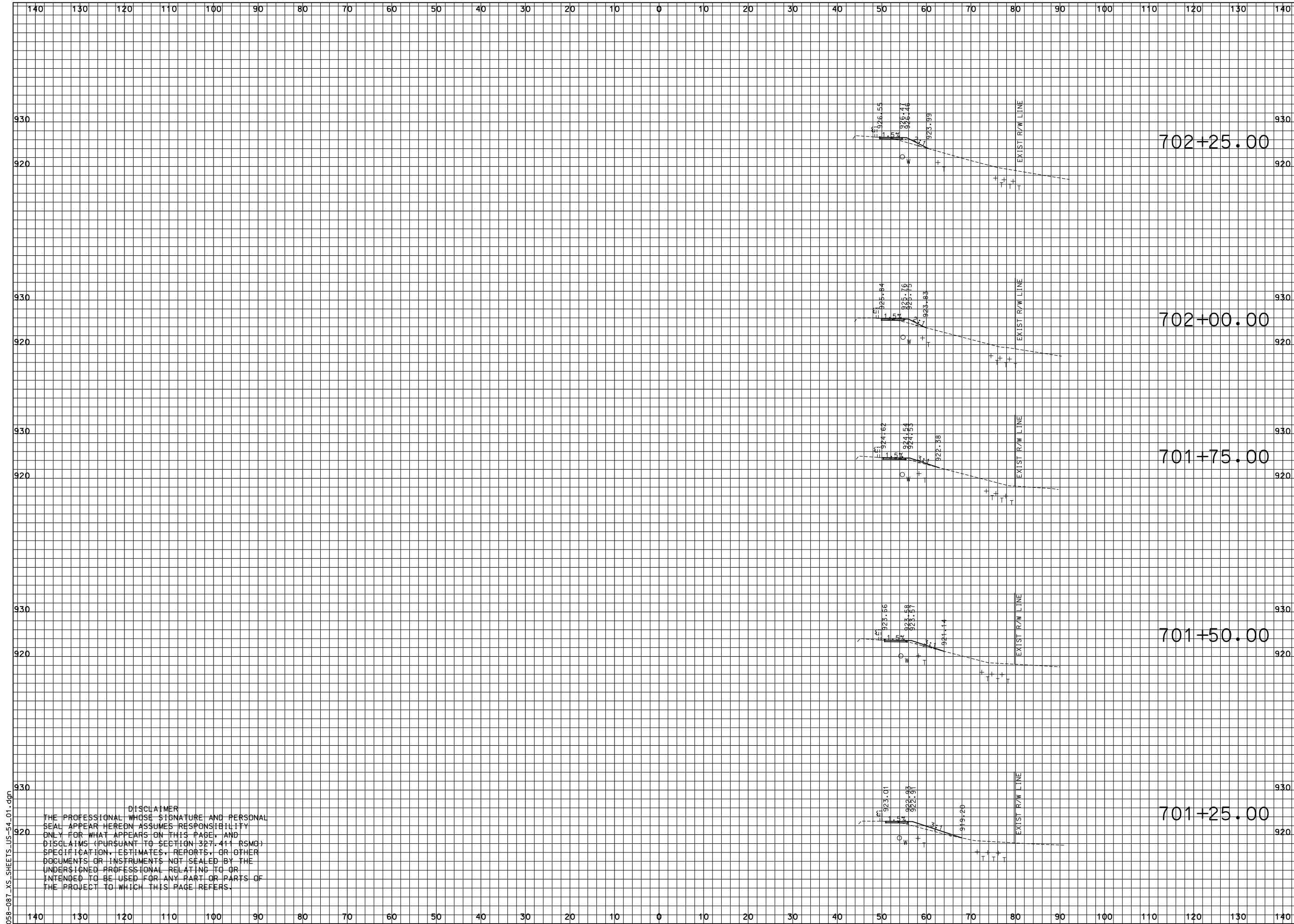
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 US-54 SIDEWALK PROJECT
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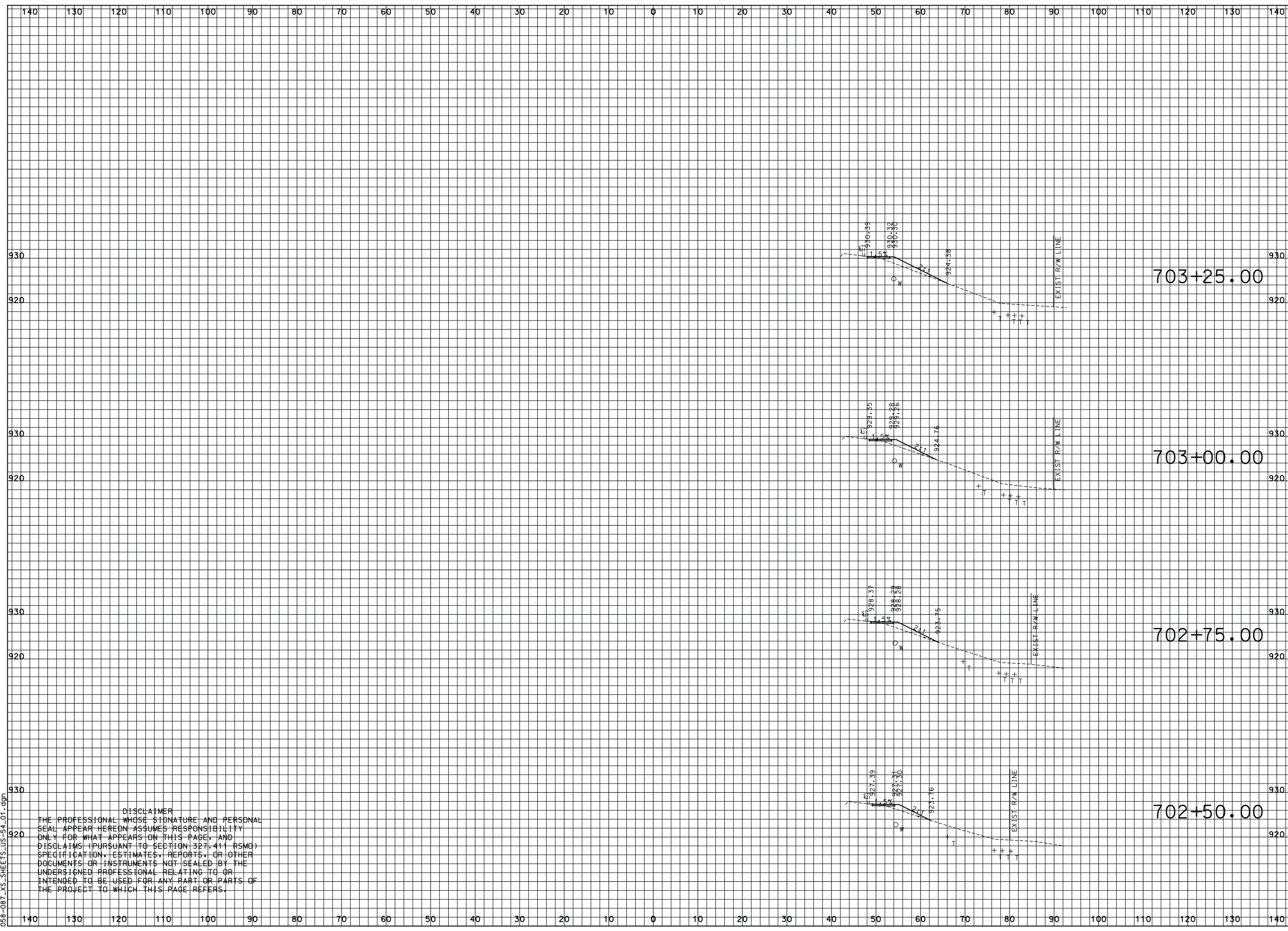


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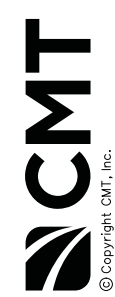
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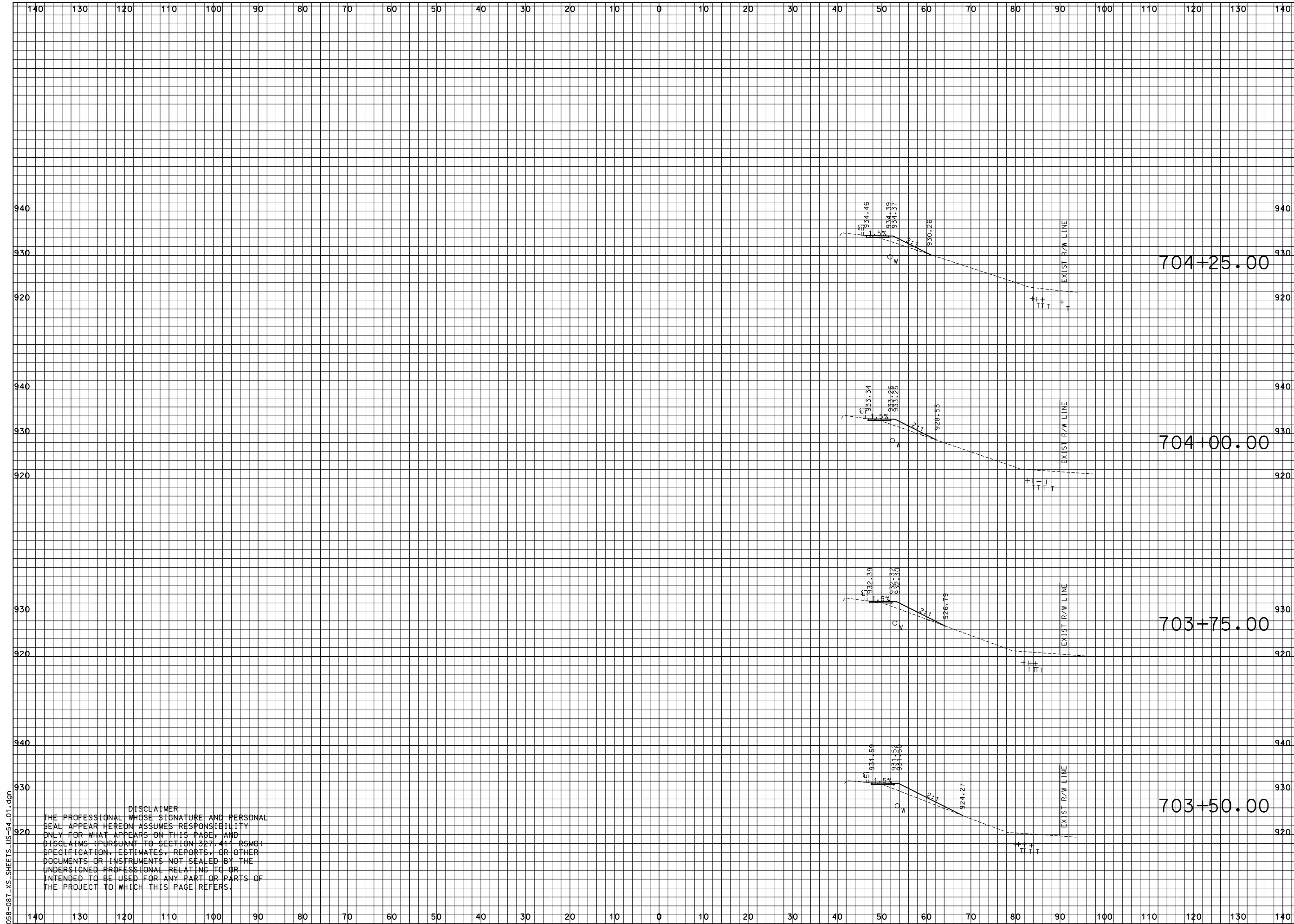
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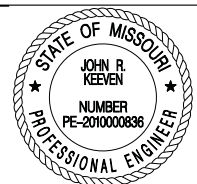
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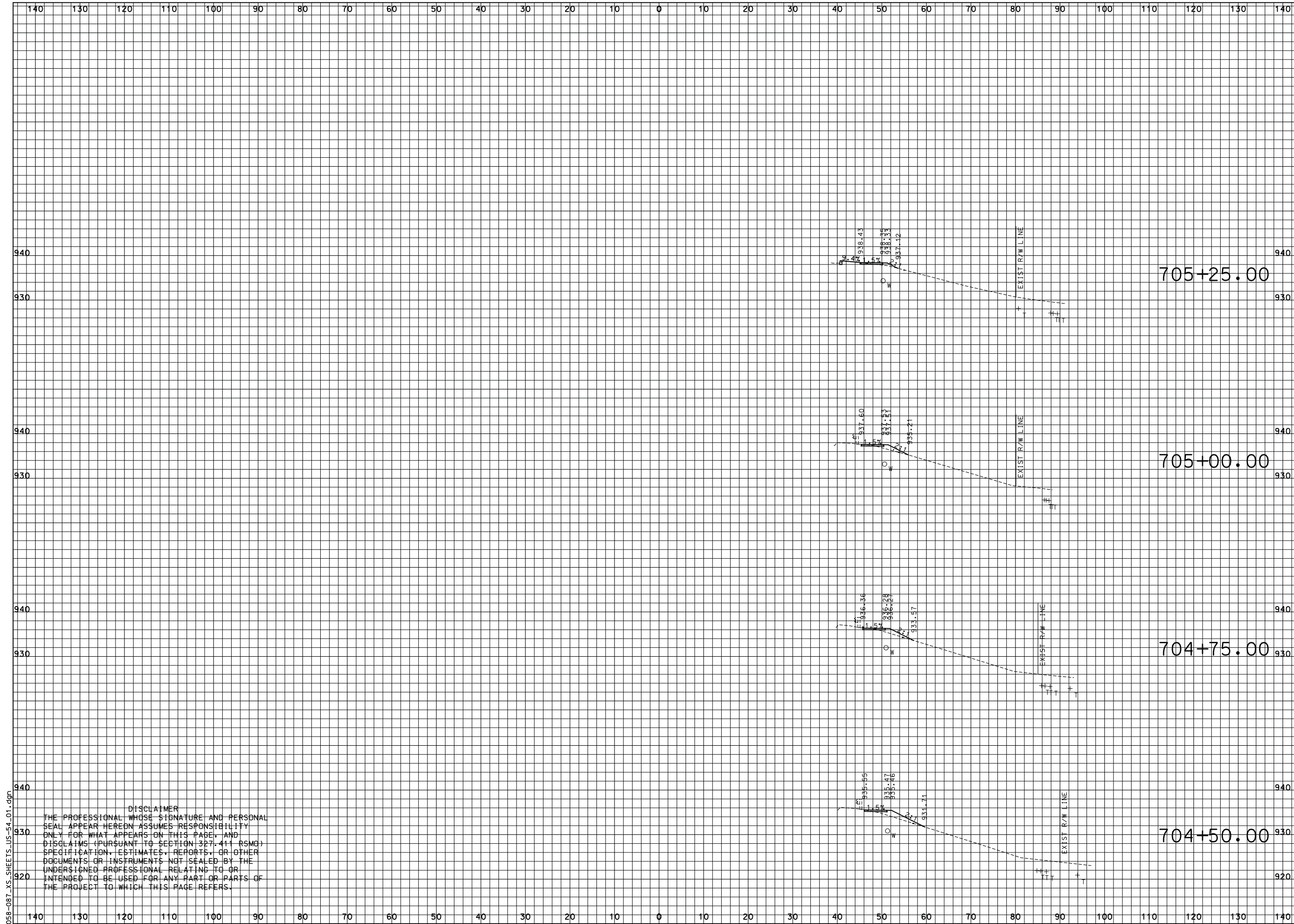
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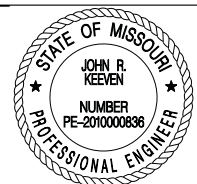
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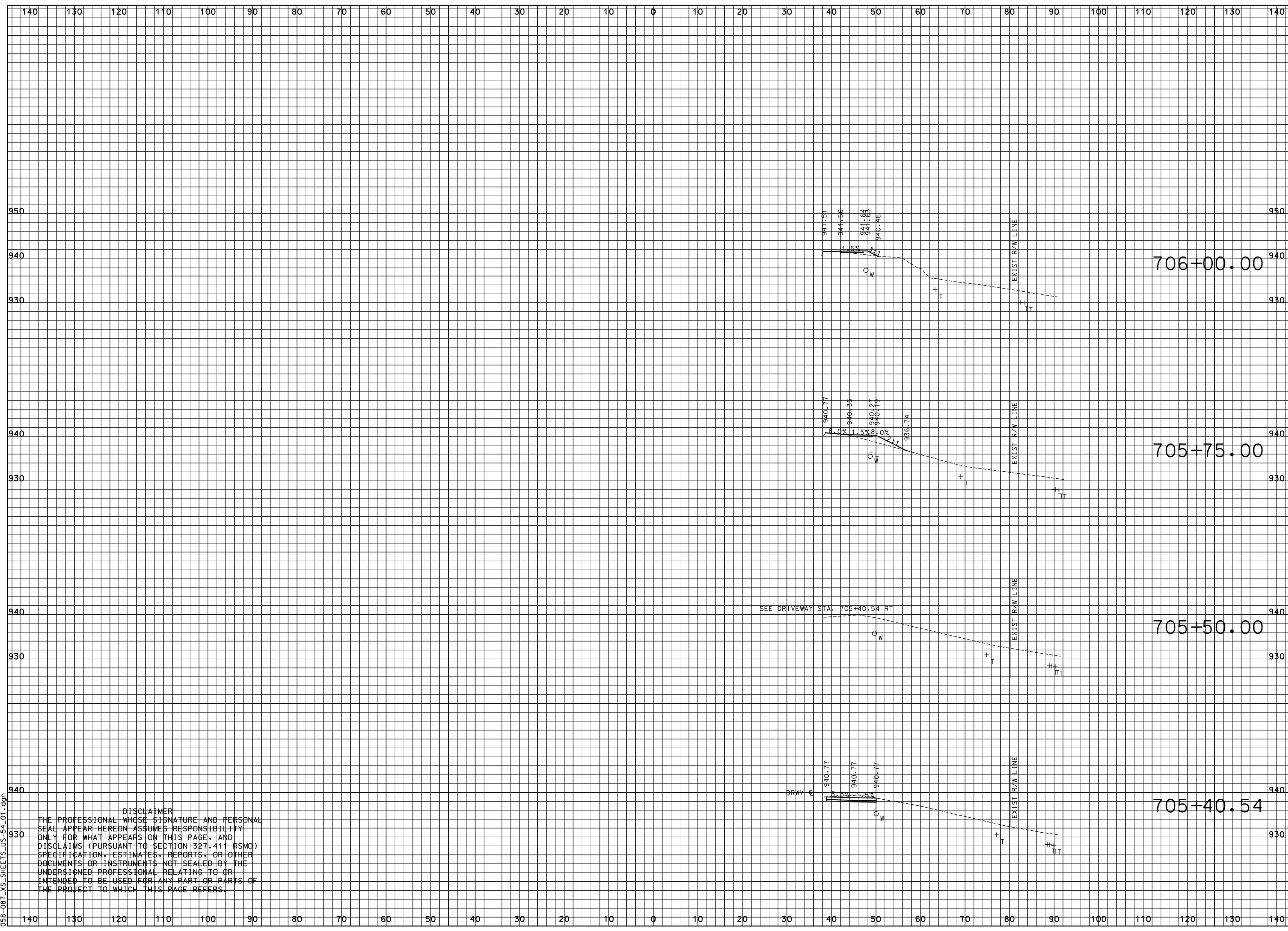
CROSS SECTIONS
 SHEET 6 OF 65

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 US-54 SIDEWALK PROJECT
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 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



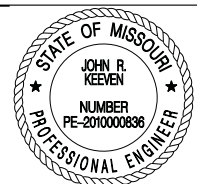
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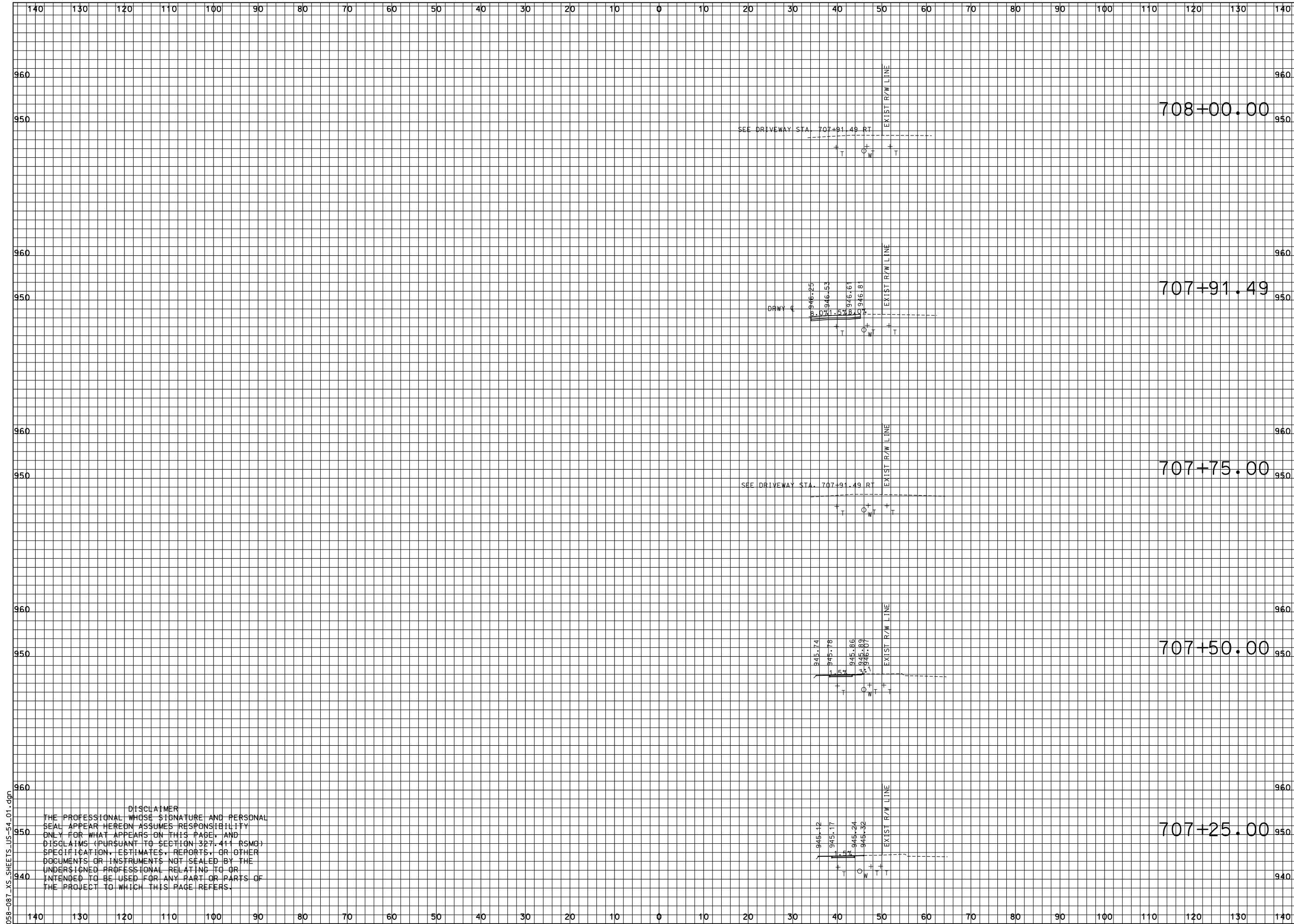
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 SHEET 9 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



