

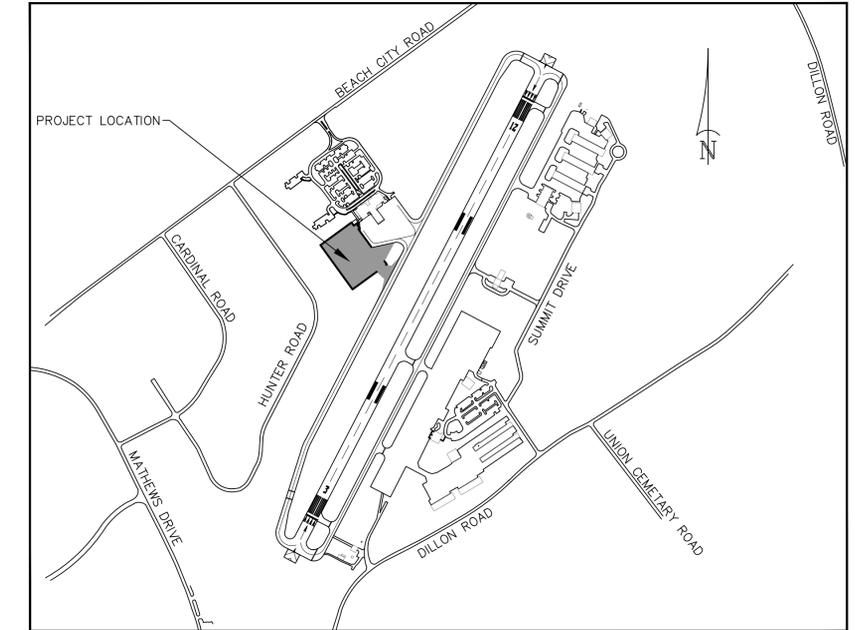
# CONSTRUCTION PLANS FOR COMMERCIAL APRON EXPANSION HILTON HEAD ISLAND

## AIRPORT

"Fly to the Fun"



VICINITY MAP  
NOT TO SCALE

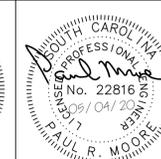
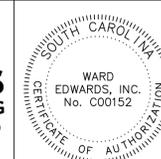
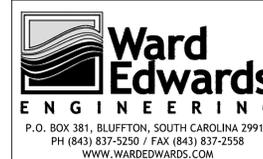


SITE MAP  
NOT TO SCALE

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SET # \_\_\_\_\_

NOTE:  
USE EXISTING PUBLIC ROADS FOR HAUL ROUTE BETWEEN  
WASTE AREA AND PROJECT LOCATION.



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& ELLINGTON**  
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**GENERAL NOTES:**

- 1. BOUNDARY INFORMATION PROVIDED BY ATLAS SURVEYING THROUGH TALBERT, BRIGHT & ELLINGTON.
2. TOPOGRAPHIC DATA PROVIDED BY ATLAS SURVEYING THROUGH TALBERT, BRIGHT & ELLINGTON.
3. APPROXIMATE LOCATION OF CERTAIN EXISTING UNDERGROUND UTILITY LINES AND STRUCTURES ARE SHOWN ON THE PLANS FOR INFORMATION ONLY. ADDITIONAL UNDERGROUND LINES OR STRUCTURES MAY EXIST THAT ARE NOT SHOWN. CALL SOUTH CAROLINA 811 AT 811 OR 1-888-721-7877 BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM MONDAY THRU FRIDAY AT LEAST THREE WORKING DAYS BEFORE COMMENCING CONSTRUCTION. REQUEST UNDERGROUND UTILITIES TO BE LOCATED AND MARKED WITHIN AND NEAR THE CONSTRUCTION SITE.
4. COMPLY WITH SOUTH CAROLINA UNDERGROUND FACILITY DAMAGE PREVENTION ACT (EFFECTIVE JUNE 7, 2012). NOTIFICATION OF INTENT TO EXCAVATE MAY BE GIVEN BY CALLING THE TOLL FREE NUMBER: 1-800-922-0983.
5. PROTECT BENCH MARKS AND PROPERTY MONUMENTS FROM DAMAGE DURING CONSTRUCTION OPERATIONS. REPLACE ANY BENCH MARKS OR MONUMENTS DAMAGED OR DESTROYED AS A RESULT OF CONTRACTOR'S OPERATIONS, AT NO COST TO THE OWNER, BY A LICENSED SURVEYOR IN THE STATE OF SOUTH CAROLINA.
6. OFF-STREET PARKING FOR THE CONTRACTOR'S EMPLOYEES AND AUTHORIZED VISITORS TO THE SITE MUST BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION.
7. THE CONTRACTOR IS RESPONSIBLE FOR ADHERING TO WEIGHT LIMITS PRESCRIBED FOR ALL PUBLIC ROADS WHEN HAULING EQUIPMENT AND MATERIALS TO AND FROM THE PROJECT SITE. DAMAGES TO EXISTING PAVEMENT DUE TO THE CONTRACTOR'S CONSTRUCTION OPERATIONS OR IMPROPER TRANSPORTATION OF MATERIALS AND EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
8. AT LEAST ONE DRIVING LANE ON PUBLIC ROADS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. TRAFFIC LANES WILL ONLY BE CLOSED WITH THE EXPRESS WRITTEN CONSENT OF THE AGENCY HAVING JURISDICTION OVER THE ROADWAY. NOTIFY AGENCY HAVING JURISDICTION AT LEAST 5 DAYS BEFORE CLOSING ANY DRIVING LANES TO TRAFFIC. PROVIDE TRAFFIC CONTROL DEVICES, SIGNS AND FLAGMEN AS REQUIRED TO ENSURE PUBLIC SAFETY.
9. CONTRACTOR SHALL COORDINATE DEMOLITION, CLEARING AND CONSTRUCTION OF IMPROVEMENTS TO MINIMIZE INTERFERENCE WITH VEHICULAR AND PEDESTRIAN TRAFFIC AND WITH OPERATIONS OF EXISTING FACILITIES.

**WATER AND SEWER LINE CONSTRUCTION:**

- 1. ALL WATER AND SEWER LINE CONSTRUCTION SHALL CONFORM TO APPLICABLE STATE AND HILTON HEAD PUBLIC SERVICE DISTRICT (HHPSD) REQUIREMENTS, STANDARDS AND SPECIFICATIONS.
2. HHPSD WILL BE RESPONSIBLE FOR INSPECTION AND APPROVAL OF ALL WATER AND SEWER SYSTEM CONSTRUCTION AND FOR ACCEPTANCE FOR OPERATION AND MAINTENANCE.
3. ALL UTILITIES SHOWN ARE APPROXIMATE LOCATIONS. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFICATION OF ALL UTILITY OWNERS AND FOR FIELD VERIFICATION OF BOTH HORIZONTAL AND VERTICAL LOCATIONS PRIOR TO COMMENCING CONSTRUCTION. ANY DAMAGES TO EXISTING UTILITIES DUE TO THIS CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
4. NOTIFY THE PROJECT ENGINEER, IF CONFLICTS WITH EXISTING STRUCTURES REQUIRE THAT PROPOSED UTILITIES BE RELOCATED.
5. THE CONTRACTOR MUST NOTIFY HHPSD FORTY-EIGHT (48) HOURS PRIOR TO ANY CONSTRUCTION, INSPECTION OR TESTING OF THE WATER DISTRIBUTION SYSTEM.
6. PIPE, FITTINGS, VALVES AND APPURTENANCES FOR WATER AND SEWER LINES SHALL ALL BE IN ACCORDANCE WITH THE REQUIREMENTS CONTAINED IN THE SPECIFICATIONS.
7. INSTALLATION OF WATER AND SEWER LINES AND APPURTENANCES SHALL BE IN ACCORDANCE WITH THE HHPSD STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
8. CONTRACTOR SHALL INSTALL MECHANICAL RESTRAINTS ON ALL BENDS, PLUGS AND TEES, 2" OR LARGER, ON WATER LINES AND SANITARY SEWER MAINS.
9. ALL WATER MAINS SHALL BE STERILIZED AND PRESSURE TESTED IN ACCORDANCE WITH HHPSD SPECIFICATIONS.
10. SEPARATION OF WATER MAINS AND SEWERS:
A. PARALLEL INSTALLATION: UNLESS OTHERWISE SPECIFICALLY SHOWN IN A SPECIAL DETAIL ON THE PLANS, INSTALL WATER MAINS AT LEAST 10-FT. HORIZONTALLY FROM ANY EXISTING OR PROPOSED SANITARY SEWER OR SANITARY SEWER FORCE MAIN, THE DISTANCE BEING MEASURED IN A HORIZONTAL PLANE BETWEEN THE OUTSIDE SURFACES OF THE PIPES.
B. CROSSINGS: UNLESS OTHERWISE SPECIFICALLY SHOWN IN A SPECIAL DETAIL ON THE PLANS, INSTALL WATER LINES CROSSING SANITARY SEWERS OR SANITARY SEWER FORCE MAINS TO PROVIDE A MINIMUM VERTICAL SEPARATION OF 18-INCHES BETWEEN THE OUTSIDE SURFACES OF THE PIPES. THIS SHALL BE THE CASE WHETHER THE WATER LINE IS ABOVE OR BELOW THE SANITARY SEWER LINE. WHENEVER POSSIBLE LOCATE THE WATER LINE ABOVE THE SEWER LINE. WHERE A NEW WATER LINE CROSSES A NEW SEWER LINE, PLACE A FULL LENGTH OF DUCTILE IRON PIPE AT THE CROSSING WITH PIPE POSITIONED SO THAT THE JOINTS ARE AS FAR AS POSSIBLE FROM THE POINT OF CROSSING. WHERE A NEW WATER LINE CROSSES AN EXISTING SEWER LINE, PLACE ONE FULL LENGTH OF DUCTILE IRON PIPE WATER LINE SO THAT THE JOINTS ARE AS FAR FROM THE POINT OF CROSSING AS POSSIBLE.
11. THE CONTRACTOR SHALL CUT AND PATCH EXISTING PAVEMENT AS REQUIRED FOR THE INSTALLATION OF UTILITY LINES.
12. SANITARY MANHOLE RIM GRADES SHOWN ARE APPROXIMATE. ADJUST RIM ELEVATIONS TO BE FLUSH WITH FINISHED GRADE.
13. THE CONTRACTOR UNDER THIS CONTRACT SHALL NOT MAKE ANY CONNECTIONS TO THE EXISTING WATER OR SANITARY SEWER SYSTEMS UNLESS EXPRESSLY AUTHORIZED TO DO SO BY THE HHPSD. ALL WATER AND SEWER IMPROVEMENTS UNDER THIS CONTRACT MUST BE CONSTRUCTED COMPLETE, TESTED, INSPECTED AND APPROVED BY THE HHPSD BEFORE ANY AUTHORIZATION TO CONNECT WILL BE GIVEN. COORDINATION OF TESTING, INSPECTION AND CONNECTIONS WITH THE HHPSD IS THE RESPONSIBILITY OF THE CONTRACTOR UNDER THIS CONTRACT.
14. ALL WATER MAINS SHALL BE INSTALLED WITH THIRTY-SIX INCHES (36") MINIMUM COVER (FROM FINISHED GRADE). MAXIMUM DEPTH SHALL BE FIVE FEET (5'). WHERE WATER MAINS MAY CONFLICT WITH OTHER UTILITIES, THE WATER MAIN CROSSING SHALL BE CONSTRUCTED WITH DUCTILE IRON PIPE, MECHANICAL JOINT 45-DEG. BENDS AND MECHANICAL RESTRAINTS.

**WORK ON SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY:**

- 1. CONTRACTOR SHALL REVIEW AND COMPLY WITH ALL CONDITIONS AND SPECIAL PROVISIONS CONTAINED IN THE SCDOT ENCROACHMENT PERMIT(S) ISSUED FOR THIS PROJECT.
2. CONTRACTOR IS RESPONSIBLE FOR SUBMITTING CONSTRUCTION NOTIFICATION FORM (48 HOUR MINIMUM) AND COORDINATION OF ALL WORK WITHIN SCDOT RIGHTS-OF-WAY WITH THE LOCAL AND/OR DISTRICT SCDOT ENGINEERING REPRESENTATIVE.
3. CONTRACTOR IS RESPONSIBLE FOR PREPARING A TRAFFIC CONTROL PLAN TO SCDOT FOR APPROVAL MINIMUM 48 HOURS PRIOR TO CONDUCTING WORK IN THE RIGHT-OF-WAY. ALL TRAFFIC CONTROL PLANS SHALL CONFORM TO CURRENT MUTCD AND CURRENT SCDOT GUIDELINES AND SPECIFICATIONS.
4. ALL SIGNAGE, PAVEMENT MARKINGS, AND MARKERS SHALL CONFORM TO CURRENT MUTCD GUIDELINES AND CURRENT SCDOT STANDARD SPECIFICATIONS AND DRAWINGS.
5. ALL PAVING AND DRAINAGE CONSTRUCTION SHALL CONFORM TO CURRENT SCDOT STANDARD SPECIFICATIONS AND DRAWINGS.
6. ALL PAVEMENT MARKINGS IN SCDOT RIGHT-OF-WAY SHALL BE THERMOPLASTIC AND CONFORM TO CURRENT MUTCD GUIDELINES AND CURRENT SCDOT STANDARD SPECIFICATIONS AND DRAWINGS.
7. REMOVAL OF PAVEMENT MARKINGS SHALL CONFORM TO CURRENT SCDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 609.4.1.2.

**TREE PROTECTION-HILTON HEAD ISLAND**

- 1. ONLY THOSE TREES APPROVED FOR REMOVAL IN ACCORDANCE WITH THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE AND SO DESIGNATED ON THE DRAWINGS FOR REMOVAL ARE TO BE REMOVED AS PART OF THE SITE CLEARING OPERATIONS. ALL TREES NOT DESIGNATED ON THE PLANS FOR REMOVAL MUST BE PRESERVED.
2. THE CONTRACTOR IS RESPONSIBLE FOR MARKING THE TREES DESIGNATED TO BE PRESERVED AND THOSE TO BE REMOVED IN ACCORDANCE WITH THE REQUIREMENTS CONTAINED IN SECTION 16-6-104 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE.
3. PRIOR TO COMMENCING ANY CLEARING OR CONSTRUCTION OPERATIONS ON THE SITE, THE CONTRACTOR SHALL ERECT TREE PROTECTION BARRIERS AROUND EACH TREE OR GROUP OF TREES DESIGNATED FOR PRESERVATION IN ACCORDANCE WITH THE DETAILS ON THE PLANS AND THE REQUIREMENTS CONTAINED IN SECTION 16-6-404 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE.
4. A TREE PROTECTION ZONE SHALL BE ESTABLISHED IN ACCORDANCE WITH THE PROVISIONS CONTAINED IN SECTION 16-6-404 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE FOR EACH EXISTING TREE DESIGNATED FOR PRESERVATION. THE MINIMUM TREE PROTECTION ZONE SHALL BE THE TREE DRIPLINE PLUS 1 FOOT. THE SIZE OR CONFIGURATION OF THE TREE PROTECTION ZONE MAY BE ADJUSTED AT THE DISCRETION OF THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR.
5. THE AREA WITHIN THE TREE PROTECTION ZONE MUST REMAIN OPEN AND UNPAVED. NO CHANGE OF GRADE WILL BE ALLOWED WITHIN THE TREE PROTECTION ZONE. ANY ACTIVITY WITHIN THE TREE PROTECTION ZONE IS SUBJECT TO APPROVAL BY THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR. THE FOLLOWING ACTIVITIES ARE PROHIBITED WITHIN THE TREE PROTECTION ZONE:
A. PLACEMENT OR STORAGE OF ANY SOIL, DEBRIS, OILS, FUEL, PAINTS, BUILDING MATERIALS OR ANY OTHER MATERIALS.
B. BURNING
C. VEHICLE PARKING
D. PAVING
E. TRENCHING FOR UTILITIES
6. WHERE UTILITY LINES MUST PASS THRU THE TREE PROTECTION ZONE, THEY SHALL BE INSTALLED BY HORIZONTAL BORING BENEATH THE ROOTS OF THE TREE.
7. WHERE IT IS NECESSARY FOR MACHINERY AND EQUIPMENT TO PASS WITHIN THE TREE PROTECTION ZONE, APPROVAL MUST BE OBTAINED FROM THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR. SPECIAL MEASURES WILL BE REQUIRED TO PROTECT THE ROOTS FROM EXCESSIVE COMPACTION.
8. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING A STAKING INSPECTION WITH THE TOWN OF HILTON HEAD ISLAND PRIOR TO CLEARING. THE FOLLOWING MUST BE COMPLETED BEFORE THE INSPECTION:
A. ALL BUILDING EDGES, SIDEWALKS, PAVED AREAS AND UTILITY LINES STAKED.
B. TREE PROTECTION BARRIERS INSTALLED.
C. TREES TO BE PRESERVED MARKED WITH BLUE FLAGGING AND THOSE TO BE REMOVED MARKED WITH RED FLAGGING.
D. ALL SILT FENCE INSTALLED PER THE APPROVED PLAN.

**SITE GRADING AND DRAINAGE:**

- 1. ALL UTILITIES SHOWN ARE APPROXIMATE LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING 72-HOUR NOTICE TO ALL RESPECTIVE UTILITY COMPANIES FOR FIELD VERIFICATION OF EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY DAMAGES TO EXISTING UTILITIES DUE TO THIS CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. TEMPORARY CONTROL OF STORM WATER DRAINAGE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. SEQUENCING AND CONSTRUCTION TECHNIQUES SHALL PREVENT OBSTRUCTION OF STORM SEWERS, PONDING IN TRAFFIC AREAS OR RISING OF WATER LEVELS WHICH WOULD ENTER ADJACENT BUILDINGS OR STRUCTURES.
3. FULL WIDTH OF STREET AND ROAD RIGHTS-OF-WAY MUST BE CLEARED AND GRADED AS SHOWN IN THE DETAILS ON THE DRAWINGS
4. SUBGRADE PREPARATION: TOP SOIL SHALL BE REMOVED FROM PAVED AREAS TO A MINIMUM DEPTH AS RECOMMENDED IN THE PROJECT'S GEOTECHNICAL REPORT. ALL EXCAVATION SHALL BE TO SUBGRADE LIMITS.
5. ALL UTILITY PIPE LINES, CONDUITS AND SLEEVES UNDER PAVED AREAS MUST BE IN PLACE PRIOR TO COMPLETION OF THE ROADWAY SUBGRADE
6. FINISH GRADING SHALL INCLUDE THE PLACEMENT OF TOPSOIL OVER ALL UNPAVED AREAS NOT OCCUPIED BY BUILDINGS OR STRUCTURES AND FINE GRADING AROUND BUILDINGS, ADJACENT TO WALKS, CURBS, GUTTERS AND STRUCTURES TO ASSURE POSITIVE DRAINAGE.

**SCDHEC/OCRM SEDIMENT AND EROSION CONTROL STANDARD NOTES (REVISED DEC-2012):**

- 1. IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS, IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
2. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
A. WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
B. WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
3. ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED ONCE EVERY CALENDAR WEEK. IF PERIODIC INSPECTION OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY OR INCORRECTLY INSTALLED, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
4. PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEARED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION. FILL COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE SEDIMENT BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
5. ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
6. THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAY(S) FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
7. RESIDENTIAL SUBDIVISIONS REQUIRE EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION. INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C. REG. 72-300 ET SEQ. AND SCD100000.
8. TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
9. ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CANNOT BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
10. LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
11. COVER TO PROTECT PROFILES, GASKET JOINTS WILL NOT BE CONSIDERED AN APPROVED O-RING JOINTS. THE CONTRACTOR SHALL INSTALL STANDARD STEPS IN ALL MANHOLES AND DROP INLETS. THE CONTRACTOR SHALL INSTALL INLET SHAPING IN ALL DRAINAGE MANHOLES AND DROP INLETS. NO SEPARATE MEASUREMENT WILL BE MADE FOR THE ABOVE REFERENCED ITEMS.
12. INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (3H:1V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
13. MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
14. MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATER MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
15. MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BMPs (SEDIMENT BASIN, FILTER BAG, ETC.).
16. THE FOLLOWING DISCHARGES FROM SITES ARE PROHIBITED:
A. WASTEWATER FROM WASHOUT OF CONCRETE, UNLESS MANAGED BY AN APPROPRIATE CONTROL.
B. WASTEWATER FROM WASHOUT AND CLEANTOUP OF STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS.
C. FUELS, OILS, OR OTHER POLLUTANTS USED IN VEHICLE AND EQUIPMENT OPERATION AND MAINTENANCE.
D. SOAPS OR SOLVENTS USED IN VEHICLE AND EQUIPMENT WASHING.
17. AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE.
18. IF EXISTING BMPs NEED TO BE MODIFIED OR IF ADDITIONAL BMPs ARE NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT AND/OR SC'S WATER QUALITY STANDARDS, IMPLEMENTATION MUST BE COMPLETED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE, IF IMPLEMENTATION BEFORE THE NEXT STORM EVENT IS IMPRACTICABLE, THE SITUATION MUST BE DOCUMENTED IN THE SWPPP AND ALTERNATIVE BMPs MUST BE IMPLEMENTED AS SOON AS REASONABLY POSSIBLE.
19. A PRE-CONSTRUCTION CONFERENCE MUST BE HELD FOR EACH CONSTRUCTION SITE WITH AN APPROVED ON-SITE SWPPP PRIOR TO THE IMPLEMENTATION OF CONSTRUCTION ACTIVITIES. FOR NON-LINEAR PROJECTS THAT DISTURB 10 ACRES OR MORE THIS CONFERENCE MUST BE HELD ON-SITE UNLESS THE DEPARTMENT HAS APPROVED OTHERWISE.

**DRY UTILITY CONDUITS FOR ELECTRIC, TELEPHONE AND CABLE TV:**

- 1. ALL DRY UTILITY CONDUIT ENDS SHALL BE CAPPED AND MARKED WITH A STEEL REBAR STAKE IMBEDDED ONE (1) FOOT BELOW GROUND SURFACE.
2. 48" MINIMUM BURY DEPTH FOR ALL ELECTRICAL CONDUITS.
3. MAINTAIN MINIMUM 12" VERTICAL CLEARANCE WHEN CROSSING WATER, SEWER, AND STORM DRAIN LINES.
4. MAINTAIN MINIMUM 18" HORIZONTAL CLEARANCE WHEN PARALLELING WATER, SEWER AND STORM DRAIN LINES.
5. EXTEND CONDUIT BEYOND PAVEMENT, CURB, AND SIDEWALKS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF THE INSTALLATION OF ALL UTILITY SERVICE CONNECTIONS. REFER TO APPROVED BUILDING PLANS FOR THE EXACT LOCATION OF ALL SERVICE CONNECTIONS. THE CONTRACTOR MUST INSTALL ALL CONDUITS, AS SHOWN ON THE PLANS OR AS REQUIRED BY RESPECTIVE UTILITY COMPANIES. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE STRICT COMPLIANCE WITH ALL APPLICABLE CODES AND REGULATIONS WITH REGARDS TO THE INSTALLATION OF UTILITIES AND CONDUIT.
7. LOCATIONS SHOWN ON THE PLANS FOR PROPOSED DRY UTILITY CONDUITS ARE APPROXIMATE ONLY. ALL DIMENSIONING AND STAKING SHOULD BE BASED ON ECONOMIC AND PRACTICAL CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH THE RESPECTIVE UTILITY REPRESENTATIVES, PRIOR TO ANY CONDUIT INSTALLATION.
8. TRANSFORMER PADS SHALL BE LOCATED AS DIRECTED BY THE RESPECTIVE UTILITY REPRESENTATIVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH APPLICABLE CODE REQUIREMENTS.
9. NOTIFY THE ENGINEER IF CONFLICTS WITH EXISTING OR PROPOSED STRUCTURES REQUIRE PROPOSED UTILITIES BE RELOCATED.

**SITE CLEARING AND DEMOLITION:**

- 1. NO CLEARING SHALL OCCUR WITHIN DESIGNATED BUFFER ZONES, TREE PROTECTION ZONES, OUTSIDE OF THE PROPERTY LINES OR BEYOND THE CLEARING LIMITS UNLESS OTHERWISE SPECIFICALLY SHOWN ON THE PLANS.
2. ONLY THOSE TREES DESIGNATED ON THE DRAWINGS FOR REMOVAL ARE TO BE REMOVED AS PART OF THE SITE CLEARING OPERATIONS.
3. THE CONTRACTOR SHALL INSTALL A CONTINUOUS LINE OF FLAGGING OR FENCING ALONG THE LIMITS OF CLEARING PRIOR TO COMMENCING ANY CLEARING, DEMOLITION, OR CONSTRUCTION WORK ON THE PROJECT.
4. EXERCISE CAUTION DURING CLEARING OPERATIONS TO AVOID FELLING TREES INTO DESIGNATED TREE PROTECTION ZONES.
5. NO BURNING WILL BE ALLOWED WITHIN 50 FEET OF A TREE PROTECTION ZONE OR TREE DRIP LINE. CONTRACTOR SHALL COORDINATE ANY BURNING OPERATIONS WITH LOCAL JURISDICTION AND FIRE DEPARTMENTS.
6. SELECTIVE CLEARING AREAS SHALL BE CLEARED OF ALL BRUSH AND UNDERSTORY GROWTH.

**UTILITY CONTACTS:**

Table with utility contact information including Palmetto Electric, Dominion Energy, Broad Creek PSD, South Island PSD, Hilton Head Island PSD, Hargor Communications, Time Warner Cable, Century Link, and Santee Cooper.