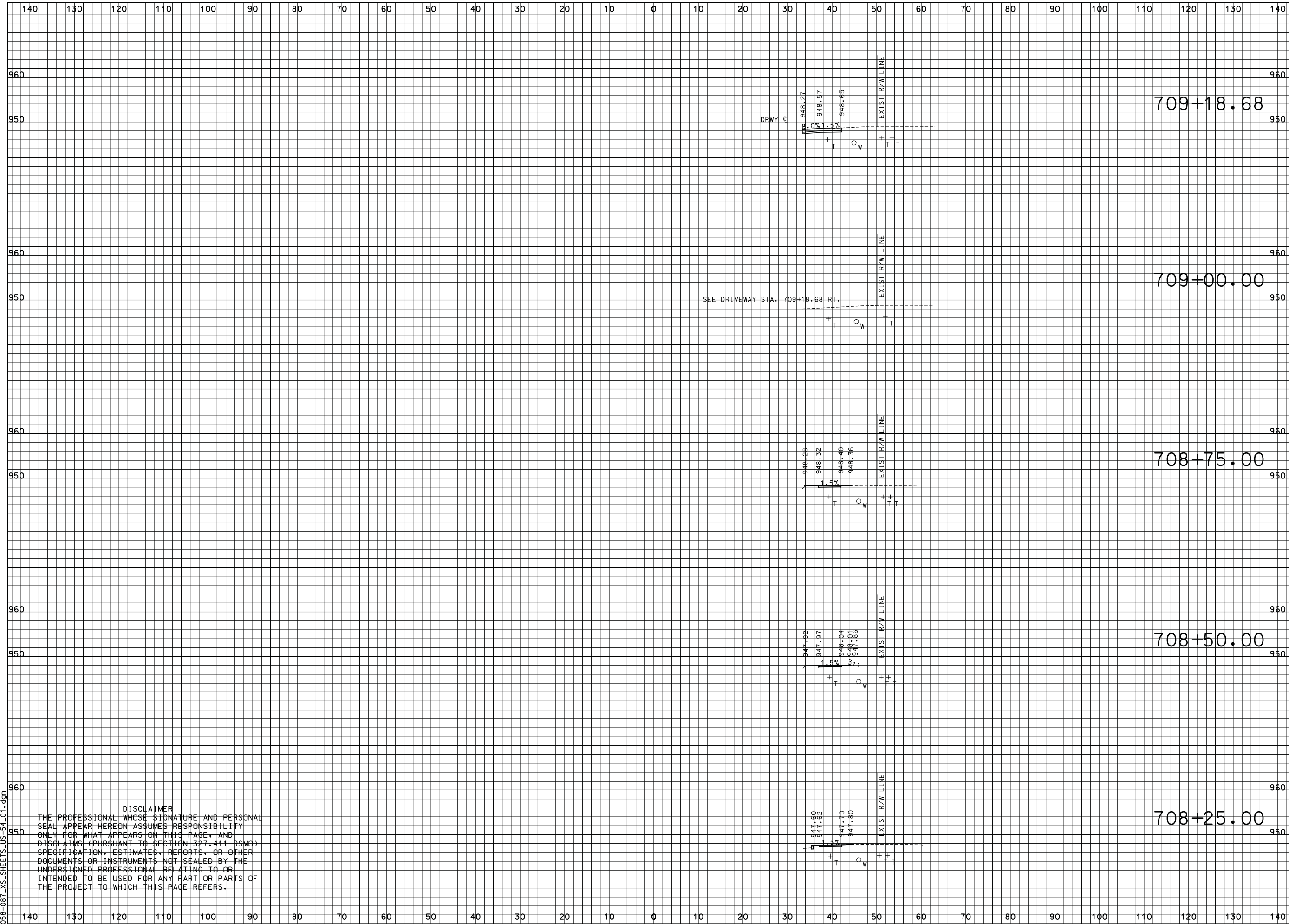
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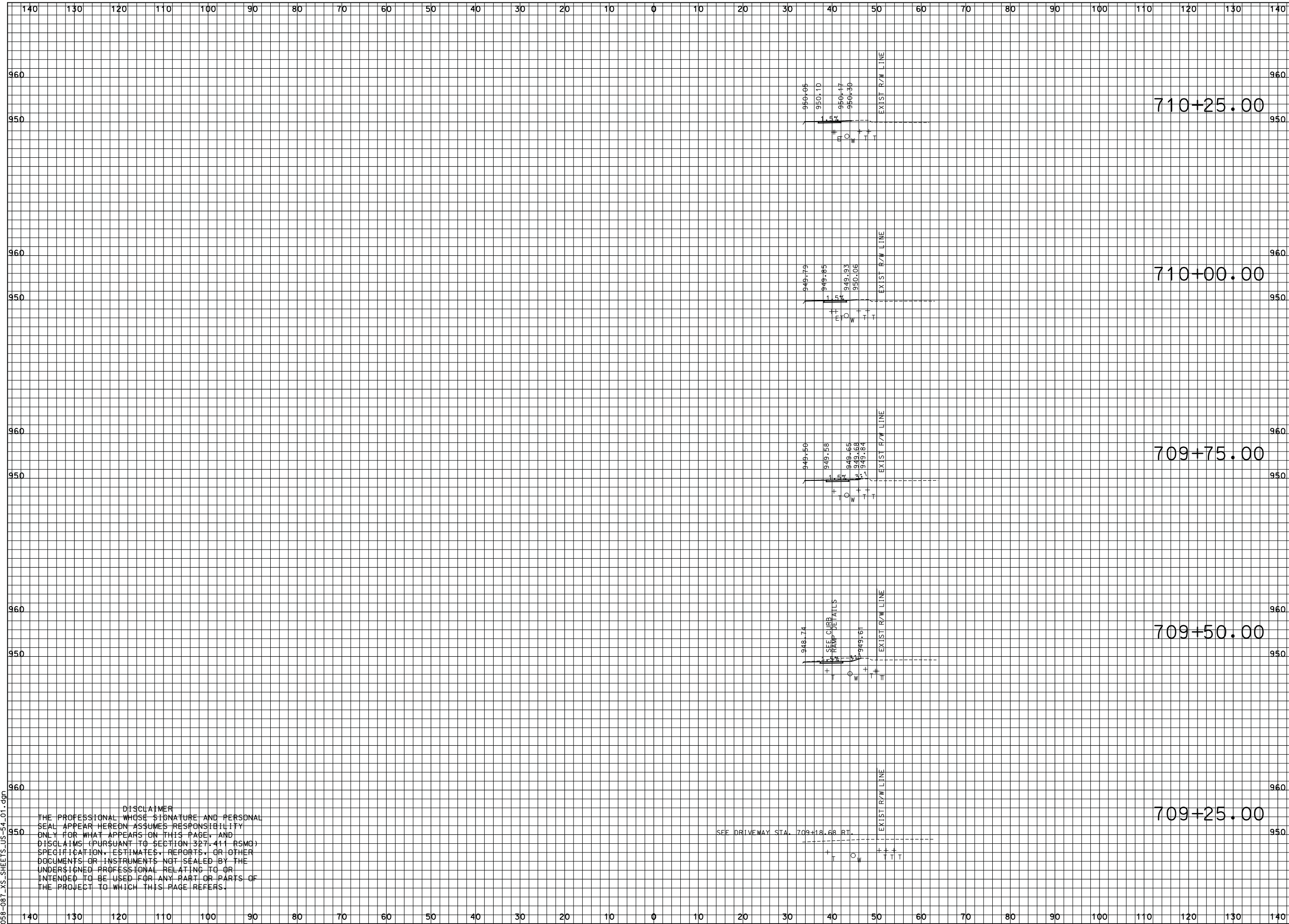
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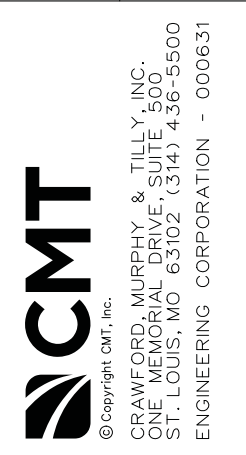
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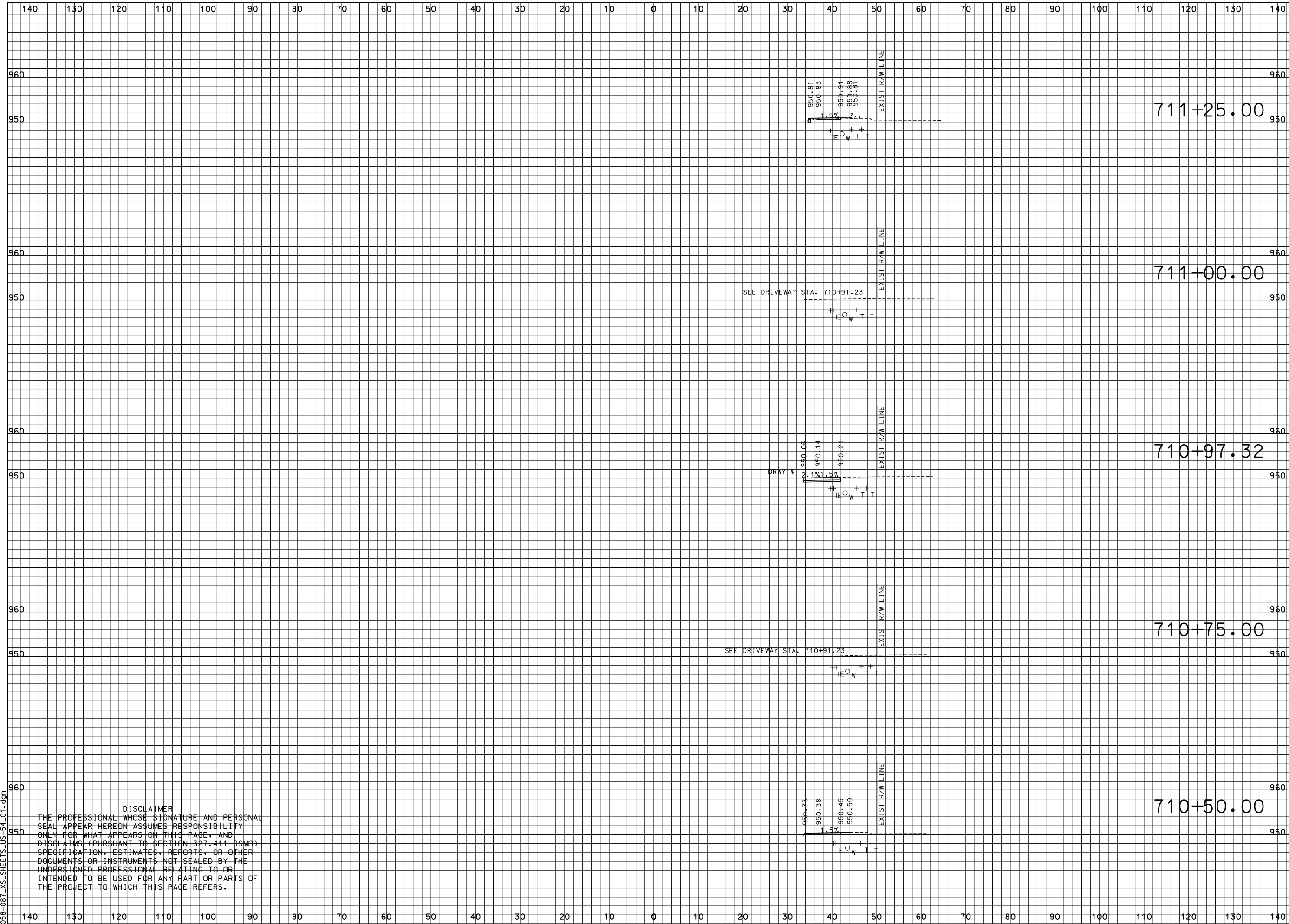
CROSS SECTIONS
 SHEET 11 OF 65

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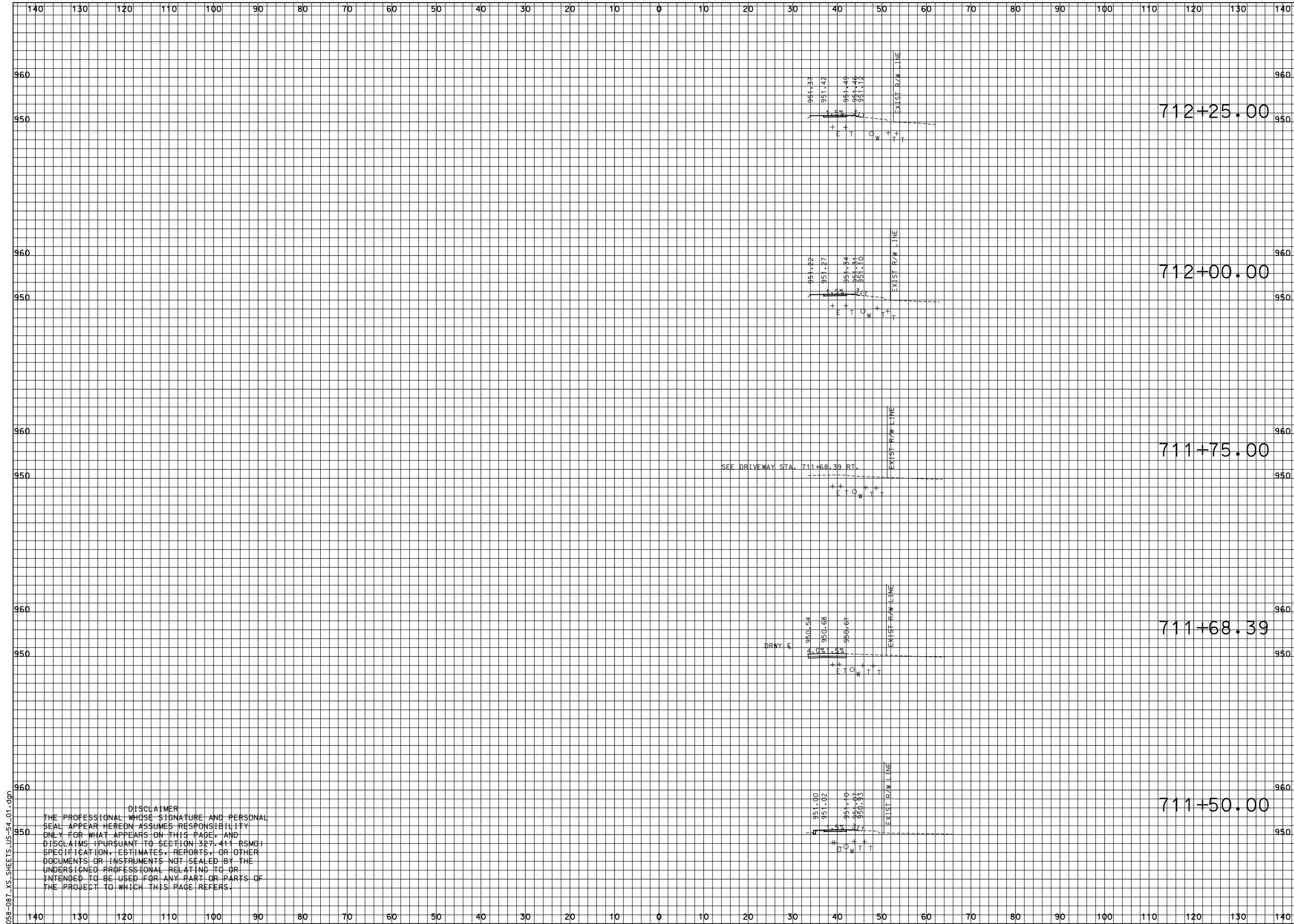
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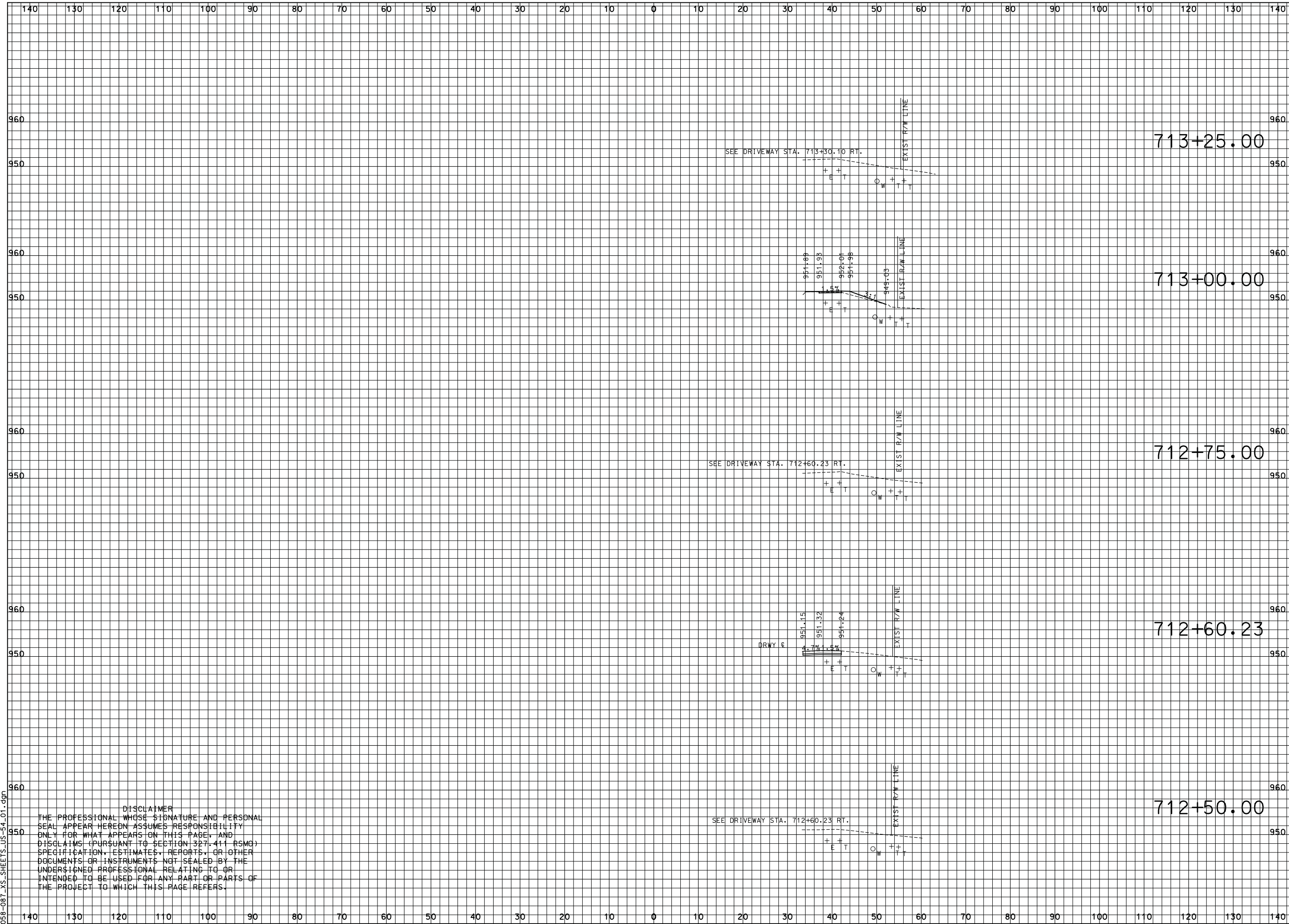
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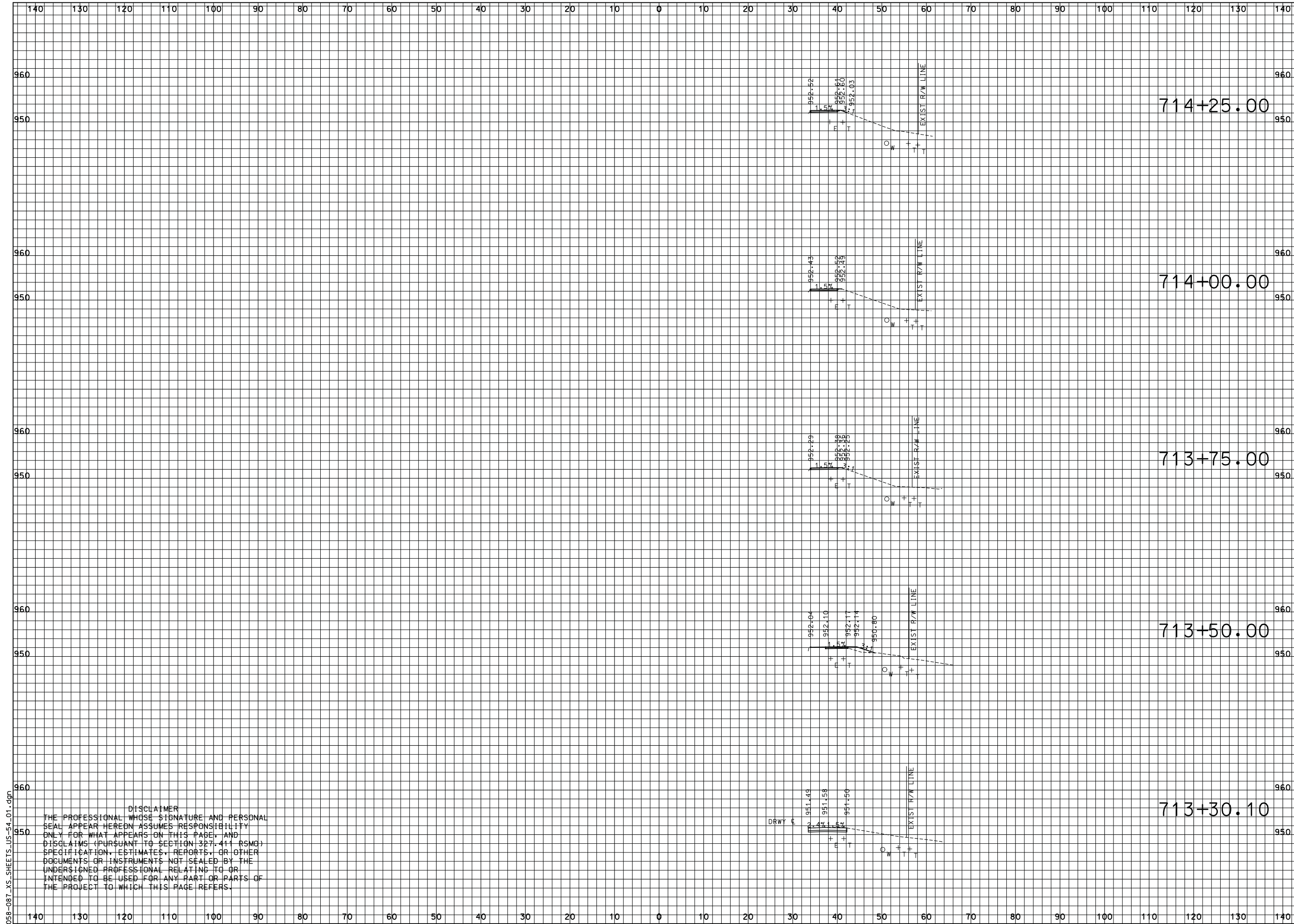
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SHEET 71 OF 122

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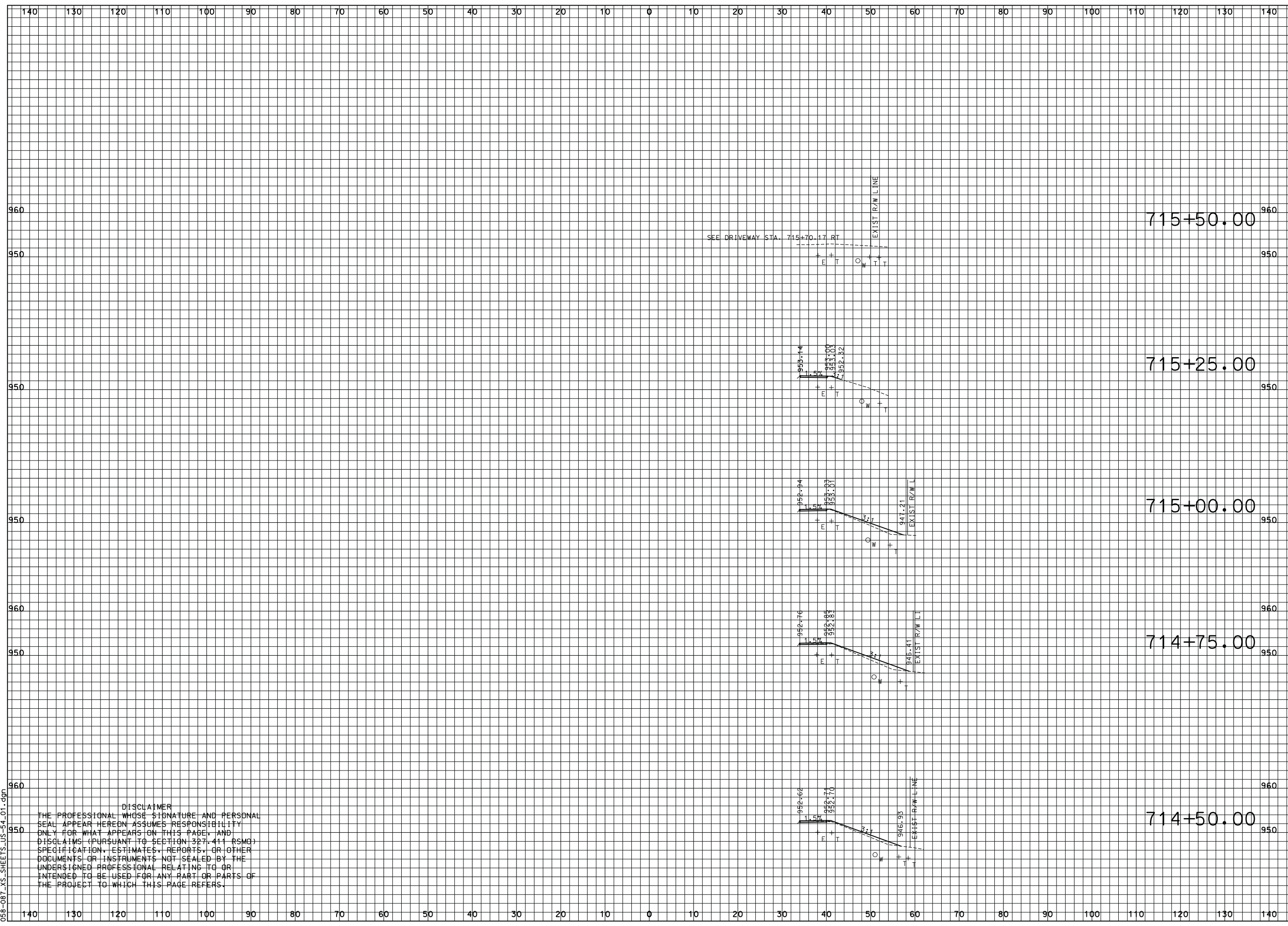
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 SHEET 15 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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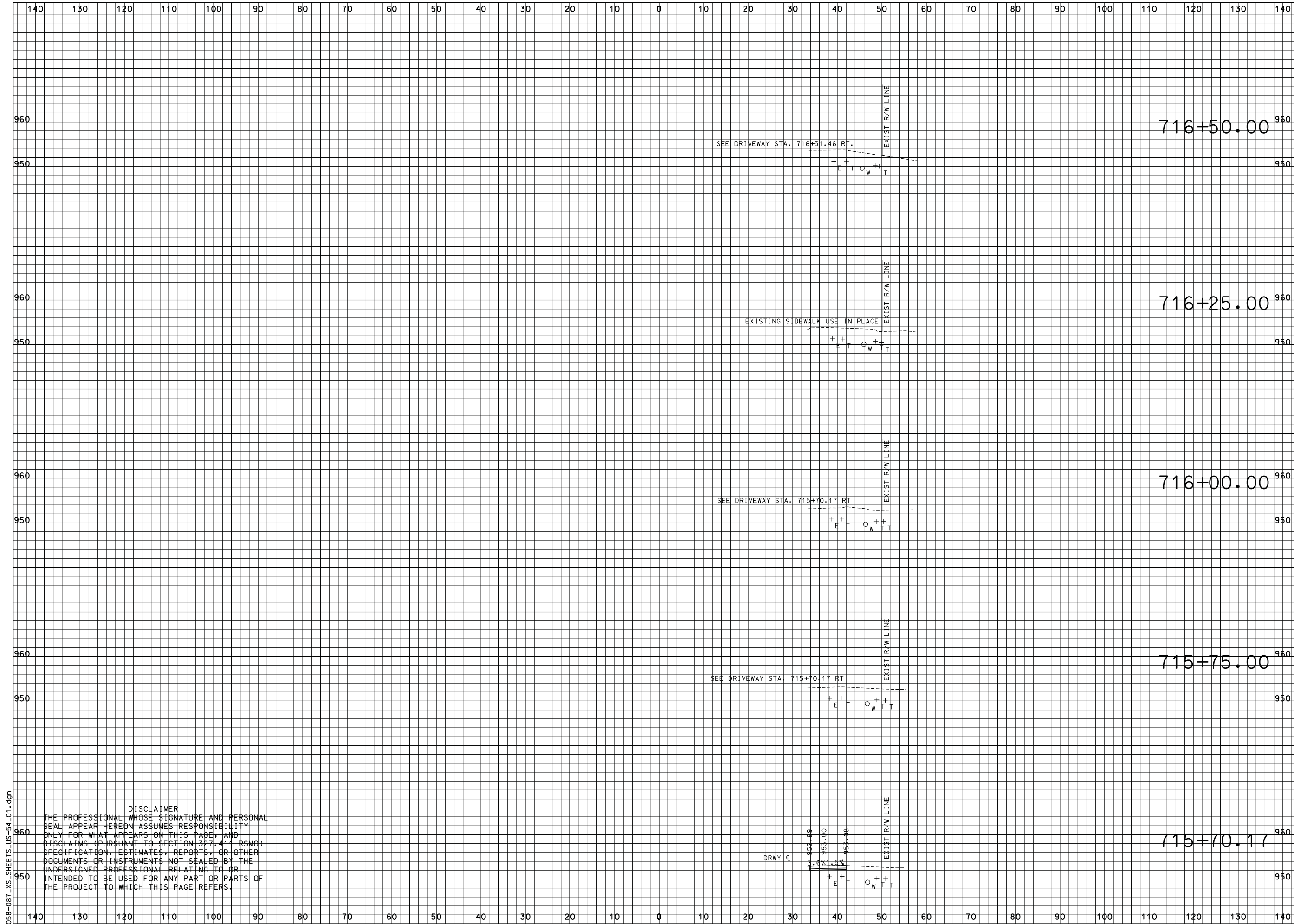
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SHEET 17 OF 65