**SUPPLEMENTAL GENERAL NOTES:**

- 1. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR HAVING VISITED THE SITE AND HAVING FAMILIARIZED HIMSELF WITH EXISTING CONDITIONS PRIOR TO SUBMITTING HIS BID. ANY DISCREPANCIES BETWEEN EXISTING FIELD CONDITIONS AND WHAT IS SHOWN ON THE PLAN SHEETS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE BID.
2. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL DESIGNATE A REPRESENTATIVE AND ALTERNATE TO CONTACT ON A 24 HOUR BASIS SHOULD PROBLEMS ARISE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS, LICENSES, ETC. REQUIRED BY LOCAL, STATE AND FEDERAL AGENCIES.
4. VEHICLES AND PUBLIC SAFETY SHALL NOT BE COMPROMISED AND PROPER SEPARATION SHALL BE MAINTAINED AT ALL TIMES. REFERENCE IS DIRECTED TO "SAFETY" SECTION OF THE SPECIFICATIONS. EQUIPMENT AND VEHICLES SHALL BE MARKED WITH 3' X 3' ORANGE AND WHITE CHECKER FLAGS (DAY ONLY) OR YELLOW FLASHING DOME TYPE LIGHT (DAY OR NIGHT).
5. THE CONTRACTOR IS ADVISED THAT AIRCRAFT MAINTENANCE OPERATIONS ARE CONDUCTED ADJACENT TO THE PROJECT. SPECIAL ATTENTION TO DUST CONTROL WILL BE REQUIRED DURING THE COURSE OF THE PROJECT. THE USE OF WATER AND CALCIUM CHLORIDE SHALL BE ANTICIPATED. THE ENGINEER RESERVES THE RIGHT TO HALT WORK OR HAULING IN NON-CONFORMING AREAS, IF CORRECTIVE ACTIONS ARE NOT PROMPTLY TAKEN BY THE CONTRACTOR.
6. PRIOR TO ENTERING ANY SAFETY AREA, THE CONTRACTOR SHALL OBTAIN CLEARANCE FROM THE AIRPORT OPERATION STAFF AND AIR TRAFFIC CONTROL TOWER. TWO-WAY RADIO COMMUNICATIONS SHALL BE REQUIRED AT ALL TIMES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING THE TWO-WAY RADIOS. NO EQUIPMENT OR PERSONNEL SHALL ENTER AN OPEN RUNWAY OR TAXIWAY SAFETY AREA UNLESS PROPERLY COORDINATED WITH THE OWNER. THE CONTRACTOR SHALL HAVE TWO (2) NEW TWO-WAY RADIO(S) AT THE JOB SITE AT ALL TIMES WHILE WORK IS IN PROGRESS. THE CONTRACTOR SHALL MONITOR THE GROUND FREQUENCY (121.10 MHZ). THE RADIOS SHALL BE TURNED OVER TO THE OWNER AT THE END OF THE PROJECT.
7. THE CONTRACTOR SHALL KEEP ALL ROADWAYS CLEAR OF DEBRIS, STONES, LITTER, ETC., DURING CONSTRUCTION ON A DAILY BASIS, OR AS DIRECTED BY THE ENGINEER. ALL PAVEMENTS SHALL BE CLEANED OF CONSTRUCTION DEBRIS AND SPILLAGE IMMEDIATELY. CONTRACTOR SHALL VISUALLY INSPECT PAVEMENTS DURING THE HAULING OPERATIONS.
8. THE CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES (I.E., AIRPORT LIGHTING, NAVAIDS, ETC.) FROM DAMAGE BY EQUIPMENT OR PERSONNEL. THE CONTRACTOR SHALL ADVISE THE ENGINEER, IN WRITING, OF ANY EXISTING DAMAGED UTILITIES PRIOR TO BEGINNING CONSTRUCTION. ANY UTILITIES OR FACILITIES DAMAGED DURING THE PROJECT BY THE CONTRACTOR'S MEN OR EQUIPMENT SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE. HAND DIGGING TO PROTECT UTILITIES FROM DAMAGE SHOULD BE ANTICIPATED.
9. THE OWNER RESERVES THE RIGHT TO CONTRACT AND PERFORM OTHER OR ADDITIONAL WORK ADJACENT TO AND WITHIN THE WORK AREA COVERED BY THIS CONTRACT. WHEN SEPARATE CONTRACTS ARE LET WITHIN THE LIMITS OF ANY ONE PROJECT, EACH CONTRACTOR SHALL CONDUCT HIS WORK SO AS NOT TO INTERFERE WITH OR HINDER THE PROGRESS OR THE COMPLETION OF THE WORK BEING COMPLETED BY OTHER CONTRACTORS. THE CONTRACTORS WORKING ON THE SAME PROJECT SHALL COOPERATE WITH EACH OTHER AS ORDERED BY THE ENGINEER. ANY ADDITIONAL EFFORT OR WORK REQUIRED FOR SUCH COORDINATION WITH OTHER CONTRACTORS SHALL BE INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE. REFERENCE IS DIRECTED TO SECTION GENERAL PROVISION 50 OF THE SPECIFICATIONS.
10. THE CONTRACTOR'S EMPLOYEE PARKING SHALL BE IN THE STAGING AREA AS SHOWN. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL POSITION ALL EQUIPMENT, TOOLS, MATERIAL, ETC., IN THE APPROVED CONTRACTOR'S STAGING AREA UNLESS OTHERWISE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
11. RUNWAY AND/OR TAXIWAY LIGHTS SHALL BE DE-ENERGIZED OR PROPERLY COVERED ON CLOSED RUNWAY(S) AND ALL CLOSED SECTIONS OF TAXIWAYS (NO SEPARATE PAY ITEM).
12. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE ON THE PROJECT FOR THE COMPLETE DURATION.
13. THE CONTRACTOR SHALL PROVIDE CONFINED O-RING JOINTS FOR ALL REINFORCED CONCRETE PIPES INSTALLED UNDER THIS PROJECT. REINFORCED CONCRETE PIPES UTILIZING PROFILE GASKET JOINTS WILL NOT BE CONSIDERED AN APPROVED O-RING JOINTS. THE CONTRACTOR SHALL INSTALL STANDARD STEPS IN ALL MANHOLES AND DROP INLETS. THE CONTRACTOR SHALL INSTALL INLET SHAPING IN ALL DRAINAGE MANHOLES AND DROP INLETS. NO SEPARATE MEASUREMENT WILL BE MADE FOR THE ABOVE REFERENCED ITEMS.
14. CONSTRUCTION DURING NIGHTTIME WORK PERIODS SHALL REQUIRE THE FOLLOWING:
A. ALL TRENCHING MACHINES, BACKHOES, TRUCKS, AND OTHER VEHICLES REQUIRED BY THE CONTRACTOR FOR HIS OPERATIONS SHALL BE EQUIPPED WITH ARTIFICIAL ILLUMINATION SUFFICIENT TO SAFELY COMPLETE THE WORK.
B. AS PARTIAL FULFILLMENT OF THE ABOVE REQUIREMENTS, THE CONTRACTOR SHALL FURNISH AND USE COMPLETE ARTIFICIAL LIGHTING UNITS WITH A MINIMUM OUTPUT OF 3,000 WATTS ELECTRICAL BEAM, AFFIXED TO ALL EQUIPMENT IN SUCH A WAY TO DIRECT ILLUMINATION ON THE AREA UNDER CONSTRUCTION.
15. ANY LISTED MANUFACTURER'S EQUIPMENT SHALL MEAN THAT LISTED ITEM OR AN APPROVED EQUIVALENT.
16. TOPSOIL WILL NOT BE MEASURED AS A SEPARATE PAY ITEM. REFERENCE IS DIRECTED TO THE SPECIFICATIONS. OVER EXCAVATION FOR REDISTRIBUTION OF TOPSOIL WILL NOT BE MEASURED FOR PAYMENT AND WILL BE CONSIDERED INCIDENTAL TO THE PROJECT. ALL TOPSOIL SHALL BE STOCKPILED ON SITE. LOCATIONS OF STOCKPILE AREAS TO BE COORDINATED WITH THE OWNER.
17. UNSUITABLE EXCAVATION SHALL BE VERIFIED BY THE OWNER'S TESTING COMPANY AND ENGINEER PRIOR TO REMOVAL. UNSUITABLE EXCAVATION SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT HIS EXPENSE.
18. AN ELECTRONIC FILE OF THESE PLANS MAY BE OBTAINED BY THE CONTRACTOR TO AID IN THE PROJECT LAYOUT AND GRADING ONCE THE PROJECT HAS BEEN AWARDED.
19. ALL CONNECTIONS TO EXISTING PAVEMENT SHALL BE MADE BY SAW CUTTING A NEAT VERTICAL FACE. MATCH EXISTING PAVEMENT ELEVATIONS. EDGE SHALL BE TACKED PRIOR TO PAVING. (NO PAY ITEM)
20. THE CONTRACTOR SHALL VERIFY EXISTING INFORMATION FOR ALL STRUCTURES PRIOR TO ORDERING AND/OR TIE-IN WORK BEGINS.
21. ALL DISTURBED AREAS, INCLUDING THE CONTRACTORS STAGING AREA, HAUL ROUTES, GRADING LIMITS, ETC. SHALL BE RESTORED TO A SMOOTH LINE AND GRADED WITH POSITIVE DRAINAGE. THE CONTRACTOR SHALL SEED AND MULCH ALL DISTURBED AREAS.
22. CONTRACTOR SHALL REMOVE ALL TEMPORARY SEDIMENT AND EROSION CONTROL DEVICES, INCLUDING BUT NOT LIMITED TO SILT FENCE, DIVERSION DITCH, SEDIMENT TRAPS, AND GRAVEL ROADS, ONCE THE SITE HAS BEEN STABILIZED.
23. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL MAKE ALL NECESSARY REPAIRS TO THE HAUL ROAD, FENCING, CONTRACTOR STAGING AREA, AND REMOVE TEMPORARY EROSION CONTROL MEASURES AS ORDERED BY THE ENGINEER.
24. ANY DISCREPANCIES IN THE BENCHMARK INFORMATION SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION PRIOR TO ANY WORK PERFORMED USING THE BENCHMARKS IN QUESTION.
25. FIELD SURVEY INFORMATION WAS PROVIDED BY: ATLAS SURVEYING, INC. 49 BROWNS COVE ROAD, SUITE 5 RIDGELAND, SC 29936 (843)645-9277

**SEQUENCE OF CONSTRUCTION ACTIVITIES**

ESTIMATED START DATE: 09-01-20 ESTIMATED COMPLETION DATE: 09-01-21

ITEMS MUST OCCUR IN THE ORDER LISTED; ITEMS CANNOT OCCUR CONCURRENTLY UNLESS SPECIFICALLY NOTED.

**PHASE 1: (INITIAL)**

- 1. RECEIVE NPDES COVERAGE FROM DHEC.
2. HOLD PRE-CONSTRUCTION MEETING.
3. NOTIFY DHEC EOC REGIONAL OFFICE OR OCRM OFFICE 48 HOURS PRIOR TO BEGINNING LAND-DISTURBING ACTIVITIES.
4. INSTALLATION OF CONSTRUCTION ENTRANCE.
5. CLEARING & GRUBBING ONLY AS NECESSARY FOR INSTALLATION OF PERIMETER CONTROLS.
6. INSTALLATION OF PERIMETER CONTROLS (E.G. SILT FENCE).
7. INSTALL TREE PROTECTION.
8. INSTALL INLET PROTECTION.
9. INSTALL SEDIMENT TUBES.
10. CLEARING & GRUBBING ONLY IN AREAS OF BASIN.

**PHASES 2 & 3: (INTERMEDIATE & FINAL)**

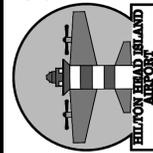
- 11. INSTALLATION OF BASIN AND INSTALLATION OF DIVERSIONS TO THOSE STRUCTURES (OUTLET STRUCTURES MUST BE COMPLETELY INSTALLED AS SHOWN ON THE DETAILS BEFORE PROCEEDING TO NEXT STEP; AREAS DRAINING TO THESE STRUCTURES CANNOT BE DISTURBED UNTIL THE STRUCTURES & DIVERSIONS TO THE STRUCTURES ARE COMPLETELY INSTALLED). INSTALL SURFACE DEWATERING SKIMMER PRIOR TO MOVING TO NEXT STEP.
12. CLEARING & GRUBBING OF SITE OR DEMOLITION (SEDIMENT & EROSION CONTROL MEASURES FOR THESE AREAS MUST ALREADY BE INSTALLED).
13. ROUGH GRADING.
14. INSTALLATION OF STORM DRAIN SYSTEM AND PLACEMENT OF INLET PROTECTION AS EACH INLET IS INSTALLED.
15. INSTALL ALL REQUIRED UTILITIES AND CURBING.
16. FINE GRADING, PAVING, ETC.
17. PLACE TOPSOIL & ESTABLISH FINISH GRADES.
18. PERMEABLE PAVERS SHALL BE LAID WITH ALL HEAVY CONSTRUCTION IS COMPLETED.
19. CLEAN-OUT OF DETENTION BASINS THAT WERE USED AS SEDIMENT CONTROL STRUCTURES AND RE-GRADING OF DETENTION POND BOTTOMS; IF NECESSARY, MODIFICATION OF SEDIMENT BASIN RISER TO CONVERT TO DETENTION BASIN OUTLET STRUCTURE.
20. INSTALL PERMANENT SEEDING.
21. FLUSH ANY SEDIMENT FROM STORM SEWER PIPES AND INLETS.
22. REMOVAL OF TEMPORARY SEDIMENT & EROSION CONTROL MEASURES (INCLUDING SKIMMER) AFTER ENTIRE AREA DRAINING TO THE STRUCTURE IS FINALLY STABILIZED (THE DEPARTMENT RECOMMENDS THAT THE PROJECT OWNER / OPERATOR HAVE THE SWPPP PREPARER OR REGISTRATION EQUIVALENT APPROVE THE REMOVAL OF TEMPORARY STRUCTURES).
23. PERFORM AS-BUILT SURVEYS OF ALL DETENTION STRUCTURES AND SUBMIT TO DHEC OR MS4 FOR ACCEPTANCE.
24. SUBMIT NOTICE OF TERMINATION (NOT) TO DHEC AS APPROPRIATE.

- NOTE: PERFORM WEEKLY SITE INSPECTIONS DURING LAND DISTURBING ACTIVITIES AND MAKE RECOMMENDATIONS FOR ADDITIONAL BMPs OR MAINTENANCE OF EXISTING BMPs
NOTE: ALL PUMPED DEWATERING SHALL BE PERFORMED USING AN APPROPRIATELY SIZED PUMPED WATER FILTER BAG.

Table with columns for REVISIONS and DATE, containing revision numbers 1 through 5.

Table with columns for DESIGNER BY, PROJECT ENG., PROJECT NO., DRAWN BY, CHECKED BY, and DATE ISSUED, with corresponding values.

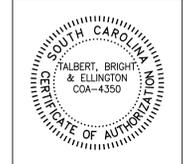
HILTON HEAD ISLAND AIRPORT "Fly to the Fun"



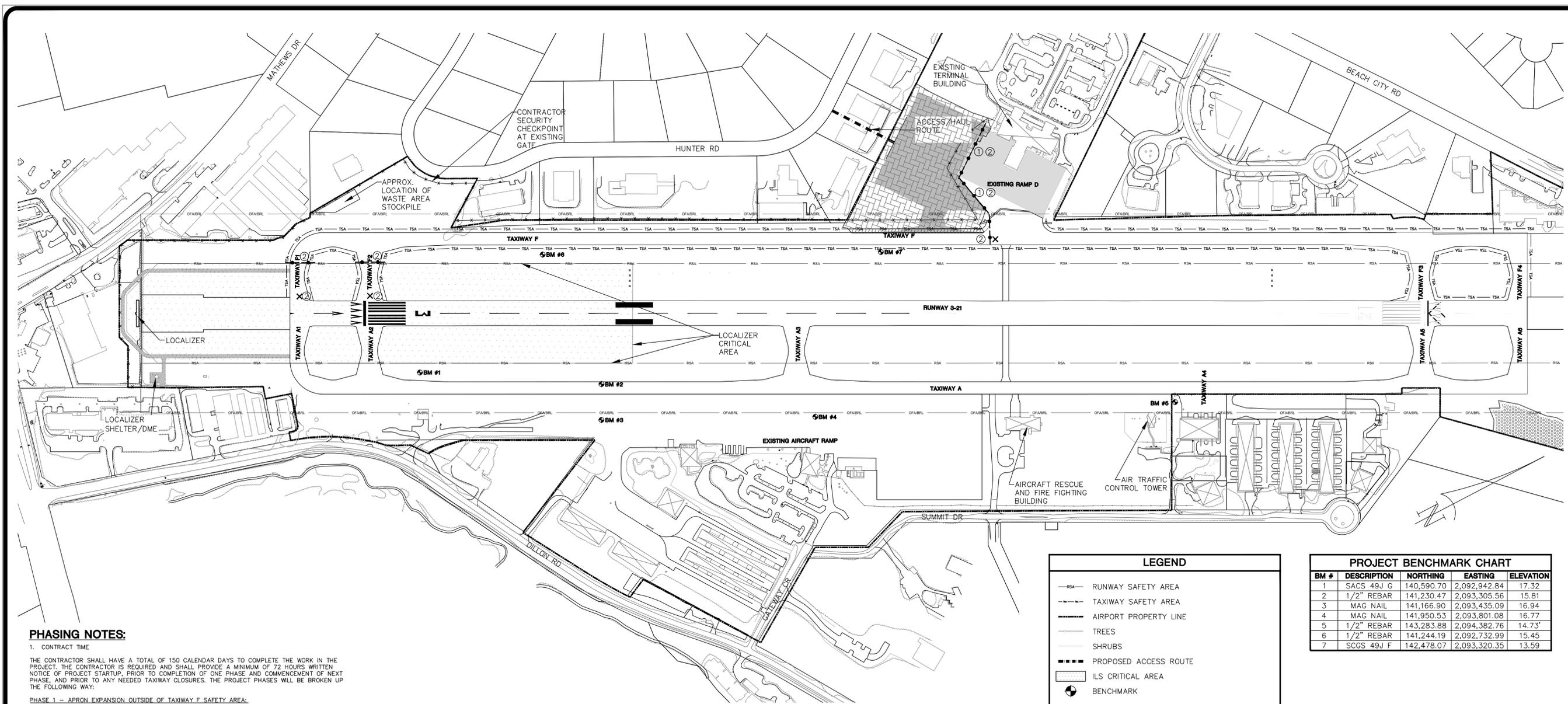
CONSTRUCTION NOTES COMMERCIAL APRON EXPANSION SCALE: N.T.S. FILE NAME: 1906-NOTES

SHT.

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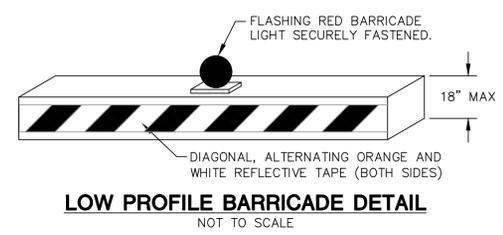


TALBERT, BRIGHT & ELLINGTON ENGINEERING & PLANNING CONSULTANTS 2000 PARK STREET, SUITE 101 COLUMBIA, SC 29201 PHONE: 803-933-9290 FAX: 803-933-9205 SC COA 4350 www.talbertandbright.com



**PHASING NOTES:**

- CONTRACT TIME  
 THE CONTRACTOR SHALL HAVE A TOTAL OF 150 CALENDAR DAYS TO COMPLETE THE WORK IN THE PROJECT. THE CONTRACTOR IS REQUIRED AND SHALL PROVIDE A MINIMUM OF 72 HOURS WRITTEN NOTICE OF PROJECT STARTUP, PRIOR TO COMPLETION OF ONE PHASE AND COMMENCEMENT OF NEXT PHASE, AND PRIOR TO ANY NEEDED TAXIWAY CLOSURES. THE PROJECT PHASES WILL BE BROKEN UP THE FOLLOWING WAY:  
**PHASE 1 - APRON EXPANSION OUTSIDE OF TAXIWAY F SAFETY AREA.**  
 ALL WORK IN THIS PHASE MAY COMMENCE AND BE COMPLETED ANYTIME WITHIN THIS PHASE. THE CONTRACTOR IS NOTIFIED OF THE PRESENCE OF THE ACTIVE COMMERCIAL AIRCRAFT APRON ADJACENT TO THE WORK AREA. PARTICULAR ATTENTION SHALL BE PROVIDED TO DUST AND FOREIGN OBJECT DEBRIS (FOD) CONTROL. IT IS POSSIBLE THAT AIRCRAFT ACTIVITY FROM THE ADJACENT COMMERCIAL AIRCRAFT PARKING APRON WILL PRODUCE JET BLAST FROM AIRCRAFT ENTERING AND EXITING THE EXISTING APRON, AND CARE SHALL BE TAKEN TO PROPERLY SECURE MATERIALS AND EQUIPMENT FROM BEING MOVED BY SUCH JET BLAST DURING AND AFTER WORK HOURS.  
**PHASE 2 - APRON EXPANSION INSIDE TAXIWAY F SAFETY AREA.**  
 THE CONTRACTOR WILL BE ALLOWED TO CLOSE TAXIWAY F DURING THIS PHASE TO MAKE REQUIRED TIE-INS TO TAXIWAY F, ALONG WITH ALL OTHER WORK WITHIN THE TAXIWAY F SAFETY AREA. ONCE THIS PHASE IS CLOSED TO AIRCRAFT WITH REQUIRED BARRICADES AND CONTRACTOR COMMENCES WORK IN THIS PHASE, ALL PAVEMENT REMOVAL AND OTHER WORK WITHIN THE CLOSED PHASE AREA MAY COMMENCE AND BE COMPLETED ANYTIME WITHIN THIS PHASE.
- CLOSED AIRFIELD MARKERS AND DE-ENERGIZED AND/OR COVERED TAXIWAY LIGHTS SHALL BE INSTALLED AS THE FIRST TASK IN EACH PHASE REQUIRING A TAXIWAY CLOSURE. CLOSED AIRFIELD MARKERS SHALL BE REMOVED AS THE LAST TASK IN EACH PHASE REQUIRING A TAXIWAY CLOSURE. TRAFFIC BARRICADES SHALL NOT BE REMOVED UNTIL PAVEMENT AND TAXIWAY SAFETY AREAS ARE IN SERVICEABLE CONDITION.
- THE CONTRACTOR SHALL SWEEP ALL AIRFIELD PAVEMENT PRIOR TO OPENING, TO CLEAR ANY FOREIGN OBJECT DEBRIS CAUSED BY THE WORK. THE SWEEPING WILL CONTINUE UNTIL APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL FOLLOW THE SAFETY GUIDELINES SET FORTH IN AC 150/5370-2G "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- AT THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING THE NEW DOUBLE SWING GATES INSTALLED IN THE EXISTING AIRPORT FENCE FOR TEMPORARY CONSTRUCTION ACCESS POINTS AND FOR THE INSTALLATION OF NEW CHAIN LINK FENCE, OR THE UNDAMAGED EXISTING CHAIN LINK FENCE REMOVED PRIOR TO THE TEMPORARY GATE INSTALLATION, IN ORDER TO RETURN THE AIRPORT FENCE TO ITS ORIGINAL SECURED CONDITION PRIOR TO THIS PROJECT.



**LOW PROFILE BARRICADE DETAIL**  
NOT TO SCALE

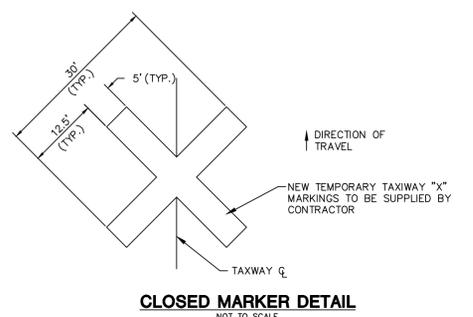
- NOTES:**
- BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
  - THE CONTRACTOR SHALL PROVIDE A SUFFICIENT NUMBER OF BARRICADES TO COMPLETE THE WORK. THIS ITEM IS INCIDENTAL TO THE PROJECT, INCLUDING MULTIPLE RELOCATIONS OF THE BARRICADES AS REQUIRED. NO SEPARATE PAY ITEM, AND ALL WORK ASSOCIATED WITH THIS ITEM SHALL BE INCIDENTAL TO THE PROJECT.
  - BARRICADES SHALL BE CONSTRUCTED OF MATERIAL THAT IS LOW IN MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE TO WITHSTAND HIGH WINDS AND/OR JET BLAST.
  - SPACING OF BARRICADES SHALL ENSURE THAT THE DISTANCE BETWEEN LIGHTS IS NO MORE THAN 10 FEET. INSTALLATION OF MULTIPLE LIGHTS ON A SINGLE BARRICADE MAY BE REQUIRED. THE BARRICADES SHALL BE A MINIMUM OF 8' IN LENGTH.
  - THE CONTRACTOR SHALL MAINTAIN AND MAKE FREQUENT INSPECTION OF THE BARRICADES THROUGHOUT THE DURATION OF THE PROJECT. PROMPT REPAIRS INCLUDING REPLACEMENT OF FLASHING RED LIGHTS SHALL BE MADE AS NECESSARY.
  - KNOWN MANUFACTURER AND MODEL: OTW SAFETY, AR10X96 O V.2.

**LEGEND**

- RSA — RUNWAY SAFETY AREA
- TSA — TAXIWAY SAFETY AREA
- AIRPORT PROPERTY LINE
- TREES
- SHRUBS
- PROPOSED ACCESS ROUTE
- ILS CRITICAL AREA
- BENCHMARK
- ① ② LOW PROFILE BARRICADES w/PHASE DESIGNATION
- ▨ DEMO, GRADING, DRAINAGE, PAVING PHASE 1
- ▩ DEMO, GRADING, DRAINAGE, PAVING PHASE 2
- ▧ WETLAND

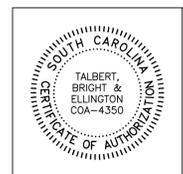
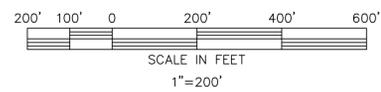
**PROJECT BENCHMARK CHART**

BM #	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	SACS 49J G	140,590.70	2,092,942.84	17.32
2	1/2" REBAR	141,230.47	2,093,305.56	15.81
3	MAG NAIL	141,166.90	2,093,435.09	16.94
4	MAG NAIL	141,950.53	2,093,801.08	16.77
5	1/2" REBAR	143,283.88	2,094,382.76	14.73
6	1/2" REBAR	141,244.19	2,092,732.99	15.45
7	SCGS 49J F	142,478.07	2,093,320.35	13.59



**CLOSED MARKER DETAIL**  
NOT TO SCALE

- NOTES:**
- CLOSED MARKERS SHALL BE INSTALLED AS THE FIRST ITEM OF WORK.
  - THE CONTRACTOR SHALL PROPERLY SECURE THE MARKINGS, AND MAKE FREQUENT INSPECTION OF THE MARKINGS AND MAKE PROMPT REPAIRS AS NECESSARY.
  - SEE ADVANCE NOTICE REQUIREMENTS FOR TAXIWAY CLOSURE PROVIDED ON THIS SHEET IN THE PHASING NOTES.
  - TAXIWAY CLOSED MARKERS SHALL BE PAY ITEMS.



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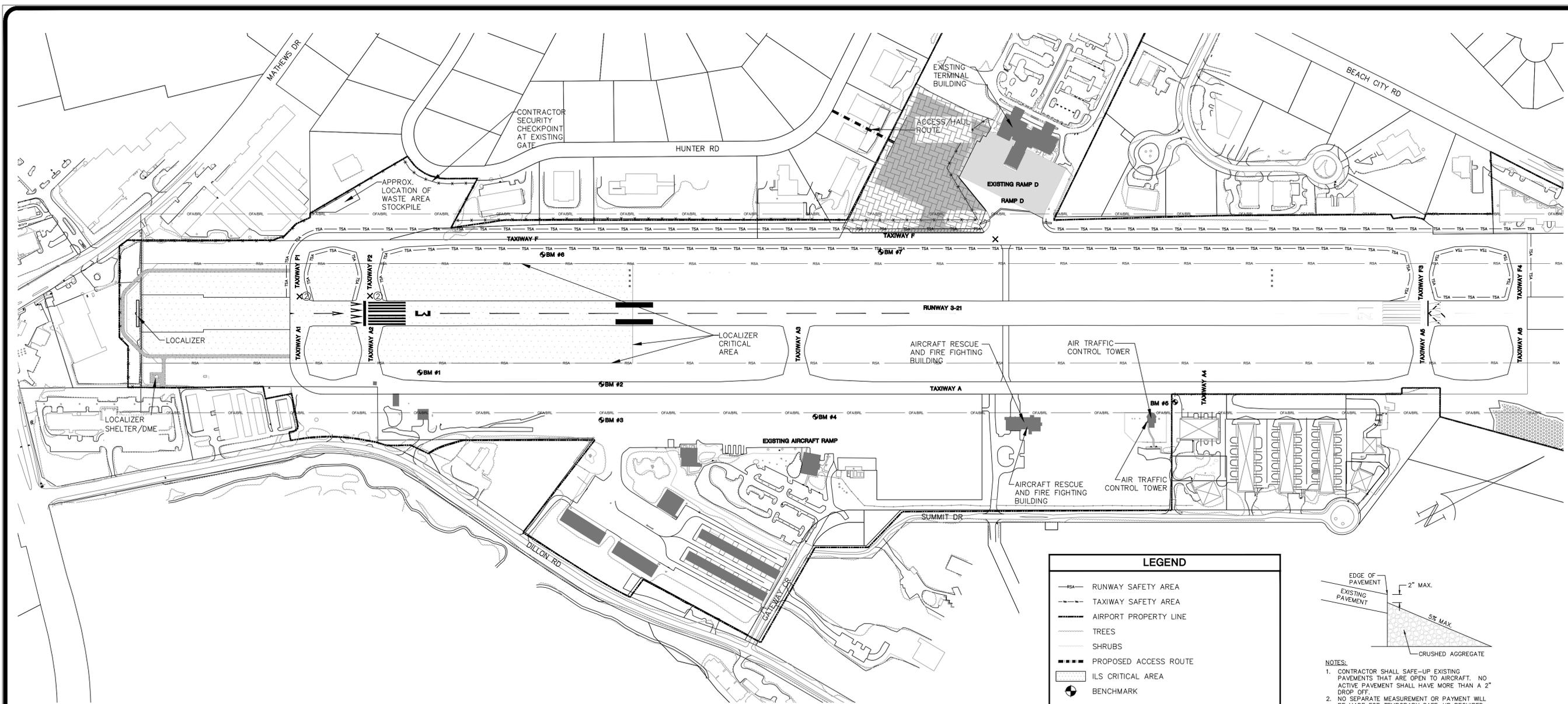
REVISIONS

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

DESIGNED BY: PET  
 PROJECT ENG.: JAS  
 PROJECT NO.: 2119-1906  
 DRAWN BY: PET  
 CHECKED BY: JAS  
 DATE ISSUED: MAY 2020

**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun!"*

**CONSTRUCTION SAFETY AND PHASING PLAN**  
**COMMERCIAL APRON EXPANSION**  
 SCALE: 1"=200'  
 FILE NAME: 1906-PHASE



**PHASING AND SAFETY NOTES:**

THE INTENT OF THIS PLAN IS TO ESTABLISH CERTAIN PHASING REQUIREMENTS THAT SHALL BE ADHERED TO BY THE CONTRACTOR DURING CONSTRUCTION OF THE PROJECT.