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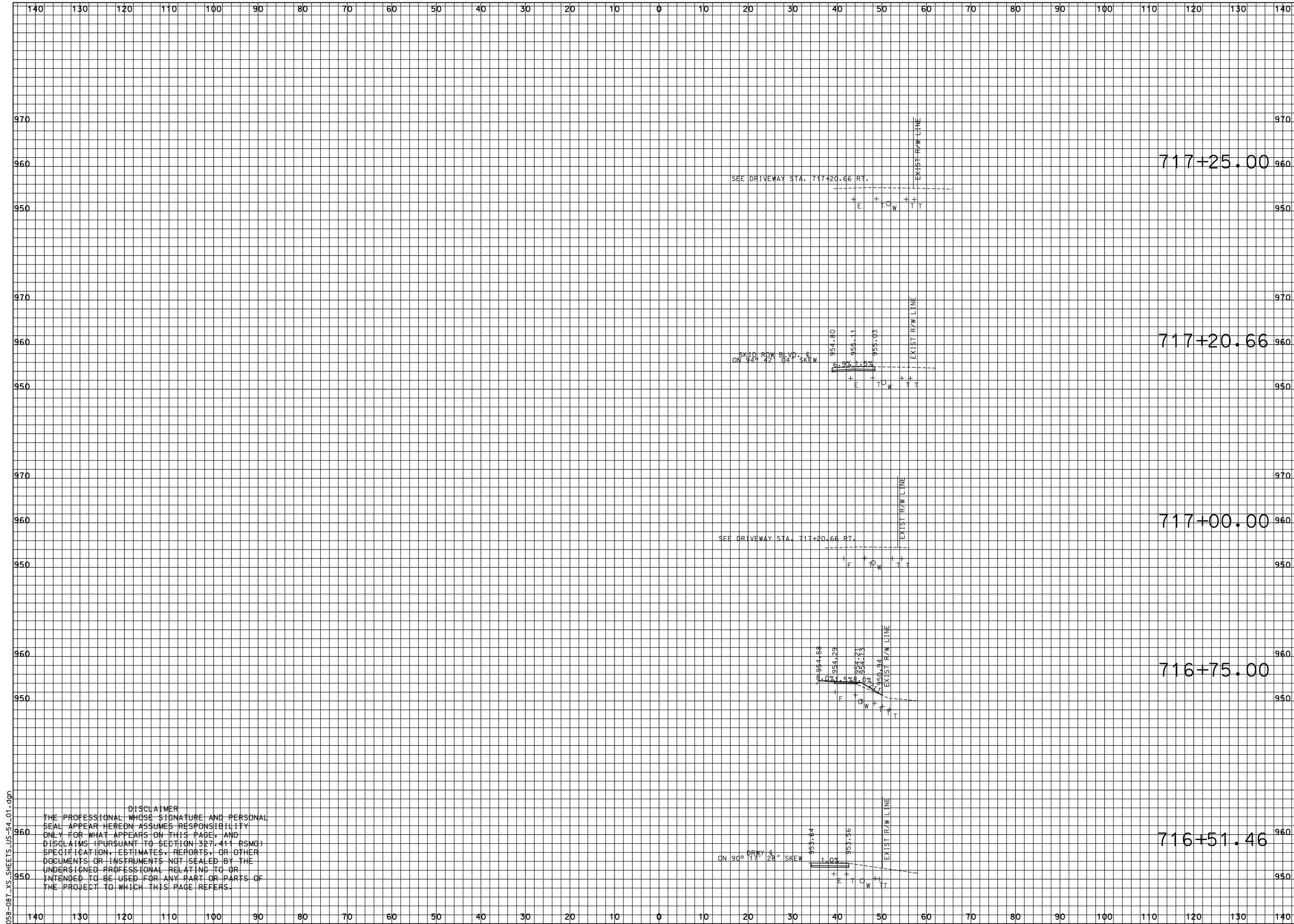


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SHEET 18 OF 65

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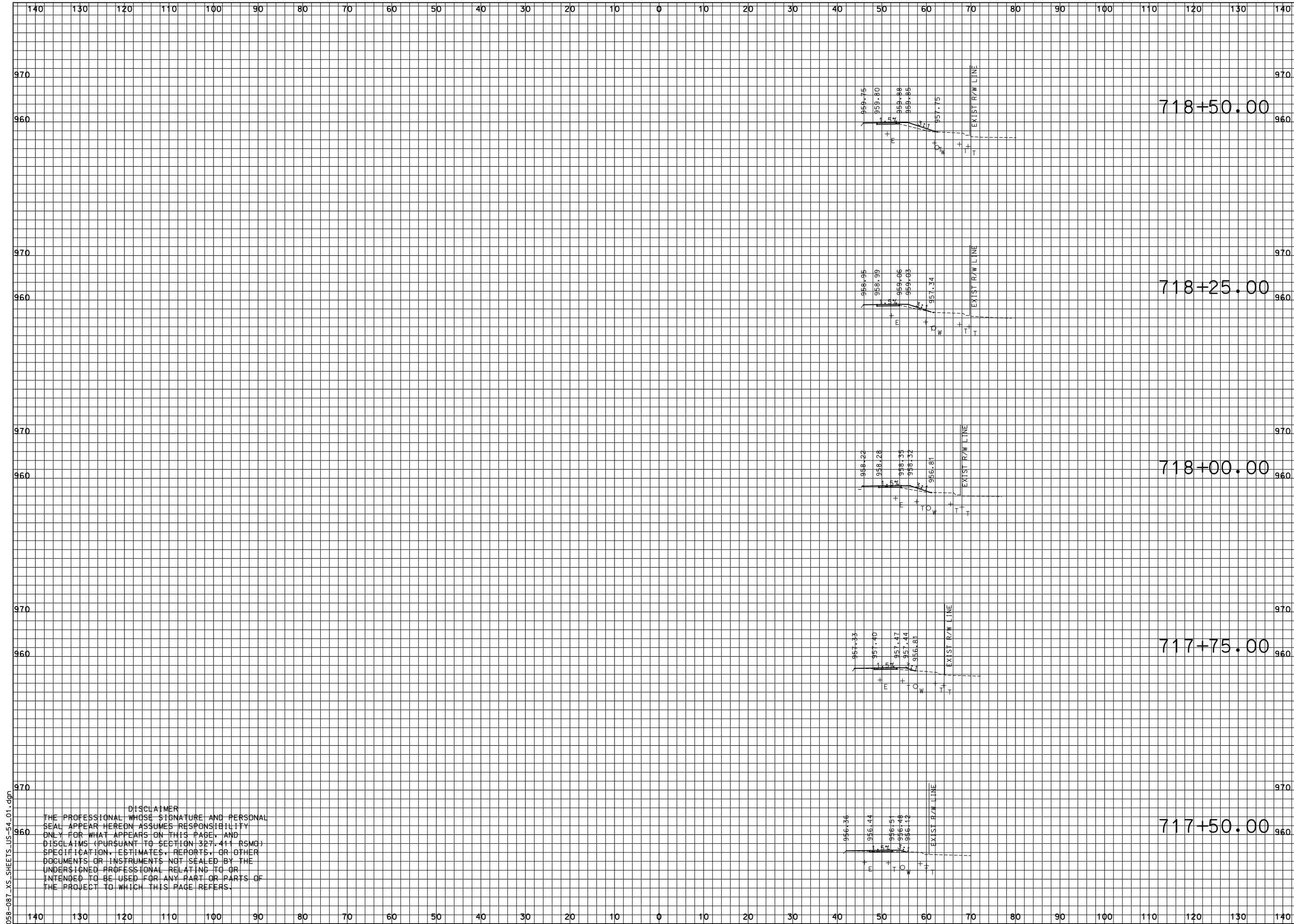


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 SHEET 19 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

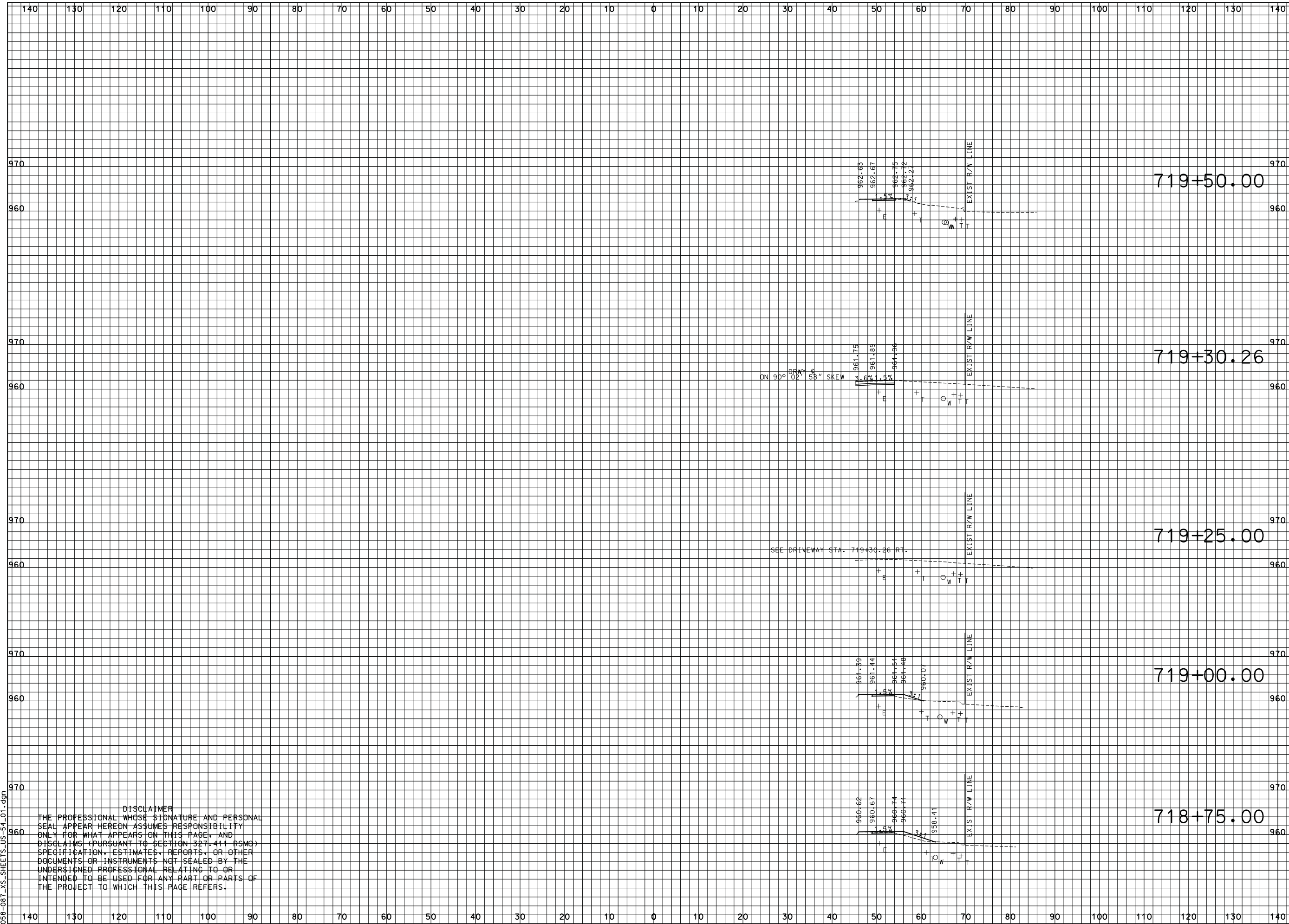


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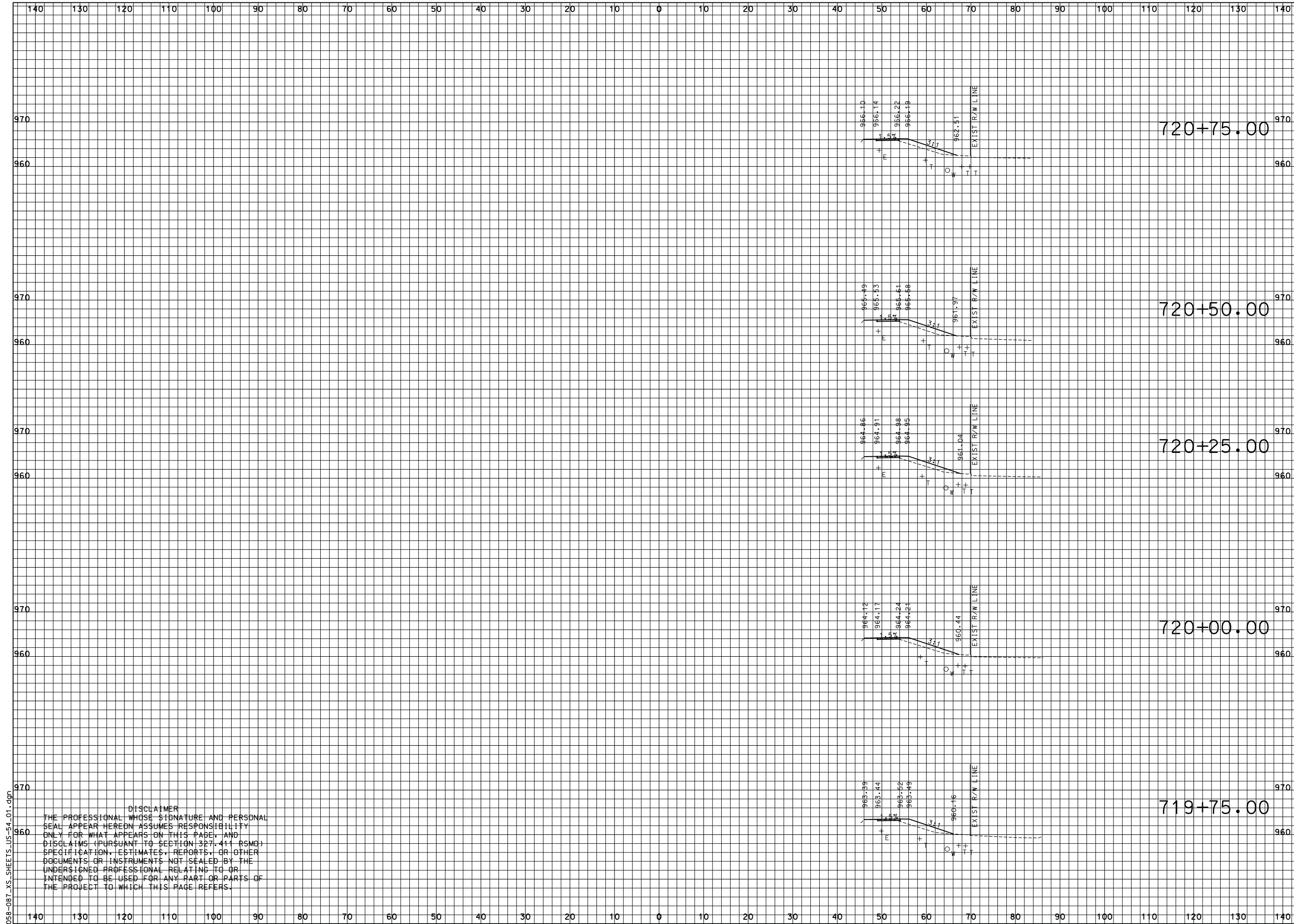
CROSS SECTIONS
 SHEET 20 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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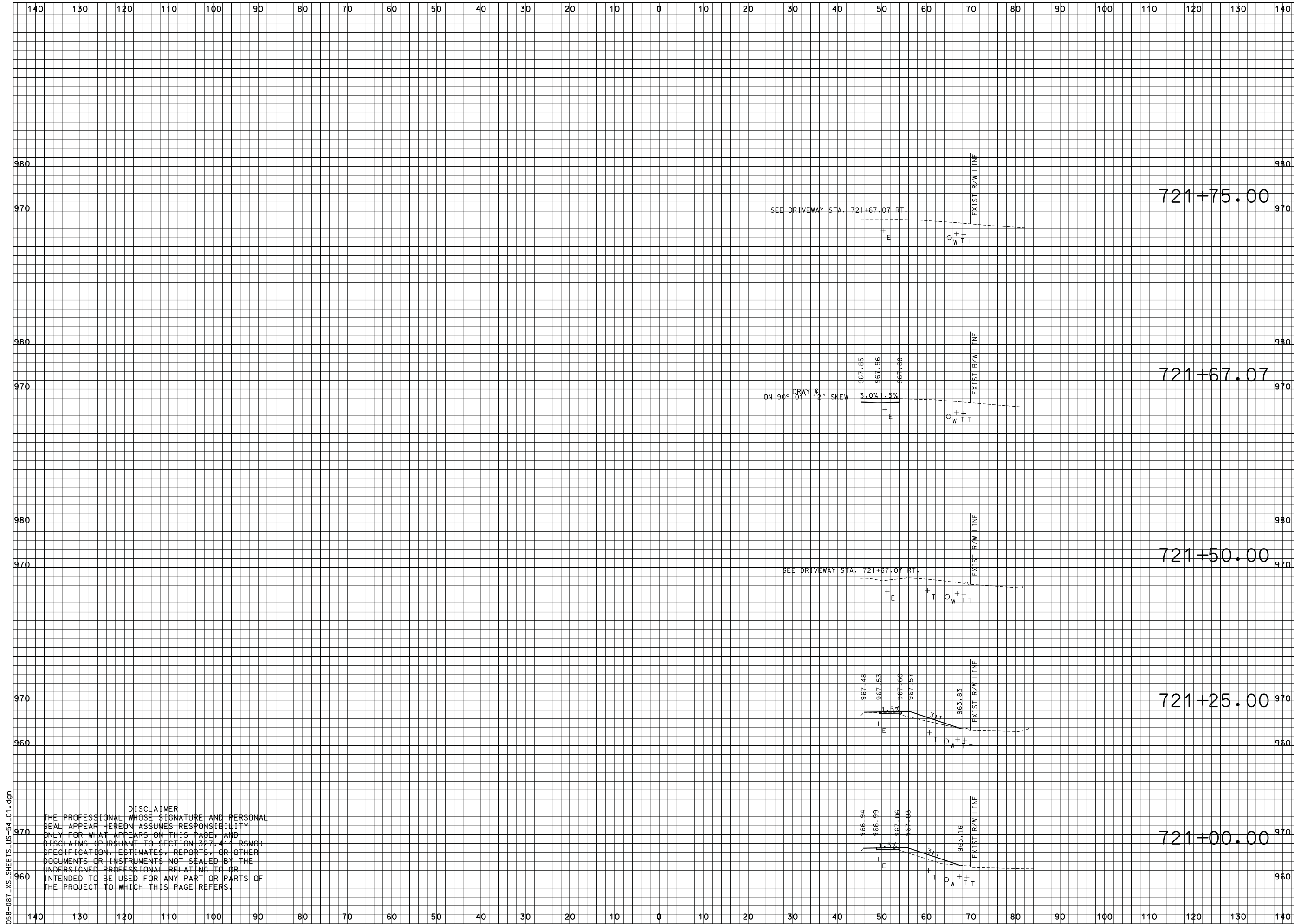
CROSS SECTIONS
 SHEET 21 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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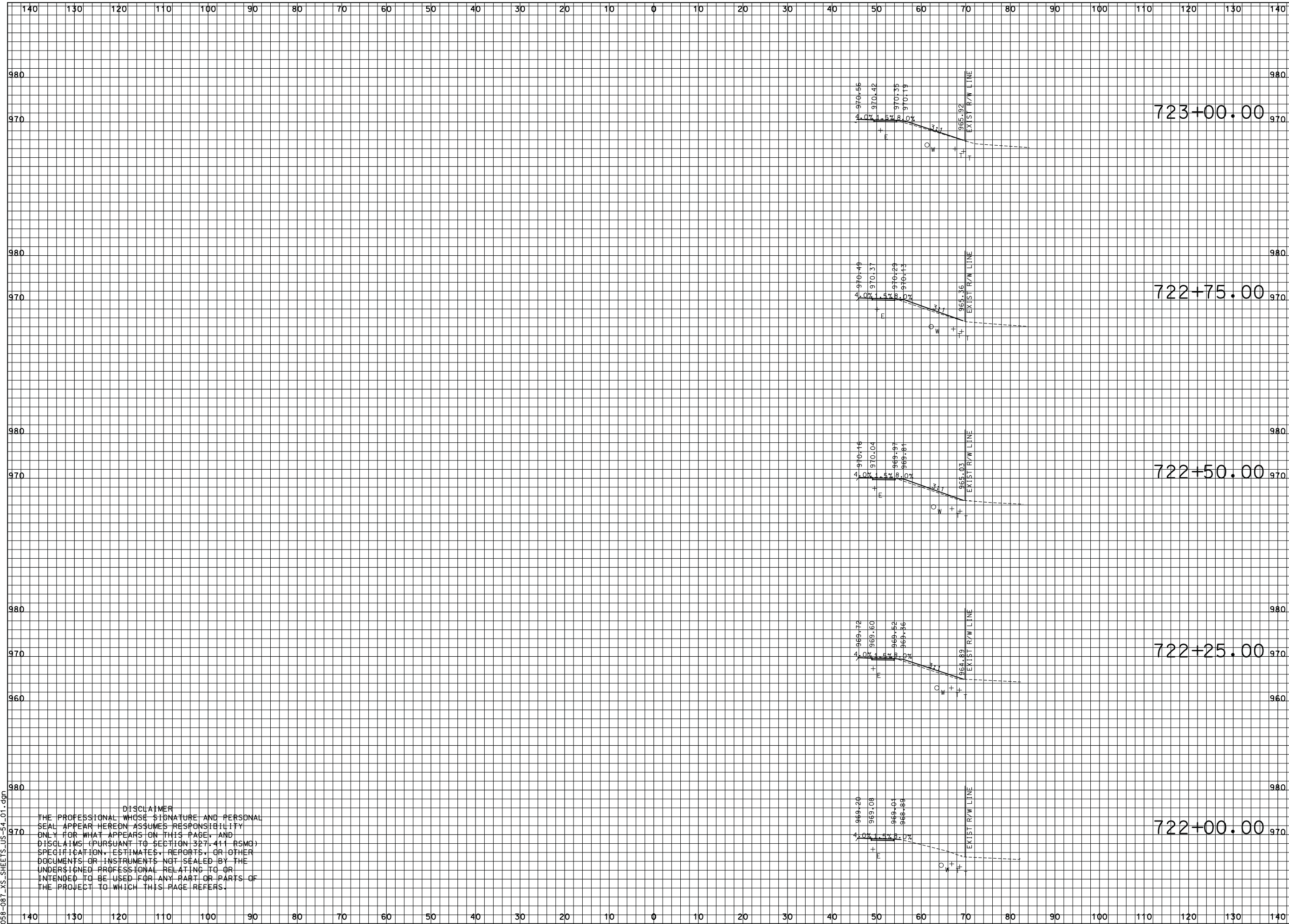
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 SHEET 22 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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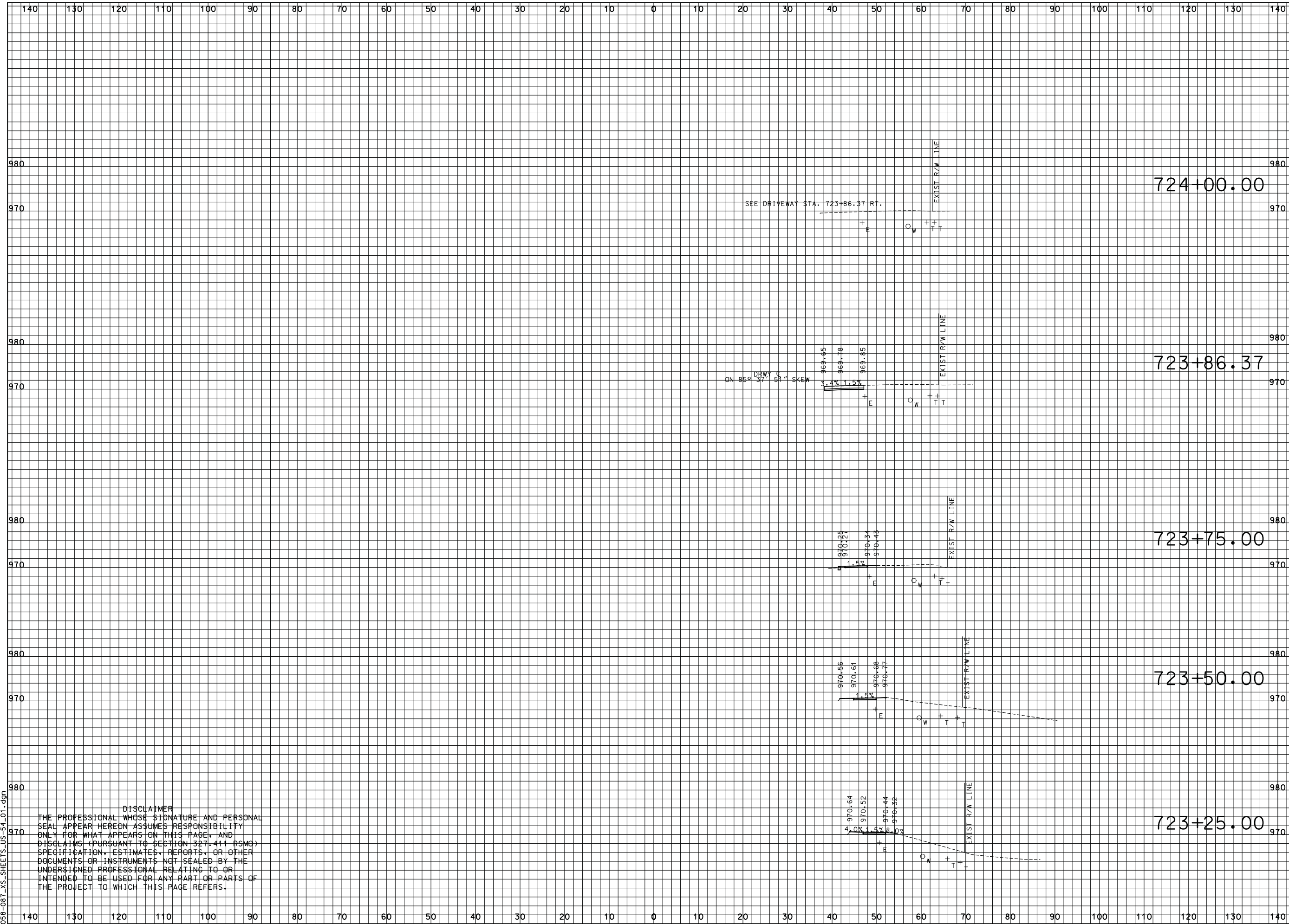
CROSS SECTIONS
 SHEET 23 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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CROSS SECTIONS