- CONTRACT TIME: THE CONTRACTOR SHALL HAVE A TOTAL OF 150 CALENDAR DAYS TO COMPLETE THE WORK IN THE PROJECT. THE CONTRACTOR IS REQUIRED AND SHALL PROVIDE A MINIMUM OF 72 HOURS WRITTEN NOTICE OF PROJECT STARTUP, PRIOR TO COMPLETION OF ONE PHASE AND COMMENCEMENT OF NEXT PHASE, AND PRIOR TO ANY NEEDED TAXIWAY CLOSURES. SEE SHEET 150 FOR ADDITIONAL ADVANCE NOTICE REQUIREMENTS FOR NEEDED TAXIWAY CLOSURES.
- LIQUIDATED DAMAGES: LIQUIDATED DAMAGES IN THE AMOUNT OF \$1,500.00 PER CALENDAR DAY WILL BE ASSESSED BEYOND THE TOTAL CONTRACT TIME ALLOTTED AND MILESTONE CONTRACT TIME.
- ALL WORK OUTSIDE THE RUNWAY AND TAXIWAY SAFETY AREAS (RSA, TSA) MAY BE COMPLETED AT ANY TIME OF THE DAY. NO WORK MAY OCCUR WITHIN THE RSA AND TSA WHILE AIRCRAFT OPERATIONS ARE ACTIVE.
- ALL WORK WITHIN THE SAFETY AREA OF TAXIWAY F SHALL BE COMPLETED DURING SCHEDULED TAXIWAY CLOSURE ONLY AVAILABLE IN PHASE 2 LISTED ON SHEET 150. THE USE OF THE TEMPORARY SAFE-UP DETAIL IS REQUIRED AT THE END OF EACH DAY'S WORK TO ASSURE THAT THERE IS NEVER MORE THAN A 2" DROP FROM THE EDGE OF PAVEMENT OF ACTIVE RUNWAY, TAXIWAY AND AIRCRAFT PARKING RAMP.
- CONTRACTOR SHALL PROVIDE A MINIMUM OF SEVEN (7) DAYS WRITTEN NOTICE PRIOR TO ANY RUNWAY, TAXIWAY OR AIRCRAFT PARKING RAMP CLOSURE.
- THE CONTRACTOR SHALL NOT BEGIN WORK WITHIN ANY AIR OPERATIONS AREA UNTIL 72 HOURS NOTICE HAS BEEN GIVEN TO THE ENGINEER AND AIRPORT MANAGEMENT. AIRPORT MANAGEMENT WILL THEN COORDINATE WITH AIR TRAFFIC CONTROL TOWER FOR EACH NIGHTLY CLOSURE.
- ALL WORK INVOLVING CONSTRUCTION OPERATIONS WITHIN THE SECURED AREA SHALL BE COORDINATED WITH THE OWNER.
- OPEN TRENCHES, EXCAVATION, DROP-OFFS, AND STOCKPILED MATERIAL WILL NOT BE PERMITTED WITHIN THE RSA, TSA OR WITHIN 50 FEET OF EDGE OF ACTIVE APRONS. COVERINGS FOR OPEN TRENCHES MUST BE OF SUCH STRENGTH TO SUPPORT CRITICAL AIRCRAFT AS DETERMINED BY THE ENGINEER AND AIRPORT DIRECTOR.
- A DAILY STARTUP AND SHUTDOWN CHECKLIST WILL BE JOINTLY PREPARED BY THE CONTRACTOR AND AIRPORT MANAGEMENT, WHICH WILL BE FOLLOWED THROUGHOUT THE PROJECT. THIS CHECKLIST SHALL INCLUDE, BUT NOT BE LIMITED TO, BARRICADES, FLAGS, HAUL ROUTES, SECURING ALL GATES, CLEAN UP, ETC.

- THE CONTRACTOR SHALL COORDINATE WITH THE OWNER TO DETERMINE A SUITABLE LOCATION FOR THE PROJECT STAGING AREAS. THE AREAS SHOWN ON THIS SHEET ARE APPROXIMATE. AIRPORT TO APPROVE FINAL STAGING AREA LOCATIONS AND LIMITS.
- THE CONTRACTOR SHALL COORDINATE INGRESS-EGRESS REQUIREMENTS WITH THE AIRPORT MANAGEMENT. ALL OPEN GATES TO BE SECURED. AIRPORT AREAS SHALL BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PROPERLY BADGED GATE GUARD PERSONNEL TO CONTROL ACCESS TO SECURED AREA. GATE GUARD MUST SPEAK ENGLISH PROPRIETLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL GATES AT THE END OF EACH DAY'S OPERATIONS.
- ALL CONSTRUCTION VEHICLES MUST BE CLEARED FOR ACCESS BY AIRPORT MANAGEMENT. PERSONAL CARS SHALL BE PARKED OUTSIDE OF SECURED AIRFIELD AREAS. ALL VEHICLES OPERATING IN ACTIVE AIR OPERATIONS AREAS SHALL BE LIGHTED OR FLAGGED IN ACCORDANCE WITH ADVISORY CIRCULAR 150/5370-2, CURRENT EDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PENALTIES ASSOCIATED WITH CONTRACTOR AND SUBCONTRACTOR FORCES THAT ARE INVOLVED IN A VEHICLE DEVIATION/RUNWAY INCURSION.
- EACH CONTRACTOR VEHICLE DRIVER WITHIN THE AIRPORT SECURITY FENCE MUST HAVE COMPLETED (OR BE ESCORTED BY A CONTRACTOR VEHICLE DRIVER THAT HAS COMPLETED) THE AIRPORT'S GROUND VEHICLE DRIVER TRAINING PROGRAM.
- ALL CONTRACTOR AND SUBCONTRACTOR EMPLOYEES SHALL ENTER THE AIRPORT WITHIN CONTRACTOR VEHICLES. CONTRACTOR EMPLOYEE PARKING SHALL BE OUTSIDE OF THE AIRPORT SECURITY FENCING.
- UNLESS DIRECTED OTHERWISE BY OWNER, CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING RUNWAYS OR TAXIWAYS AS PART OF THE HAUL ROAD.
- AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL POSITION ALL EQUIPMENT, TOOLS, MATERIAL, ETC. IN THE APPROVED CONTRACTOR'S STAGING AREAS UNLESS OTHERWISE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGEMENT FOR NECESSARY SECURITY FENCE INSTALLATION AND REMOVAL. CONTRACTOR SHALL MAINTAIN FULL PERIMETER FENCING AT ALL TIMES. NEW FENCE MUST BE INSTALLED PRIOR TO FENCE REMOVAL TO MAINTAIN THE SECURED AREA. SEE PLAN SHEETS FOR PROPOSED FENCING AND FENCING REMOVAL.
- LOW PROFILE BARRICADES SHALL BE INSTALLED AS THE FIRST TASK IN ANY PHASE. BARRICADES SHALL NOT BE REMOVED UNTIL PAVEMENT AND APRON

SAFETY AREAS ARE IN SERVICEABLE CONDITION. PLACEMENT OF THE LIGHTED BARRICADES SHALL BE AT THE EXTREME EDGE OF THE EXISTING TAXIWAY WHILE THE TAXIWAY IS OPEN TO AIRCRAFT. THE CONTRACTOR SHALL INCLUDE IN HIS BID PROPOSAL ALL COSTS ASSOCIATED WITH PROVIDING, PLACING, RELOCATING, AND REMOVAL OF BARRICADES TO MEET THE PROJECT SAFETY REQUIREMENTS.

- THE CONTRACTOR SHALL BE REQUIRED, AS NECESSARY, TO RELOCATE THE LOW PROFILE BARRICADES DURING THE PROJECT. MULTIPLE RELOCATIONS OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT. THE LOCATION OF THE BARRICADES AS SHOWN ON THE PLANS REPRESENTS THE MOST TYPICAL LOCATION. BARRICADES ARE REQUIRED UNTIL THE NEW PAVEMENT AND ADJACENT SAFETY AREAS ARE COMPLETED. NO BARRICADE REMOVAL OR RELOCATION WILL OCCUR UNTIL APPROVED BY THE OWNER.
- DURING ALL WORK WITHIN THE SECURED AREA, THE CONTRACTOR'S EMPLOYEES SHALL BE BADGED IN ACCORDANCE WITH OWNER'S REQUIREMENTS.
- THE CONTRACTOR SHALL NOTIFY AIRPORT MANAGEMENT IMMEDIATELY IN THE EVENT OF AN EMERGENCY SITUATION.
- AREAS OUTSIDE THE PROJECT LIMITS ARE DESIGNATED AS RESTRICTED AREAS. THE CONTRACTOR'S FORCES ARE PROHIBITED FROM ENTERING RESTRICTED AREAS AT ANY TIME, UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEER OR AIRPORT MANAGEMENT. ANYONE FOUND IN A RESTRICTED AREA WITHOUT PRIOR WRITTEN PERMISSION FROM THE OWNER WILL BE PROMPTLY AND PERMANENTLY REMOVED FROM THE PROJECT.
- EXISTING RUNWAY AND TAXIWAY EDGE LIGHT CIRCUITS SHALL REMAIN IN SERVICE THROUGHOUT THE DURATION OF THE PROJECT. CONTRACTOR SHALL PROVIDE AND MAINTAIN ABOVE GROUND JUMPER CABLES AS REQUIRED. ALL COSTS FOR TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO PROJECT.
- CONTRACTOR SHALL REMOVE ALL FOREIGN OBJECT DEBRIS (FOD) CONTINUOUSLY THROUGHOUT THE PROJECT ON ALL ACTIVE AIRFIELD PAVEMENTS.
- CONTRACTOR PERSONNEL MUST UTILIZE TWO-WAY AVIATION BAND RADIO COMMUNICATIONS WITH AIRPORT AIR TRAFFIC CONTROL TOWER ON FREQUENCY OF 121.1 MHZ. WHILE WORKING ON ACTIVE AIRFIELD AREAS.

**LEGEND**

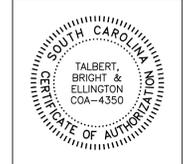
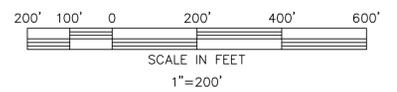
- RSA — RUNWAY SAFETY AREA
- TSA — TAXIWAY SAFETY AREA
- AIRPORT PROPERTY LINE
- TREES
- SHRUBS
- PROPOSED ACCESS ROUTE
- ILS CRITICAL AREA
- BENCHMARK
- LOW PROFILE BARRICADES w/PHASE DESIGNATION
- DEMO, GRADING, DRAINAGE, PAVING PHASE 1
- DEMO, GRADING, DRAINAGE, PAVING PHASE 2
- WETLAND

**TEMPORARY PAVEMENT EDGE SAFE-UP DETAIL**  
NOT TO SCALE

EDGE OF PAVEMENT — 2" MAX.  
EXISTING PAVEMENT — 5% MAX.  
CRUSHED AGGREGATE

**NOTES:**

- CONTRACTOR SHALL SAFE-UP EXISTING PAVEMENTS THAT ARE OPEN TO AIRCRAFT. NO ACTIVE PAVEMENT SHALL HAVE MORE THAN A 2" DROP OFF.
- NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE FOR TEMPORARY SAFE-UP REQUIRED DURING CONSTRUCTION. CONTRACTOR WILL BE ALLOWED TO RE-USE SAFE-UP MATERIAL IN THE PROJECT PROVIDED THE MATERIAL MEETS THE SPECIFICATION REQUIREMENTS FOR THE RESPECTIVE ITEMS OF WORK.



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SC COA 4350  
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DESIGNED BY:	PET
PROJECT ENG.:	JAS
PROJECT NO.:	2119-1906
DRAWN BY:	PET
CHECKED BY:	JAS
DATE ISSUED:	MAY 2020

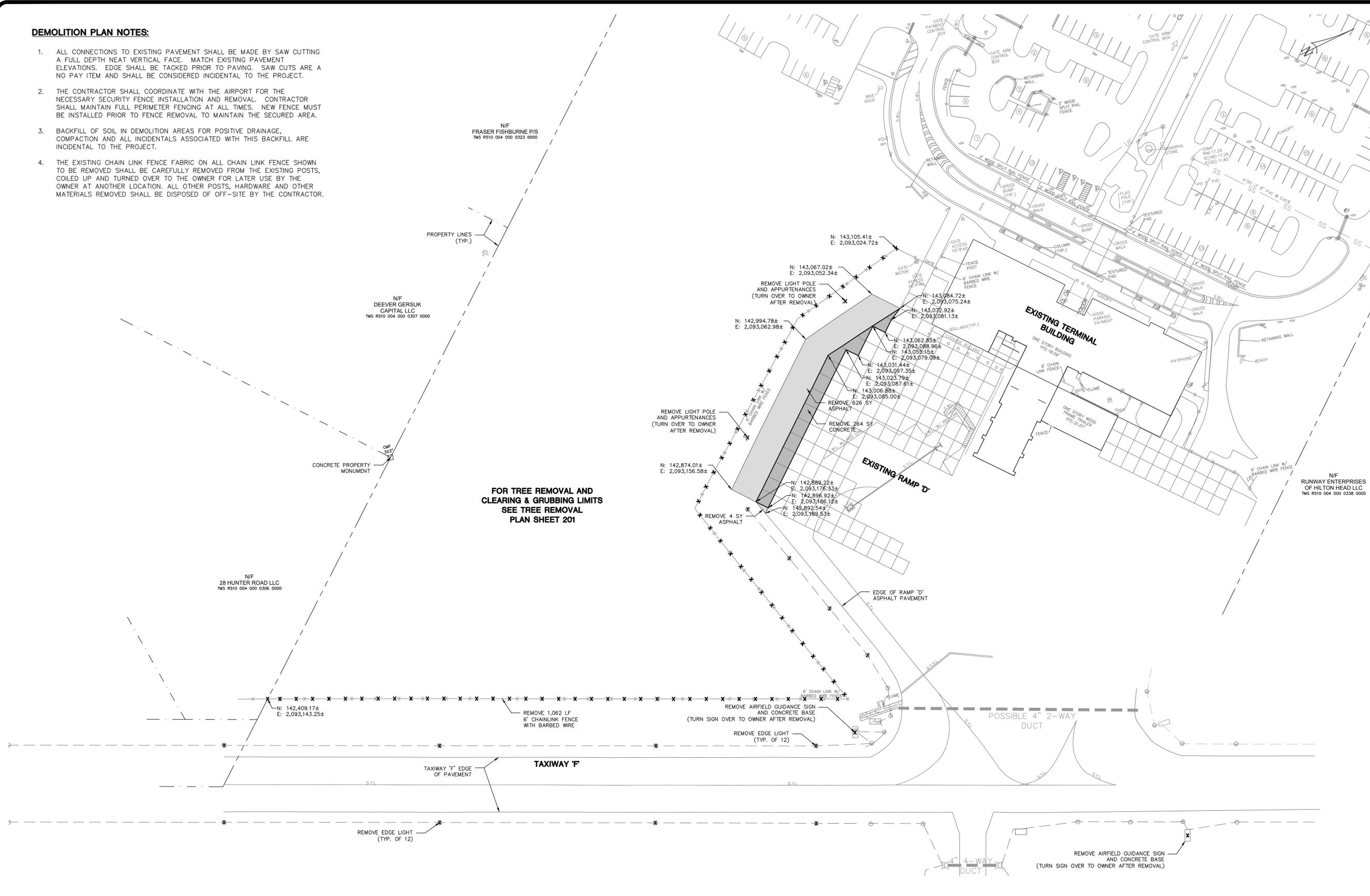
REVISIONS	DATE
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**HILTON HEAD ISLAND AIRPORT**  
"Fly to the Fun!"

**CONSTRUCTION SAFETY AND PHASING PLAN**  
**COMMERCIAL APRON EXPANSION**  
SCALE: 1"=200'  
FILE NAME: 1906-PHASE

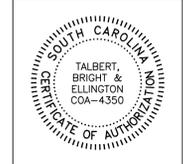
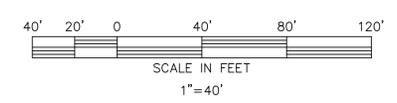
**DEMOLITION PLAN NOTES:**

1. ALL CONNECTIONS TO EXISTING PAVEMENT SHALL BE MADE BY SAW CUTTING A FULL DEPTH NEAT VERTICAL FACE. MATCH EXISTING PAVEMENT ELEVATIONS. EDGE SHALL BE TACKED PRIOR TO PAVING. SAW CUTS ARE A NO PAY ITEM AND SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT FOR THE NECESSARY SECURITY FENCE INSTALLATION AND REMOVAL. CONTRACTOR SHALL MAINTAIN FULL PERIMETER FENCING AT ALL TIMES. NEW FENCE MUST BE INSTALLED PRIOR TO FENCE REMOVAL TO MAINTAIN THE SECURED AREA.
3. BACKFILL OF SOIL IN DEMOLITION AREAS FOR POSITIVE DRAINAGE, COMPACTION AND ALL INCIDENTALS ASSOCIATED WITH THIS BACKFILL ARE INCIDENTAL TO THE PROJECT.
4. THE EXISTING CHAIN LINK FENCE FABRIC ON ALL CHAIN LINK FENCE SHOWN TO BE REMOVED SHALL BE CAREFULLY REMOVED FROM THE EXISTING POSTS, COILED UP AND TURNED OVER TO THE OWNER FOR LATER USE BY THE OWNER AT ANOTHER LOCATION. ALL OTHER POSTS, HARDWARE AND OTHER MATERIALS REMOVED SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR.



**FOR TREE REMOVAL AND  
CLEARING & GRUBBING LIMITS  
SEE TREE REMOVAL  
PLAN SHEET 201**

LEGEND	
	PROPOSED AIRFIELD CONCRETE TO BE REMOVED
	PROPOSED AIRFIELD ASPHALT PAVEMENT TO BE REMOVED
	MISCELLANEOUS ITEM TO BE REMOVED



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DESIGNED BY: CEV	PROJECT ENG.: JAS	PROJECT NO.: 2119-1906	DRAWN BY: UPB	CHECKED BY: PET	DATE ISSUED: MAY 2020
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**HILTON HEAD ISLAND AIRPORT**  
"Fly to the Fun!"

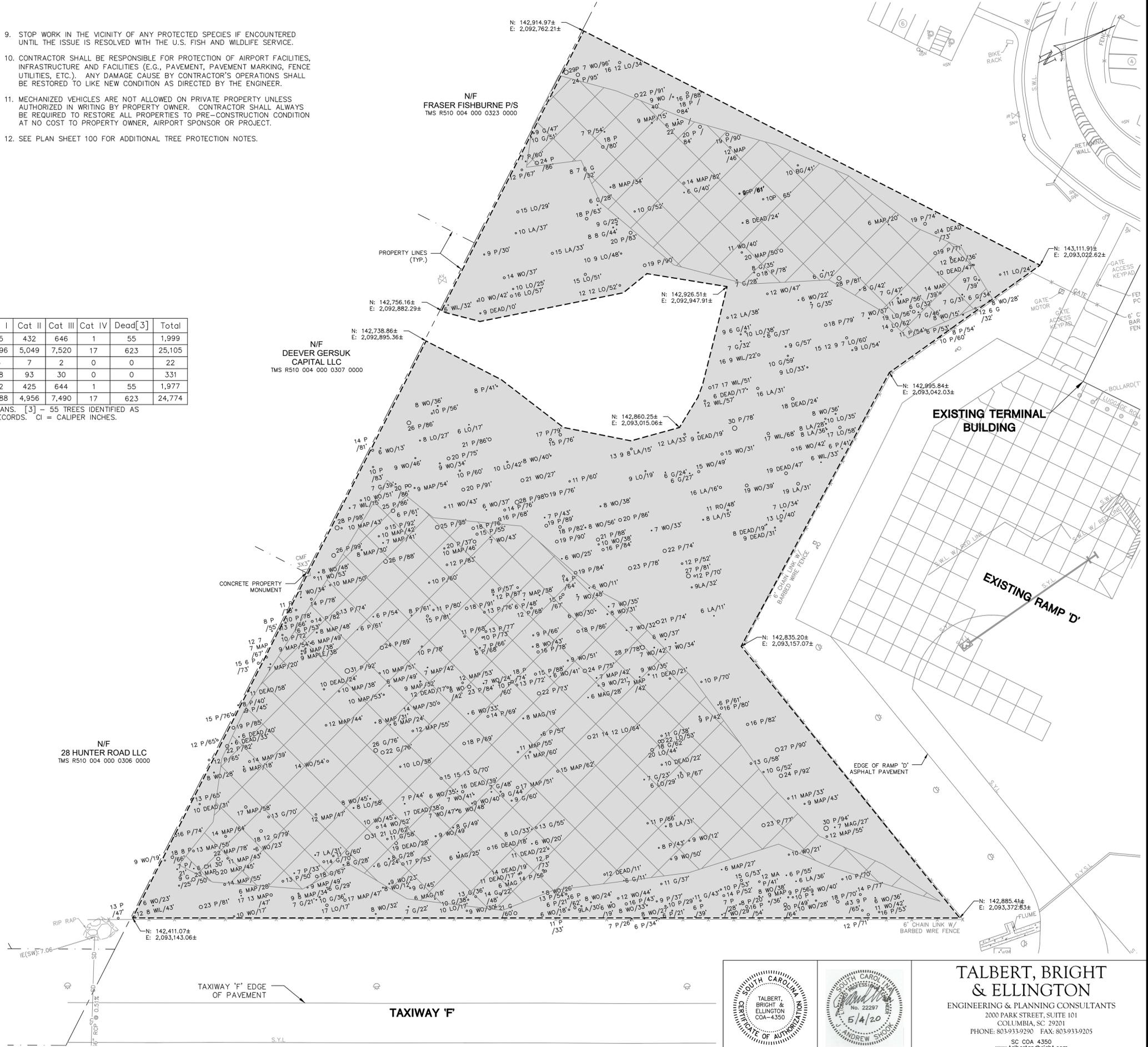
**DEMOLITION PLAN**  
**COMMERCIAL APRON EXPANSION**  
FILE NAME: 1906-DEMO  
SCALE: 1"=40'

**TREE REMOVAL NOTES:**

- ALL WORK SHALL COMPLY WITH THE SAFETY PLAN AND FAA ADVISORY CIRCULAR AC 150/5370-26, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION."
- AT NO TIME SHALL THE CONTRACTOR PARK ANY VEHICLES OR EQUIPMENT IN AIRPORT OR PUBLIC ROADWAYS, OR BLOCK IN ANY MANNER THE ACCESS ROADS TO AIRPORT FACILITIES, PARTICULARLY ACCESS ROADS AND FROM THE ARFF FACILITY.
- CONTRACTOR TO PROVIDE NOTICE TO ENGINEER AND OWNER THREE (3) DAYS PRIOR TO BEGINNING TREE REMOVAL OPERATIONS.
- PRIOR TO ANY TREE PRUNING OR REMOVAL THE CONTRACTOR SHALL FLAG THE AIRPORT AND OTHER PROPERTY LINES AND THE DESIGNATED WETLAND AREAS. NO TREES SHALL BE REMOVED UNTIL THE FLAGGING IS INSPECTED BY A DESIGNATED REPRESENTATIVE OF THE TOWN OF HILTON HEAD ISLAND TO ENSURE THAT IT IS ACCEPTABLE (REQUIRED 48 HOURS ADVANCE NOTIFICATION).
- THE CONTRACTOR SHALL USE A PROFESSIONAL SURVEYOR REGISTERED IN THE STATE OF SOUTH CAROLINA. THIS COST SHALL BE INCIDENTAL TO THE PROJECT. THE CONTRACTOR WILL RECEIVE A DIGITAL (CADD) FILE OF THIS TREE REMOVAL PLAN.
- THE CONTRACTOR SHALL ENTER THE PROPERTY AT LOCATIONS APPROVED BY ENGINEER AND OWNER.
- THE CONTRACTOR SHALL REMOVE ALL TIMBER, FOLIAGE AND VEGETATIVE DEBRIS UNLESS THE OWNER PROVIDES A WRITTEN NOTICE INDICATING OTHERWISE.
- TREES REMOVED SHALL BE HARVESTED AND USED AS SAW TIMBER, PULPWOOD, OR CHIPS FOR BOILER FUEL IF ECONOMICALLY VIABLE. DISPOSE OF ANY VEGETATIVE DEBRIS OFFSITE IN A MANNER PERMITTED BY THE SOUTH CAROLINA DEPARTMENT OF HEALTH AND ENVIRONMENTAL CONTROL.
- STOP WORK IN THE VICINITY OF ANY PROTECTED SPECIES IF ENCOUNTERED UNTIL THE ISSUE IS RESOLVED WITH THE U.S. FISH AND WILDLIFE SERVICE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF AIRPORT FACILITIES, INFRASTRUCTURE AND FACILITIES (E.G., PAVEMENT, PAVEMENT MARKING, FENCE UTILITIES, ETC.). ANY DAMAGE CAUSED BY CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO LIKE NEW CONDITION AS DIRECTED BY THE ENGINEER.
- MECHANIZED VEHICLES ARE NOT ALLOWED ON PRIVATE PROPERTY UNLESS AUTHORIZED IN WRITING BY PROPERTY OWNER. CONTRACTOR SHALL ALWAYS BE REQUIRED TO RESTORE ALL PROPERTIES TO PRE-CONSTRUCTION CONDITION AT NO COST TO PROPERTY OWNER, AIRPORT SPONSOR OR PROJECT.
- SEE PLAN SHEET 100 FOR ADDITIONAL TREE PROTECTION NOTES.

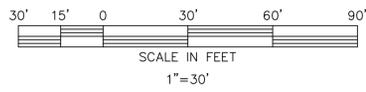
Description	Unit	Cat I	Cat II	Cat III	Cat IV	Dead [3]	Total
Pre-Removal (Existing Trees Prior to Removal)[1]	EA	865	432	646	1	55	1,999
	CI	11,896	5,049	7,520	17	623	25,105
Post-Removal (Remaining Trees After Removal)[2]	EA	13	7	2	0	0	22
	CI	208	93	30	0	0	331
Difference in Pre-Removal and Post-Removal	EA	852	425	644	1	55	1,977
	CI	11,688	4,956	7,490	17	623	24,774

[1] - BASED UPON DESIGN SURVEY. [2] - BASED UPON DESIGN PLANS. [3] - 55 TREES IDENTIFIED AS DEAD. FINAL NUMBER WILL BE DERIVED FROM PROJECT ARBORIST RECORDS. CI = CALIPER INCHES.



**LEGEND**

	EXISTING WETLAND
	PROPOSED CLEARING AND GRUBBING
	PROPOSED CLEARING AND GRUBBING LIMITS
	PINE TREE TO BE REMOVED
	WATER OAK TREE TO BE REMOVED
	LIVE OAK TREE TO BE REMOVED
	LAUREL OAK TREE TO BE REMOVED
	WILLOW OAK TREE TO BE REMOVED
	RED OAK TREE TO BE REMOVED
	RED MAPLE TREE TO BE REMOVED
	SWEET GUM TREE TO BE REMOVED
	MAGNOLIA TREE TO BE REMOVED
	BLACK GUM TREE TO BE REMOVED
	CHERRY TREE TO BE REMOVED



REVISIONS	DATE
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DESIGNED BY:	CEV
PROJECT ENG.:	JAS
PROJECT NO.:	2119-1906
DRAWN BY:	UPB
CHECKED BY:	PET
DATE ISSUED:	MAY 2020

**HILTON HEAD ISLAND AIRPORT**

*"Fly to the Fun!"*

**TREE REMOVAL PLAN**

**COMMERCIAL APRON EXPANSION**

FILE NAME: 1906-TREEDENO SCALE: 1"=30'

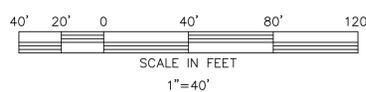


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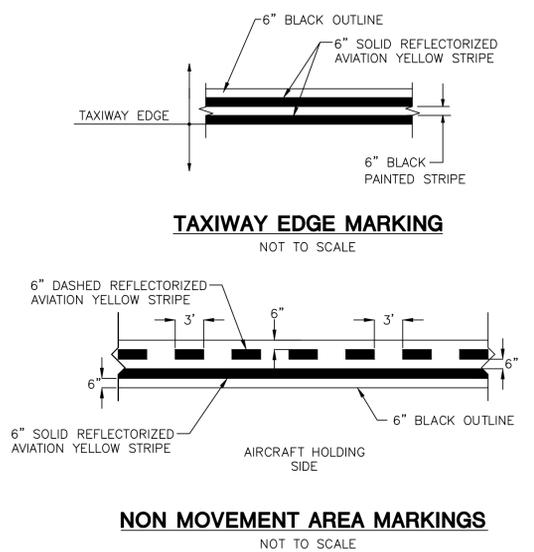


**GEOMETRIC & MARKING PLAN NOTES:**

- DISTANCES SHOWN ON EXISTING CONCRETE/ASPHALT SHALL BE CONSIDERED APPROXIMATE AND FIELD VERIFIED PRIOR TO ANY REMOVAL/PAVING OPERATIONS.
- ALL AVIATION YELLOW MARKINGS SHOWN SHALL BE REFLECTORIZED, UNLESS OTHERWISE NOTED ON PLAN.
- CONTRACTOR SHALL RECEIVE A DIGITAL AUTOCAD DRAWING OF THIS PLAN TO AID IN THE STAKING/LAYOUT OF THE PROPOSED ITEMS TO BE CONSTRUCTED.



LEGEND	
	PROPOSED 12" CONCRETE PAVEMENT
	PROPOSED 8" CONCRETE PAVEMENT
	PROPOSED AIRFIELD ASPHALT PAVEMENT
	PROPOSED STANDARD ASPHALT PAVEMENT

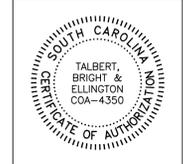


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PROJECT ENG.:	JAS
PROJECT NO.:	2119-1906
DRAWN BY:	UPB
CHECKED BY:	PET
DATE ISSUED:	MAY 2020

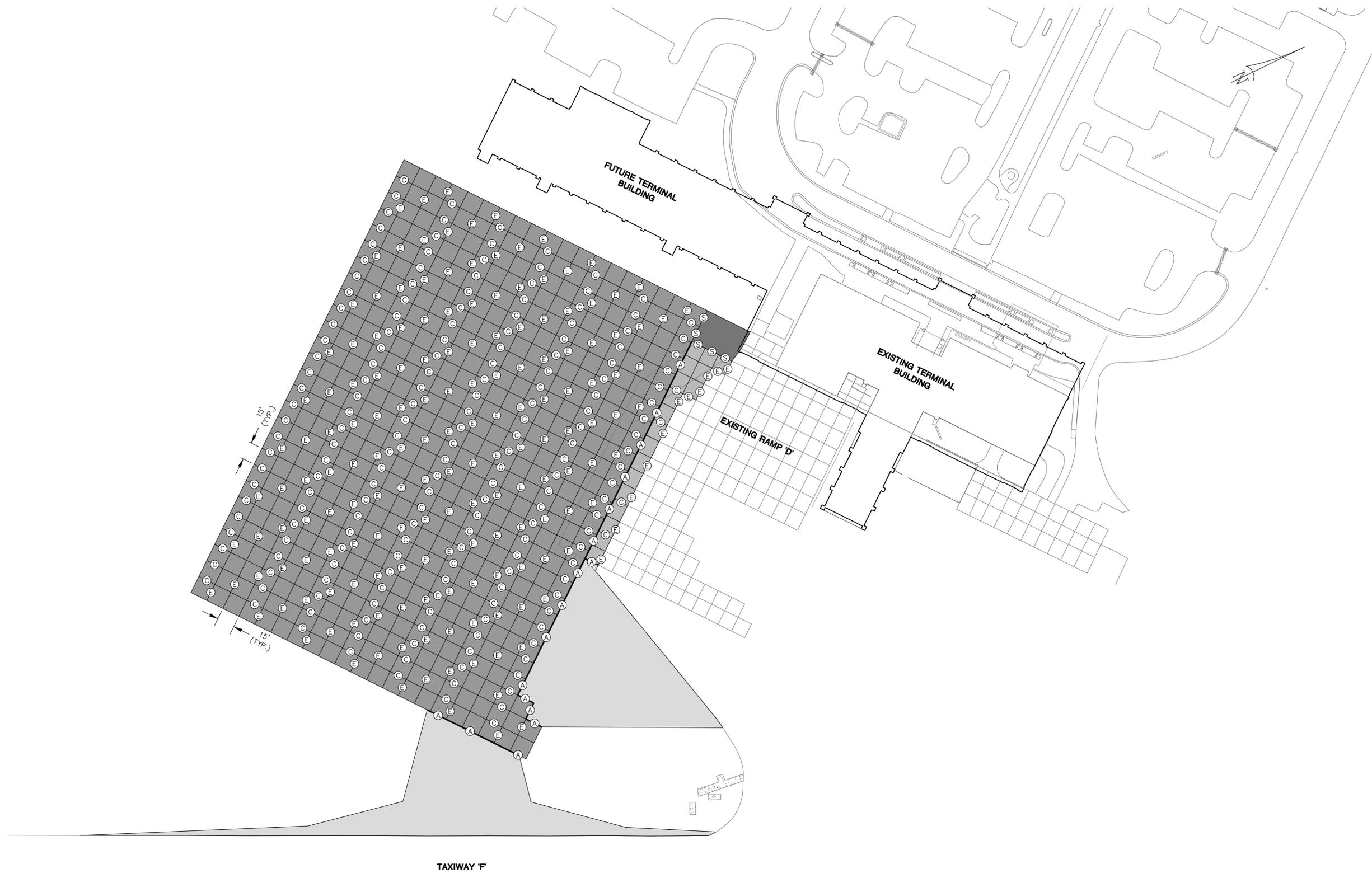
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**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*

**GEOMETRIC & MARKING LAYOUT PLAN**  
**COMMERCIAL APRON EXPANSION**  
 SCALE: 1"=40'  
 FILE NAME: 1906-GEOM



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15'  
(TYP.)

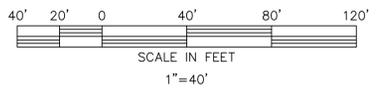
15'  
(TYP.)

TAXIWAY F

FUTURE TERMINAL BUILDING

EXISTING TERMINAL BUILDING

EXISTING RAMP 'D'



- JOINT LAYOUT NOTES:**
1. AT ALL CONCRETE PAVEMENT TIE-IN LOCATIONS TO EXISTING CONCRETE PAVEMENT, CONTRACTOR SHALL MATCH EXISTING JOINT PATTERN.
  2. THE CIRCLES SHOWN REPRESENT TYPICAL JOINT TYPES. EACH CONCRETE JOINT IS NOT BEING LABELED FOR CLARITY REASONS. ALL CONCRETE JOINTS SHALL BE EITHER DOWELED CONSTRUCTION, DOWELED CONTRACTION, THICKENED EDGE EXPANSION, OR SHOULDER JOINT, PER DETAILS ON SHEETS 600 AND 601.
  3. 12" CONCRETE PAVEMENT SLABS SHALL BE 15'X15' UNLESS OTHERWISE SHOWN.

LEGEND			
(A)	THICKENED EDGE ISOLATION JOINT		PROPOSED 12" CONCRETE PAVEMENT
(E)	DOWELED CONSTRUCTION JOINT		PROPOSED 8" CONCRETE PAVEMENT
(C)	DOWELED CONTRACTION JOINT		PROPOSED AIRFIELD ASPHALT PAVEMENT
(S)	STANDARD ASPHALT JOINT		PROPOSED STANDARD ASPHALT PAVEMENT



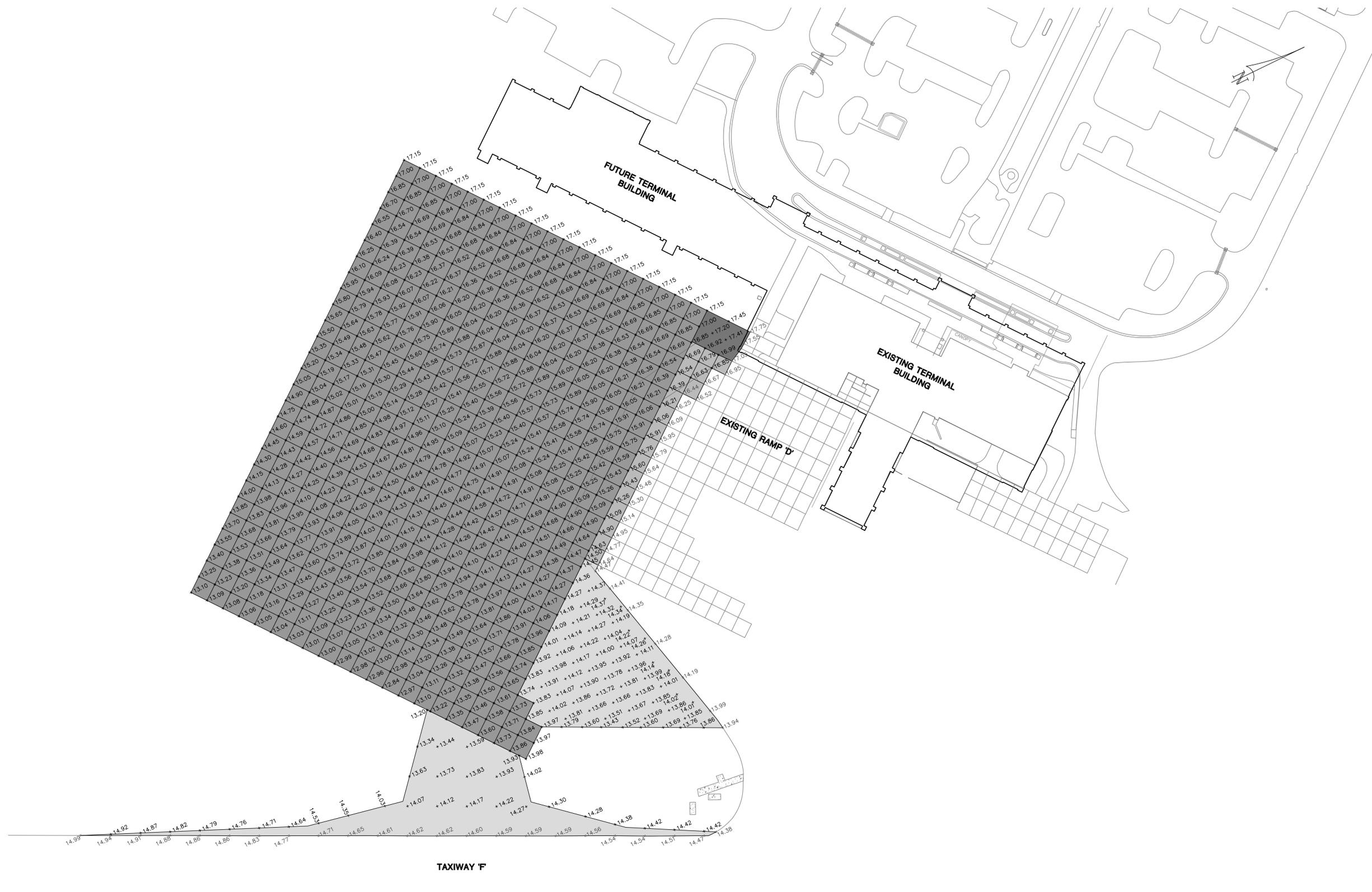
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DESIGNED BY: CEV	PROJECT ENG.: JAS
PROJECT NO.: 2119-1906	DRAWN BY: JPB
CHECKED BY: PET	DATE ISSUED: MAY 2020

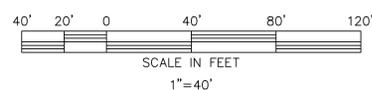
**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*

**JOINT LAYOUT PLAN**  
**COMMERCIAL APRON EXPANSION**  
 SCALE: 1"=40'  
 FILE NAME: 1906-GEOM

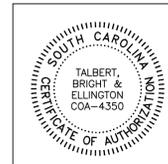


**JOINT ELEVATION PLAN NOTES:**

1. ALL EXISTING SPOT ELEVATIONS LISTED SHALL BE CONSIDERED APPROXIMATE AND FIELD VERIFIED PRIOR TO ANY CONCRETE PAVING OPERATIONS.



LEGEND	
	PROPOSED 12" CONCRETE PAVEMENT
	PROPOSED 8" CONCRETE PAVEMENT
	PROPOSED AIRFIELD ASPHALT PAVEMENT
	PROPOSED STANDARD ASPHALT PAVEMENT
x 24.75	PROPOSED SPOT ELEVATION
x 24.75	EXISTING SPOT ELEVATION



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PROJECT ENG.:	JAS
PROJECT NO.:	2119-1906
DRAWN BY:	UPB
CHECKED BY:	PET
DATE ISSUED:	MAY 2020

**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*

**JOINT ELEVATION PLAN**  
**COMMERCIAL APRON EXPANSION**  
 FILE NAME: 1906-JUNTELEV SCALE: 1"=40'

LIMITS OF DISTURBANCE: NPDES

EROSION PREVENTION	
LAND GRADING:	LD OR (LD)
SURFACE ROUGHENING:	(SR)
TOPSOILING:	(TS)
TEMPORARY SEEDING:	TS
MULCHING:	M
ECB OR TRM:	(ECB/TRM)
FGM:	(FGM)
BFM:	(BFM)
PERMANENT SEEDING:	PS
SODDING:	SO
RIPRAP:	(RIPRAP)
OUTLET PROTECTION:	RIPRAP ECB or TRM
DUST CONTROL:	DC
POLYACRYLAMIDE (PAM):	PAM

SEDIMENT CONTROL	
SEDIMENT BASIN:	(SB)
TEMPORARY SEDIMENT TRAP:	(TST)
ROCK SEDIMENT DIKE:	(RSD)
ROCK CHECK DAM:	(RCD)
SEDIMENT TUBE:	(ST)
SILT FENCE:	(SF)
REINFORCED SILT FENCE:	(RSF)
TYPE A - FABRIC INLET PROTECTION:	(A)
TYPE A - SEDIMENT TUBE INLET PROTECTION:	(A-ST)
TYPE B - WIRE MESH AND STONE DROP INLET PROTECTION:	(B)
TYPE C - BLOCK AND GRAVEL INLET PROTECTION:	(C)
TYPE D - RIGID INLET FILTERS:	(D)
TYPE E - SURFACE COURSE CURB INLET FILTER:	(E)
TYPE F - INLET TUBE:	(F)
TYPE FC - FILTER BAG CURB INLET PROTECTION:	(FC)
TYPE FB - FILTER BAG CURB INLET PROTECTION:	(FB)
CONCRETE WASHOUT:	(CW)

RUNOFF CONVEYANCE MEASURES	
VEGETATED CHANNELS:	(VC)
RIPRAP-LINED CHANNELS:	(RLC)
ECB OR TRM-LINED CHANNELS:	(ELC)
PAVED CHANNELS:	(PC)
PIPE SLOPE DRAINS:	(PSD)
TEMPORARY STREAM CROSSING:	(TSC)
TEMPORARY DIVERSION DITCH OR SWALE:	(TDD)
PERMANENT DIVERSION DITCH:	(PDD)
DIVERSION DIKE OR BERM:	(DD)
LEVEL SPREADER:	(LS)
SUBSURFACE DRAIN:	(SSD)



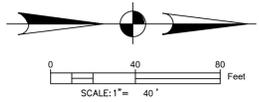
NOT FOR CONSTRUCTION

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DESIGNED BY:	BCB
PROJECT ENG.:	PRM
PROJECT NO.:	100266J
DRAWN BY:	BCB
CHECKED BY:	PRM
DATE ISSUED:	05/04/20

**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*

INITIAL EROSION CONTROL PLAN  
 COMMERCIAL APRON EXPANSION  
 FILE NAME: 1906-GEOM  
 SCALE: 1" = 40'



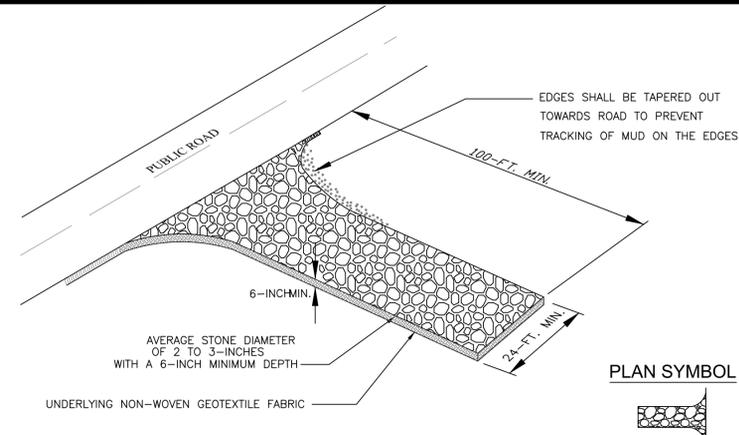
**Ward Edwards ENGINEERING**  
 P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910  
 PH (843) 837-5290 / FAX (843) 837-2558  
 WWW.WARDEDWARDS.COM

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**SOUTH CAROLINA PROFESSIONAL ENGINEER**  
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 06/01/04/2000  
 PAUL R. MOORE

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SHT. 400



SPECIFICATION	SIZE
ROCK PAD THICKNESS	6 INCHES
ROCK PAD WIDTH	24 FEET
ROCK PAD LENGTH	100 FEET
ROCK PAD STONE SIZE	D = 2-3 INCHES

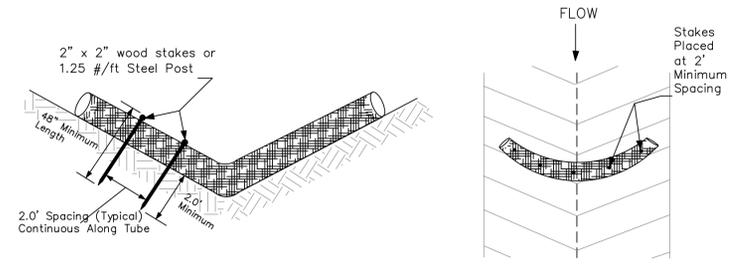
South Carolina Department of Health and Environmental Control  
**CONSTRUCTION ENTRANCE**  
 STANDARD DRAWING NO. SC-06 PAGE 1 of 2  
 FEBRUARY 2014 DATE  
 NOT TO SCALE

- CONSTRUCTION ENTRANCE - GENERAL NOTES**
1. Stabilized construction entrances should be used at all points where traffic will egress/ingress a construction site onto a public road or any impervious surfaces, such as parking lots.
  2. Install a non-woven geotextile fabric prior to placing any stone.
  3. Install a culvert pipe across the entrance when needed to provide positive drainage.
  4. The entrance shall consist of 2-inch to 3-inch D50 stone placed at a minimum depth of 6-inches.
  5. Minimum dimensions of the entrance shall be 24-feet wide by 100-feet long, and may be modified as necessary to accommodate site constraints.
  6. The edges of the entrance shall be tapered out towards the road to prevent tracking at the edge of the entrance.
  7. Divert all surface runoff and drainage from the stone pad to a sediment trap or basin or other sediment trapping structure.
  8. Limestone may not be used for the stone pad.

- CONSTR. ENTRANCE - INSPECTION & MAINTENANCE**
1. The key to functional construction entrances is weekly inspections, routine maintenance, and regular sediment removal.
  2. Regular inspections of construction entrances shall be conducted once every calendar week and, as recommended, within 24-hours after each rainfall event that produces 1/2-inch or more of precipitation.
  3. During regular inspections, check for mud and sediment buildup and pad integrity. Inspection frequencies may need to be more frequent during long periods of wet weather.
  4. Reshape the stone pad as necessary for drainage and runoff control.
  5. Wash or replace stones as needed and as directed by site inspector. The stone in the entrance should be washed or replaced whenever the entrance fails to reduce the amount of mud being carried off-site by vehicles. Frequent washing will extend the useful life of stone pad.
  6. Immediately remove mud and sediment tracked or washed onto adjacent impervious surfaces by brushing or sweeping. Flushing should only be used when the water can be discharged to a sediment trap or basin.
  7. During maintenance activities, any broken pavement should be repaired immediately.
  8. Construction entrances should be removed after the site has reached final stabilization. Permanent vegetation should replace areas from which construction entrances have been removed, unless area will be converted to an impervious surface to serve post-construction.

South Carolina Department of Health and Environmental Control  
**CONSTRUCTION ENTRANCE**  
 STANDARD DRAWING NO. SC-06 PAGE 2 of 2  
 FEBRUARY 2014 DATE  
 GENERAL NOTES

**SEDIMENT TUBE INSTALLATION**



**SEDIMENT TUBE SPACING**

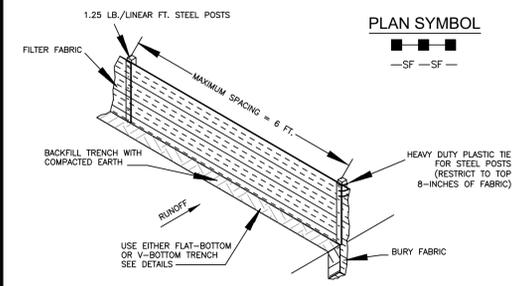
SLOPE	MAX. SEDIMENT TUBE SPACING
LESS THAN 2%	150-FEET
2%	100-FEET
3%	75-FEET
4%	50-FEET
5%	40-FEET
6%	30-FEET
GREATER THAN 6%	25-FEET

South Carolina Department of Health and Environmental Control  
**SEDIMENT TUBES**  
 STANDARD DRAWING NO. SC-05 PAGE 1 of 2  
 FEBRUARY 2014 DATE  
 NOT TO SCALE

- SEDIMENT TUBES - GENERAL NOTES**
1. Sediment tubes may be installed along contours, in drainage conveyance channels, and around inlets to help prevent off-site discharge of sediment-laden stormwater runoff.
  2. Sediment tubes are elongated tubes of compacted geotextiles, curled excelsior wood, natural coconut fiber, or hardwood mulch. Straw, pine needles, and leaf mulch-filled sediment tubes are not permitted.
  3. The outer netting of the sediment tube should consist of seamless, high-density polyethylene photodegradable materials treated with ultraviolet stabilizers or a seamless, high-density polyethylene non-degradable material.
  4. Sediment tubes, when used as checks within channels, should range between 18-inches and 24-inches depending on channel dimensions. Diameters outside this range may be allowed where necessary when approved.
  5. Curled excelsior wood, or natural coconut products that are rolled up to create a sediment tube are not allowed.
  6. Sediment tubes should be staked using wooden stakes (2-inch X 2-inch) or steel posts (standard "U" or "T" sections with a minimum weight of 1.25 pounds per foot) at a minimum of 48-inches in length placed on 2-foot centers.
  7. Install all sediment tubes to ensure that no gaps exist between the soil and the bottom of the tube. Manufacturer's recommendations should always be consulted before installation.
  8. The ends of adjacent sediment tubes should be overlapped 6-inches to prevent flow and sediment from passing through the field joint.
  9. Sediment tubes should not be stacked on top of one another, unless recommended by manufacturer.
  10. Each sediment tube should be installed in a trench with a depth equal to 1/5 the diameter of the sediment tube.
  11. Sediment tubes should continue up the side slopes a minimum of 1-foot above the design flow depth of the channel.
  12. Install stakes at a diagonal facing incoming runoff.

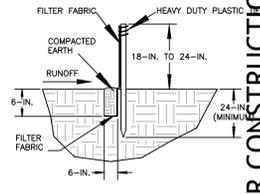
South Carolina Department of Health and Environmental Control  
**SEDIMENT TUBES**  
 STANDARD DRAWING NO. SC-05 PAGE 2 of 2  
 FEBRUARY 2014 DATE  
 GENERAL NOTES

**SILT FENCE INSTALLATION**

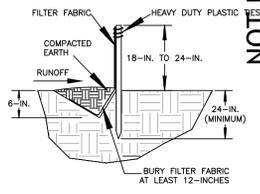


- SILT FENCE - GENERAL NOTES**
1. Do not place silt fence across channels or in other areas subject to concentrated flows. Silt fence should not be used as a velocity control BMP. Concentrated flows are any flows greater than 0.5 cfs.
  2. Maximum sheet or overland flow path length to the silt fence shall be 100-feet.
  3. Maximum slope steepness (normal [perpendicular] to the fence line) shall be 2:1.
  4. Silt fence joints, when necessary, shall be completed by one of the following options:
    - Wrap each fabric together at a support post with both ends fastened to the post, with a 1-foot minimum overlap;
    - Overlap silt fence by installing 3-feet passed the support post to which the new silt fence roll is attached. Attach old roll to new roll with heavy-duty plastic ties; or,
    - Overlap entire width of each silt fence roll from one support post to the next support post.
  5. Attach filter fabric to the steel posts using heavy-duty plastic ties that are evenly spaced within the top 8-inches of the fabric.
  6. Install the silt fence perpendicular to the direction of the stormwater flow and place the silt fence the proper distance from the toe of steep slopes to provide sediment storage and access for maintenance and cleanout.
  7. Install Silt Fence Checks (Tie-Backs) every 50-100 feet, dependent on slope, along silt fence that is installed with slope and where concentrated flows are expected or are documented along the proposed/installed silt fence.

**FLAT-BOTTOM TRENCH DETAIL**



**V-SHAPED TRENCH DETAIL**



South Carolina Department of Health and Environmental Control  
**SILT FENCE**  
 STANDARD DRAWING NO. SC-03 PAGE 1 of 2  
 FEBRUARY 2014 DATE  
 NOT TO SCALE

- SILT FENCE - POST REQUIREMENTS**
1. Silt fence posts must be 48-inch long steel posts that meet, at a minimum, the following physical characteristics:
    - Composed of a high strength steel with a minimum yield strength of 50,000 psi.
    - Include a standard "T" section with a nominal face width of 1.38-inches and a nominal "T" length of 1.48-inches.
    - Weigh 1.25 pounds per foot (± 8%)
  2. Posts shall be equipped with projections to aid in fastening of filter fabric.
  3. Steel posts may need to have a metal soil stabilization plate welded near the bottom when installed along steep slopes or installed in loose soils. The plate should have a minimum cross section of 17-square inches and be composed of 15 gauge steel, at a minimum. The metal soil stabilization plate should be completely buried.
  4. Install posts to a minimum of 24-inches. A minimum height of 1- to 2-inches above the fabric shall be maintained, and a maximum height of 3 feet shall be maintained above the ground.
  5. Post spacing shall be at a maximum of 6-feet on center.