SHEET 24 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



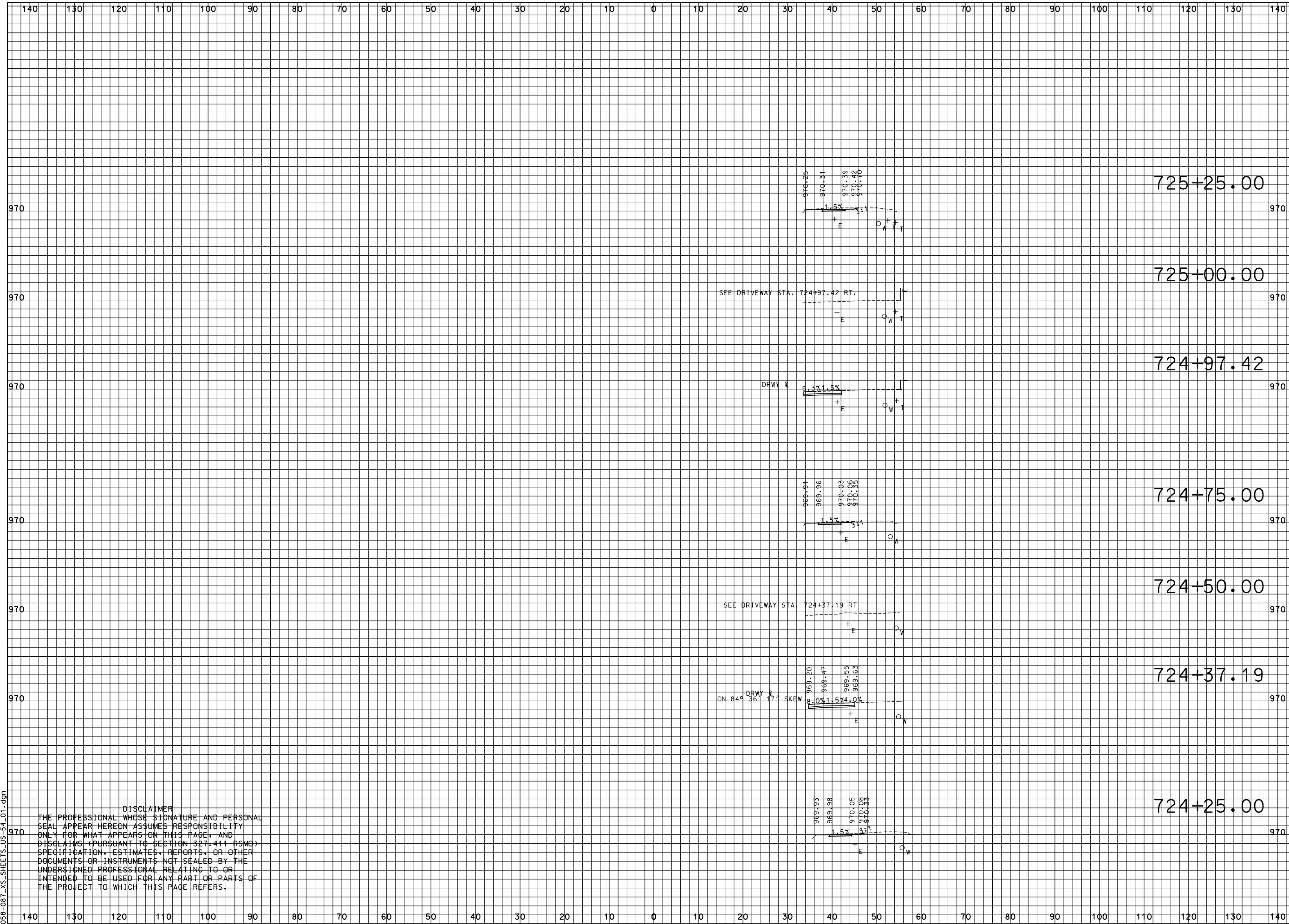
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SHEET 81 OF 122

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CROSS SECTIONS
SHEET 25 OF 65

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US-54 SIDEWALK PROJECT
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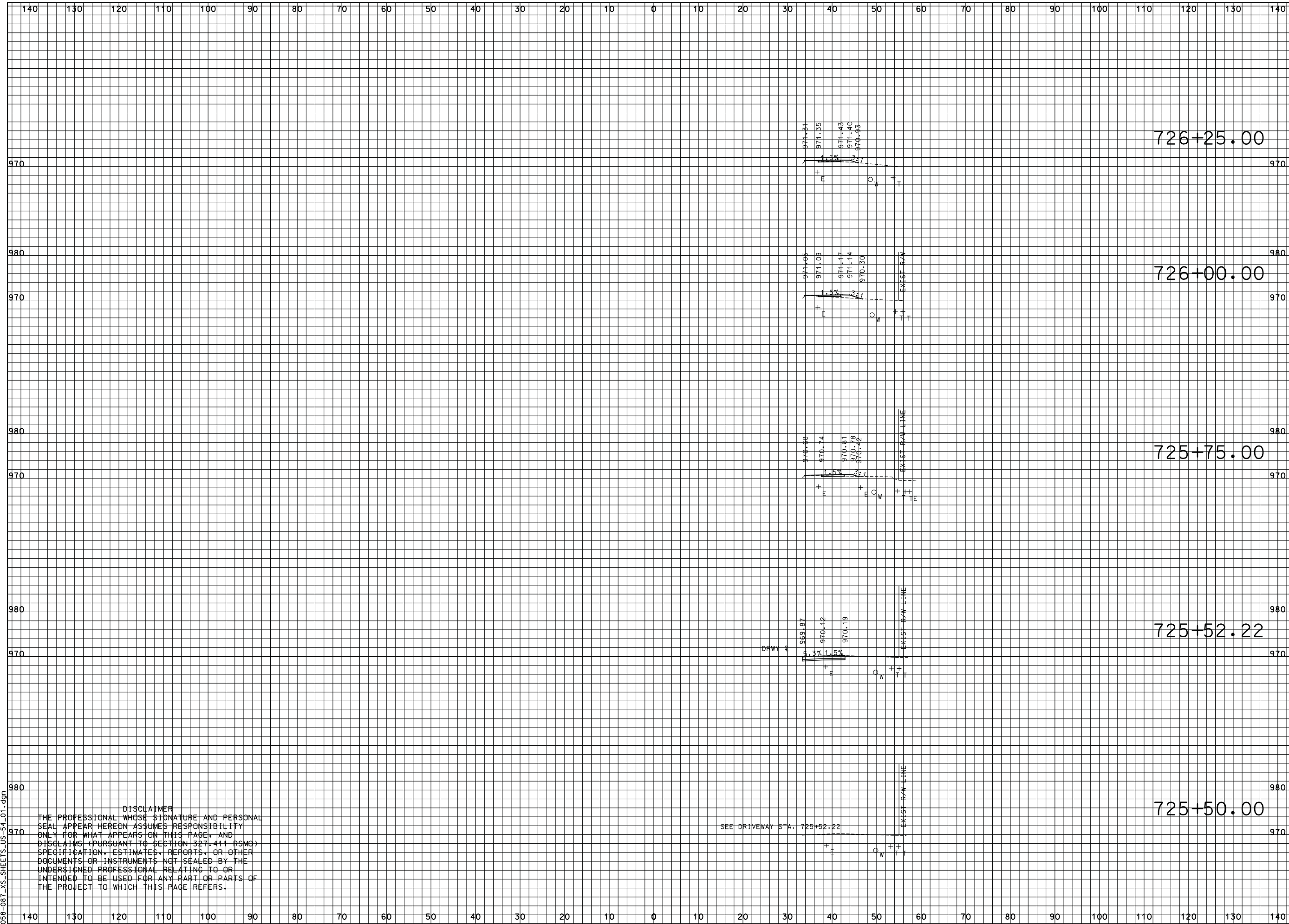
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 SHEET 26 OF 65

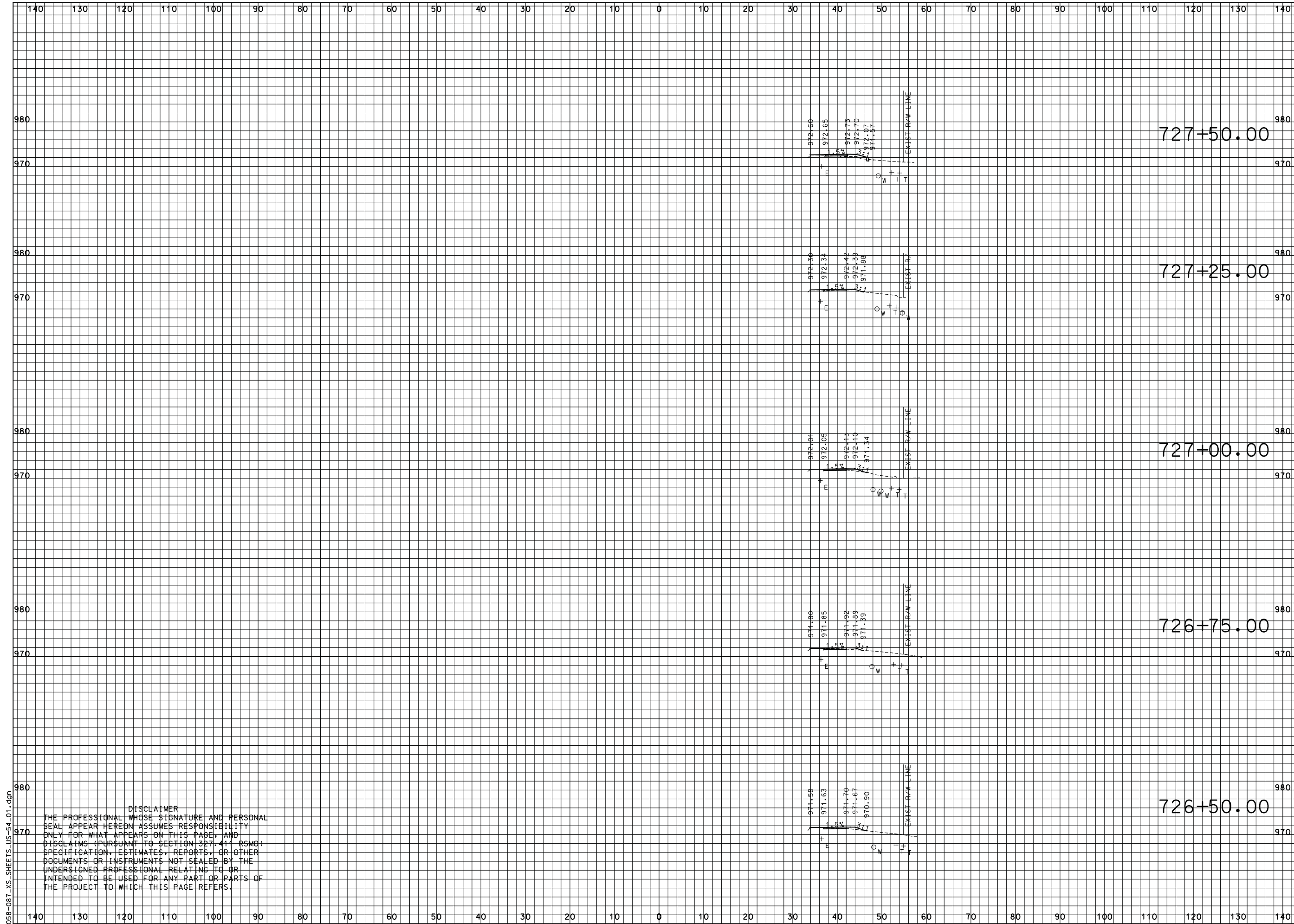
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REVISIONS		
NUMBER	BY	DATE

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 SHEET 27 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

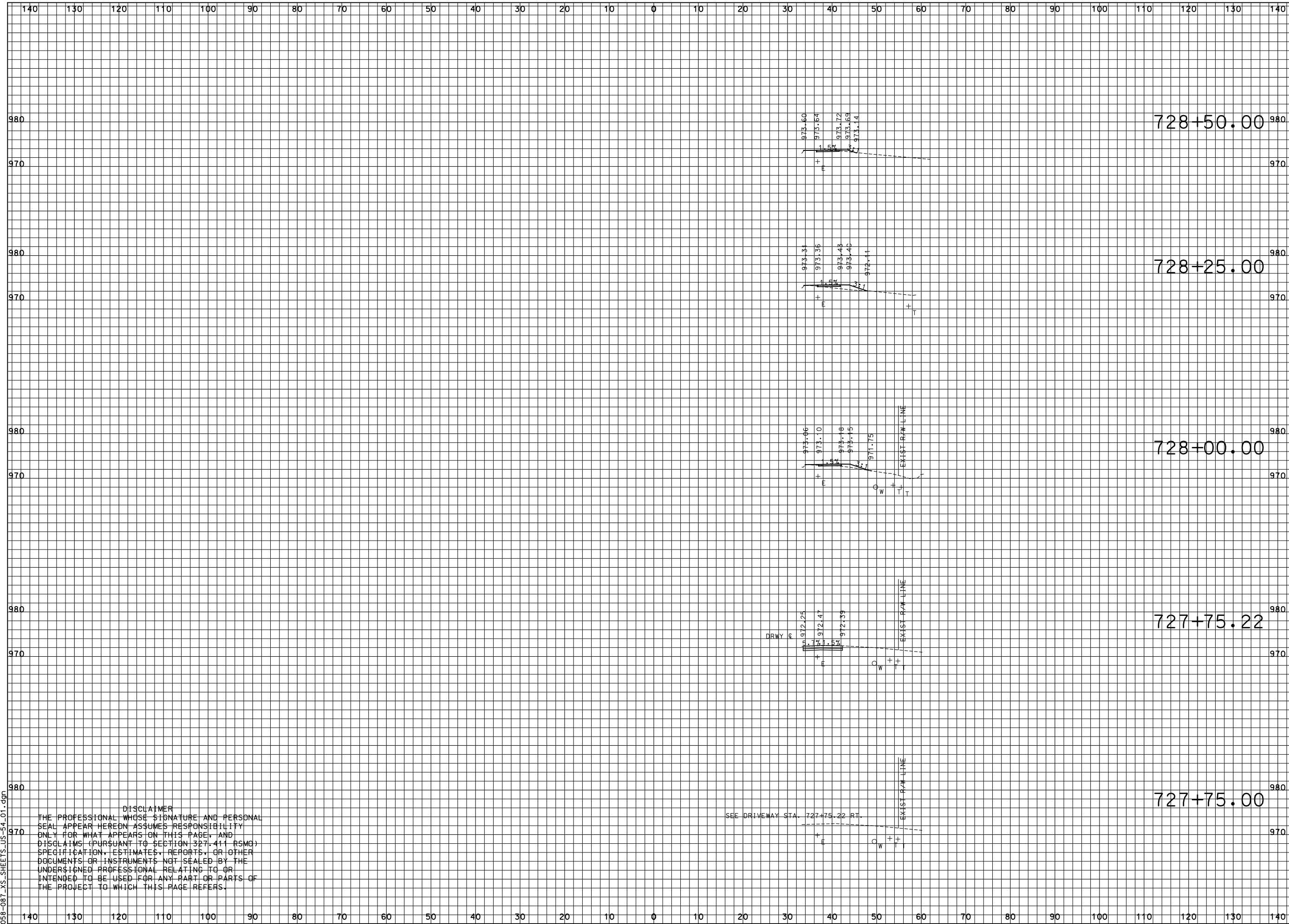


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SHEET 84 OF 122

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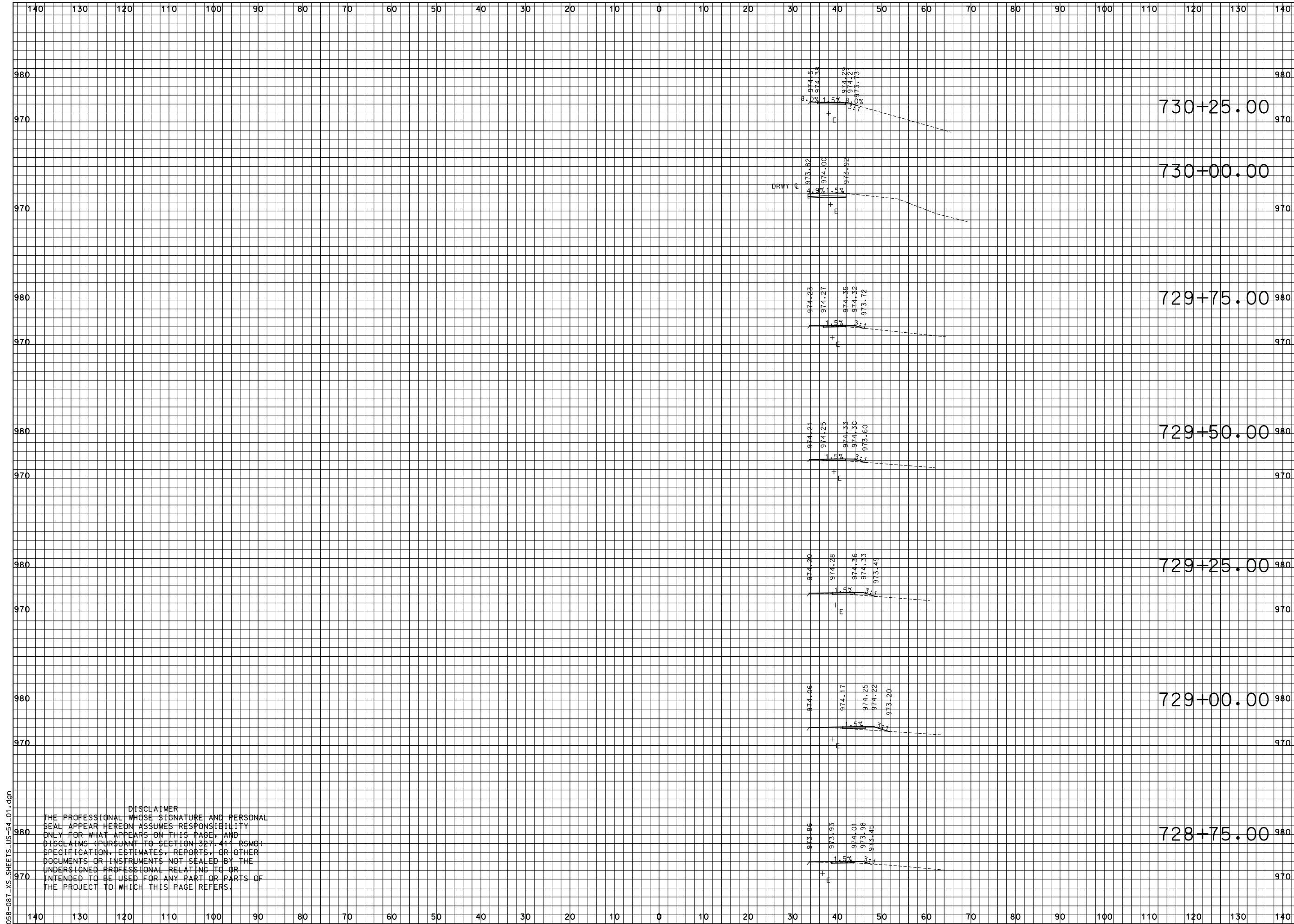
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 SHEET 28 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



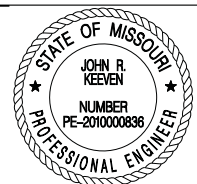
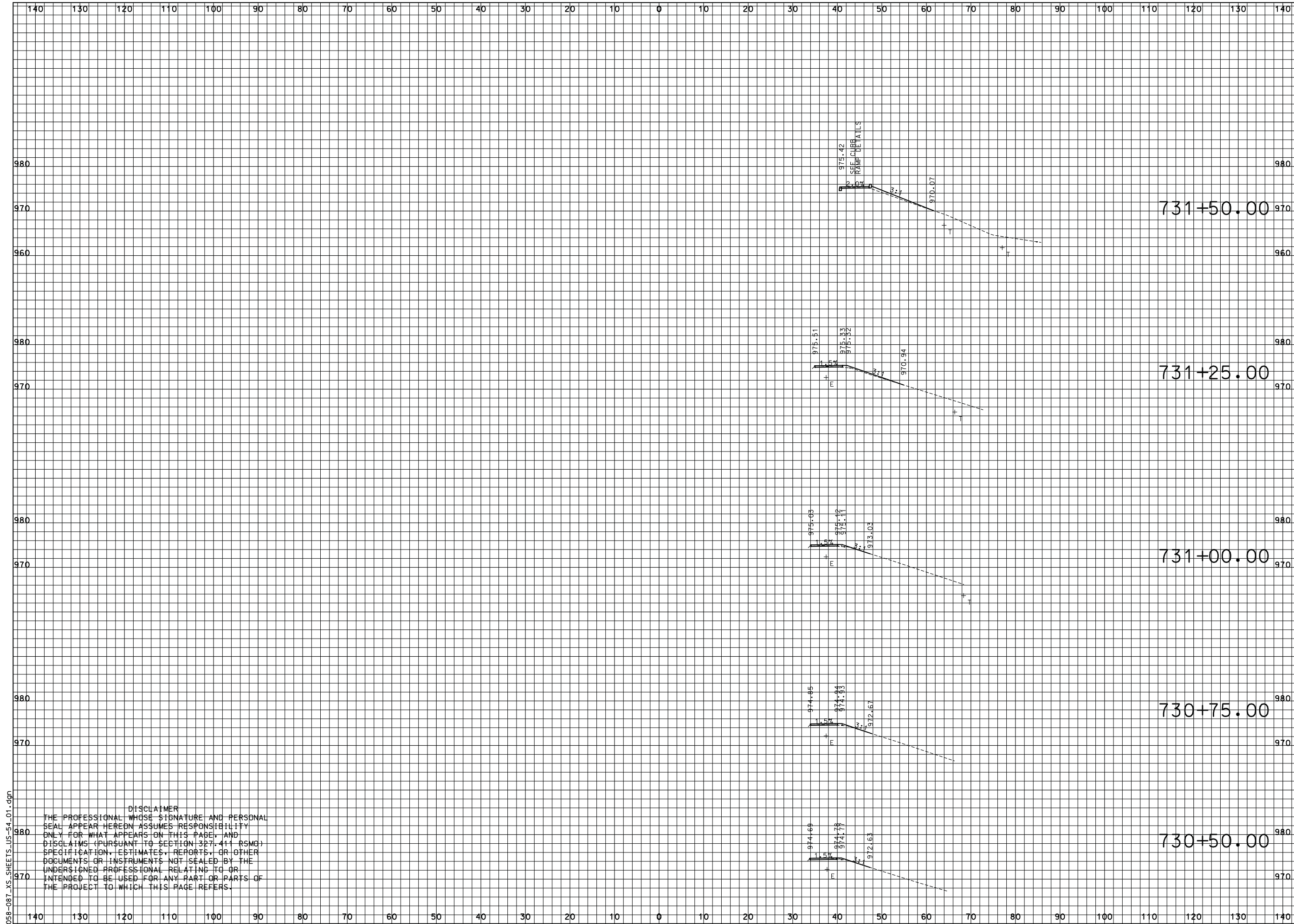
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SHEET 30 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

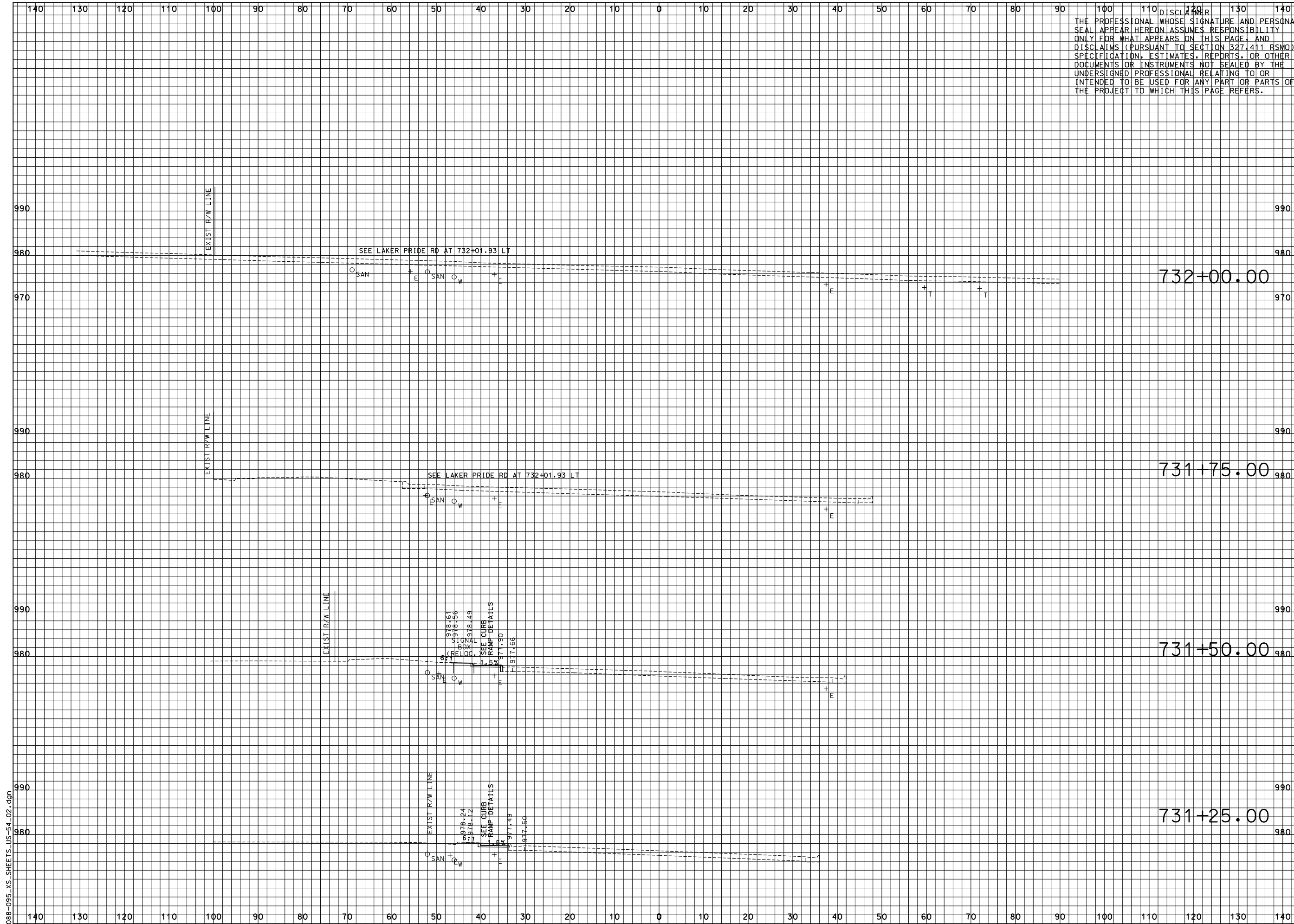


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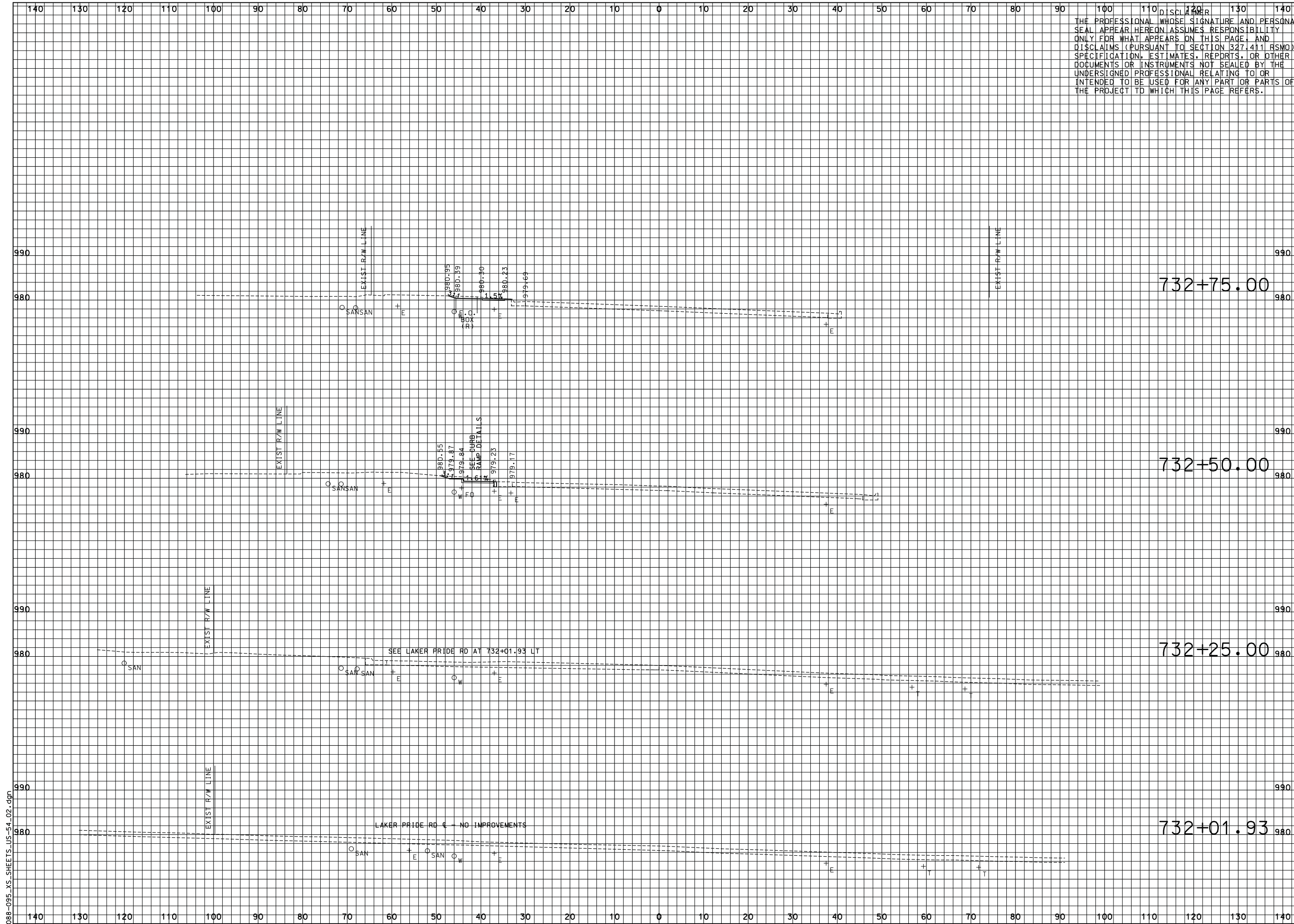
CROSS SECTIONS
 SHEET 31 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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NUMBER	BY	DATE

CROSS SECTIONS
 SHEET 32 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

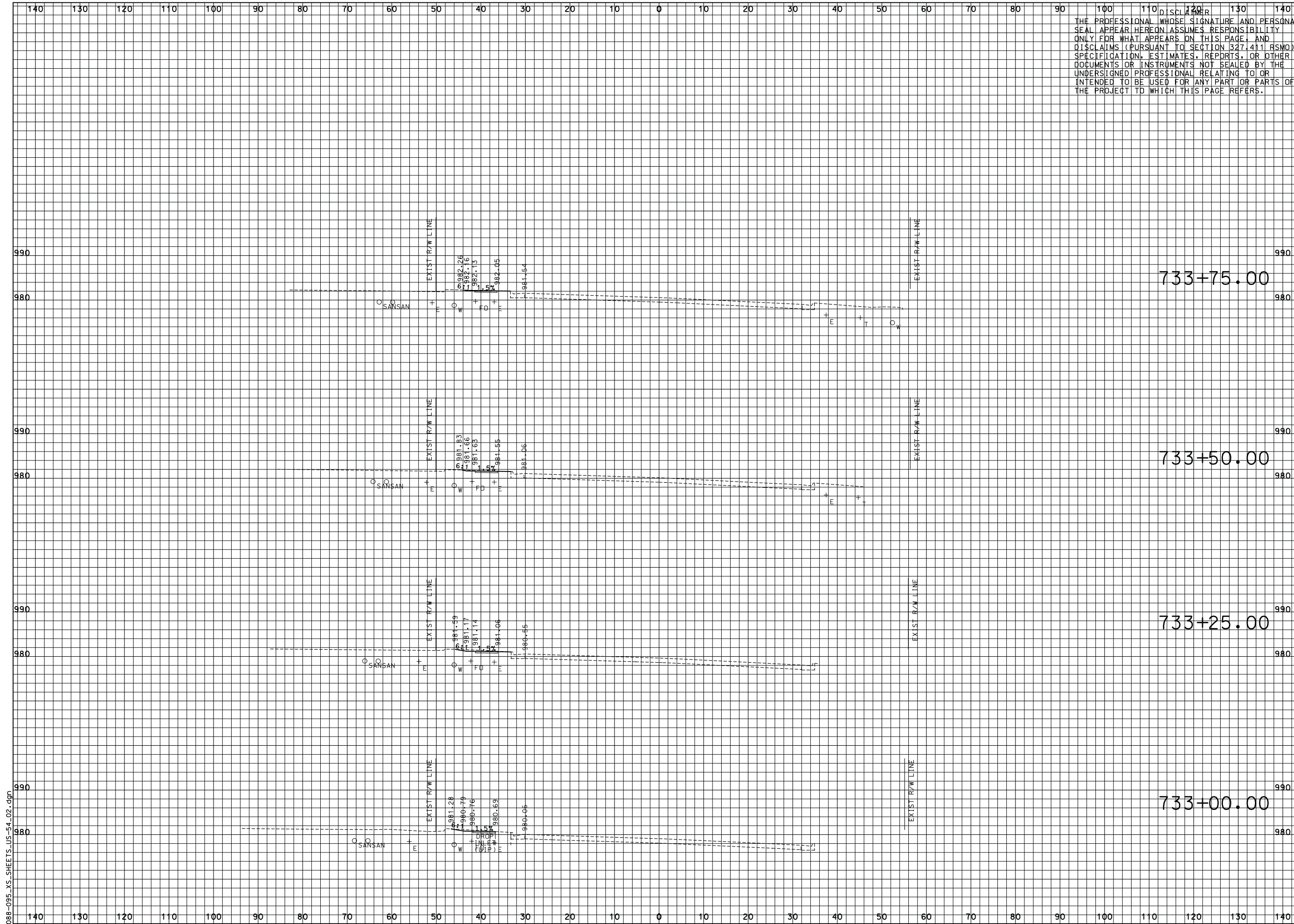


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 SHEET 33 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

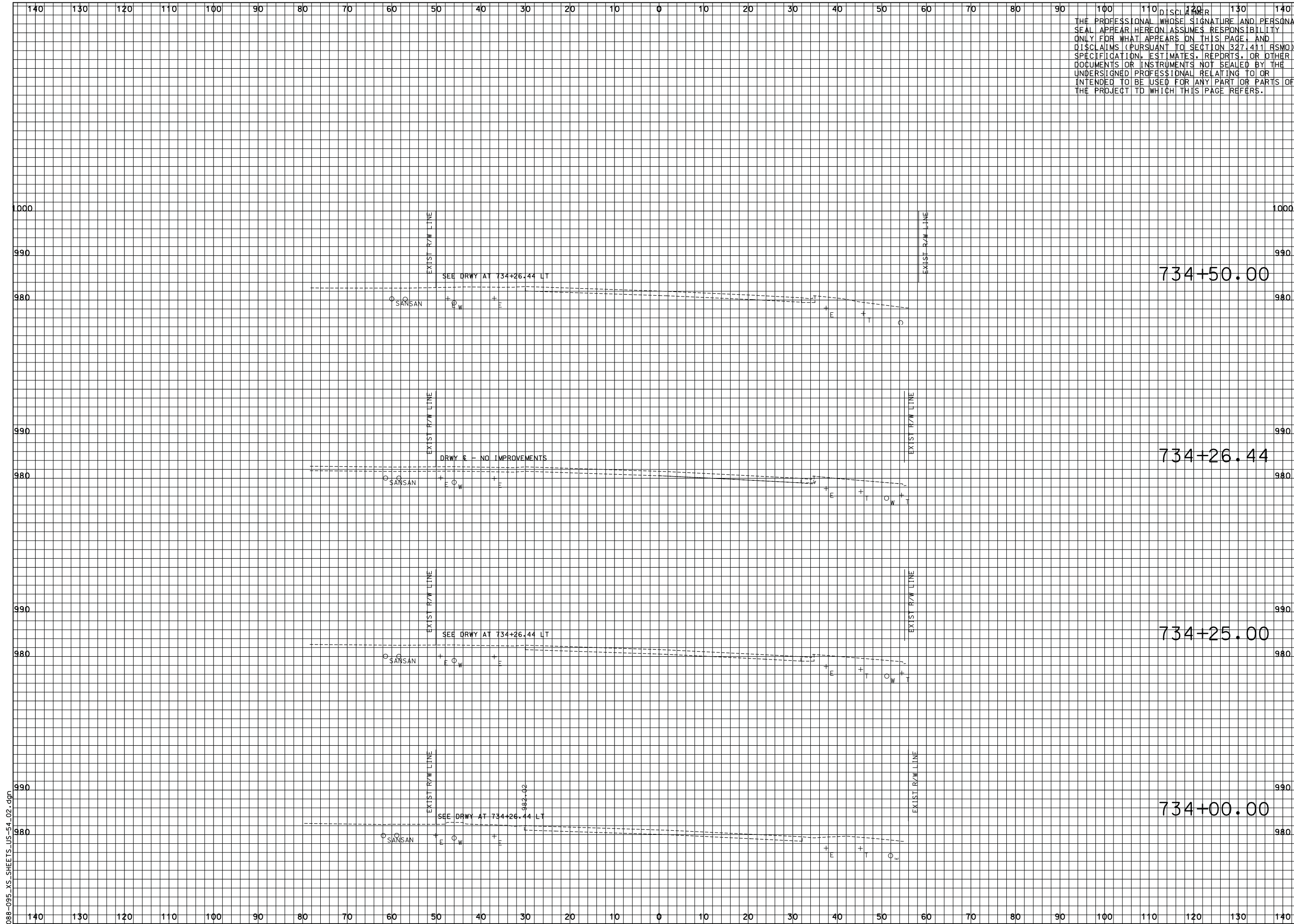


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CROSS SECTIONS
 SHEET 34 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

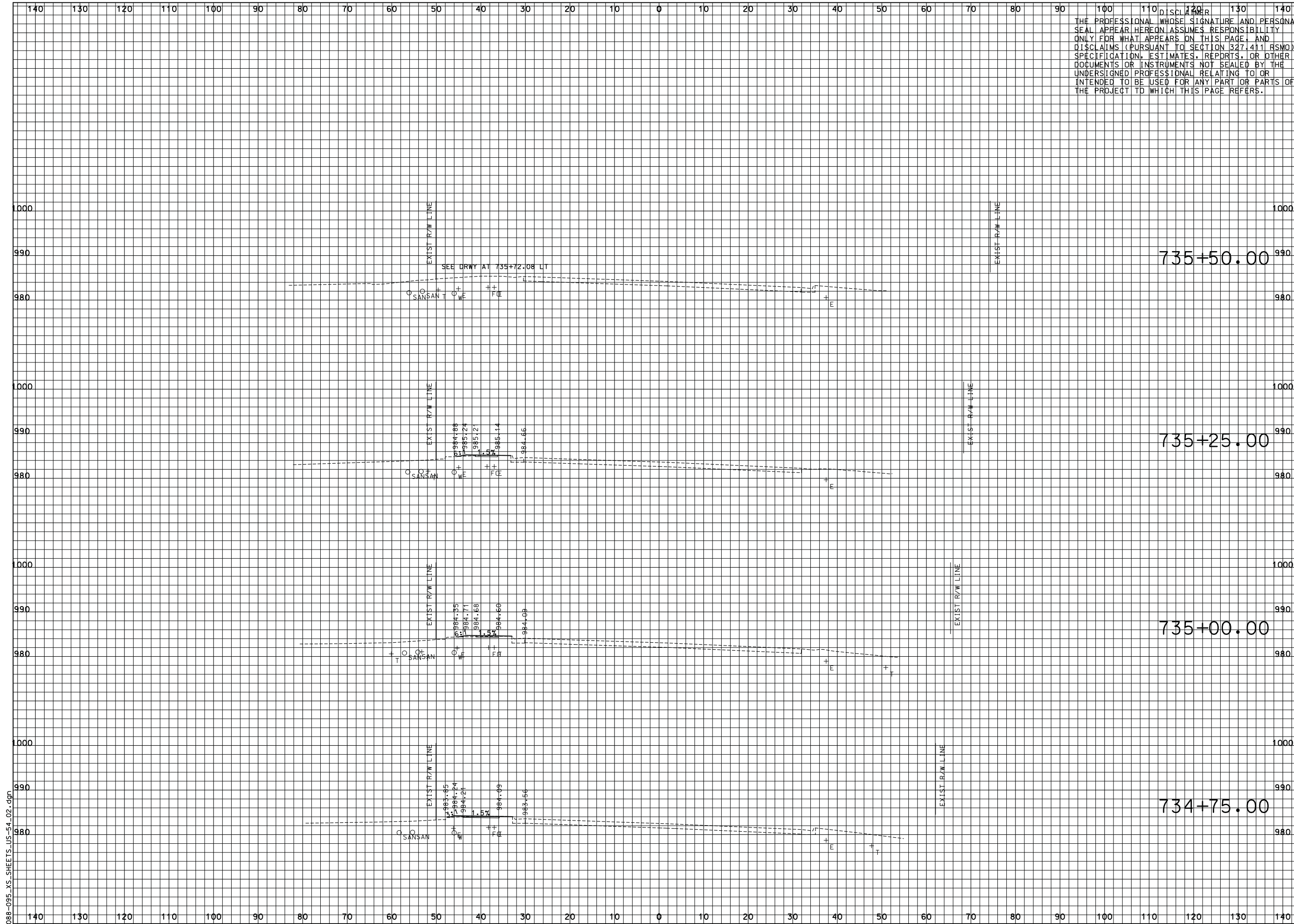


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NUMBER	BY	DATE

CROSS SECTIONS

SHEET 35 OF 65

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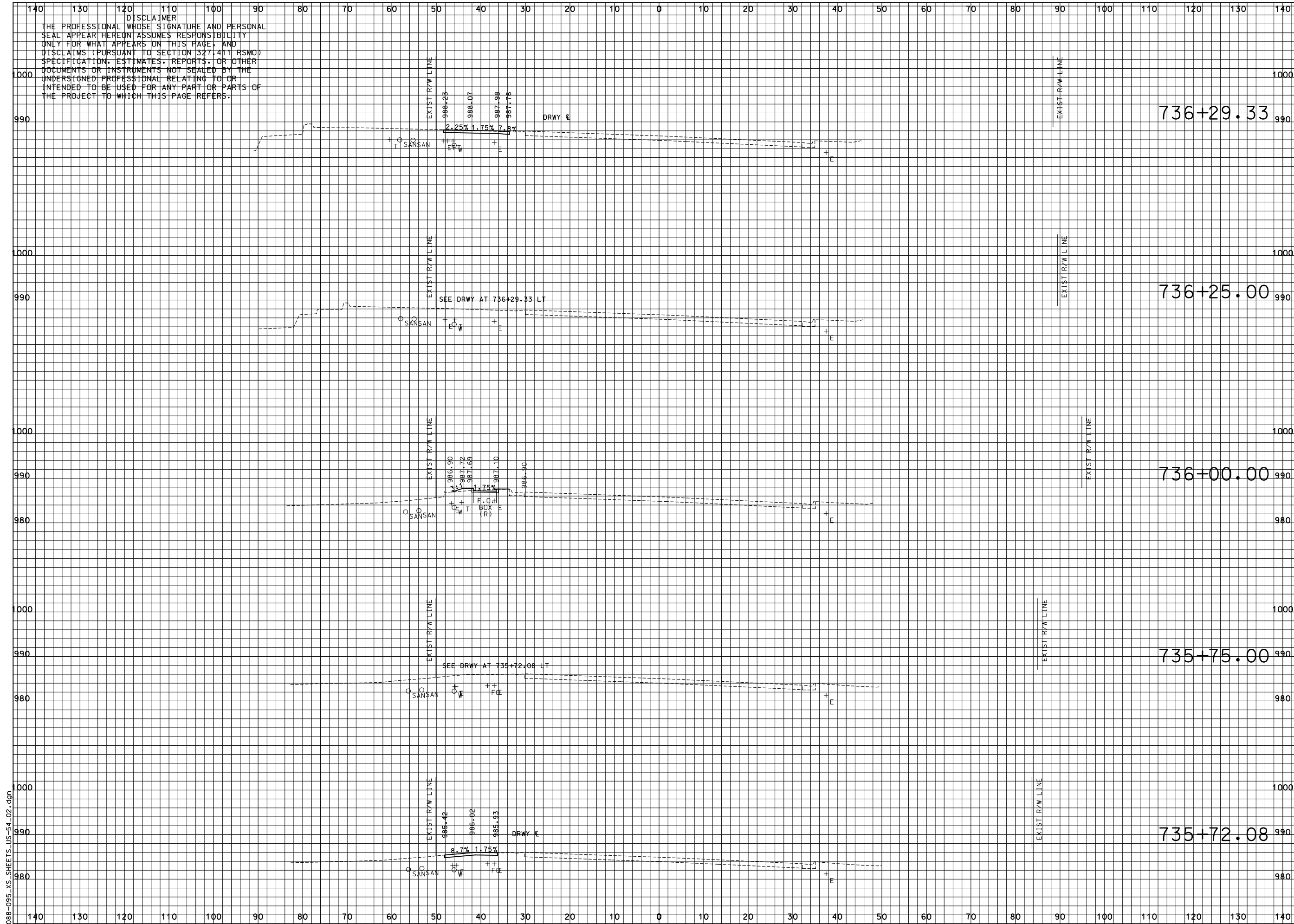
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SHEET 92 OF 122

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NUMBER	BY	DATE

CROSS SECTIONS

SHEET 36 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



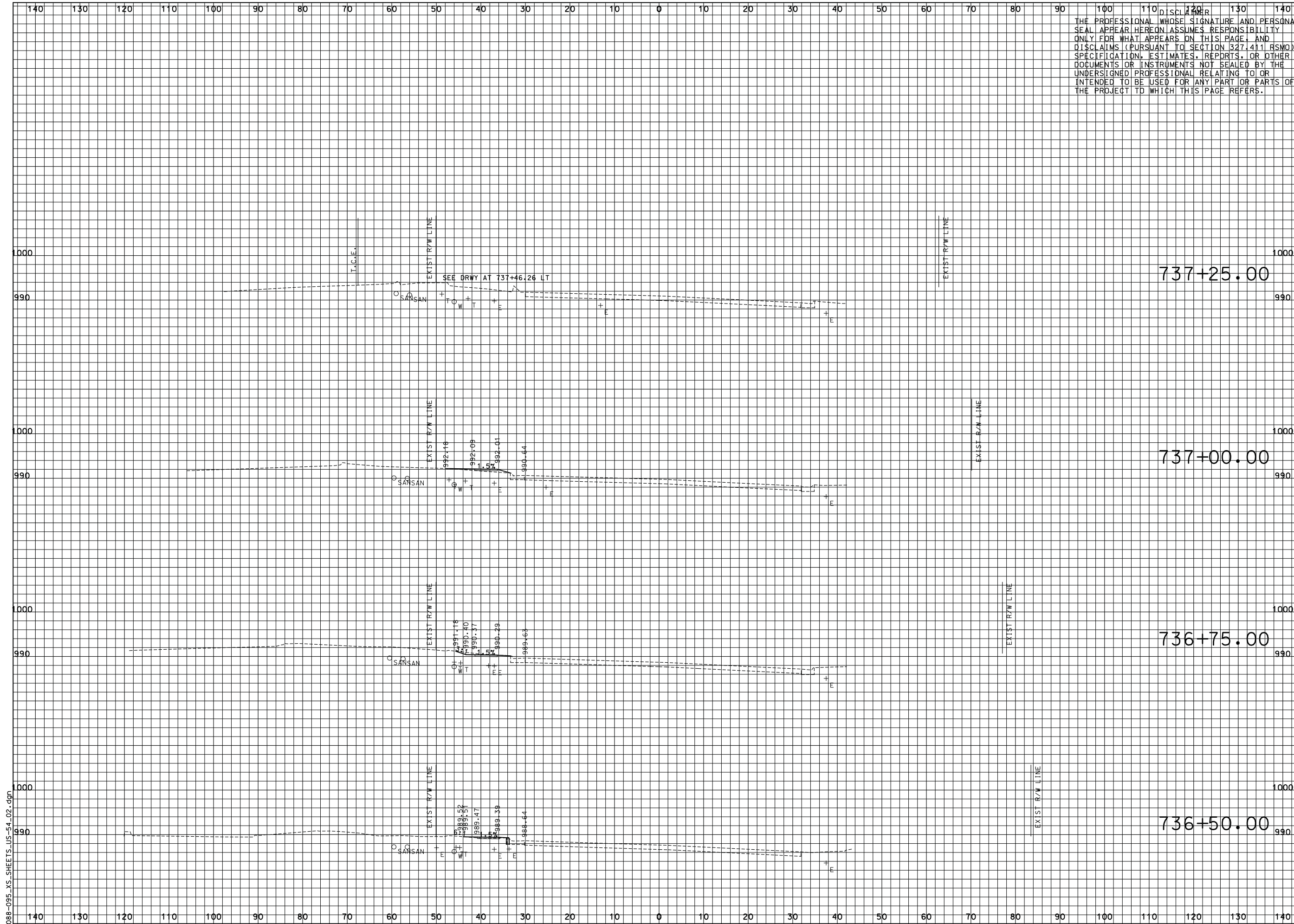
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REVISIONS		
NUMBER	BY	DATE

CROSS SECTIONS
 SHEET 37 OF 65

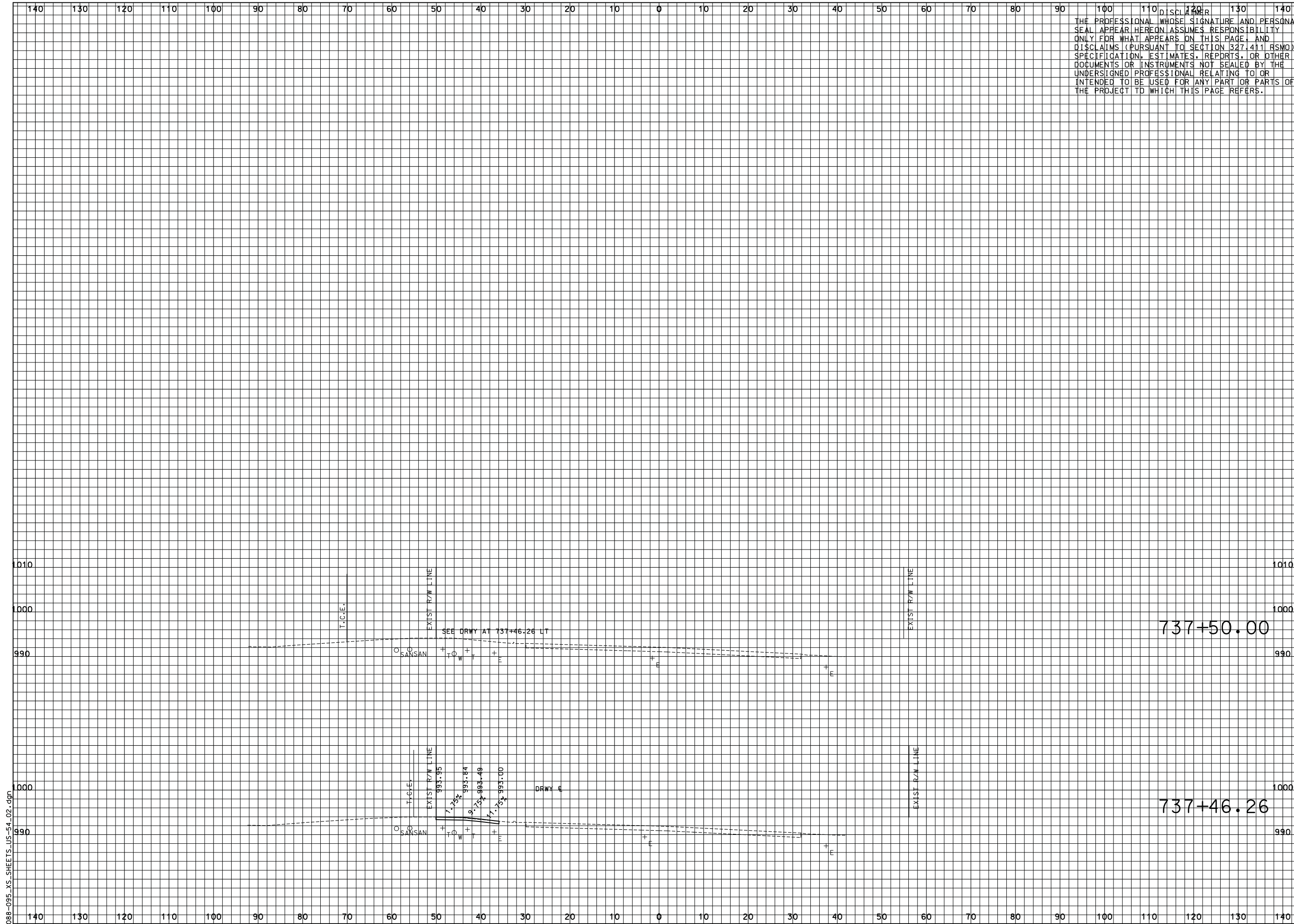
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 US-54 SIDEWALK PROJECT
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REVISIONS		
NUMBER	BY	DATE