- SILT FENCE - FABRIC REQUIREMENTS**
1. Silt fence must be composed of woven geotextile filter fabric that consists of the following requirements:
    - Composed of fibers consisting of long chain synthetic polymers of at least 85% by weight of polypropylene, polyesters, or polyamides that are formed into a network such that the filaments or yarns retain dimensional stability relative to each other;
    - Free of any treatment or coating which might adversely affect its physical properties after installation;
    - Free of any defects or flaws that significantly affect its physical and/or filtering properties; and,
    - Have a minimum width of 36-inches.
  2. Use only fabric appearing on SC DOT's Qualified Products Listing (QPL), Approval Sheet #34, meeting the requirements of the most current edition of the SC DOT Standard Specifications for Highway Construction.
  3. 12-inches of the fabric should be placed within excavated trench and loed in when the trench is backfilled.
  4. Filter Fabric shall be purchased in continuous rolls and cut to the length of the barrier to avoid joints.
  5. Filter Fabric shall be installed at a minimum of 24-inches above the ground.

- SILT FENCE - INSPECTION & MAINTENANCE**
1. The key to functional silt fence is weekly inspections, routine maintenance, and regular sediment removal.
  2. Regular inspections of silt fence shall be conducted once every calendar week and, as recommended, within 24-hours after each rainfall event that produces 1/2-inch or more of precipitation.
  3. Attention to sediment accumulations along the silt fence is extremely important. Accumulated sediment should be continually monitored and removed when necessary.
  4. Remove accumulated sediment when it reaches 1/3 the height of the silt fence.
  5. Removed sediment shall be placed in stockpile storage areas or spread thinly across disturbed area. Stabilize the removed sediment after it is relocated.
  6. Check for areas where stormwater runoff has eroded a channel beneath the silt fence, or where the fence has sagged or collapsed due to runoff overlapping the silt fence. Install checks/tie-backs and/or reinstall silt fence, as necessary.
  7. Check for tears within the silt fence, areas where silt fence has begun to decompose, and for any other circumstances that may render the silt fence ineffective. Removed damaged silt fence and reinstall new silt fence immediately.
  8. Silt fence should be removed within 30 days after final stabilization is achieved and once it is removed, the resulting disturbed area shall be permanently stabilized.

South Carolina Department of Health and Environmental Control  
**SILT FENCE**  
 STANDARD DRAWING NO. SC-03 PAGE 2 of 2  
 FEBRUARY 2014 DATE  
 GENERAL NOTES

**NOT FOR CONSTRUCTION**

REVISIONS	DATE
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DESIGNED BY: BCB	PROJECT ENG.: PRM	PROJECT NO.: 100266J	DRAWN BY: BCB	CHECKED BY: PRM	DATE ISSUED: 05/04/20
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**HILTON HEAD ISLAND AIRPORT**

"Fly to the Fun"

INITIAL EROSION CONTROL DETAILS

COMMERCIAL APRON EXPANSION

SCALE: AS SHOWN

FILE NAME: 1906-GEOM

**Ward Edwards ENGINEERING**

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No. 22816

06/01/2008

PAUL R. MOORE

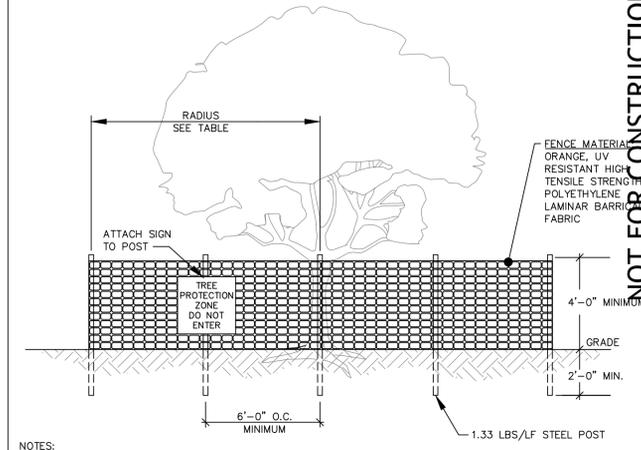
**TALBERT, BRIGHT & ELLINGTON**

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SHT. 401



NOT FOR CONSTRUCTION

- NOTES:**
1. ALL TREES DESIGNATED TO BE SAVED SHALL BE PROTECTED BY FENCING.
  2. INSTALL TREE PROTECTION FENCE TO RADIUS INDICATED IN TABLE UNLESS OTHERWISE INDICATED ON PLANS.
  3. WARNING SIGNS TO BE MADE OF DURABLE WATERPROOF MATERIAL.
  4. ALL WARNING SIGN LETTERS TO BE AT LEAST 3 INCHES HIGH, CLEARLY LEGIBLE AND SPACED A MINIMUM OF ONE EVERY 40 FT. FOR PROTECTION AREAS LESS THAN 40 FT. IN PERIMETER, PROVIDE NO LESS THAN ONE SIGN PER SIDE.
  5. THE SIZE OF EACH WARNING SIGN MUST BE A MINIMUM OF 2' x 2' AND BE VISIBLE FROM BOTH SIDES OF THE FENCE.
  6. ATTACH SIGNS SECURELY TO FENCE POSTS AND FABRIC.
  7. THERE SHALL BE NO STORAGE OF MATERIAL WITHIN THE BOUNDARIES OF THE TREE PROTECTION FENCING.
  8. TREE PROTECTION FENCING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT. FENCING MUST REMAIN UPRIGHT AND SLACK FREE.

JURISDICTION	RADIUS OF CIRCULAR TPZ
BEAUFORT COUNTY BEAUFORT CO. DEV. CODE 5.11.100	1 FOOT PER INCH OF TRUNK DBH
TOWN OF BLUFFTON UDO 5.3.3	1.5 FEET PER INCH OF TRUNK DBH OR 10 FEET WHICHEVER IS GREATER
TOWN OF HILTON HEAD LMO 16-6-104, J-3A	FENCING AT DRIP LINE FOR ALL TREES TO BE RETAINED
CITY OF BEAUFORT BEAUFORT CODE 5.3.3	0.5 FOOT PER INCH OF TRUNK DBH
JASPER COUNTY ZONING ORD. ART. 13.5	FENCING AT DRIP LINE FOR ALL TREES TO BE RETAINED
TOWN OF PORT ROYAL PORT ROYAL CODE 5.7.70	1.5 FEET PER INCH OF TRUNK DBH OR 5 FEET WHICHEVER IS GREATER
CITY OF HARDEEVILLE MZ&DD 4.8, F-3	FENCING AT DRIP LINE FOR ALL TREES TO BE RETAINED

DBH = TRUNK DIAMETER AT BREAST HEIGHT

**TREE PROTECTION FENCE**

DETAIL #02915-008

REVISIONS	DATE
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DESIGNED BY: BCB	PROJECT ENG.: PRM
PROJECT NO.: 100266J	DRAWN BY: BCB
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**HILTON HEAD ISLAND AIRPORT**

*"Fly to the Fun"*

INITIAL EROSION CONTROL DETAILS
COMMERCIAL APRON EXPANSION
FILE NAME: 1906-GEOM
SCALE: AS SHOWN

**Ward Edwards**  
ENGINEERING

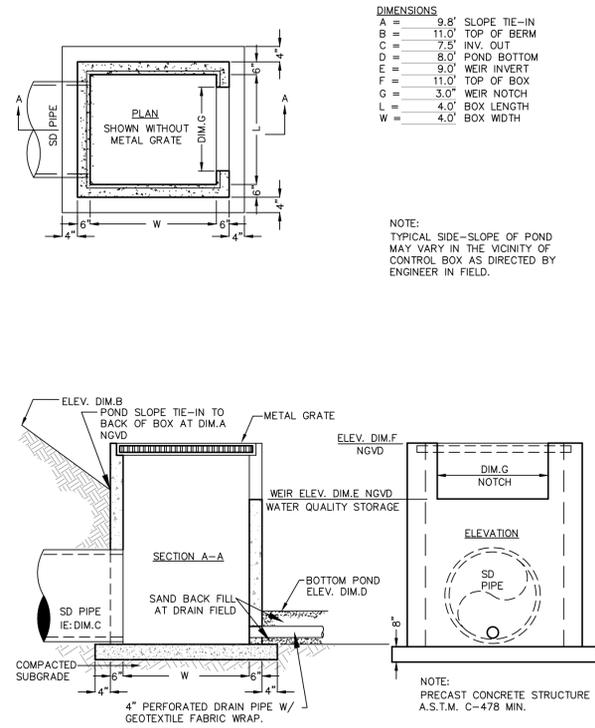
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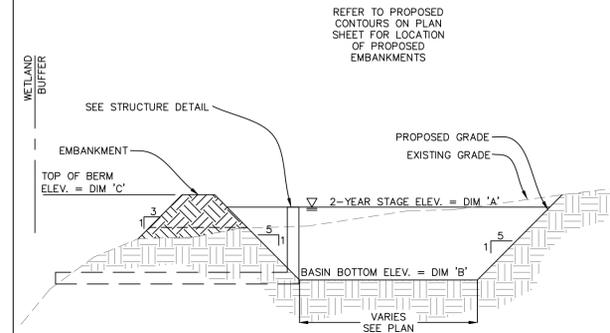
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INFILTRATION STRUCTURE

DETAIL 02630-031



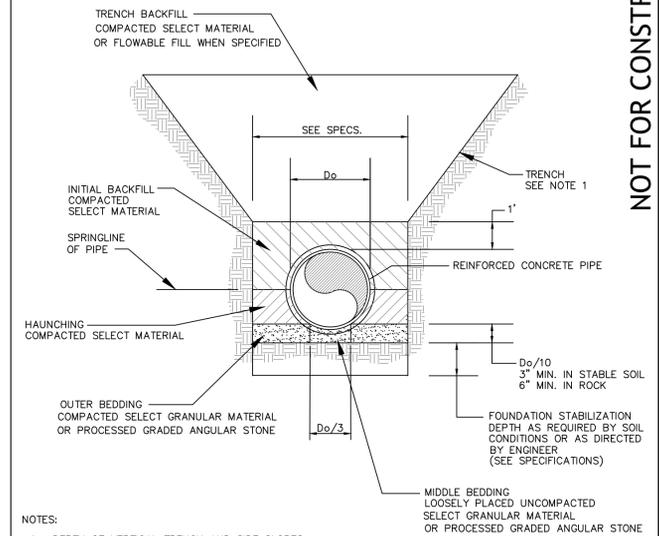
**DIMENSION DATA**

	BASIN 1	2 YR. PEAK
DIM 'A'	10.92'	8.00'
DIM 'B'	8.00'	12.00'
DIM 'C'	12.00'	

- EMBANKMENT CONSTRUCTION NOTES:**
- ALL VEGETATION ROOTS, BRUSH, SOD, RUBBISH, DEBRIS, AND ALL OTHER UNSUITABLE MATERIALS SHALL BE STRIPPED AND REMOVED FROM THE LIMITS OF THE PROPOSED EMBANKMENT CONSTRUCTION.
  - THE MATERIAL FOR THE EMBANKMENT CONSTRUCTION SHALL CONSIST OF SAND-CLAY OR CLAYEY SAND. THE SOIL FILL SHALL BE FREE OF ORGANICS AND SENSITIVE CLAY MATERIALS.
  - THE EMBANKMENT AND BACKFILL MATERIAL SHALL BE COMPACTED TO 95% MAXIMUM DRY DENSITY, AS DETERMINED BY THE STANDARD PROCTOR TEST. ALL FILL SHALL BE COMPACTED TO THE LINES, DEPTHS, AND GRADES AS INDICATED. EXCESS EXCAVATION OF THE EXISTING SOILS MAY BE NECESSARY TO OBTAIN THE REQUIRED DENSITY AT THE LIMITS OF EXCAVATION.
- EROSION & SEDIMENT CONTROL NOTE:**
- SEE PLANS FOR INDICATION OF EROSION CONTROL MEASURES AND SURFACE STABILIZATION METHOD TO BE USED

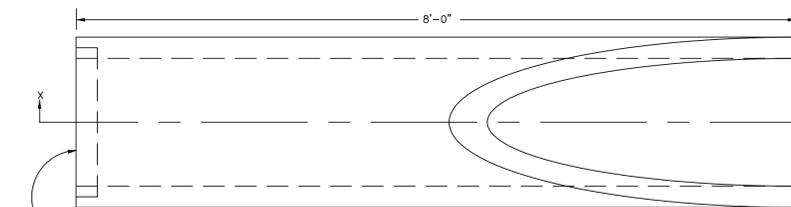
DRY DETENTION BASIN CROSS SECTION

DETAIL 02630-033

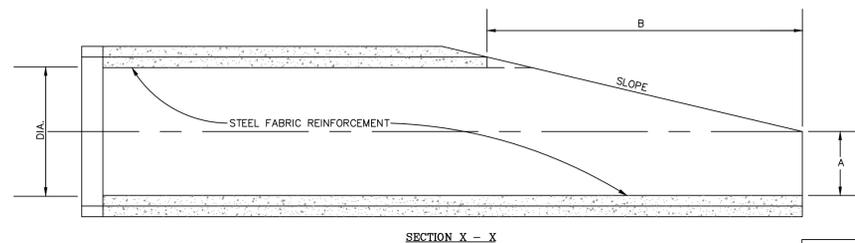


- NOTES:**
- DEPTH OF VERTICAL TRENCH AND SIDE SLOPES OF TRENCH MAY VARY DEPENDING ON DEPTH OF EXCAVATION AND TRENCH PROTECTION SYSTEM USED. COMPLY WITH OSHA REGULATIONS (STANDARDS - 29 CFR) PART 1956 SUBPART P, EXCAVATIONS.
  - WHEN PROCESSED ANGULAR STONE IS USED FOR MIDDLE AND OUTER BEDDING, THE STONE SHALL BE COMPLETELY WRAPPED IN FILTER FABRIC TO PREVENT MIGRATION OF SURROUNDING SOILS.

EMBEDMENT DETAIL FOR REINFORCED CONCRETE PIPE



GROOVE END ON OUTLET END SECTIONS  
 SPIGOT END ON INLET END SECTIONS



- NOTE:**
- BEVELED END SECTIONS WILL BE MANUFACTURED IN ACCORDANCE WITH SECTION 714 OF THE S.C. STATE HIGHWAY DEPARTMENT STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION. THESE SPECIAL PIPE SECTIONS WILL BE MADE DURING THE MANUFACTURING OF OTHER STATE APPROVED REINFORCED CONCRETE PIPE.
- BEVELED ENDS ON ALUMINUM AND STEEL PIPE ARE TO CONFORM TO DIMENSIONS SHOWN FOR CONCRETE PIPE.
- TOTAL LINE LENGTH OF ALTERNATE ALUMINUM AND STEEL PIPE MAY BE PLACED IN ONE OR TWO LENGTHS AT THE DISCRETION OF THE CONTRACTOR.

**END SECTION DIMENSIONS**

DIA.	A	B	SLOPE
15"	0'- 6"	2'- 0"	4 : 1
18"	0'- 9"	3'- 0"	4 : 1
24"	0'- 10"	3'- 4"	4 : 1
30"	1'- 0"	4'- 6"	3 : 1
36"	1'- 3"	5'- 3"	3 : 1
42"	1'- 8"	5'- 6"	3 : 1
48"	2'- 0"	6'- 0"	3 : 1

BEVELED END PIPE SECTION

DETAIL 02630-026

NOT FOR CONSTRUCTION

REVISIONS	DATE
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DESIGNED BY:	BCB
PROJECT ENG.:	FRM
PROJECT NO.:	100266J
DRAWN BY:	BCB
CHECKED BY:	FRM
DATE ISSUED:	05/04/20

**HILTON HEAD ISLAND AIRPORT**

"Fly to the Fun"

GRADING & DRAINAGE DETAILS

COMMERCIAL APRON EXPANSION

SCALE: AS SHOWN

FILE NAME: 1906-GEOM

**Ward Edwards ENGINEERING**

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**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*



DRAINAGE & EROSION CONTROL NOTES

COMMERCIAL APRON EXPANSION

SCALE: N/A

FILE NAME: 1906-GEOM

SHT.

452

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**GENERAL NOTES:**

- BOUNDARY INFORMATION PROVIDED BY ATLAS SURVEYING THROUGH TALBERT, BRIGHT & ELLINGTON.
- TOPOGRAPHIC DATA PROVIDED BY ATLAS SURVEYING THROUGH TALBERT, BRIGHT & ELLINGTON.
- APPROXIMATE LOCATION OF CERTAIN EXISTING UNDERGROUND UTILITY LINES AND STRUCTURES ARE SHOWN ON THE PLANS FOR INFORMATION ONLY. ADDITIONAL UNDERGROUND LINES OR STRUCTURES MAY EXIST THAT ARE NOT SHOWN. CALL SOUTH CAROLINA 811 AT 811 OR 1-888-721-7877 BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM MONDAY THRU FRIDAY AT LEAST THREE WORKING DAYS BEFORE COMMENCING CONSTRUCTION. REQUEST UNDERGROUND UTILITIES TO BE LOCATED AND MARKED WITHIN AND NEAR THE CONSTRUCTION SITE.
- COMPLY WITH "SOUTH CAROLINA UNDERGROUND FACILITY DAMAGE PREVENTION ACT (EFFECTIVE JUNE 7, 2012). NOTIFICATION OF INTENT TO EXCAVATE MAY BE GIVEN BY CALLING THE TOLL FREE NUMBER: 1-800-922-0983.
- PROTECT BENCH MARKS AND PROPERTY MONUMENTS FROM DAMAGE DURING CONSTRUCTION OPERATIONS. REPLACE ANY BENCH MARKS OR MONUMENTS DAMAGED OR DESTROYED AS A RESULT OF CONTRACTOR'S OPERATIONS, AT NO COST TO THE OWNER, BY A LICENSED SURVEYOR IN THE STATE OF SOUTH CAROLINA.
- OFF-STREET PARKING FOR THE CONTRACTOR'S EMPLOYEES AND AUTHORIZED VISITORS TO THE SITE MUST BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR ADHERING TO WEIGHT LIMITS PRESCRIBED FOR ALL PUBLIC ROADS WHEN HAULING EQUIPMENT AND MATERIALS TO AND FROM THE PROJECT SITE. DAMAGES TO EXISTING PAVEMENT DUE TO THE CONTRACTOR'S CONSTRUCTION OPERATIONS OR IMPROPER TRANSPORTATION OF MATERIALS AND EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- AT LEAST ONE DRIVING LANE ON PUBLIC ROADS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. TRAFFIC LANES WILL ONLY BE CLOSED WITH THE EXPRESS WRITTEN CONSENT OF THE AGENCY HAVING JURISDICTION OVER THE ROADWAY. NOTIFY AGENCY HAVING JURISDICTION AT LEAST 5 DAYS BEFORE CLOSING ANY DRIVING LANES TO TRAFFIC. PROVIDE TRAFFIC CONTROL DEVICES, SIGNS AND FLAGMEN AS REQUIRED TO ENSURE PUBLIC SAFETY.
- CONTRACTOR SHALL COORDINATE DEMOLITION, CLEARING AND CONSTRUCTION OF IMPROVEMENTS TO MINIMIZE INTERFERENCE WITH VEHICULAR AND PEDESTRIAN TRAFFIC AND WITH OPERATIONS OF EXISTING FACILITIES.

**WATER AND SEWER LINE CONSTRUCTION:**

- ALL WATER AND SEWER LINE CONSTRUCTION SHALL CONFORM TO APPLICABLE STATE AND HILTON HEAD PUBLIC SERVICE DISTRICT (HHPSD) REQUIREMENTS, STANDARDS AND SPECIFICATIONS.
- HHPSD WILL BE RESPONSIBLE FOR INSPECTION AND APPROVAL OF ALL WATER AND SEWER SYSTEM CONSTRUCTION AND FOR ACCEPTANCE FOR OPERATION AND MAINTENANCE.
- ALL UTILITIES SHOWN ARE APPROXIMATE LOCATIONS. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFICATION OF ALL UTILITY OWNERS AND FOR FIELD VERIFICATION OF HORIZONTAL AND VERTICAL LOCATIONS PRIOR TO COMMENCING CONSTRUCTION. ANY DAMAGES TO EXISTING UTILITIES DUE TO THIS CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- NOTIFY THE PROJECT ENGINEER, IF CONFLICTS WITH EXISTING STRUCTURES REQUIRE THAT PROPOSED UTILITIES BE RELOCATED.
- THE CONTRACTOR MUST NOTIFY HHPSD FORTY-EIGHT (48) HOURS PRIOR TO ANY CONSTRUCTION, INSPECTION OR TESTING OF THE WATER DISTRIBUTION SYSTEM.
- PIPE, FITTINGS, VALVES AND APPURTENANCES FOR WATER AND SEWER LINES SHALL ALL BE IN ACCORDANCE WITH THE REQUIREMENTS CONTAINED IN THE HHPSD TECHNICAL SPECIFICATIONS.
- INSTALLATION OF WATER AND SEWER LINES AND APPURTENANCES SHALL BE IN ACCORDANCE WITH THE HHPSD STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
- CONTRACTOR SHALL INSTALL MECHANICAL RESTRAINTS ON ALL BENDS, PLUGS AND TEES, 2" OR LARGER, ON WATERLINES AND SANITARY SEWER FORCE MAINS.
- ALL WATER MAINS SHALL BE STERILIZED AND PRESSURE TESTED IN ACCORDANCE WITH HHPSD SPECIFICATIONS.
- SEPARATION OF WATER MAINS AND SEWERS:
  - PARALLEL INSTALLATION: UNLESS OTHERWISE SPECIFICALLY SHOWN IN A SPECIAL DETAIL ON THE PLANS, INSTALL WATER MAINS AT LEAST 10-FT. HORIZONTALLY FROM ANY EXISTING OR PROPOSED SANITARY SEWER OR SANITARY SEWER FORCE MAIN. THE DISTANCE BEING MEASURED IN A HORIZONTAL PLANE BETWEEN THE OUTSIDE SURFACES OF THE PIPES.
  - CROSSINGS: UNLESS OTHERWISE SPECIFICALLY SHOWN IN A SPECIAL DETAIL ON THE PLANS, INSTALL WATER LINES CROSSING SANITARY SEWERS OR SANITARY SEWER FORCE MAINS TO PROVIDE A MINIMUM VERTICAL SEPARATION OF 18-INCHES BETWEEN THE OUTSIDE SURFACES OF THE PIPES. THIS SHALL BE THE CASE WHETHER THE WATER LINE IS ABOVE OR BELOW THE SANITARY SEWER LINE. WHENEVER POSSIBLE, LOCATE THE WATER LINE WHERE A NEW WATER LINE CROSSES AN EXISTING SEWER LINE. PLACE ONE FULL LENGTH OF DUCTILE IRON PIPE FOR WATER LINE AT THE CROSSING WITH PIPE POSITIONED SO THAT THE JOINTS ARE AS FAR AS POSSIBLE FROM THE POINT OF CROSSING. WHERE A NEW WATER LINE CROSSES AN EXISTING SEWER LINE, PLACE ONE FULL LENGTH OF DUCTILE IRON PIPE WATER LINE SO THAT THE JOINTS ARE AS FAR FROM THE POINT OF CROSSING AS POSSIBLE.
- THE CONTRACTOR SHALL CUT AND PATCH EXISTING PAVEMENT AS REQUIRED FOR THE INSTALLATION OF UTILITY LINES.
- SANITARY MANHOLE RIM GRADES SHOWN ARE APPROXIMATE. ADJUST RIM ELEVATIONS TO BE FLUSH WITH FINISHED GRADE.
- THE CONTRACTOR UNDER THIS CONTRACT SHALL NOT MAKE ANY CONNECTIONS TO THE EXISTING WATER OR SANITARY SEWER SYSTEMS UNLESS EXPRESSLY AUTHORIZED TO DO SO BY THE HHPSD. ALL WATER AND SEWER IMPROVEMENTS UNDER THIS CONTRACT MUST BE CONSTRUCTED COMPLETELY TESTED AND APPROVED BY THE HHPSD BEFORE ANY AUTHORIZATION TO CONNECT WILL BE GIVEN. COORDINATION OF TESTING, INSPECTION AND CONNECTIONS WITH THE HHPSD IS THE RESPONSIBILITY OF THE CONTRACTOR UNDER THIS CONTRACT.
- ALL WATER MAINS SHALL BE INSTALLED WITH THIRTY-SIX INCHES (36") MINIMUM COVER (FROM FINISHED GRADE). MAXIMUM DEPTH SHALL BE FIVE FEET (5'). WHERE WATER MAINS MAY CONFLICT WITH OTHER UTILITIES, THE WATER MAIN CROSSING SHALL BE CONSTRUCTED WITH DUCTILE IRON PIPE, MECHANICAL JOINT 45-DEG. BENDS AND MECHANICAL RESTRAINTS.

**WORK ON SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY:**

- CONTRACTOR SHALL REVIEW AND COMPLY WITH ALL CONDITIONS AND SPECIAL PROVISIONS CONTAINED IN THE SCOTD ENFORCEMENT PERMIT(S) ISSUED FOR THIS PROJECT.
- CONTRACTOR IS RESPONSIBLE FOR SUBMITTING CONSTRUCTION NOTIFICATION FORM (48 HOUR MINIMUM) AND COORDINATION OF ALL WORK WITHIN SCOTD RIGHT-OF-WAY WITH THE LOCAL AND/OR DISTRICT SCOTD ENGINEERING REPRESENTATIVE.
- CONTRACTOR IS RESPONSIBLE FOR PREPARING AND SUBMITTING A TRAFFIC CONTROL PLAN TO SCOTD FOR APPROVAL MINIMUM 48 HOURS PRIOR TO CONDUCTING WORK IN THE RIGHT-OF-WAY. ALL TRAFFIC CONTROL PLANS SHALL CONFORM TO CURRENT MUTCD AND CURRENT SCOTD GUIDELINES AND SPECIFICATIONS.
- ALL SIGNAGE, PAVEMENT MARKINGS, AND MARKERS SHALL CONFORM TO CURRENT MUTCD GUIDELINES AND CURRENT SCOTD STANDARD SPECIFICATIONS AND DRAWINGS.
- ALL PAVEMENT MARKINGS IN SCOTD RIGHT-OF-WAY SHALL BE THERMOPLASTIC AND CONFORM TO CURRENT MUTCD GUIDELINES AND CURRENT SCOTD STANDARD SPECIFICATIONS AND DRAWINGS.
- REMOVAL OF PAVEMENT MARKINGS SHALL CONFORM TO CURRENT SCOTD STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 609.41.2.