CROSS SECTIONS
 SHEET 38 OF 65

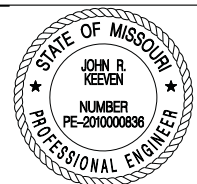
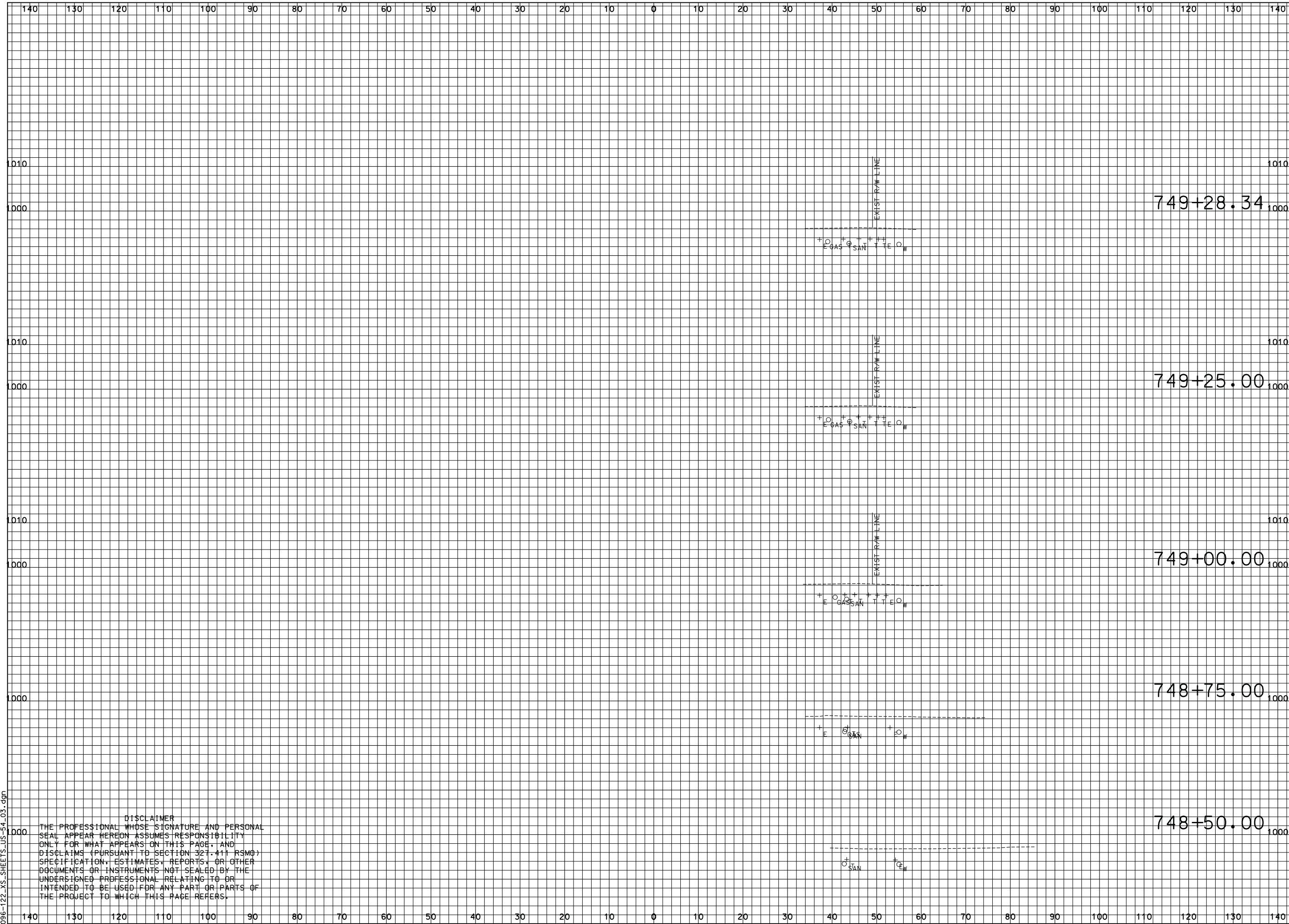
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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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REVISIONS		
NUMBER	BY	DATE

CROSS SECTIONS
SHEET 39 OF 65

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



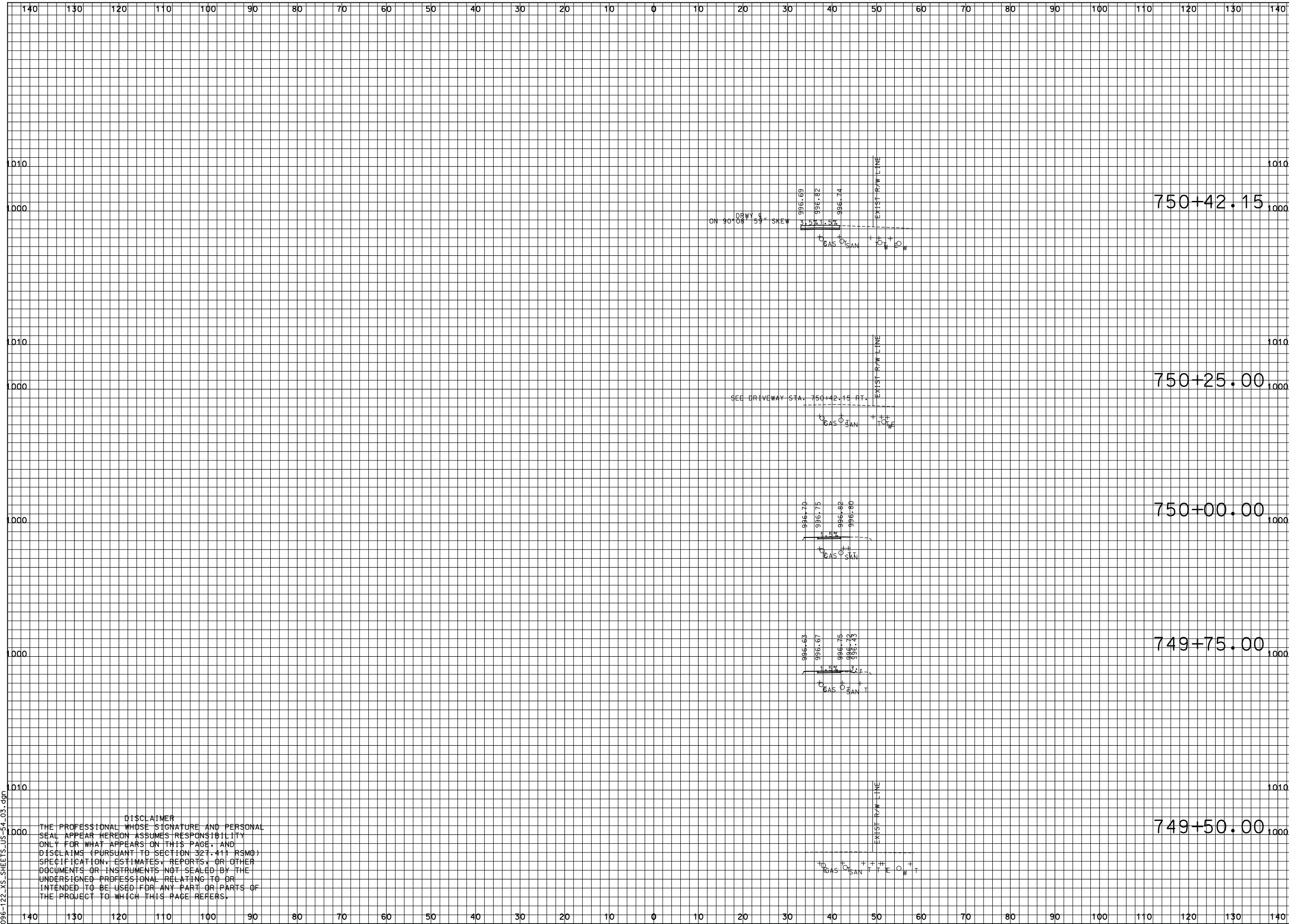
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JOB No: 15480-02-00
15480-03-00

SHEET 96 OF 122

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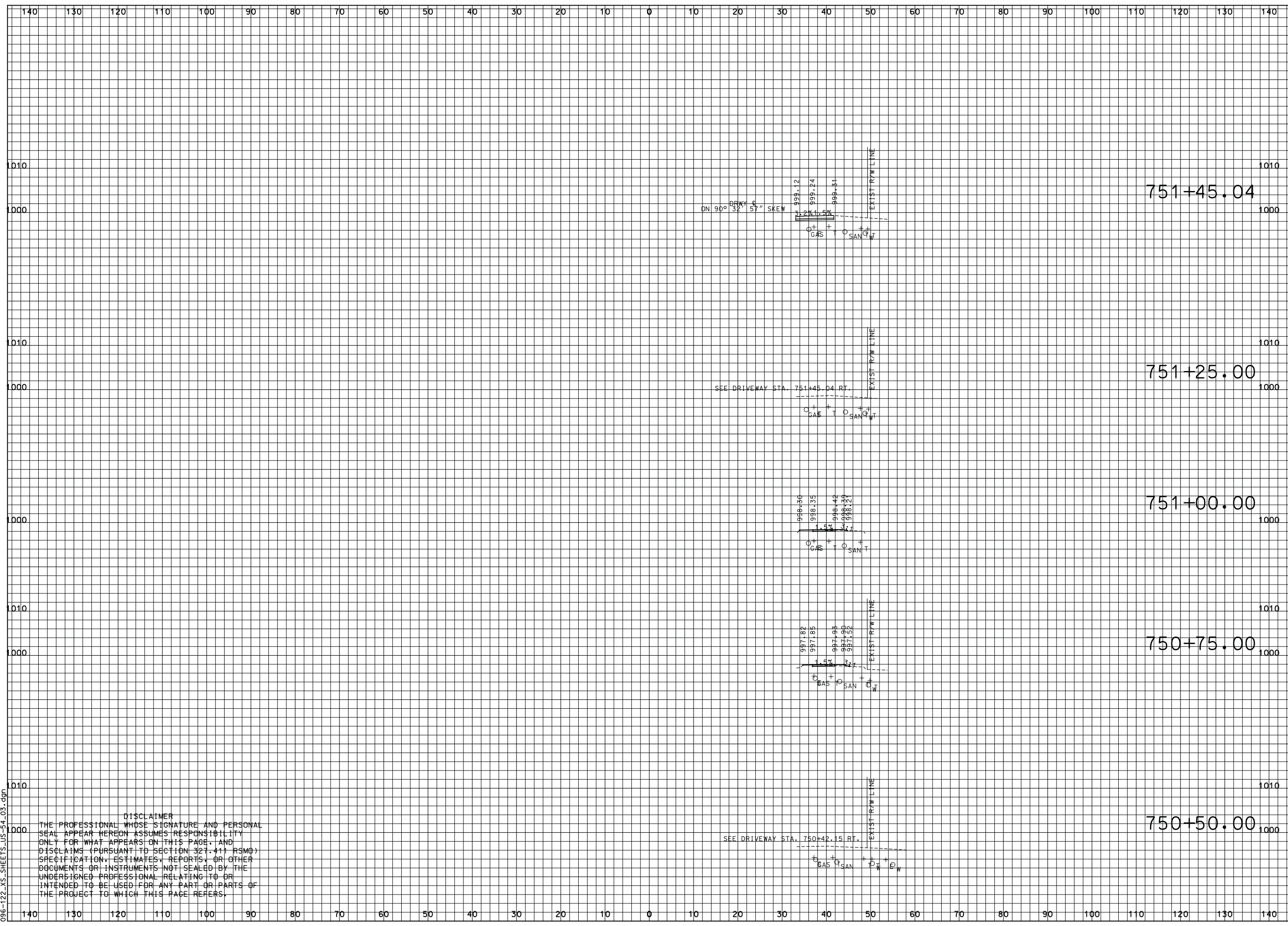
CROSS SECTIONS
 SHEET 40 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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 SHEET 41 OF 65

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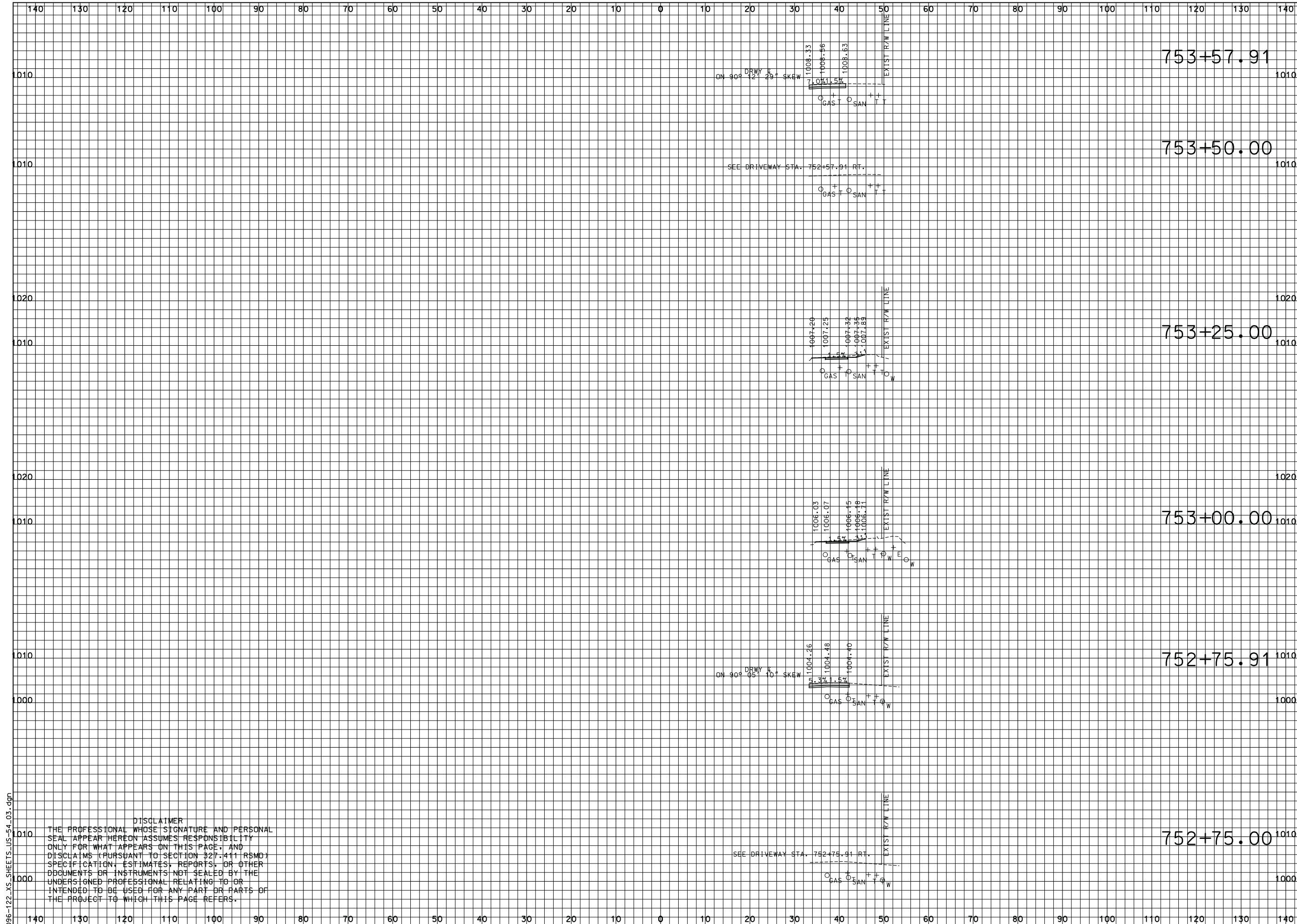
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 SHEET 42 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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CROSS SECTIONS
SHEET 43 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

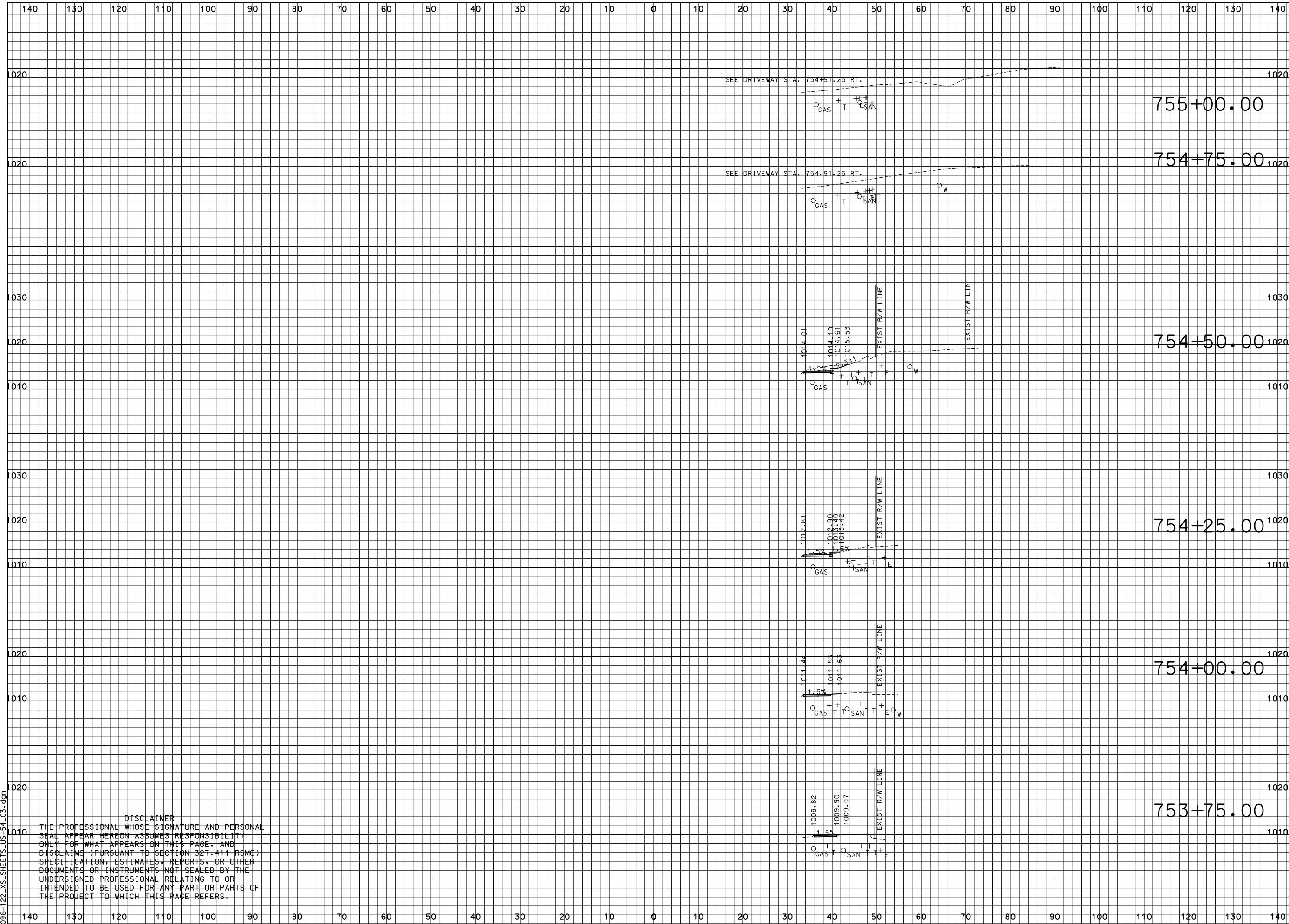


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NUMBER	BY	DATE

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 SHEET 44 OF 65

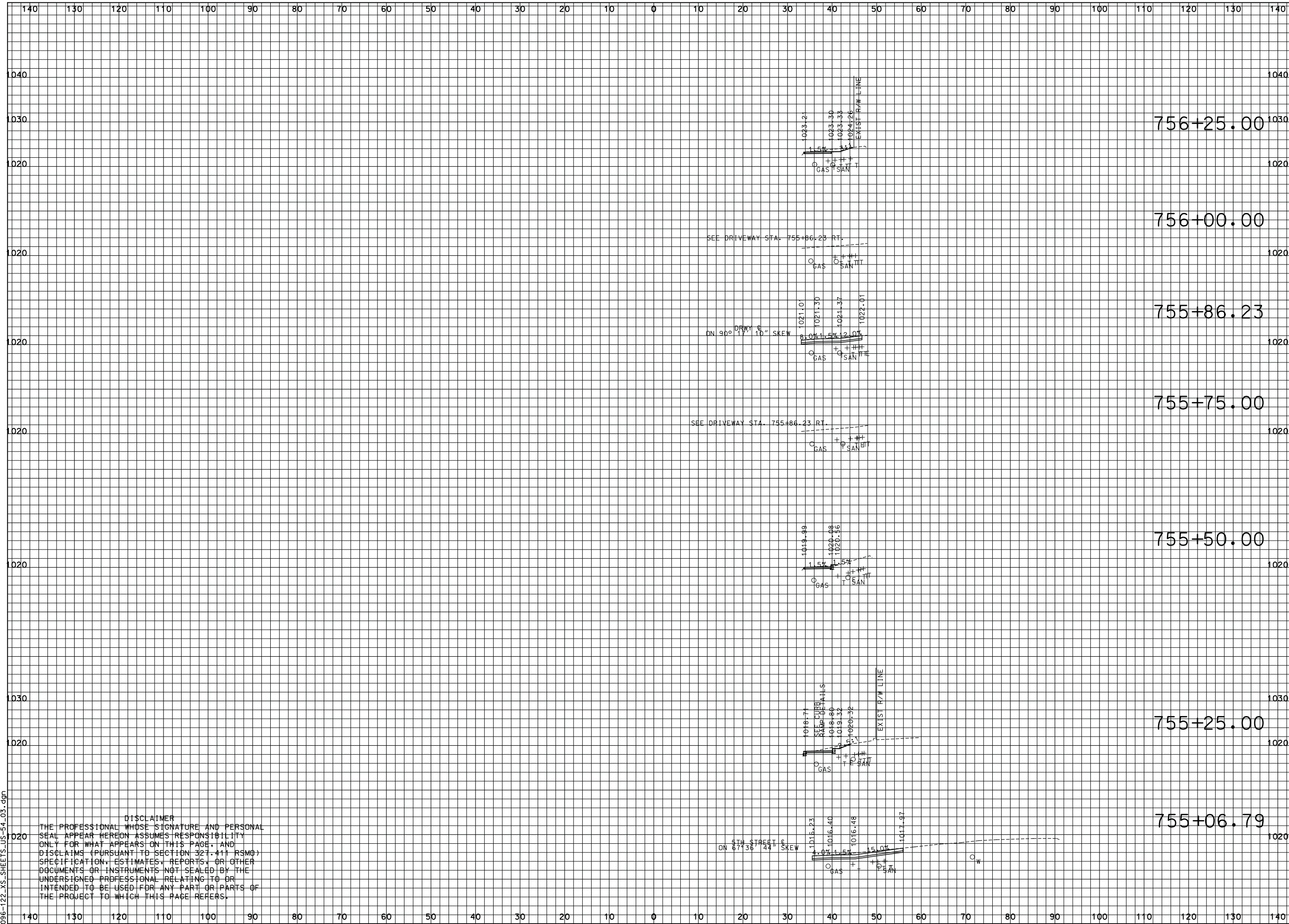
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REVISIONS		
NUMBER	BY	DATE

CROSS SECTIONS
SHEET 45 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

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NUMBER	BY	DATE

CROSS SECTIONS
SHEET 46 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

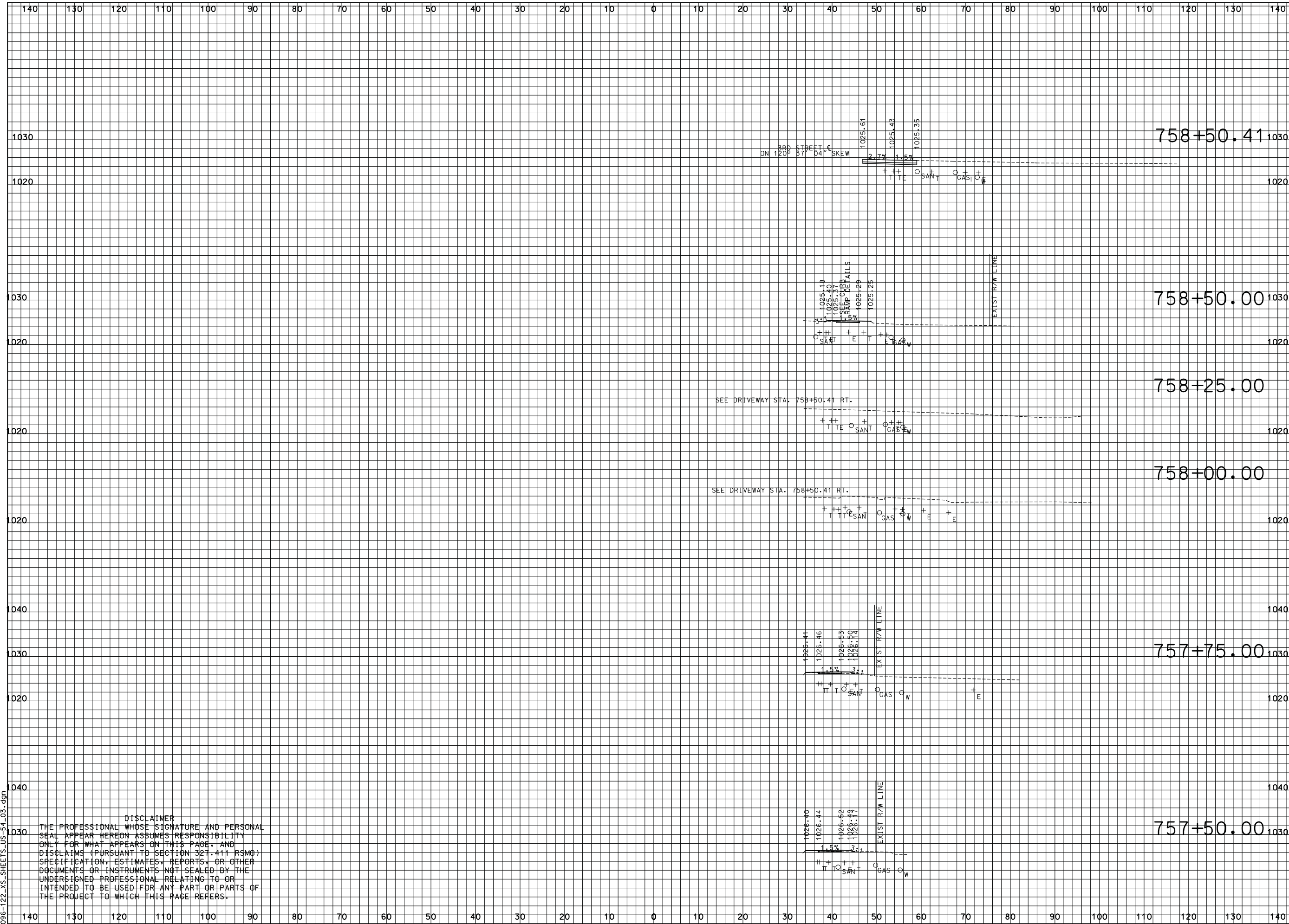


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NUMBER	BY	DATE

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 SHEET 47 OF 65

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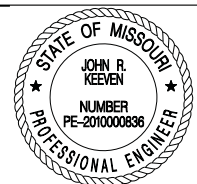
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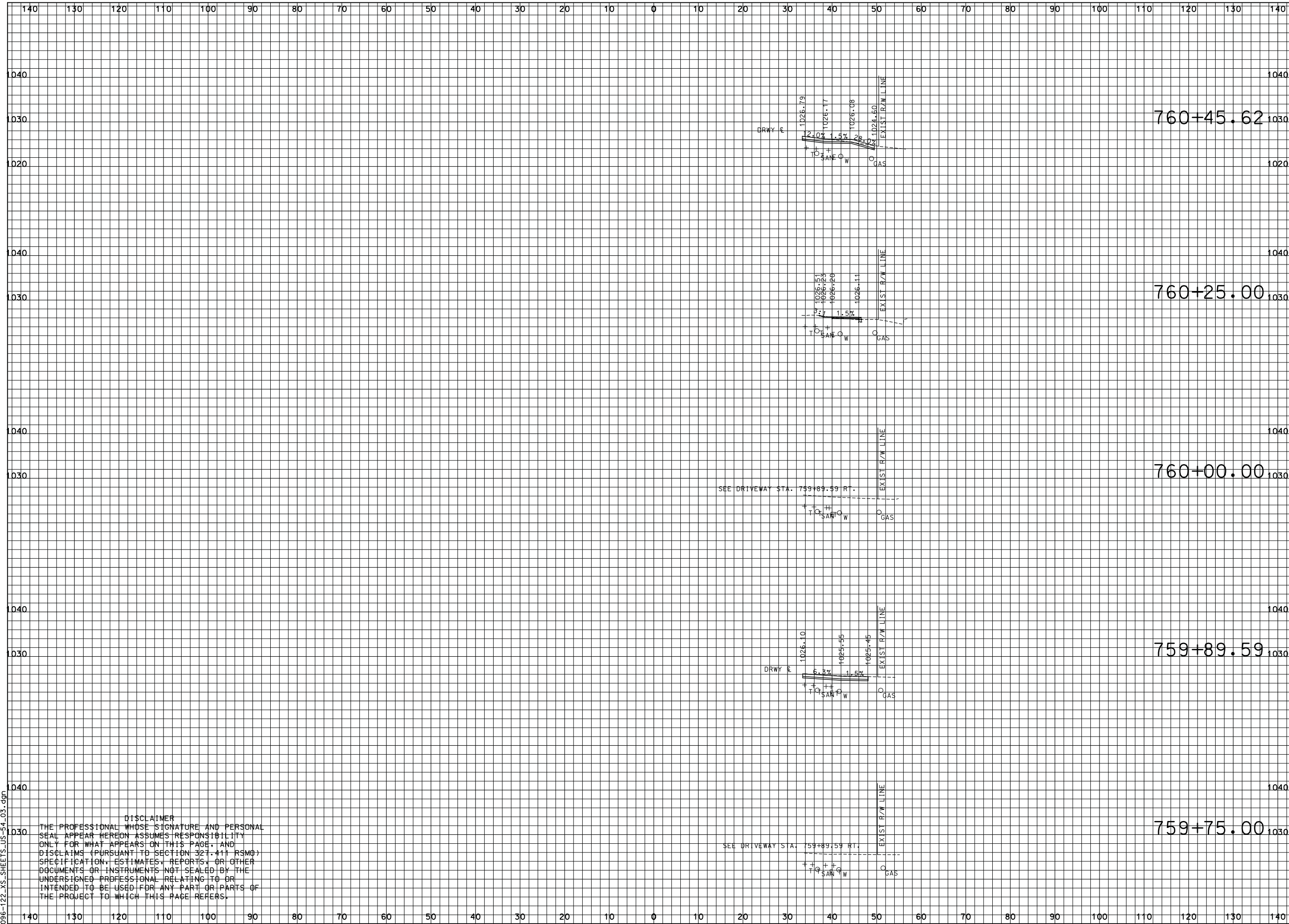
CROSS SECTIONS
 SHEET 48 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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NUMBER	BY	DATE