**TREE PROTECTION-HILTON HEAD ISLAND**

- ONLY THOSE TREES APPROVED FOR REMOVAL IN ACCORDANCE WITH THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE AND SO DESIGNATED ON THE DRAWINGS FOR REMOVAL ARE TO BE REMOVED AS PART OF THE SITE CLEARING OPERATIONS. ALL TREES NOT DESIGNATED ON THE PLANS FOR REMOVAL MUST BE PRESERVED.
- THE CONTRACTOR IS RESPONSIBLE FOR MARKING THE TREES DESIGNATED TO BE PRESERVED AND THOSE TO BE REMOVED IN ACCORDANCE WITH THE REQUIREMENTS CONTAINED IN SECTION 16-6-104 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE.
- PRIOR TO COMMENCING ANY CLEARING OR CONSTRUCTION OPERATIONS ON THE SITE, THE CONTRACTOR SHALL ERECT TREE PROTECTION BARRIERS AROUND EACH TREE OR GROUP OF TREES DESIGNATED FOR PRESERVATION IN ACCORDANCE WITH THE DETAILS ON THE PLANS AND THE REQUIREMENTS CONTAINED IN SECTION 16-6-404 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE.
- A TREE PROTECTION ZONE SHALL BE ESTABLISHED IN ACCORDANCE WITH THE PROVISIONS CONTAINED IN SECTION 16-6-404 OF THE TOWN OF HILTON HEAD ISLAND LAND MANAGEMENT ORDINANCE FOR EACH EXISTING TREE DESIGNATED FOR PRESERVATION. THE MINIMUM TREE PROTECTION ZONE SHALL BE THE TREE DRUPLINE PLUS 1 FOOT. THE SIZE OR CONFIGURATION OF THE TREE PROTECTION ZONE MAY BE ADJUSTED AT THE DISCRETION OF THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR.
- THE AREA WITHIN THE TREE PROTECTION ZONE MUST REMAIN OPEN AND UNPAVED. NO CHANGE OF GRADE WILL BE ALLOWED WITHIN THE TREE PROTECTION ZONE. ANY ACTIVITY WITHIN THE TREE PROTECTION ZONE IS SUBJECT TO APPROVAL BY THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR. THE FOLLOWING ACTIVITIES ARE PROHIBITED WITHIN THE TREE PROTECTION ZONE:
  - PLACEMENT OR STORAGE OF ANY SOIL, DEBRIS, OILS, FUEL, PAINTS, BUILDING MATERIALS OR ANY OTHER MATERIALS.
  - BURNING
  - VEHICLE PARKING
  - PAVING
  - TRENCHING FOR UTILITIES
  - WHERE UTILITY LINES MUST PASS THRU THE TREE PROTECTION ZONE, THEY SHALL BE INSTALLED BY HORIZONTAL BORING BENEATH THE ROOTS OF THE TREE.
  - WHERE IT IS NECESSARY FOR MACHINERY AND EQUIPMENT TO PASS WITHIN THE TREE PROTECTION ZONE, APPROVAL MUST BE OBTAINED FROM THE TOWN OF HILTON HEAD ISLAND PLANNING DEPARTMENT DIRECTOR. SPECIAL MEASURES WILL BE REQUIRED TO PROTECT THE ROOTS FROM EXCESSIVE COMPACTION.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING A STAKING INSPECTION WITH THE TOWN OF HILTON HEAD ISLAND PRIOR TO CLEARING. THE FOLLOWING MUST BE COMPLETED BEFORE THE INSPECTION:
  - ALL BUILDING EDGES, SIDEWALKS, PAVED AREAS AND UTILITY LINES STAKED.
  - TREE PROTECTION BARRIERS INSTALLED.
  - TREES TO BE PRESERVED MARKED WITH BLUE FLAGGING AND THOSE TO BE REMOVED MARKED WITH RED FLAGGING.
  - ALL SILT FENCE INSTALLED PER THE APPROVED PLAN.

**SITE GRADING AND DRAINAGE:**

- ALL UTILITIES SHOWN ARE APPROXIMATE LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING 72-HOUR NOTICE TO ALL RESPECTIVE UTILITY COMPANIES FOR FIELD VERIFICATION OF EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY DAMAGES TO EXISTING UTILITIES DUE TO THIS CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- TEMPORARY CONTROL OF STORM WATER DRAINAGE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. SEQUENCING AND CONSTRUCTION TECHNIQUES SHALL PREVENT OBSTRUCTION OF STORM SEWERS, PONDING IN GRADED AREAS OR RISING OF WATER LEVELS WHICH WOULD ENDANGER ADJACENT BUILDINGS OR STRUCTURES.
- FULL WIDTH OF STREET AND ROAD RIGHTS-OF-WAY MUST BE CLEARED AND TRAFFIC AS SHOWN IN THE DETAILS ON THE DRAWINGS.
- SUBGRADE PREPARATION: TOP SOIL SHALL BE REMOVED FROM PAVED AREAS TO A MINIMUM DEPTH AS RECOMMENDED IN THE PROJECT'S GEOTECHNICAL REPORT. ALL EXCAVATION SHALL BE TO SUBGRADE LIMITS.
- ALL UTILITY PIPE LINES, CONDUITS AND SLEEVES UNDER PAVED AREAS MUST BE IN PLACE PRIOR TO COMPLETION OF THE ROADWAY SUBGRADE COMPACTION.
- FINISH GRADING SHALL INCLUDE THE PLACEMENT OF TOPSOIL OVER ALL UNPAVED AREAS NOT OCCUPIED BY BUILDINGS OR STRUCTURES AND FINE GRADING AROUND BUILDINGS, ADJACENT TO WALKS, CURBS, GUTTERS AND STRUCTURES TO ASSURE POSITIVE DRAINAGE.

**SCDHEC/OCRM SEDIMENT AND EROSION CONTROL STANDARD NOTES (REVISED DEC-2012):**

- IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS, IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
  - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
  - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
  - ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED ONCE EVERY CALENDAR WEEK. IF PERIODIC INSPECTION OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY, OR INCORRECTLY INSTALLED, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEARED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION. FILL, COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE SEDIMENT BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS ARE STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAY(S) FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- RESIDENTIAL SUBDIVISIONS REQUIRE EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION. INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C. REG. 72-300 ET SEQ. AND SCR10000.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LOADED WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CAN'T BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
- A COPY OF THE SWPPP, INSPECTIONS RECORDS, AND RAINFALL DATA MUST BE RETAINED AT THE CONSTRUCTION SITE OR A NEARBY LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS, FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE THAT FINAL STABILIZATION IS COMPLETED.
- INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (2H:1V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL NOT RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
- MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS SHALL BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BUMPS (SEDIMENT BUMP, FILTER BAG, ETC.).
- THE FOLLOWING DISCHARGES FROM SITES ARE PROHIBITED:
  - WASTEWATER FROM WASHOUT OF CONCRETE, UNLESS MANAGED BY AN APPROPRIATE CONTROL.
  - WASTEWATER FROM WASHOUT AND CLEANOUT OF STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS.
  - FUELS, OILS, OR OTHER POLLUTANTS USED IN VEHICLE AND EQUIPMENT OPERATION AND MAINTENANCE.
  - SOAP/SOLVENTS USED IN VEHICLE AND EQUIPMENT WASHING.
- AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE.
- IF EXISTING BUMPS NEED TO BE MODIFIED OR IF ADDITIONAL BUMPS ARE NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT AND/OR SC'S WATER QUALITY STANDARDS, IMPLEMENTATION MUST BE COMPLETED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE. IF IMPLEMENTATION BEFORE THE NEXT STORM EVENT IS IMPRACTICABLE, THE SITUATION MUST BE DOCUMENTED IN THE SWPPP AND ALTERNATIVE BUMPS MUST BE IMPLEMENTED AS SOON AS REASONABLY POSSIBLE.
- A PRE-CONSTRUCTION CONFERENCE MUST BE HELD FOR EACH CONSTRUCTION SITE WITH AN APPROVED ON-SITE SWPPP PREPARED BY THE IMPLEMENTATION OF CONSTRUCTION ACTIVITIES. FOR NON-LINEAR PROJECTS THAT DISTURB TO ACRES OR MORE, THIS CONFERENCE MUST BE HELD ON-SITE UNLESS THE DEPARTMENT HAS APPROVED OTHERWISE.

**DRY UTILITY CONDUITS FOR ELECTRIC, TELEPHONE AND CABLE TV:**

- ALL DRY UTILITY CONDUIT ENDS SHALL BE CAPPED AND MARKED WITH A STEEL REBAR STAKE IMBEDDED ONE (1) FOOT BELOW GROUND SURFACE.
- 4" MINIMUM BORE DIAMETER CONCRETE CONDUIT.
- MAINTAIN MINIMUM 12" VERTICAL CLEARANCE WHEN CROSSING WATER, SEWER, AND STORM DRAIN LINES.
- MAINTAIN MINIMUM 18" HORIZONTAL CLEARANCE WHEN PARALLELING WATER, SEWER AND STORM DRAIN LINES.
- EXTEND CONDUIT BEYOND PAVEMENT, CURBS, AND SIDEWALKS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF THE INSTALLATION OF ALL UTILITY SERVICE CONNECTIONS. REFER TO APPROVED BUILDING PLANS FOR THE EXACT LOCATION OF ALL SERVICE CONNECTIONS. THE CONTRACTOR MUST INSTALL ALL CONDUITS, AS SHOWN ON THE PLANS OR AS REQUIRED BY RESPECTIVE UTILITY COMPANIES. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE STRICT COMPLIANCE WITH ALL APPLICABLE CODES AND REGULATIONS WITH REGARDS TO THE INSTALLATION OF UTILITIES AND CONDUIT.
- LOCATIONS SHOWN ON THE PLANS FOR PROPOSED DRY UTILITY CONDUITS ARE APPROXIMATE ONLY. ALL DIMENSIONING AND STAKING SHOULD BE BASED ON ECONOMICAL AND PRACTICAL CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH THE RESPECTIVE UTILITY REPRESENTATIVES. PRIOR TO ANY CONDUIT INSTALLATION.
- TRANSFER PADS SHALL BE LOCATED AS DIRECTED BY THE RESPECTIVE UTILITY REPRESENTATIVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH APPLICABLE CODE REQUIREMENTS.
- NOTIFY THE ENGINEER IF CONFLICTS WITH EXISTING OR PROPOSED STRUCTURES REQUIRE PROPOSED UTILITIES BE RELOCATED.

**SITE CLEARING AND DEMOLITION:**

- NO CLEARING OR DEMOLITION OCCUR WITHIN DESIGNATED BUFFER ZONES, TREE PROTECTION ZONES, OUTSIDE OF THE PROPERTY LINES OR BEYOND THE CLEARING LIMITS UNLESS OTHERWISE SPECIFICALLY SHOWN ON THE PLANS.
- ONLY THOSE TREES DESIGNATED ON THE DRAWINGS FOR REMOVAL ARE TO BE REMOVED AS PART OF THE SITE CLEARING OPERATIONS.
- THE CONTRACTOR SHALL INSTALL A CONTINUOUS LINE OF FLAGGING OR FENCING ALONG THE LIMITS OF CLEARING PRIOR TO COMMENCING ANY CLEARING, DEMOLITION, OR CONSTRUCTION WORK ON THE PROJECT.
- EXERCISE CAUTION DURING CLEARING OPERATIONS TO AVOID FELLING TREES INTO DESIGNATED TREE PROTECTION ZONES.
- NO BURNING WILL BE ALLOWED WITHIN 50 FEET OF A TREE PROTECTION ZONE OR TREE DRIP LINE. CONTRACTOR SHALL COORDINATE ANY BURNING OPERATIONS WITH LOCAL JURISDICTION AND FIRE DEPARTMENTS.
- SELECTIVE CLEARING AREAS SHALL BE CLEARED OF ALL BRUSH AND UNDERSTORY GROWTH.

**UTILITY CONTACTS:**

PALMETTO ELECTRIC	843-208-5512	1 COOPERATIVE WAY, HARDEEVILLE, SC 29927
DOMINION ENERGY	800-251-7234	PO BOX 100255 COLUMBIA, SC 29202
BROAD CREEK PSD	843-785-7582	PO BOX 5878, HILTON HEAD, SC 29938
SOUTH ISLAND PSD	843-785-8224	PO BOX 5148, HILTON HEAD, SC 29938
HILTON HEAD ISLAND PSD	843-881-5295	PO BOX 21284, HILTON HEAD, SC 29925
HARGRAV COMMUNICATIONS	843-815-1675	PO BOX 3380, BLUFFTON, SC 29910
TIME WARNER CABLE	843-913-7940	11 OFFICE PARK ROAD, HILTON HEAD, SC 29928
CENTURY LINK	843-525-0044	2127 BOUNDARY ST #16, BEAUFORT, SC 29902
SANTIE COOPER	843-761-8000	1 RIVERWOOD DRIVE, MONCKS CORNER, SC 29461

**CONTRACTOR NOTE:**

CONTRACTOR TO OBTAIN AND BECOME FAMILIAR WITH GEOTECHNICAL REPORT #HG205014 PREPARED BY TERRACON.

ALL WORK MUST CONFORM TO PROJECT TECHNICAL SPECIFICATIONS FOR COMMERCIAL APRON EXPANSION HILTON HEAD ISLAND AIRPORT PREPARED BY WARD EDWARDS ENGINEERING. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING A COPY OF THE TECHNICAL SPECIFICATIONS IF NOT PROVIDED WITH THE DRAWINGS.



**PROJECT INFORMATION**

<b>DEVELOPER:</b> BEAUFORT COUNTY, SOUTH CAROLINA 120 BEACH CITY ROAD HILTON HEAD ISLAND, SC 29926 JON REMBOLD 843.255.2952 JREMBOLD@BCGOV.NET	<b>PROPERTY OWNER:</b> BEAUFORT COUNTY, SOUTH CAROLINA 120 BEACH CITY ROAD HILTON HEAD ISLAND, SC 29926
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**SOURCE OF TITLE:**  
BEAUFORT COUNTY REGISTER OF DEEDS,  
DEED BOOK 3644 PAGE 1816

**PROJECT STREET ADDRESS:**  
27 DILLON ROAD

**PROPERTY IDENTIFICATION NO.:**  
County I.D. #R510 008 000 0085 0000

**LATITUDE / LONGITUDE:**  
N 32° 15' 32"  
W 80° 41' 48"

**DEVELOPMENT PERMIT JURISDICTION:**  
TOWN OF HILTON HEAD ISLAND

**PROPERTY ZONING:**  
IL LIGHT INDUSTRIAL

**FLOOD ZONE:**  
A7, B, C (BASE FLOOD ELEV.14)

**VERTICAL CONTROL DATUM:**  
CHOOSE NGVD29 / NAVD88

**SITE AREA:**  
TOTAL: 193.93 ACRES  
DISTURBED: 5.6 ACRES

**USE:**  
EXISTING: AIRPORT  
PROPOSED: AIRPORT

**REQUIRED SETBACKS:**

FRONT:	40 FEET	FRONT:	15 FEET (TYPE B-OPTION 2)
REAR:	N/A FEET	REAR:	N/A
SIDE:	20 FEET	SIDE:	10 FEET (TYPE A OPTION 2)
STREET:	40 FEET	STREET:	15 FEET (TYPE B-OPTION 2)

**SURFACE COVERAGE:**

MAX IMPERVIOUS ALLOWED:	65 %
EXISTING IMPERVIOUS:	58.50 AC. (30 %)
PROPOSED IMPERVIOUS:	62.21 AC (32 %)
OPEN SPACE PROVIDED:	131.72 AC. (68 %)

**REQUIRED BUFFERS:**

FRONT:	15 FEET (TYPE B-OPTION 2)
REAR:	N/A
SIDE:	10 FEET (TYPE A OPTION 2)
STREET:	15 FEET (TYPE B-OPTION 2)

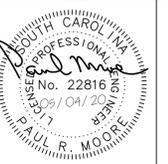
**SEQUENCE OF CONSTRUCTION ACTIVITIES**

- ESTIMATED START DATE: 09-01-20 ESTIMATED COMPLETION DATE: 09-01-21
- ITEMS MUST OCCUR IN THE ORDER LISTED; ITEMS CANNOT OCCUR CONCURRENTLY UNLESS SPECIFICALLY NOTED.
- PHASE 1: (INITIAL)**
- RECEIVE NPDES COVERAGE FROM DHEC.
  - HOLD PRE-CONSTRUCTION MEETING.
  - NOTIFY DHEC EDC REGIONAL OFFICE OR OCRM OFFICE 48 HOURS PRIOR TO BEGINNING LAND-DISTURBING ACTIVITIES.
  - INSTALLATION OF CONSTRUCTION ENTRANCE.
  - CLEARING & GRUBBING ONLY AS NECESSARY FOR INSTALLATION OF PERIMETER CONTROLS.
  - INSTALLATION OF PERMETER CONTROLS (E.G. SILT FENCE).
  - INSTALL TREE PROTECTION.
  - INSTALL INLET PROTECTION.
  - INSTALL SEDIMENT TUBES.
  - CLEARING & GRUBBING ONLY IN AREAS OF BASIN.
- PHASES 2 & 3: (INTERMEDIATE & FINAL)**
- INSTALLATION OF BASIN AND INSTALLATION OF DIVERSIONS TO THOSE STRUCTURES (OUTLET STRUCTURES MUST BE COMPLETELY INSTALLED AS SHOWN ON THE DETAILS BEFORE PROCEEDING TO NEXT STEP; AREAS DRAINING TO THESE STRUCTURES CANNOT BE DISTURBED UNTIL THE STRUCTURES & DIVERSIONS TO THE STRUCTURES ARE COMPLETELY INSTALLED). INSTALL SURFACE DEWATERING SKIMMER PRIOR TO MOVING TO NEXT STEP.
  - CLEARING & GRUBBING OF SITE OR DEMOLITION (SEDIMENT & EROSION CONTROL MEASURES FOR THESE AREAS MUST ALREADY BE INSTALLED).
  - ROUGH GRADING.
  - INSTALLATION OF STORM DRAIN SYSTEM AND PLACEMENT OF INLET PROTECTION AS EACH INLET IS INSTALLED.
  - INSTALL ALL REQUIRED UTILITIES AND CURBING.
  - FINE GRADING, PAVING, ETC.
  - PLACE TOPSOIL & ESTABLISH FINISH GRADES.
  - PERMEABLE PAVERS SHALL BE LAID WHEN ALL HEAVY CONSTRUCTION IS COMPLETED.
  - CLEAN-OUT OF DETENTION BASINS THAT WERE USED AS SEDIMENT CONTROL STRUCTURES AND RE-GRADE OF DETENTION POND BOTTOMS; IF NECESSARY, MODIFICATION OF SEDIMENT BASIN RISER TO CONVERT TO DETENTION BASIN OUTLET STRUCTURE.
  - INSTALL PERMANENT SEEDING.
  - FLUSH ANY SEDIMENT FROM STORM SEWER PIPES AND INLETS.
  - REMOVAL OF TEMPORARY SEDIMENT & EROSION CONTROL MEASURES (INCLUDING SKIMMER) AFTER ENTIRE AREA DRAINING TO THE STRUCTURE IS FINALLY STABILIZED (THE DEPARTMENT RECOMMENDS THAT THE PROJECT OWNER / OPERATOR HAVE THE SWPPP PREPARED OR REGISTRATION EQUIVALENT APPROVE THE REMOVAL OF TEMPORARY STRUCTURES).
  - PERFORM AS-BUILT SURVEYS OF ALL DETENTION STRUCTURES AND SUBMIT TO DHEC OR MSA FOR ACCEPTANCE.
  - SUBMIT NOTICE OF TERMINATION (NOT) TO DHEC AS APPROPRIATE.
- NOTE: PERFORM WEEKLY SITE INSPECTIONS DURING LAND DISTURBING ACTIVITIES AND MAKE RECOMMENDATIONS FOR ADDITIONAL BMPs OR MAINTENANCE OF EXISTING BMPs
  - NOTE: ALL PUMPED DEWATERING SHALL BE PERFORMED USING AN APPROPRIATELY SIZED PUMPED WATER FILTER BAG.

**SCDHEC-OCRM CERTIFICATION:**

"I HAVE PLACED MY SIGNATURE AND SEAL ON THE DESIGN DOCUMENTS SUBMITTED SIGNIFYING THAT I ACCEPT RESPONSIBILITY FOR THE DESIGN OF THE SYSTEM. FURTHER, I CERTIFY TO THE BEST OF MY KNOWLEDGE AND BELIEF THAT THE DESIGN IS CONSISTENT WITH THE REQUIREMENTS OF TITLE 48, CHAPTER 14 OF THE CODE OF LAWS OF SC, 1976 AS AMENDED, PURSUANT TO REGULATION 72-300 ET SEQ. (IF APPLICABLE), AND IN ACCORDANCE WITH THE TERMS AND CONDITIONS OF SCR100000."

**Ward Edwards ENGINEERING**  
 P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910  
 PH (843) 837-5290 / FAX (843) 837-2558  
 WWW.WARDEDWARDS.COM



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 SC COA 4350  
 www.talbertandbright.com

LIMITS OF DISTURBANCE: <b>NPDES</b>	
<b>EROSION PREVENTION</b>	
LAND GRADING:	LG OR 
SURFACE ROUGHENING:	
TOPSOILING:	
TEMPORARY SEEDING:	TS
MULCHING:	M
ECB OR TRM:	
FGM:	
BFM:	
PERMANENT SEEDING:	PS
SODDING:	SO
RIPRAP:	
OUTLET PROTECTION:	 RIPRAP  ECB or TRM
DUST CONTROL:	DC
POLYACRYLAMIDE (PAM):	PAM

<b>SEDIMENT CONTROL</b>	
SEDIMENT BASIN:	
TEMPORARY SEDIMENT TRAP:	
ROCK SEDIMENT DIKE:	
ROCK CHECK DAM:	
SEDIMENT TUBE:	
SILT FENCE:	
REINFORCED SILT FENCE:	
TYPE A - FABRIC INLET PROTECTION:	
TYPE A - SEDIMENT TUBE INLET PROTECTION:	
TYPE B - WIRE MESH AND STONE DROP INLET PROTECTION:	
TYPE C - BLOCK AND GRAVEL INLET PROTECTION:	
TYPE D - RIGID INLET FILTERS:	
TYPE E - SURFACE COURSE CURB INLET FILTER:	
TYPE F - INLET TUBE:	
TYPE FC - FILTER BAG CURB INLET PROTECTION:	
TYPE FB - FILTER BAG CURB INLET PROTECTION:	
CONCRETE WASHOUT:	

<b>RUNOFF CONVEYANCE MEASURES</b>	
VEGETATED CHANNELS:	
RIPRAP-LINED CHANNELS:	
ECB OR TRM-LINED CHANNELS:	
PAVED CHANNELS:	
PIPE SLOPE DRAINS:	
TEMPORARY STREAM CROSSING:	
TEMPORARY DIVERSION DITCH OR SWALE:	
PERMANENT DIVERSION DITCH:	
DIVERSION DIKE OR BERM:	
LEVEL SPREADER:	
SUBSURFACE DRAIN:	



**NOT FOR CONSTRUCTION**

REVISIONS	DATE
1	
2	
3	
4	
5	

DESIGNED BY:	BCB
PROJECT ENG.:	PRM
PROJECT NO.:	100266J
DRAWN BY:	BCB
CHECKED BY:	PRM
DATE ISSUED:	05/04/20

**HILTON HEAD ISLAND AIRPORT**  
"Fly to the Fun!"



INTERMEDIATE & FINAL EROSION CONTROL PLAN  
COMMERCIAL APRON EXPANSION  
SCALE: 1" = 40'

SHT. 470  
FILE NAME: 1906-GEOM

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PERMANENT SEEDING - COASTAL

SPECIES	LBS/AC	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
SANDY, DROUGHTY SITES													
BROWNTOP MILLET	10 LBS/AC												
BAHIAGRASS	40 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
BAHIAGRASS	30 LBS/AC												
SERICEA LESPEDEZA	40 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
ATLANTIC COASTAL PANICGRASS	15 LBS/AC												
PLS													
BROWNTOP MILLET	10 LBS/AC												
SWITCHGRASS (ALAMO)	8 LBS/AC												
PLS													
LITTLE BLUESTEM	4 LBS/AC												
SERICEA LESPEDEZA	20 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
WEEPING LOVEGRASS	8 LBS/AC												
WELL DRAINED, CLAYEY/LOAMEY SITES													
BROWNTOP MILLET	10 LBS/AC												
BAHIAGRASS	40 LBS/AC												
RYE, GRAIN	10 LBS/AC												
BAHIAGRASS	40 LBS/AC												
CLOVER, CRIMSON (ANNUAL)	5 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
BAHIAGRASS	30 LBS/AC												
SERICEA LESPEDEZA	40 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
BERMUDA, COMMON	10 LBS/AC												
SERICEA LESPEDEZA	40 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
BERMUDA, COMMON	12 LBS/AC												
KOBE LESPEDEZA (ANNUAL)	10 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
BAHIAGRASS	20 LBS/AC												
BERMUDA, COMMON	6 LBS/AC												
SERICEA LESPEDEZA	40 LBS/AC												
BROWNTOP MILLET	10 LBS/AC												
SWITCHGRASS	8 LBS/AC												
LITTLE BLUESTEM	3 LBS/AC												
PLS													
INDIANGRASS	3 LBS/AC												
PLS													

PS PERMANENT SEEDING - COASTAL

DETAIL 02370-010

TEMPORARY SEEDING - COASTAL

SPECIES	LBS/AC	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
SANDY, DROUGHTY SITES													
BROWNTOP MILLET	40 LBS/AC												
RYE, GRAIN	56 LBS/AC												
RYEGRASS	50 LBS/AC												
WELL DRAINED, CLAYEY/LOAMEY SITES													
BROWNTOP MILLET	40 LBS/AC												
OR JAPANESE MILLET													
RYE, GRAIN OR OATS	56 LBS/AC												
75 LBS/AC													
RYEGRASS	50 LBS/AC												

TS TEMPORARY SEEDING - COASTAL

DETAIL 02370-011

NOT FOR CONSTRUCTION

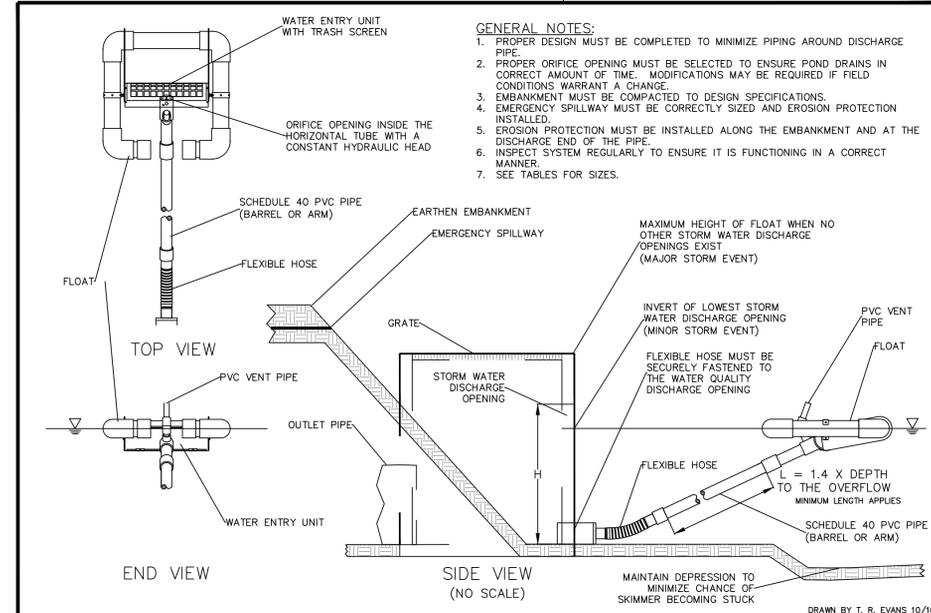
REVISIONS	DATE
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DESIGNED BY: BCB
PROJECT ENG.: PRM
PROJECT NO.: 100266J
DRAWN BY: BCB
CHECKED BY: PRM
DATE ISSUED: 05/04/20

HILTON HEAD ISLAND AIRPORT "Fly to the Fun"