CROSS SECTIONS
SHEET 49 OF 65

CITY OF CAMDENTON, MISSOURI
US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

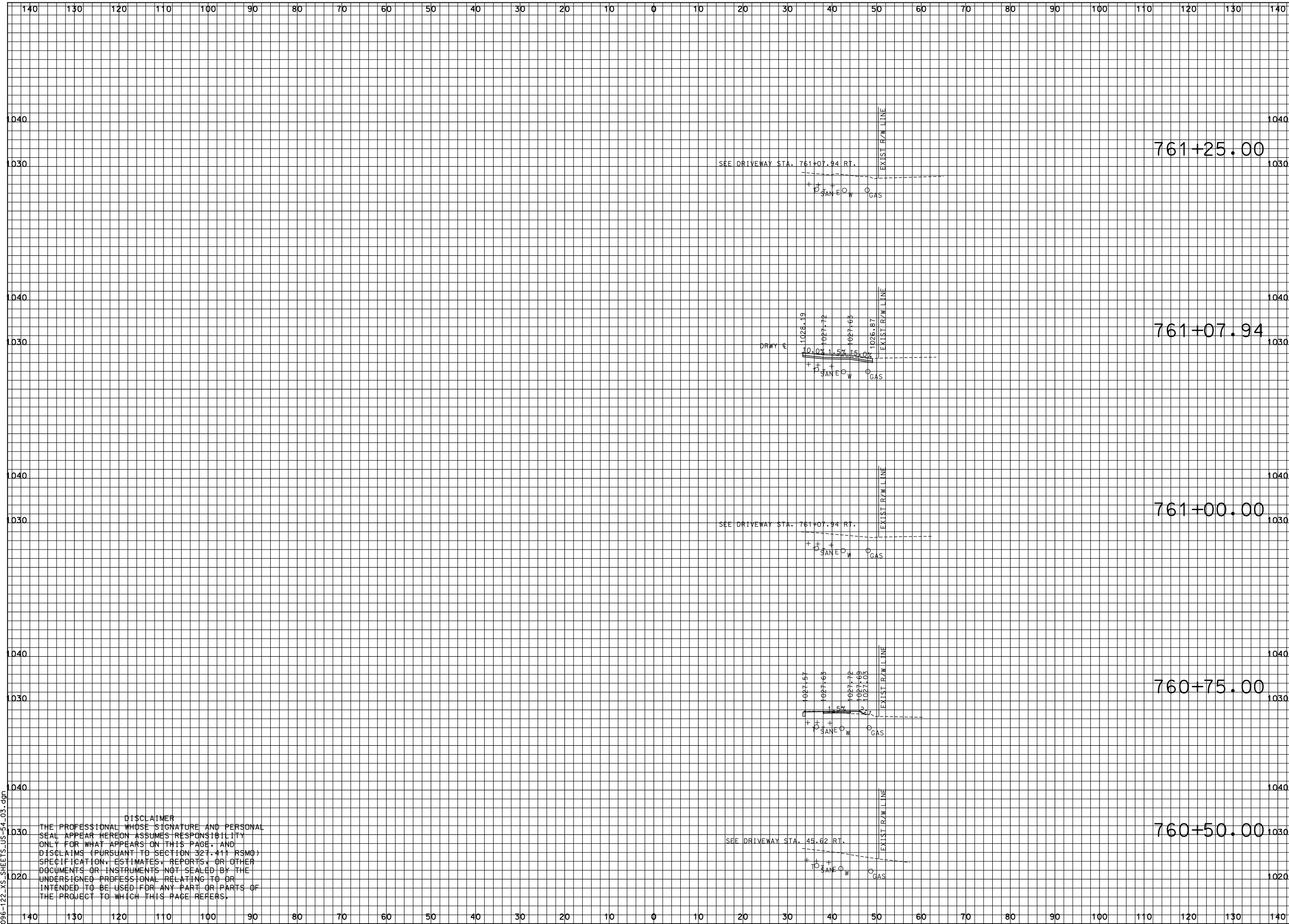


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SHEET 50 OF 65

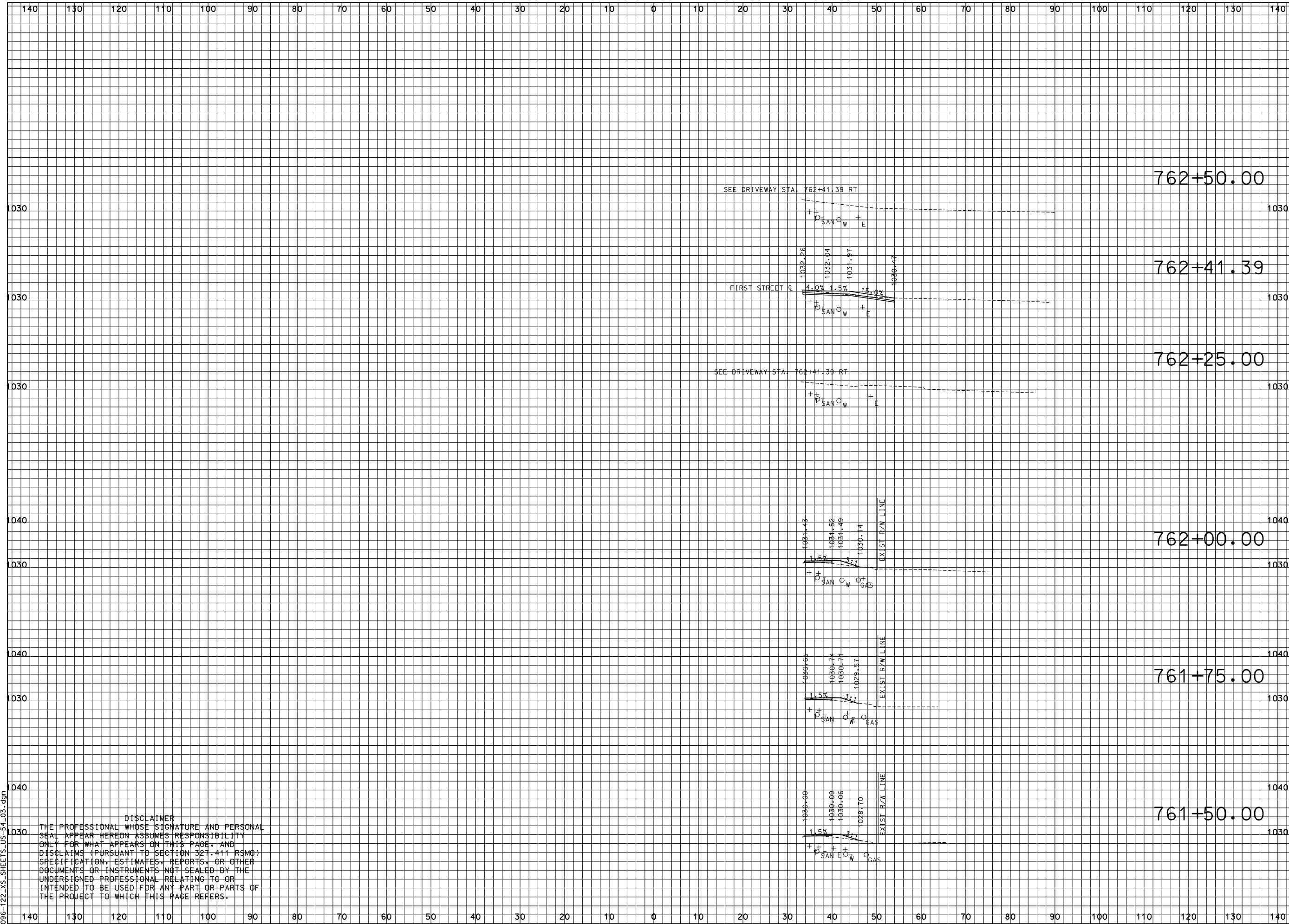
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CROSS SECTIONS
SHEET 51 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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JOB No: 15480-02-00
15480-03-00

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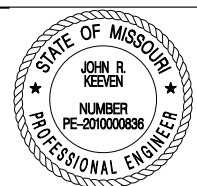
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 SHEET 52 OF 65

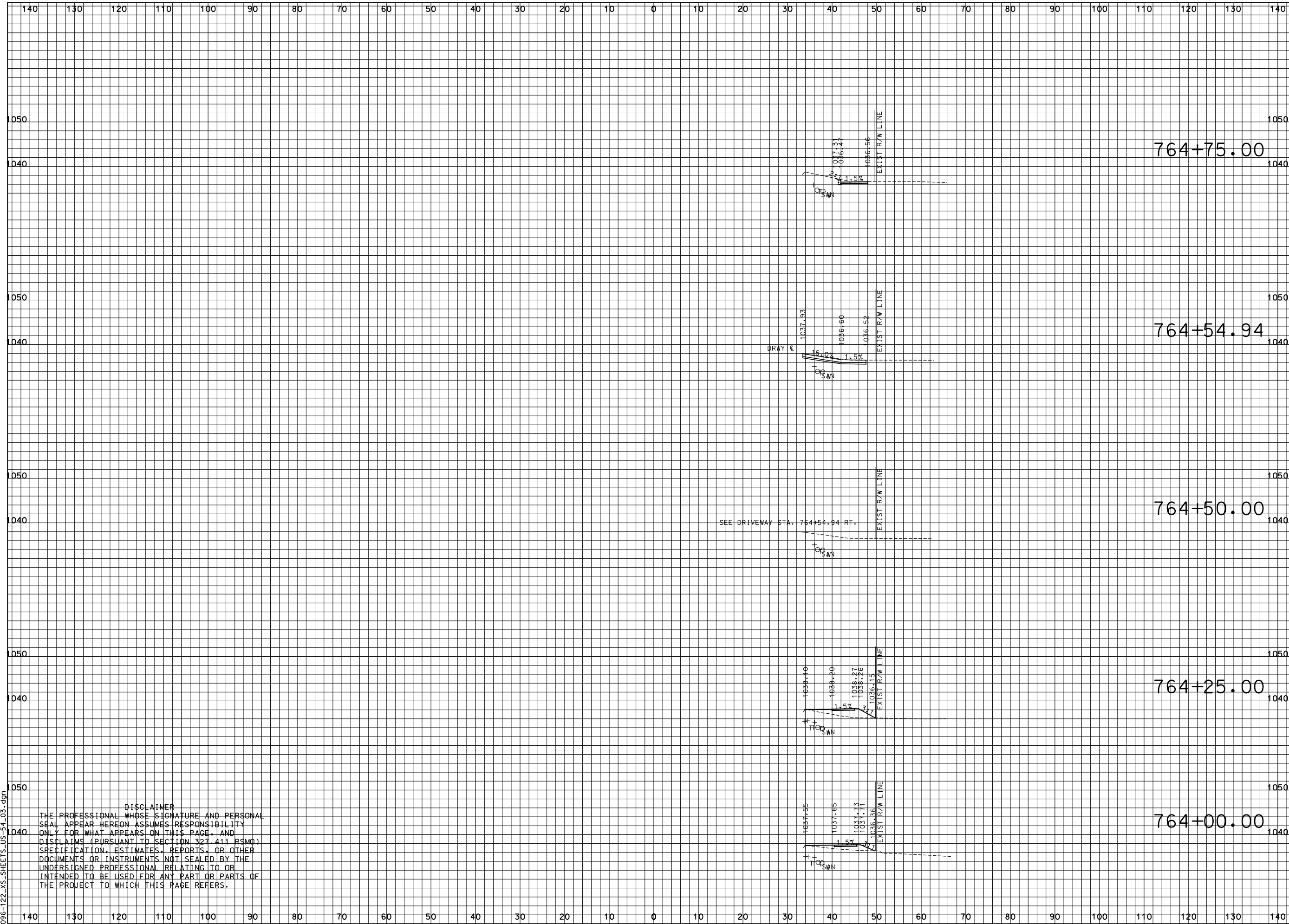
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NUMBER	BY	DATE

CROSS SECTIONS

SHEET 53 OF 65

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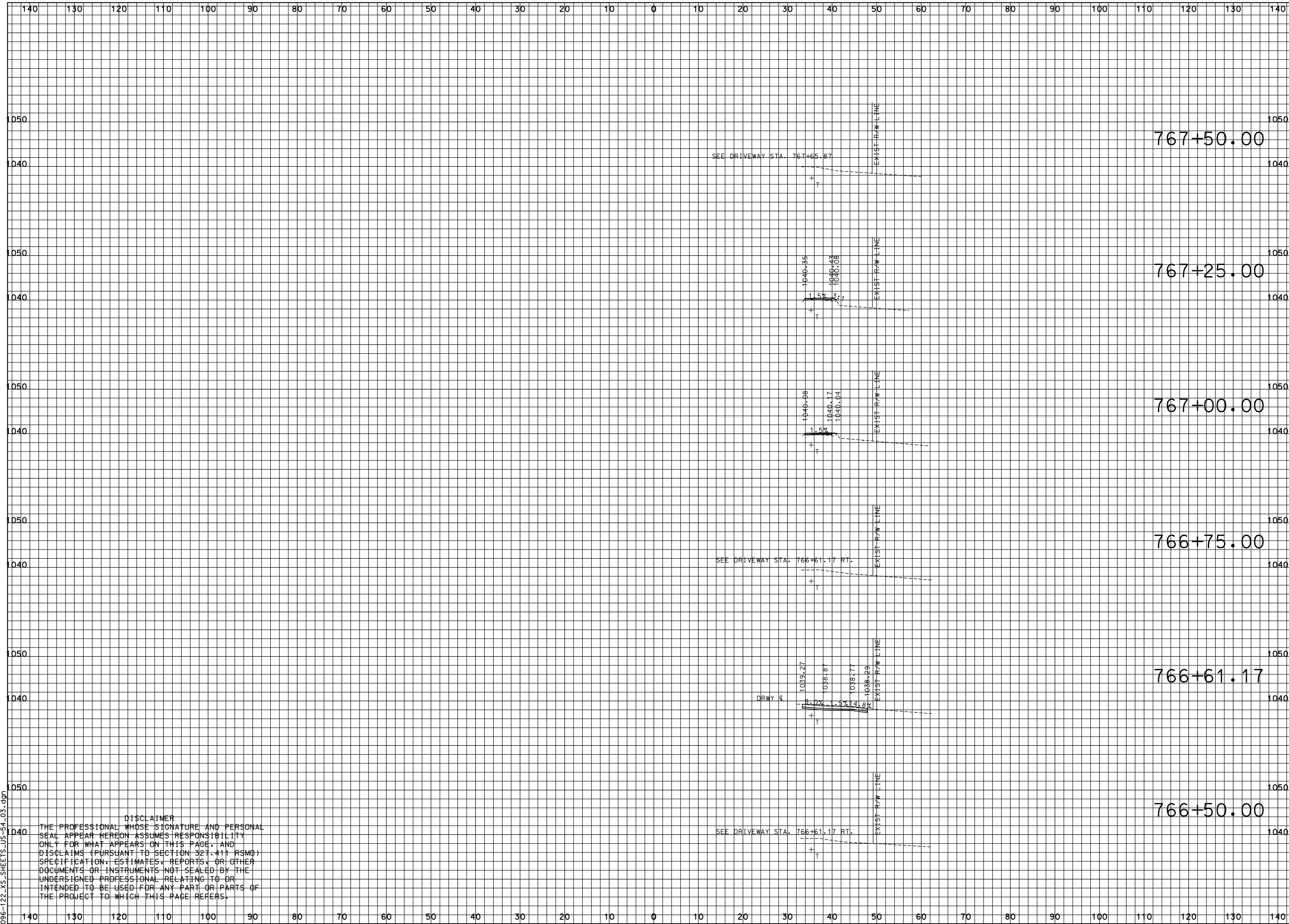
CROSS SECTIONS
 SHEET 54 OF 65

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 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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CROSS SECTIONS
SHEET 55 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



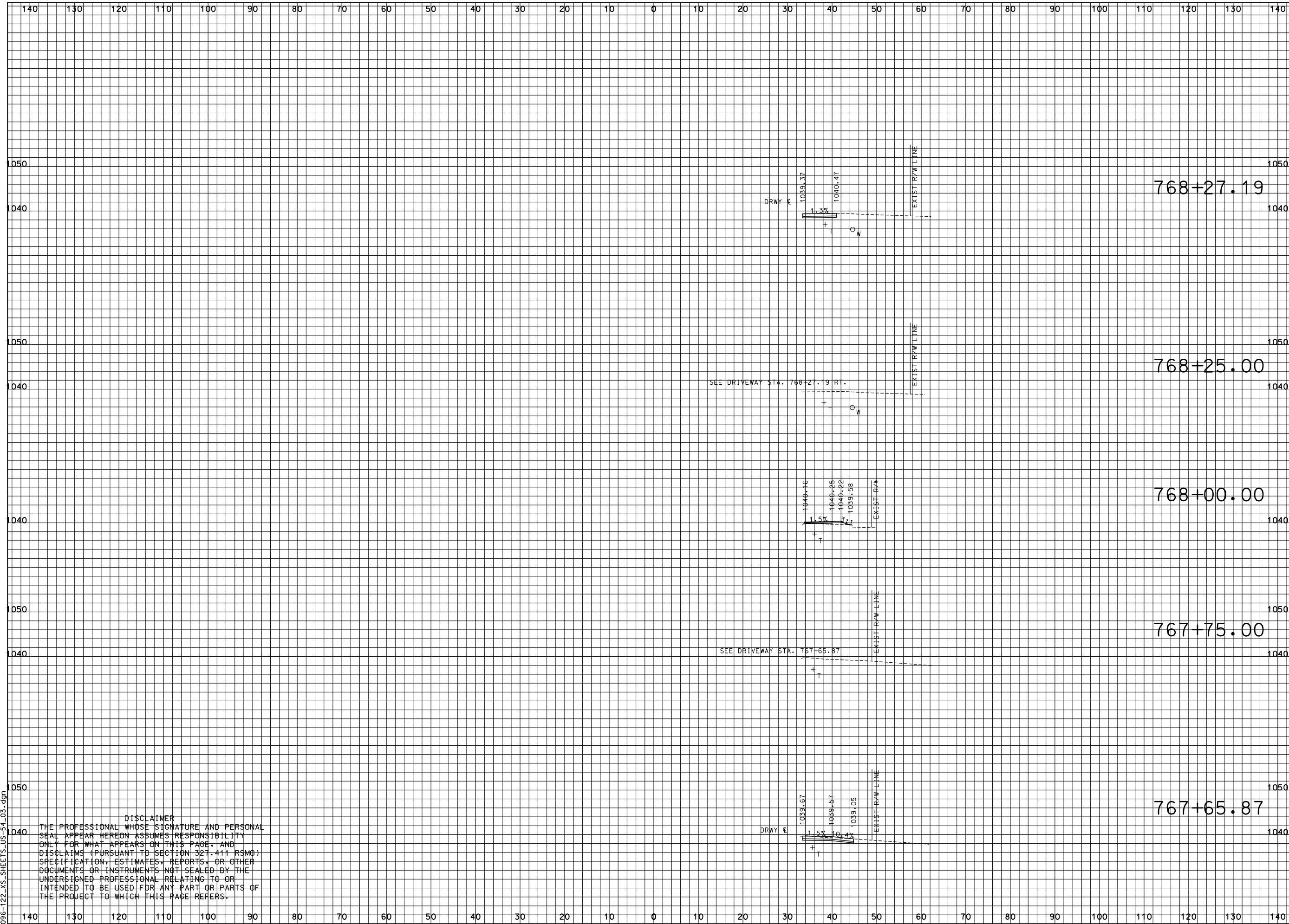
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NUMBER	BY	DATE