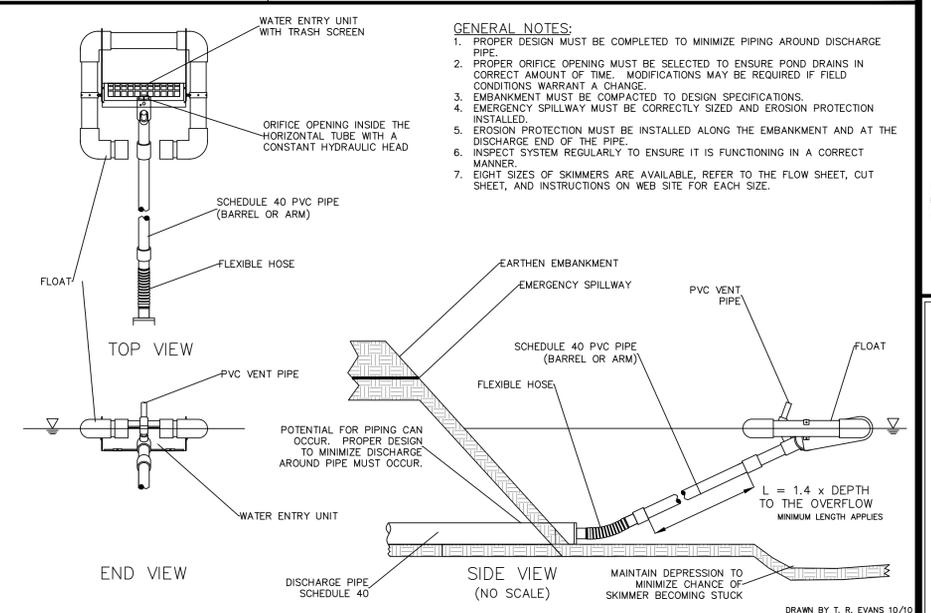


INTERMEDIATE & FINAL EROSION CONTROL DETAILS COMMERCIAL APRON EXPANSION SCALE: AS SHOWN FILE NAME: 1906-GEOM



FAIRCLOTH SKIMMER® DISCHARGE SYSTEM WITH OUTLET STRUCTURE

J. W. FAIRCLOTH & SON INC.  
WWW.FAIRCLOTHSKIMMER.COM  
TELEPHONE: (919) 732-1244  
FAX: (919) 732-1266  
EMAIL: WARREN@FAIRCLOTHSKIMMER.COM

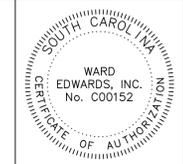


FAIRCLOTH SKIMMER® DISCHARGE SYSTEM WITH EMBANKMENT

J. W. FAIRCLOTH & SON INC.  
WWW.FAIRCLOTHSKIMMER.COM  
TELEPHONE: (919) 732-1244  
FAX: (919) 732-1266  
EMAIL: WARREN@FAIRCLOTHSKIMMER.COM

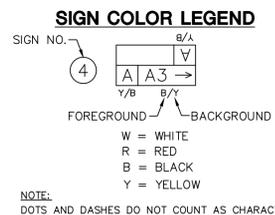


P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910  
PH (843) 837-5290 / FAX (843) 837-2558  
WWW.WARDEDWARDS.COM

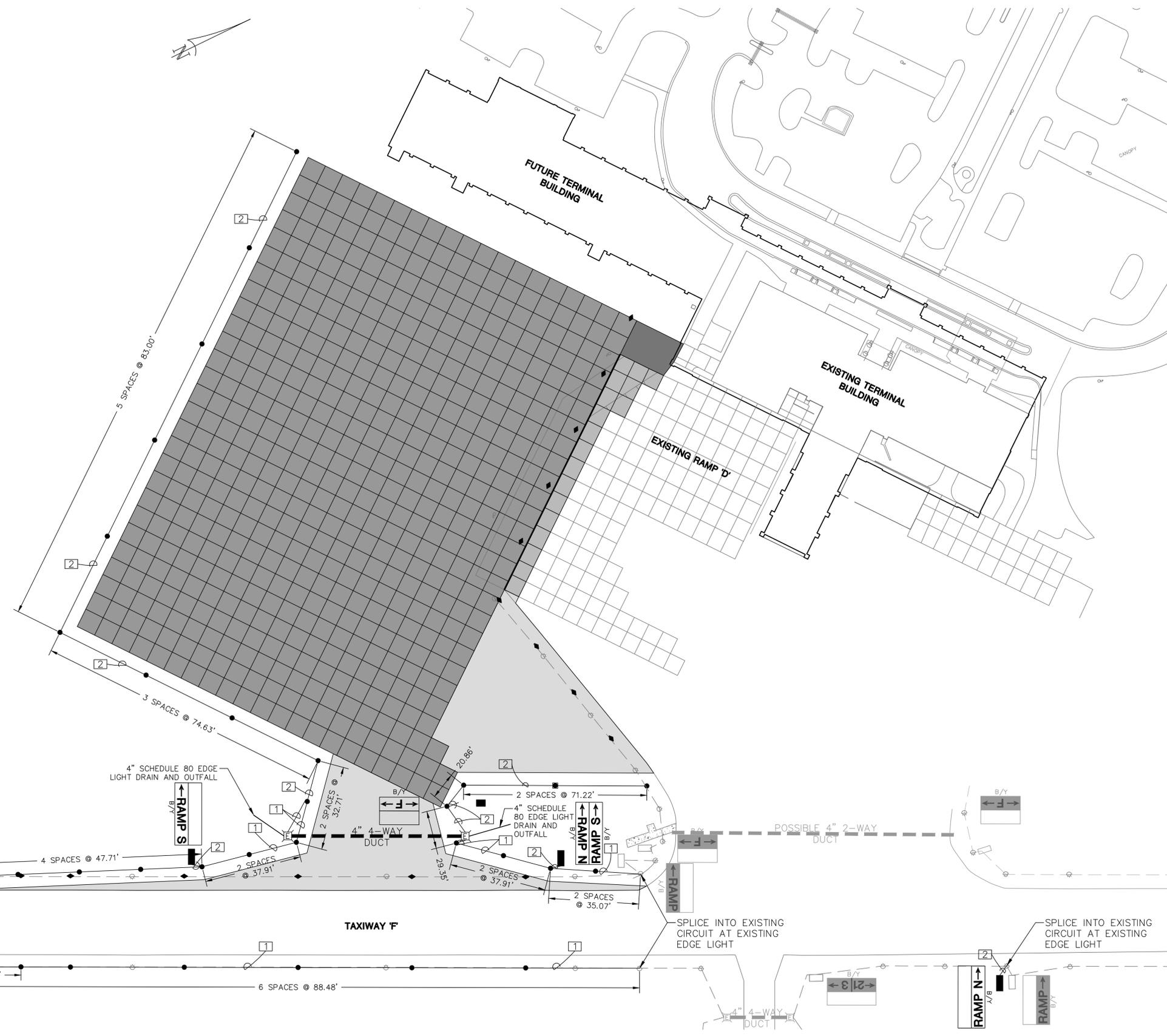


**TALBERT, BRIGHT & ELLINGTON**  
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PHONE: 803-933-9290 FAX: 803-933-9205  
SC COA 4350  
www.talbertandbright.com

LEGEND		
	EXISTING	PROPOSED
AIRFIELD GUIDANCE SIGN	—	—
BASE MOUNTED TAXIWAY EDGE LIGHT MITL - LED	⊙	⊙
SURFACE MOUNTED L-853 ELEVATED REFLECTOR	•	•
ELECTRICAL DUCT (AS SHOWN)	—	—
CABLE TRENCH	—	—
ELECTRICAL MANHOLE	⊠	⊠
FIBER OPTIC	—FO—	—FO—
WATERLINE	—W—	—W—
FENCE	—	—
EDGE OF PAVEMENT	—	—
PROPOSED CONCRETE PAVEMENT		▒
PROPOSED AIRFIELD ASPHALT PAVEMENT		▒
PROPOSED STANDARD ASPHALT PAVEMENT		▒



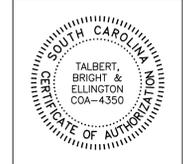
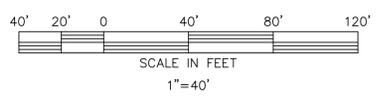
- NOTES:
- SEE SHEET C1002 FOR ELECTRICAL NOTES AND LEGEND.
  - INSTALLATION OF NEW EDGE LIGHT DRAIN OUTFALL (HEADWALL) AT THE END OF PROPOSED EDGE LIGHT DRAIN PIPE, AND INSTALLATION OF THE NEW EDGE LIGHTING DRAIN PIPE ARE INCIDENTAL TO 'ELECTRICAL MANHOLE' ITEM OF WORK. NO SEPARATE PAY.



**TRENCH SCHEDULE**

	#8, 5kV TYPE 'C' CABLE	50/C #14 TC-XHHW CONTROL CABLE	30/C #14, TC-XHHW CONTROL CABLE	6/C #14, TC-XHHW CONTROL CABLE	# PAIR FIBER OPTIC CABLE	#6 BARE CU. COUNTER-POISE	2" SCH. 40, PVC CONDUIT	REMARKS
1	1					1	1	TAXIWAY 'F' CIRCUIT
2						1	1	TAXIWAY 'F' CIRCUIT

NOTE: LIGHTNING PROTECTION BARE COPPER COUNTERPOISE NOT SHOWN. SEE NOTE E20 ON SHEET 550.



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REVISIONS	DATE
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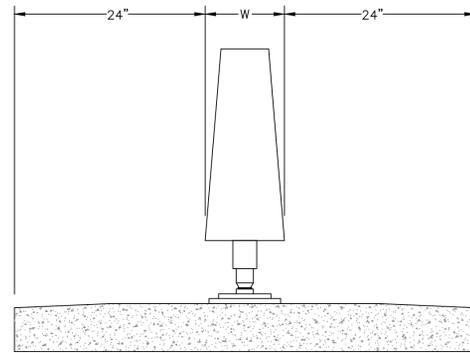
DESIGNED BY: CEV	PROJECT ENG.: JAS	DRAWN BY: UPB	CHECKED BY: PET	DATE ISSUED: MAY 2020
PROJECT NO.: 2119-1906				

**HILTON HEAD ISLAND AIRPORT**  
*"Fly to the Fun"*

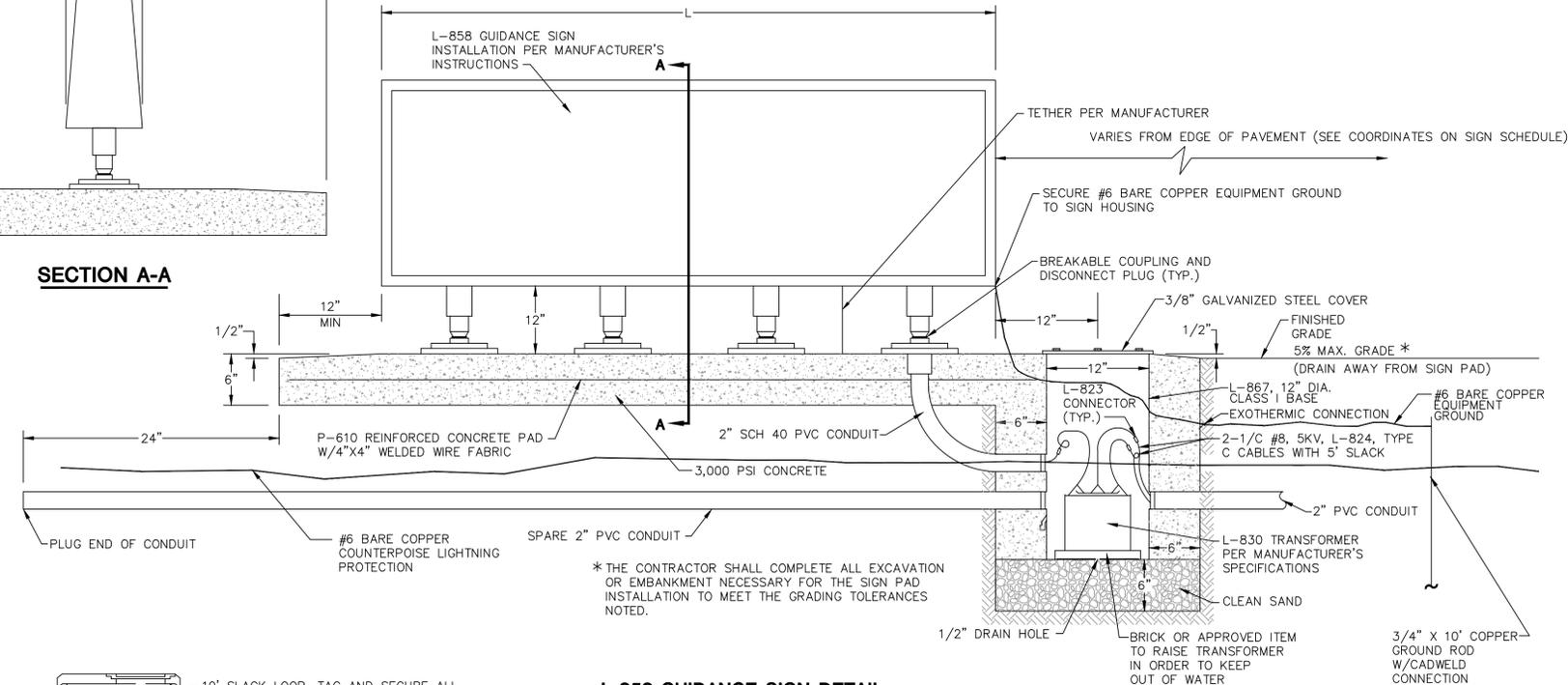
**ELECTRICAL LAYOUT PLAN**  
**COMMERCIAL APRON EXPANSION**  
 FILE NAME: 1906-ELEC  
 SCALE: 1"=40'

**ELECTRICAL NOTES**

- E1. THE CONTRACTOR SHALL LOCATE EXISTING DUCTS PRIOR TO USE. APPROXIMATE LOCATIONS ARE SHOWN.
- E2. ANY LISTED MANUFACTURER'S EQUIPMENT SHALL MEAN THAT LISTED ITEM OR AN APPROVED EQUIVALENT.
- E3. EXCAVATION FOR CABLE INSTALLATION IN EXISTING DUCT BANKS SHALL BE INCIDENTAL TO THE PROJECT.
- E4. ALL ELECTRICAL WORK SHALL BE COMPLETED IN ACCORDANCE WITH LOCAL CODES AND THE NEC HANDBOOK (CURRENT EDITION).
- E5. ALL CABLES SHALL BE TAGGED AT EACH ENTRANCE TO DUCTS, MANHOLES, OR HANDHOLES.
- E6. ANY INTERRUPTION TO ACTIVE AIRPORT LIGHTING CIRCUITS CAUSED BY THE CONTRACTOR'S EQUIPMENT OR PERSONNEL SHALL BE IMMEDIATELY REPAIRED AT THE CONTRACTOR'S EXPENSE.
- E7. ALL UNDERGROUND CONDUIT BENDS SHALL BE LONG RADIUS BENDS.
- E8. THE CONTRACTOR SHALL FIELD STAKE ALL FIXTURES PRIOR TO INSTALLATION. ENGINEER SHALL APPROVE ALIGNMENT AND LOCATION AND ANY DISCREPANCIES SHALL BE RESOLVED PRIOR TO COMMENCING CONSTRUCTION.
- E9. RUNWAY AND/OR TAXIWAY LIGHTS SHALL BE DE-ENERGIZED OR PROPERLY COVERED ON CLOSED RUNWAY(S) AND ALL CLOSED SEGMENTS OF TAXIWAYS.
- E10. THE CONTRACTOR SHALL HAVE A CABLE TRACER ON SITE AVAILABLE TO LOCATE THE EXISTING CABLES.
- E11. ANY EXISTING AIRFIELD LIGHTING EQUIPMENT OR FIXTURES TO BE REMOVED SHALL BE STORED ON SITE AS ORDERED BY THE OWNER AND REMAIN PROPERTY OF THE OWNER. THE OWNER MAY SELECT INDIVIDUAL PIECES OF EQUIPMENT OR FIXTURES TO BE DISPOSED OF OFFSITE BY THE CONTRACTOR.
- E12. THE CONTRACTOR SHALL BE REQUIRED TO CONNECT THE PROPOSED LIGHTING CIRCUITS TO THE EXISTING CIRCUITS AS ORDERED BY THE ENGINEER. ALL WORK REQUIRED TO MAKE THE CONNECTIONS TO THE EXISTING CIRCUITS SHALL BE INCIDENTAL TO THE PROJECT (I.E., PUNCHING INTO EXISTING CANS FOR CONDUITS).
- E13. AN ELECTRONIC FILE WILL BE PROVIDED FOR USE IN STAKING THE LIGHTS, SIGNS, DUCT BANKS, AND MANHOLES.
- E14. THE CONTRACTOR SHALL MEGGAR THE EXISTING TAXIWAY "T" CABLES PRIOR TO COMMENCING WORK ON THIS EXISTING CIRCUIT. THE MEGGAR READINGS SHALL BE DONE WITH A REPRESENTATIVE OF THE OWNER PRESENT AND LOGGED IN A BOOK WITH THE READINGS. MEGGAR READINGS ON MODIFIED CIRCUITS SHALL BE NO LESS THAN THE INITIAL READINGS.
- E15. ALL GROUND RODS AND OTHER UNDERGROUND GROUNDING CONNECTIONS SHALL BE CADWELDED OR APPROVED EQUIVALENT. CADWELDED CONNECTIONS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES. THE PROPOSED COUNTERPOISE SYSTEM SHALL BE CONNECTED WITH THE EXISTING SYSTEM AT ALL CROSSING POINTS.
- E16. THE CONTRACTOR SHALL MANUALLY LOCK OUT EACH CIRCUIT AT THE VAULT WHEN WORK IS BEING PERFORMED ON THE CIRCUIT. THE CIRCUIT SHALL BE TAGGED AND THE CONTRACTOR'S NAME SHALL BE CLEARLY IDENTIFIED ON EACH TAG. THE CONTRACTOR SHALL HAVE A LOCK OUT KIT ON SITE AT ALL TIMES. THE OWNER SHALL BE NOTIFIED EACH TIME A CIRCUIT IS SECURED AND EACH TIME THE CIRCUIT IS RETURNED TO REMOTE CONTROL.
- E17. THE CONTRACTOR SHALL INSTALL A POLYPROPYLENE PULL CORD IN ALL EMPTY DUCTS AND CONDUITS.
- E18. ALL CONTROL CABLES AND FIBER OPTIC CABLE, IF DEPICTED, SHALL BE INSTALLED FROM THE EQUIPMENT TO THE CONTROL PANEL, UNLESS SHOWN OTHERWISE.
- E19. CONNECTION TO EXISTING EQUIPMENT AND SYSTEMS SHALL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE NEW CONDUIT AND CABLES. ALL CORING INTO EXISTING LIGHT BASES AND SEALING OF NEW CONDUIT ENTRIES INTO EXISTING LIGHT BASES SHALL BE INCIDENTAL TO THE PROJECT AND NO SEPARATE PAY.
- E20. A LIGHTNING PROTECTION BARE COUNTERPOISE SHALL BE INSTALLED 6' FROM THE PROPOSED TAXIWAY PAVEMENT EDGE AS SHOWN IN THE TRENCHING DETAIL EDGE OF PAVEMENT SECTION ON SHEET 551.
- E21. SEE DEMOLITION PLAN SHEETS FOR REMOVAL OF EXISTING ELECTRICAL ITEMS.
- E22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING OPERATIONAL FOR THE DURATION OF THE ENTIRE PROJECT THE EXISTING TAXIWAY "T" EDGE LIGHTS. THIS SHALL BE ACCOMPLISHED BY MAINTAINING POWER FEEDS TO THE ACTIVE CIRCUITS INCLUDING, BUT NOT LIMITED TO, THE USE OF TEMPORARY JUMPER CABLES MEETING THE SAME CABLE TYPES AS INSTALLED WITH THESE SYSTEMS. THESE TEMPORARY JUMPER CABLES MAY BE DISCONNECTED TEMPORARILY EACH DAY IF REQUIRED FOR CONSTRUCTION, PROVIDED THEY ARE RE-CONNECTED BY THE CONTRACTOR AND THESE SYSTEMS ARE TESTED TO VERIFY PROPER OPERATION BY THE CONTRACTOR EACH DAY WITH THE ENGINEER PRESENT BEFORE DEPARTING THE SITE. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO PROJECT (NO PAY ITEM).



**SECTION A-A**

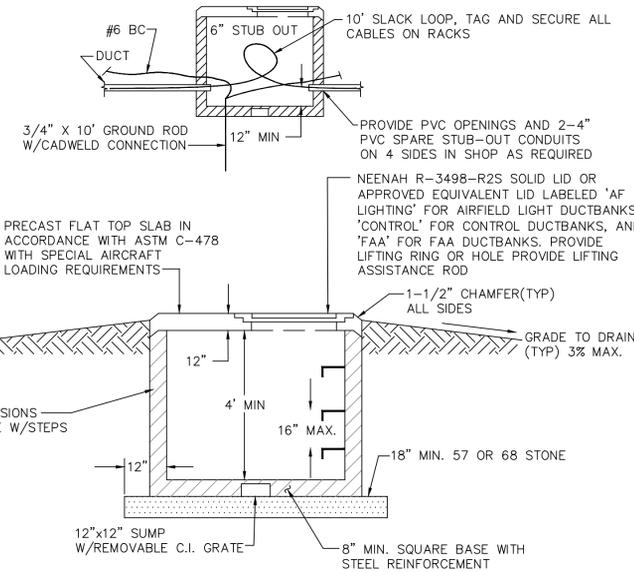


**L-858 GUIDANCE SIGN DETAIL**

NOT TO SCALE

**NOTES:**

- 1. DO NOT CONNECT LIGHTNING PROTECTION TO EQUIPMENT GROUND.
- 2. CONTRACTOR SHALL PROVIDE AND INSTALL SIZE 1, STYLE 2 OR STYLE 3, CLASS 2 LED SIGNS.

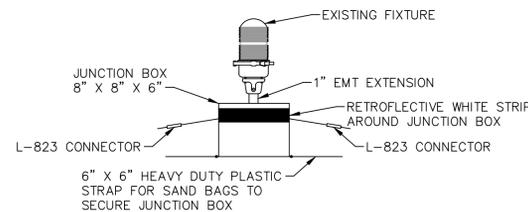


**ELECTRICAL MANHOLE DETAIL**

NOT TO SCALE

**NOTES:**

- 1. CONSTRUCTION AND STEEL REINFORCING SHALL BE IN CONFORMANCE WITH ASTM C-478 REQUIREMENTS.
- 2. CONCRETE TO BE 4,000 PSI MINIMUM.
- 3. CONTRACTOR MAY SUBMIT SHOP DRAWING FOR CAST-IN-PLACE MANHOLE.
- 4. ALL CABLES SHALL BE PROPERLY TAGGED.
- 5. THE MANHOLE, FRAME AND LID SHALL BE DESIGNED FOR 90,000 LBS. DUAL GEAR DESIGN CALCULATIONS AND STRUCTURAL DRAWINGS SHALL BE STAMPED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER.
- 6. PROVIDE END BELLS ON ALL DUCT OPENINGS.
- 7. PROVIDE THE FOLLOWING CABLE RACKS OR AN APPROVED EQUIVALENT IN EACH MANHOLE: POLYMER THREE (3) SADDLE RACK (TYP.), UNDERGROUND DEVICES INC. 2SR3 - 8 EA. MANHOLE.
- 8. INSTALL STANDARD SCODT MANHOLE STEPS THAT ARE ENCAPSULATED IN POLYPROPYLENE, RUBBER, OR OTHER NONCONDUCTIVE MATERIAL APPROVED BY THE ENGINEER. MANHOLE OPENINGS SHALL BE LOCATED FOR EASY ACCESS TO STEPS.
- 9. PROVIDE 5/8" DIAMETER X 8' GROUND ROD DRIVEN THROUGH SUMP OPENING WITH #8 SOLID BARE COPPER GROUND WIRE BONDED TO MANHOLE COVER LID AND EXOTHERMICALLY BONDED TO GROUND ROD. INSTALL GROUND WIRE ON OPPOSITE SIDE OF STEPS INSIDE MANHOLE AND ROUTE AWAY FROM INCOMING/OUTGOING POWER CABLES AND CABLE RACKS. NO SEPARATE PAY ITEM.

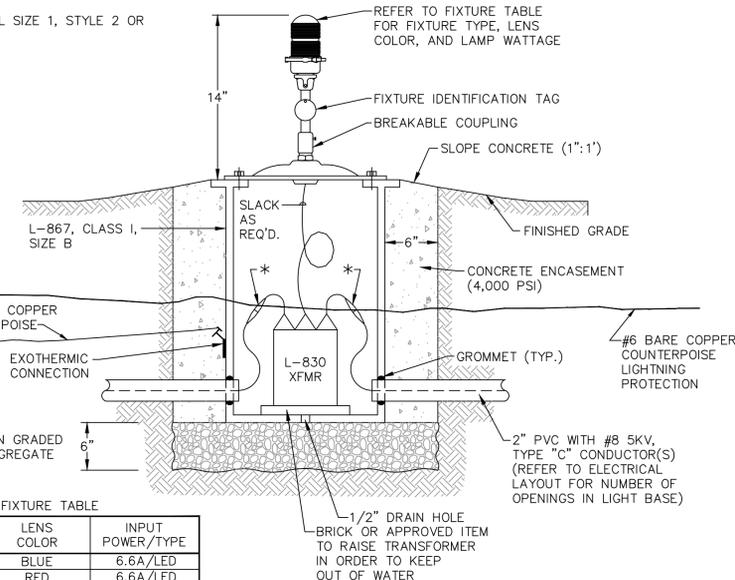


**TEMPORARY ELEVATED LIGHT**

NOT TO SCALE

**NOTES:**

- 1. ALL TEMPORARY ELEVATED LIGHTS SHALL BE SECURED WITH SANDBAGS OR AN ENGINEER APPROVED EQUAL.
- 2. TEMPORARY LIGHTS SHALL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE PROPOSED LIGHTS. (NO PAY ITEM)
- 3. CONTRACTOR MAY SUBMIT ALTERNATE METHOD FOR SECURING TEMPORARY LIGHT FOR APPROVAL. THE USE OF WOOD IS ACCEPTABLE.



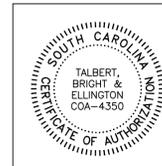
FIXTURE TYPE	LENS COLOR	INPUT POWER/TYPE
L-861T	BLUE	6.6A/LED
L-861E	RED	6.6A/LED
L-861	YELLOW/RED	6.6A/LED
L-861	YELLOW/CLEAR	6.6A/LED
L-861E	YELLOW/GREEN	6.6A/LED
L-861E	BLANK/GREEN	6.6A/LED

**BASE MOUNTED FIXTURE DETAIL**

NOT TO SCALE

**NOTES:**

- DO NOT CONNECT LIGHTNING PROTECTION TO EQUIPMENT GROUND.



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**HILTON HEAD ISLAND AIRPORT**  
 "Fly to the Fun!"

**ELECTRICAL NOTES**  
 ELECTRICAL DETAILS - SHEET 1 OF 2  
 COMMERCIAL APRON EXPANSION

SHT.

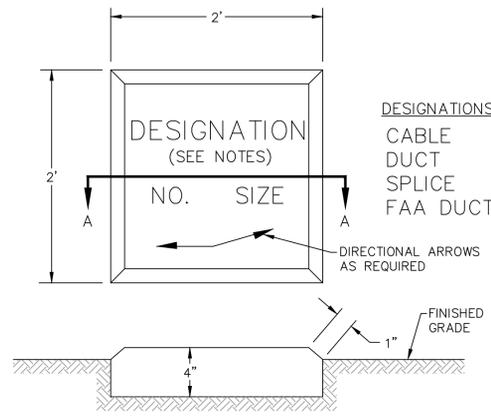
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DESIGNED BY:	CEV
PROJECT ENG.:	JAS
PROJECT NO.:	2119-1906
DRAWN BY:	UPB
CHECKED BY:	PET
DATE ISSUED:	MAY 2020

SCALE: AS SHOWN

FILE NAME: 1906-ELEC



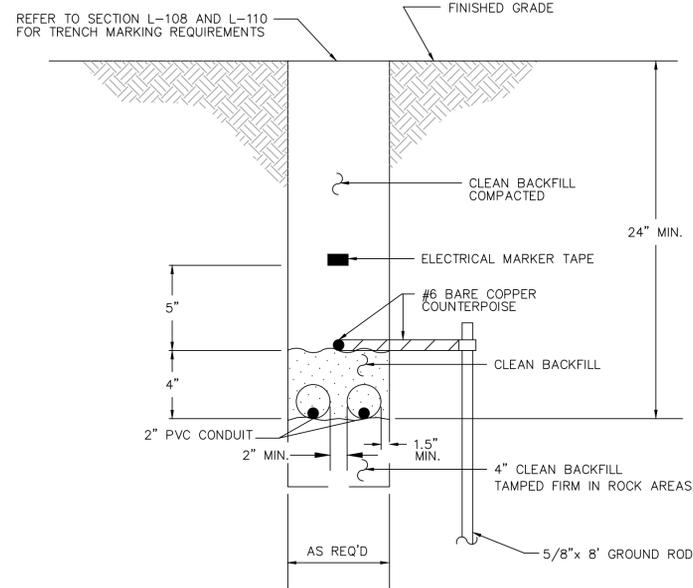
**SECTION A-A**  
**CABLE/DUCT MARKER**  
NOT TO SCALE

**NOTES:**

1. MARK DESIGNATIONS SHALL BE INSCRIBED ON MARKER IN LETTERS 4" HIGH x 3" WIDE WITH 1/2" LINE THICKNESS 1/4" DEEP, SPACED 1-1/2" APART IN A MANNER ACCEPTABLE TO THE ENGINEER. SEE ITEM L-108.3.7 AND L-110-3.4 OF THE SPECIFICATIONS. SEE LIST OF DESIGNATIONS ABOVE.
2. EACH DUCT MARKER SHOULD INDICATE NUMBER AND SIZE OF CONDUIT OR DUCT (i.e. 4"-4W).
3. ARROWS SHALL BE ADDED, WHEN NECESSARY, TO INDICATE CHANGE OF DIRECTION OF CABLE RUN.
4. MARKERS SHALL BE CONSTRUCTED OF P-610 CONCRETE (4" THICK, 3000 PSI).