CROSS SECTIONS
SHEET 56 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

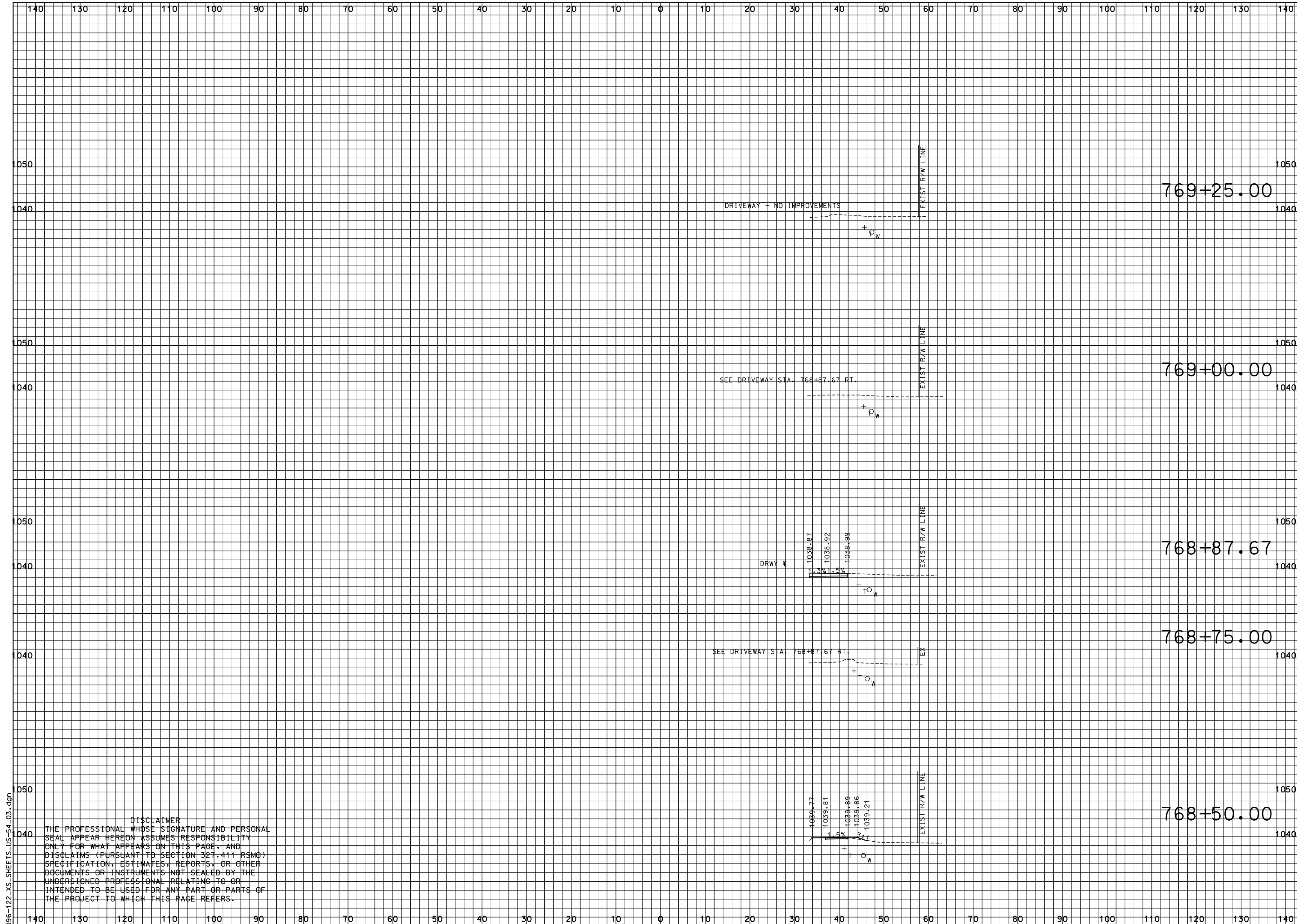


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15480-03-00

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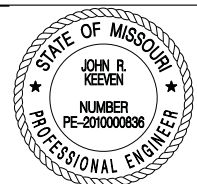
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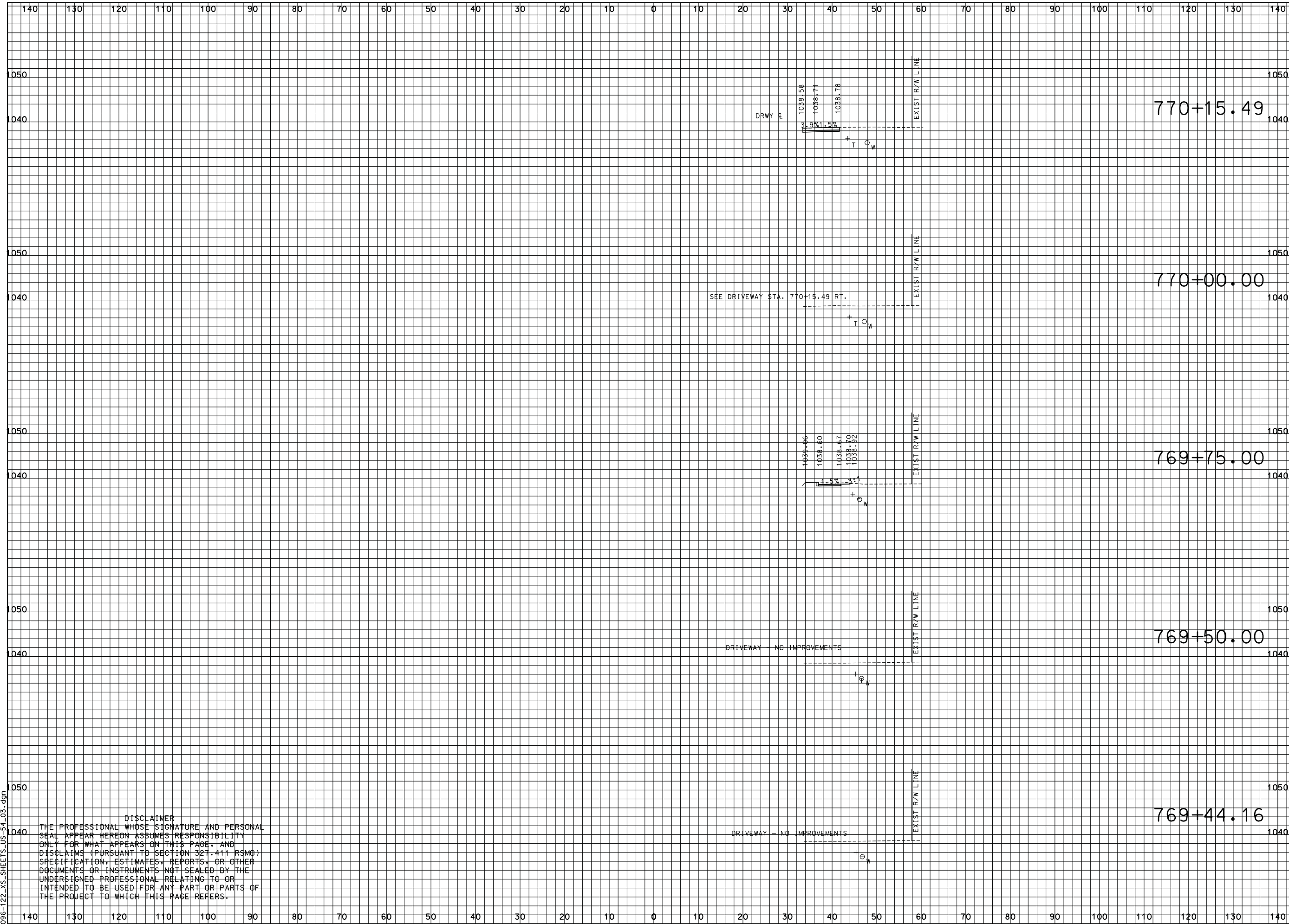
CROSS SECTIONS
 SHEET 57 OF 65

CITY OF CAMDENTON, MISSOURI
 US-54 SIDEWALK PROJECT
 PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)



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NUMBER	BY	DATE

CROSS SECTIONS
SHEET 58 OF 65

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US-54 SIDEWALK PROJECT
PHASE 1 TAP-9900 (563) & PHASE 2 TAP-9900 (529)

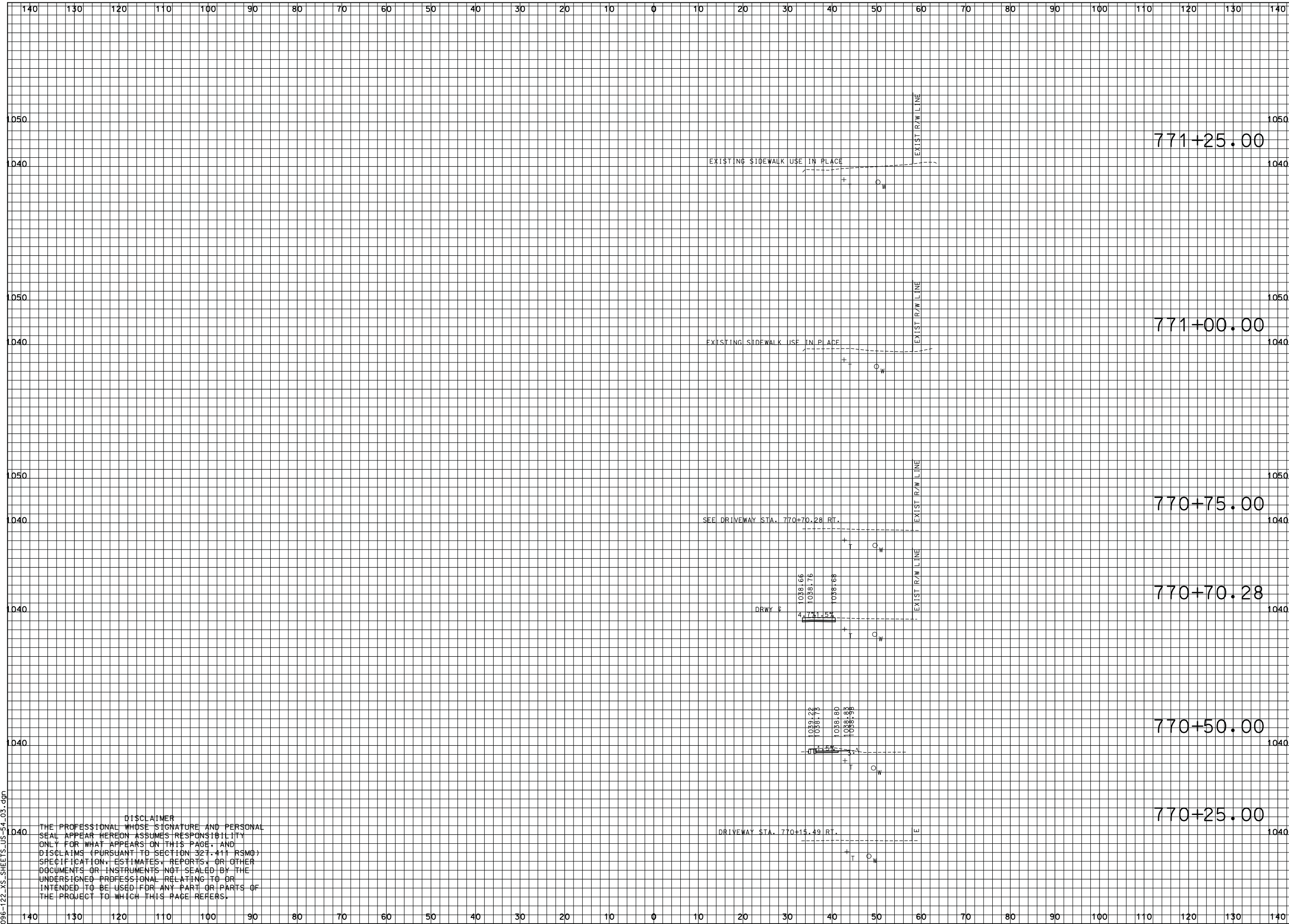


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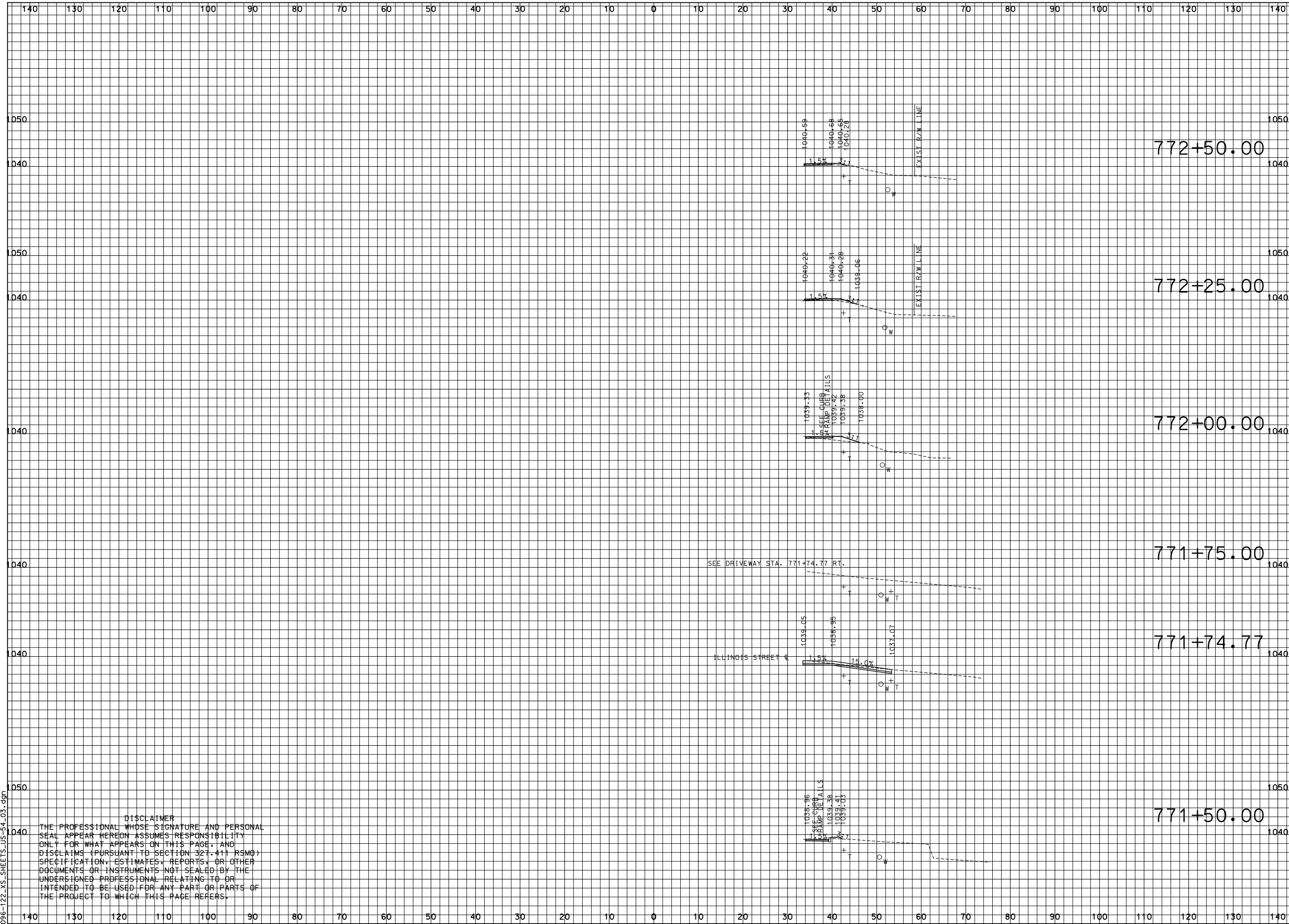
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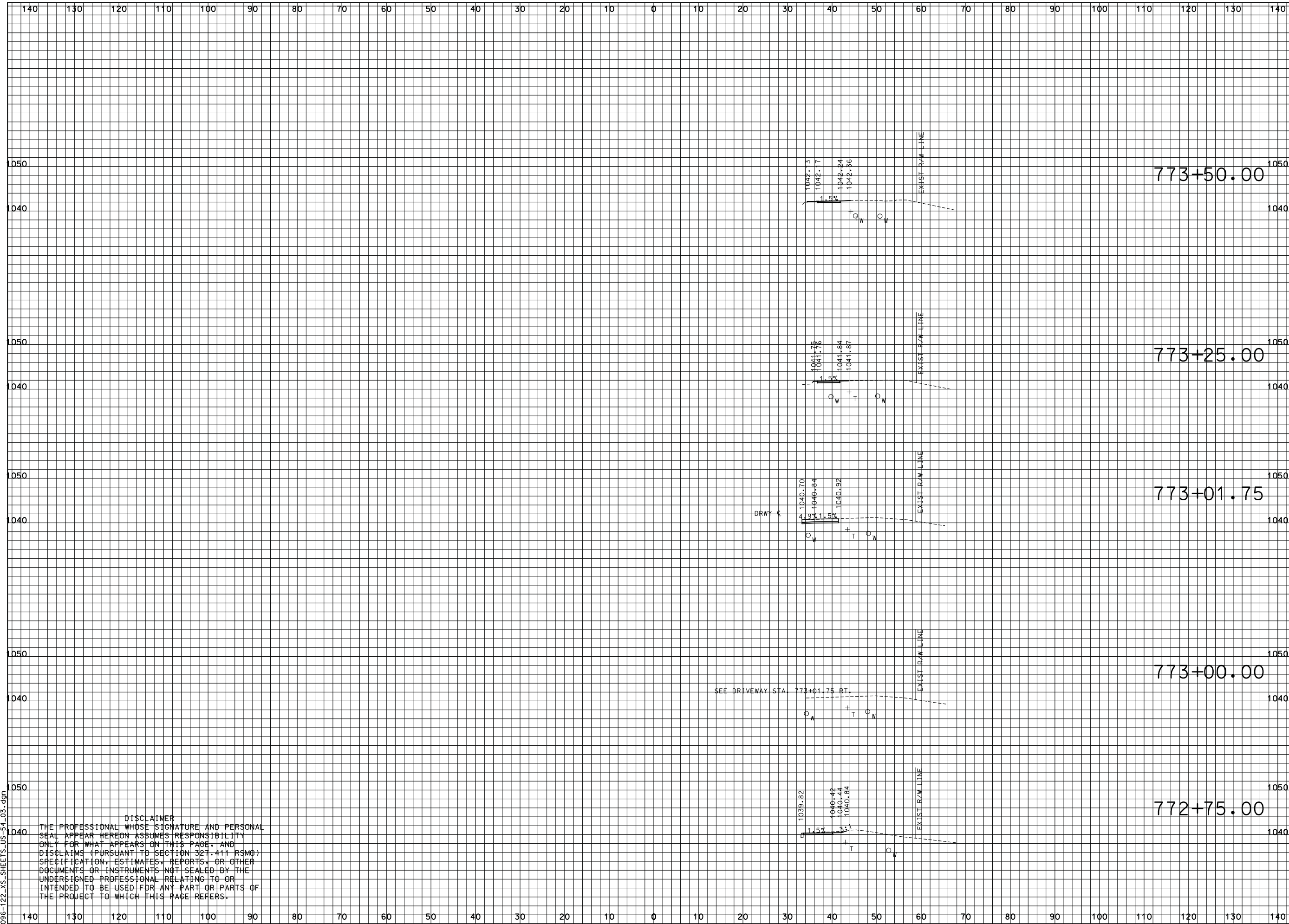
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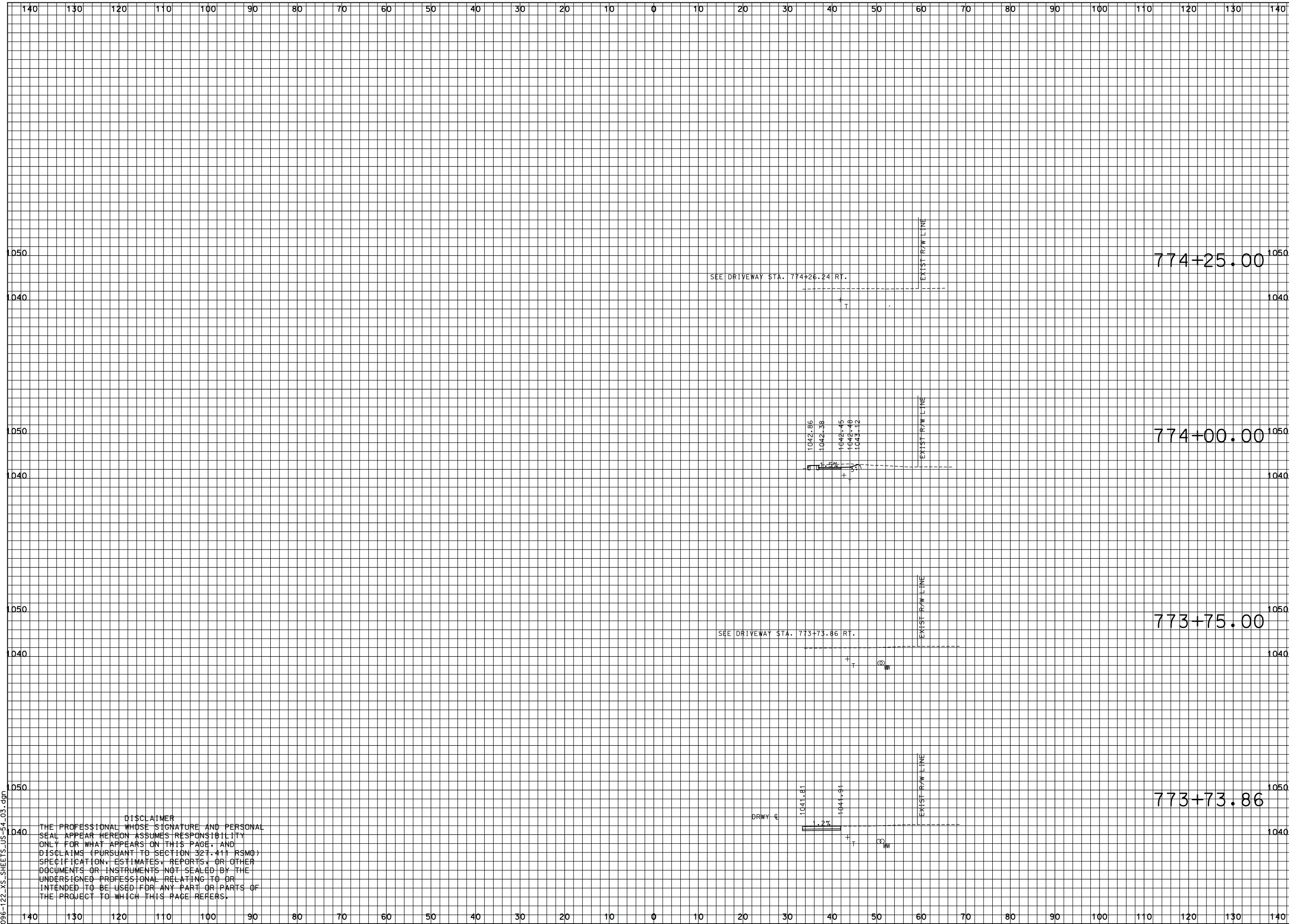
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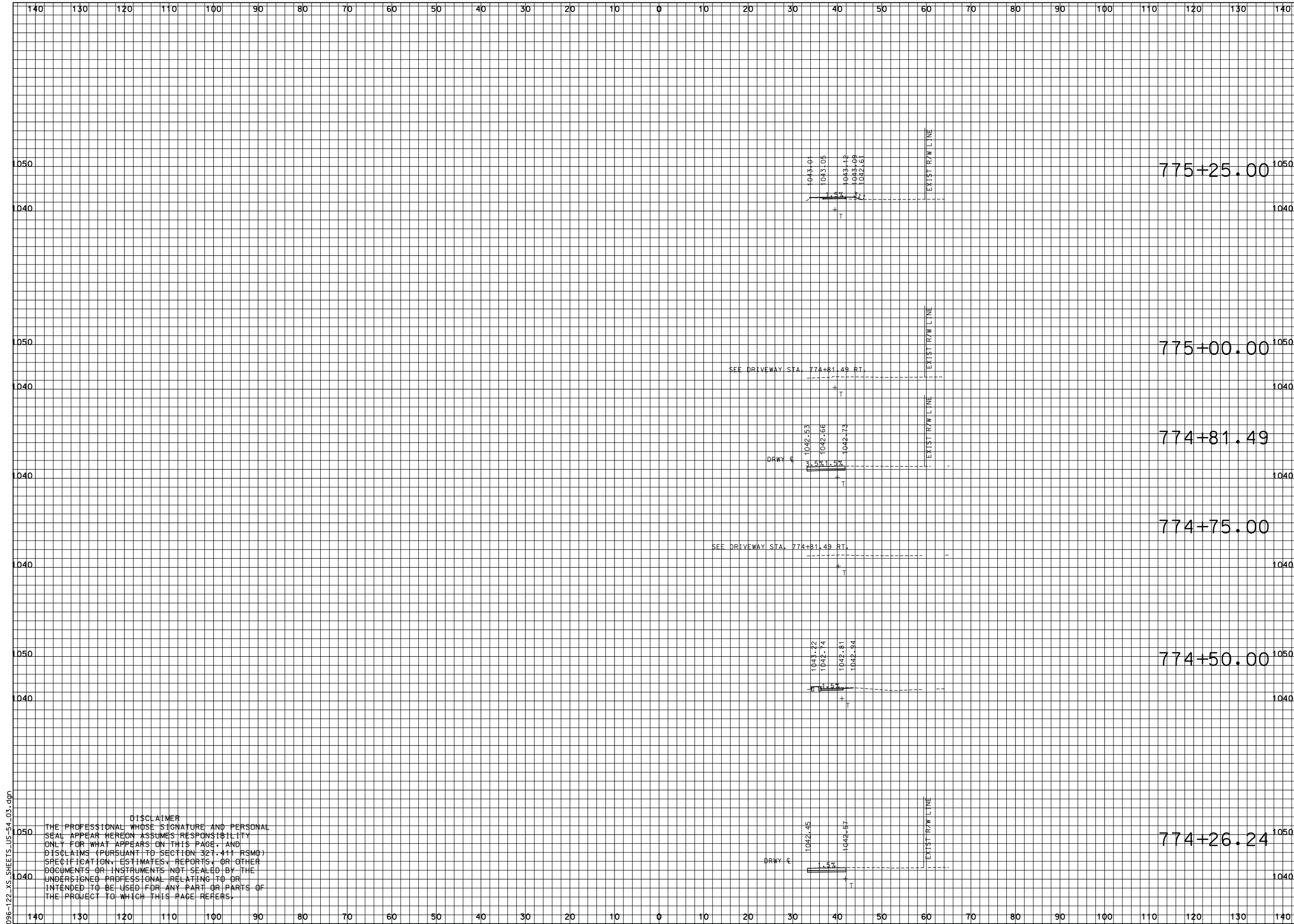
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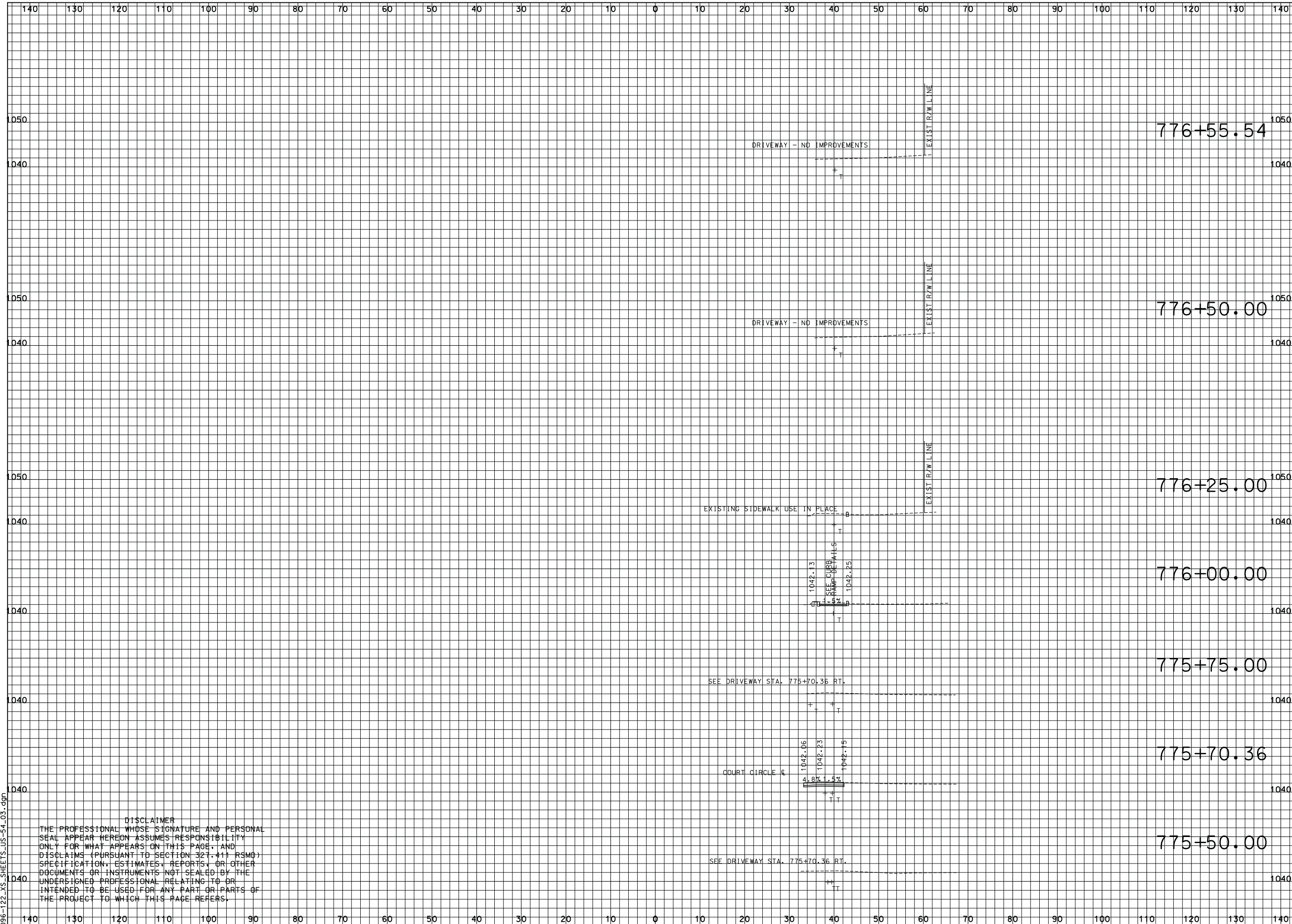


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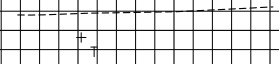


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