DESIGNATIONS  
CABLE  
DUCT  
SPLICE  
FAA DUCT

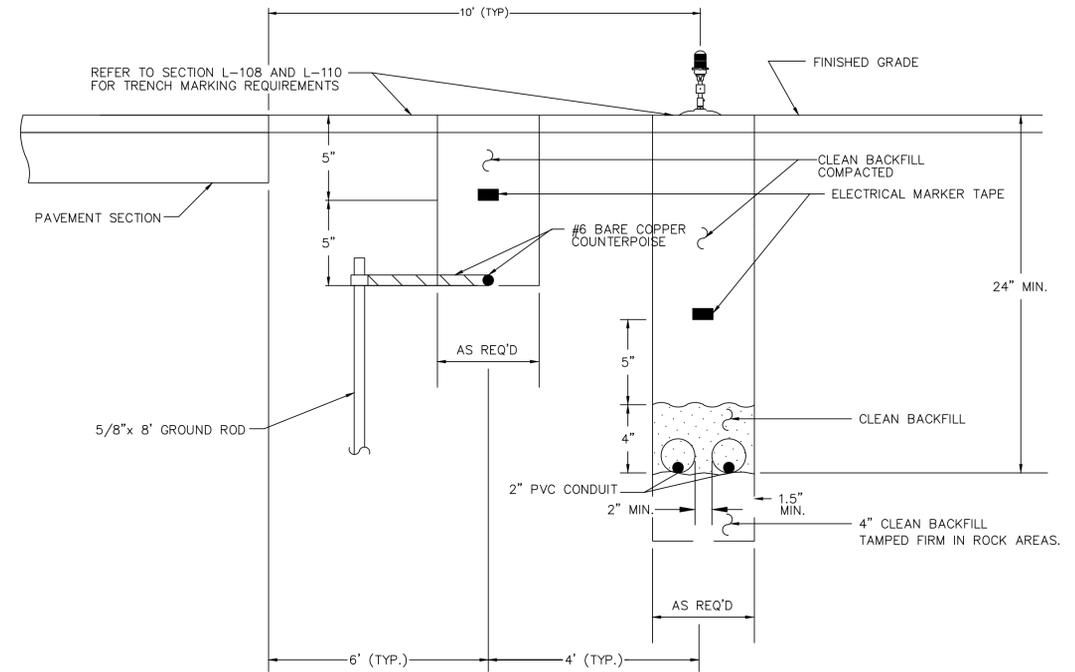
DIRECTIONAL ARROWS  
AS REQUIRED



**TRENCHING DETAIL**  
**NON EDGE OF PAVEMENT SECTION**  
NOT TO SCALE

**NOTES:**

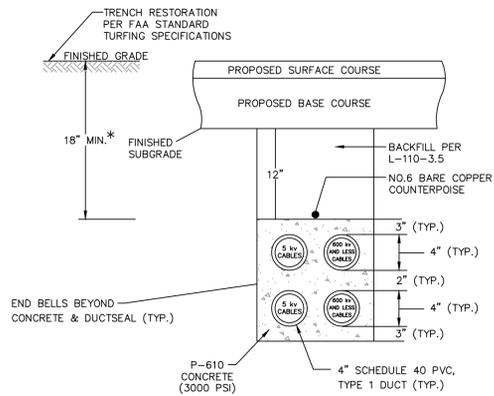
1. #6 BARE COPPER COUNTERPOISE SHALL BE CADWELD OR APPROVED EXOTHERMIC WELDED CONNECTION TO THE GROUND ROD.
2. COMPACTED BACKFILL SHALL BE PLACED IN 8" MAXIMUM LIFTS WITH A MECHANICAL COMPACTOR.
3. CONSTRUCTION AND INSTALLATION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION L-108 AND L-110.



**TRENCHING DETAIL**  
**EDGE OF PAVEMENT SECTION**  
NOT TO SCALE

**NOTES:**

1. #6 BARE COPPER COUNTERPOISE SHALL BE CADWELD OR APPROVED EXOTHERMIC WELDED CONNECTION TO THE GROUND ROD.
2. COMPACTED BACKFILL SHALL BE PLACED IN 8" MAXIMUM LIFTS WITH A MECHANICAL COMPACTOR.
3. CONSTRUCTION AND INSTALLATION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION L-108 AND L-110.



**4" - 4 WAY DUCT DETAIL**  
NOT TO SCALE

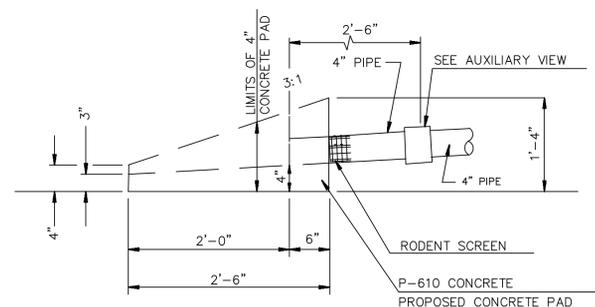
**NOTES:**

1. PROVIDE 10" MIN. OF SLACK COUNTERPOISE IN A NEAT COIL AT EACH MANHOLE, HANDHOLE, DUCT END, ETC.

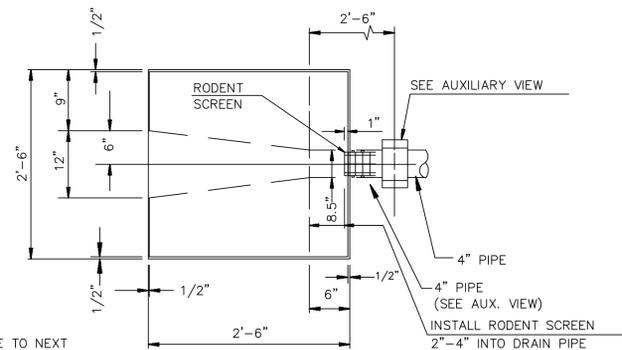


NOTE: FOLLOW HEAT SHRINK SLEEVE MANUFACTURER'S INSTRUCTIONS. PROVIDE ON ALL AIRPORT LIGHTING CONNECTIONS.

**L-823 CONNECTOR**  
**SECTION THRU HEAT SHRINK SLEEVE**  
NOT TO SCALE



**ELEVATION VIEW**

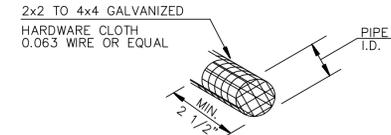


**PLAN VIEW**

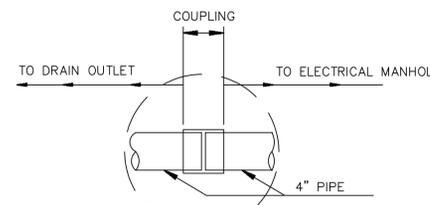
**NOTES:**

1. INSTALLATION OF NEW EDGE LIGHT DRAIN OUTFALL (HEADWALL) AT END OF PROPOSED EDGE LIGHT DRAIN PIPE, AND INSTALLATION OF NEW EDGE LIGHT DRAIN PIPE ARE INCIDENTAL TO 'ELECTRICAL MANHOLE' ITEM OF WORK. NO SEPARATE PAY.

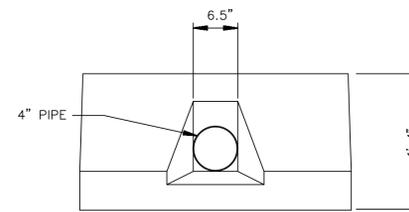
**EDGE LIGHT DRAIN OUTFALL DETAIL**  
NOT TO SCALE



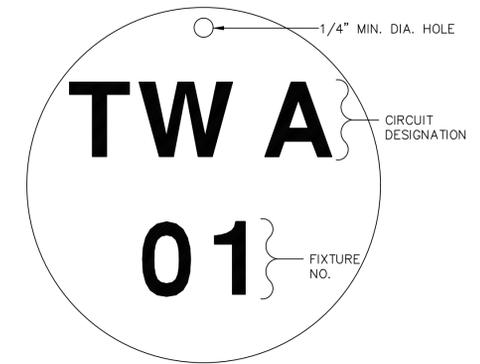
**RODENT SCREEN**



**AUXILIARY VIEW**



**END ELEVATION VIEW**



**FIXTURE IDENTIFICATION TAG**  
NOT TO SCALE

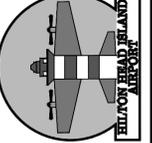
**NOTES:**

1. CHARACTERS ARE TO BE MIN. 1" HIGH IMPRESSED ON NON-CORROSIVE METAL DISC (2" MIN. DIAMETER DISC) MOUNTED ON EDGE LIGHT COLUMN WITH SELF-LOOK STAINLESS STEEL CABLE TIE. ATTACH TO GUIDANCE SIGN SIDE PANEL WITH SELF TAPPING SCREW SEALED WITH SILICONE SEALANT.
2. CIRCUIT DESIGNATIONS AND FIXTURE NUMBERS TO BE PROVIDED BY THE ENGINEER.

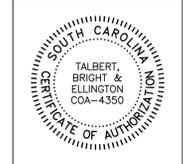
DATE	REVISIONS

DESIGNED BY: CEV	PROJECT ENG.: JAS	PROJECT NO.: 2119-1906	DRAWN BY: JPB	CHECKED BY: PET	DATE ISSUED: MAY 2020
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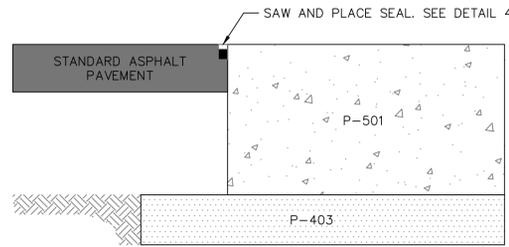
**HILTON HEAD ISLAND AIRPORT**  
"Fly to the Fun"



**ELECTRICAL DETAILS - SHEET 2 OF 2**  
**COMMERCIAL APRON EXPANSION**  
SCALE: AS SHOWN  
FILE NAME: 1906-ELEC

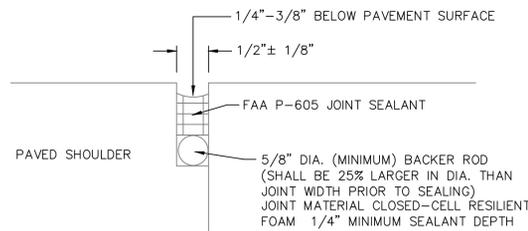


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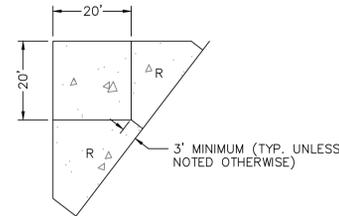
**STANDARD ASPHALT JOINT DETAIL (TYPE S)**

NOT TO SCALE  
NOTE: ALL SILICONE SEALANT SHALL BE DOW CORNING PRODUCT NO.890SL OR EQUAL.



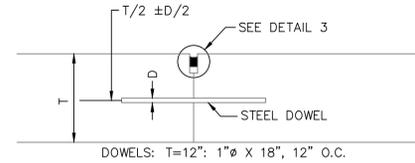
**DETAIL 4 AT ASPHALT JOINTS**

NOT TO SCALE  
NOTE: ALL SILICONE SEALANT SHALL BE DOW CORNING PRODUCT NO.890SL OR EQUAL.



**EDGE DETAIL (TYPICAL)**

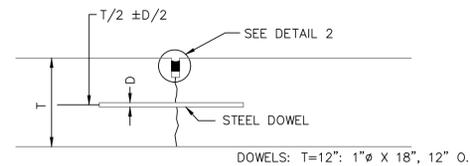
NOT TO SCALE



**DOWELED CONSTRUCTION JOINT (TYPE E)**

NOT TO SCALE

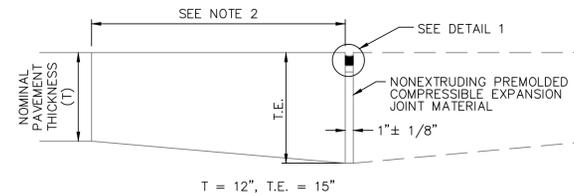
- NOTES:
- WHERE NEW PAVEMENT ADJOINS EXISTING PAVEMENT, SAW CUT EXISTING PAVEMENT FULL DEPTH TO A NEAT, UNIFORM EDGE PRIOR TO PAVING.
  - DRILL HOLES INTO EXISTING PAVEMENT AND THOROUGHLY CLEAN, FILL WITH EPOXY RESIN TO SET DOWEL.
  - GREASE ENTIRE DOWEL IN NEW PAVEMENTS.



**DOWELED CONTRACTION JOINT (TYPE C)**

NOT TO SCALE

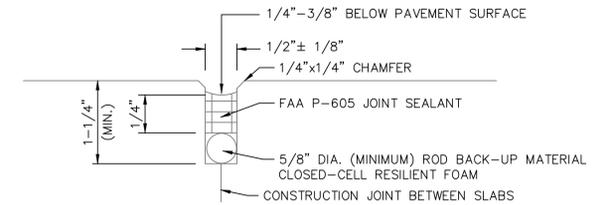
- NOTE: 1. GREASE ENTIRE DOWEL IN NEW PAVEMENTS.



**THICKENED EDGE ISOLATION JOINT (TYPE A)**

NOT TO SCALE

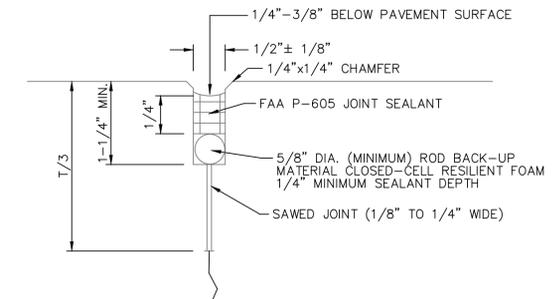
- NOTES:
- COST OF ADDITIONAL CONCRETE AT THICKENED EDGE TO BE INCLUDED IN COST OF 12" P.C. CONCRETE PAVEMENT AND 8" P.C. CONCRETE PAVEMENT.
  - 15' NORMAL (10' MIN.) TO NEAREST JOINT FOR 12" P.C.C.



**DETAIL 3**

NOT TO SCALE

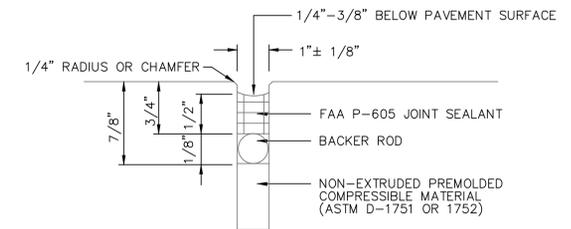
- NOTE: ALL SILICONE SEALANT SHALL BE DOW CORNING PRODUCT NO.890SL OR NO.888



**DETAIL 2**

NOT TO SCALE

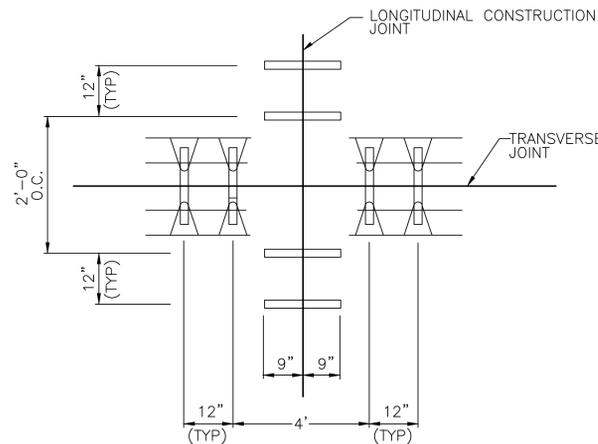
- NOTE: ALL SILICONE SEALANT SHALL BE DOW CORNING PRODUCT NO.890SL OR NO.888



**DETAIL 1**

NOT TO SCALE

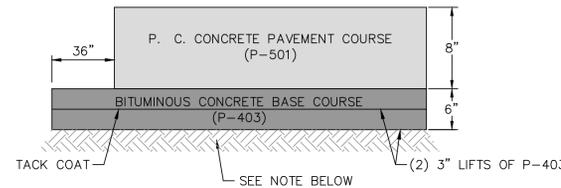
- NOTE: ALL SILICONE SEALANT SHALL BE DOW CORNING PRODUCT NO.890SL OR NO.888



**TYPICAL DOWEL BAR SPACING AT JOINT INTERSECTION**

NOT TO SCALE

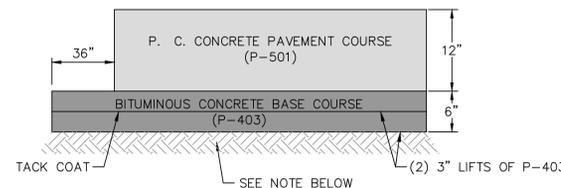
NOTE: DOWEL BAR SPACING ALONG LONGITUDINAL CONSTRUCTION JOINT IS ESTABLISHED BETWEEN INTERSECTING TRANSVERSE JOINTS USING DIMENSIONS IN THIS DETAIL.



**8" P.C. CONCRETE PAVEMENT SECTION**

NOT TO SCALE

- NOTES:
- PREPARED SUBGRADE (P-152) COMPACTED TO 100% OF MAXIMUM DENSITY AS DETERMINED BY ASTM D1557.



**12" P.C. CONCRETE PAVEMENT SECTION**

NOT TO SCALE

- NOTES:
- PREPARED SUBGRADE (P-152) COMPACTED TO 100% OF MAXIMUM DENSITY AS DETERMINED BY ASTM D1557.

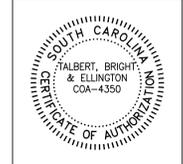
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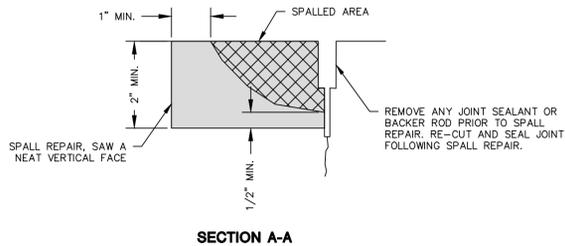
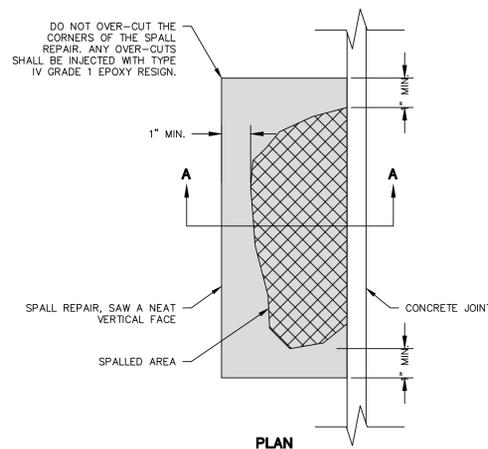
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**PAVEMENT DETAILS (SHEET 1 OF 2)**  
**COMMERCIAL APRON EXPANSION**

FILE NAME: 1906-PAVEDET SCALE: N.T.S.



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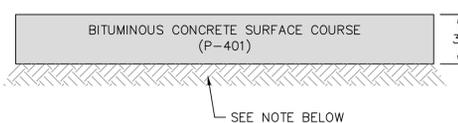


**CONCRETE PAVEMENT SPALL REPAIR DETAIL**  
NOT TO SCALE

1. CONCRETE SPALLS SHALL BE REMOVED USING VERTICAL SAW CUTS A MINIMUM OF 2" DEEP, AT LEAST 1" AWAY FROM THE SPALL. THE SAW CUTS SHALL FORM A RECTANGULAR AREA. THE BOTTOM OF THE CONCRETE AREA REMOVED SHALL BE AT LEAST 1/2" DEEPER THAN THE SPALL.
2. THE SPALL REPAIR AREA SHALL BE THOROUGHLY CLEANED PRIOR TO INSTALLING THE SPALL REPAIR MATERIAL. THE REPAIR AREA MUST BE CLEAN AND DRY. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
3. SPALL REPAIR MATERIAL SHALL BE DS BROWN DELPATCH ELASTOMERIC CONCRETE. THE SPALL REPAIR SHALL BE MIXED PER MANUFACTURER'S SPECIFICATIONS AND INSTALLED PER THIS DETAIL.

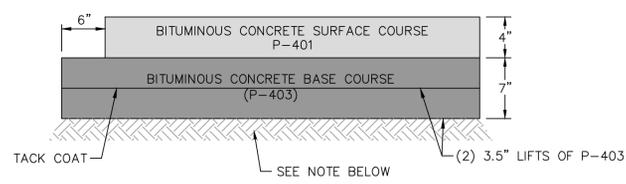
**PAVEMENT DETAIL NOTES:**

1. CONTRACTOR SHALL PLACE ALL BACKER ROD IN ALL SAW CUTS IMMEDIATELY FOLLOWING SAWING AND CLEANING OF JOINT TO KEEP MATERIAL FROM ENTERING SAWED JOINT. BACKER ROD SHALL BE SIZED TO ENSURE A TIGHT FIT AGAINST THE SIDES OF THE JOINT.
2. A MINIMUM OF SEVEN DAYS CURE TIME IS REQUIRED FOR DRILLING NEW CONCRETE UNLESS APPROVED BY THE ENGINEER, OR CONCRETE REACHES 75% OF FULL STRENGTH.
3. AFTER THE DRILLING IS COMPLETE AND PRIOR TO THE INSTALLATION OF THE DOWELS, THE HOLES SHALL BE THOROUGHLY CLEANED TO REMOVE DRILLING DUST, CONCRETE CHIPS, AND ANY MATERIAL DETRIMENTAL TO DEVELOPING BOND.
4. EPOXY MORTAR SHALL BE INSERTED INTO THE BACK OF THE DOWEL HOLE BY INJECTION NOZZLE AND SUFFICIENT MORTAR PLACED IN THE HOLE SO THAT A SLIGHT AMOUNT OF MORTAR WILL BE FORCED OUT WHEN THE DOWEL IS INSERTED AND TAPPED TO THE CORRECT POSITION. SMALL WEDGES MAY BE USED TO SUPPORT THE DOWEL IN THE CORRECT ALIGNMENT UNTIL THE MORTAR HARDENS. CONTRACTOR SHALL SUBMIT PROPOSED EPOXY MORTAR INFORMATION FOR APPROVAL BY THE ENGINEER PRIOR TO STARTING THIS WORK.
5. LONGITUDINAL AND TRANSVERSE CONSTRUCTION JOINTS SHALL NOT REQUIRE INITIAL SAWCUT.
6. TRANSVERSE CONSTRUCTION JOINTS SHALL BE USED ONLY AS APPROVED BY THE ENGINEER, OR A BULKHEAD IF APPROVED BY THE OWNER.
7. THE BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE SEALANT AND SLIGHTLY OVERSIZED TO PREVENT MOVEMENT DURING THE JOINT SEALING OPERATION.



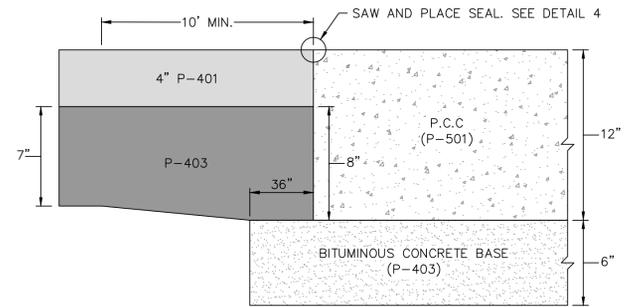
**STANDARD ASPHALT PAVEMENT SECTION**  
NOT TO SCALE

- NOTES:
1. PREPARED SUBGRADE (P-152) COMPACTED TO 100% OF MAXIMUM DENSITY AS DETERMINED BY ASTM D1557.



**AIRFIELD ASPHALT PAVEMENT SECTION**  
NOT TO SCALE

- NOTES:
1. PREPARED SUBGRADE (P-152) COMPACTED TO 100% OF MAXIMUM DENSITY AS DETERMINED BY ASTM D1557.



**TRANSITION BETWEEN P.C.C. AND AIRFIELD BITUMINOUS CONCRETE PAVEMENT SECTION**  
NOT TO SCALE

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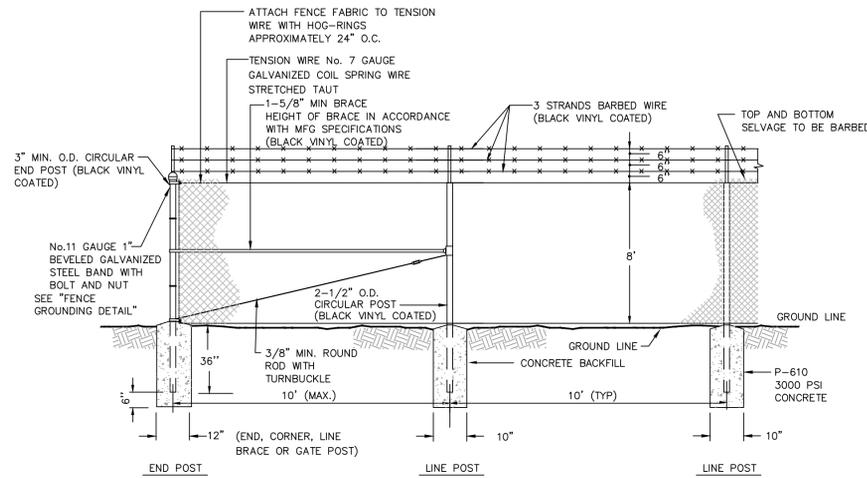
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**PAVEMENT DETAILS (SHEET 2 OF 2)**  
**COMMERCIAL APRON EXPANSION**  
SCALE: N.T.S.  
FILE NAME: 1906-PAVEDET

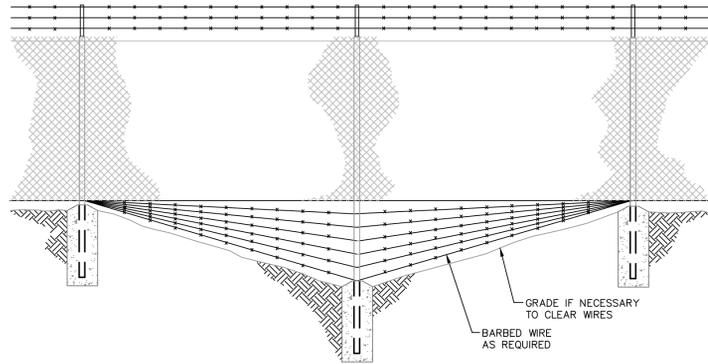


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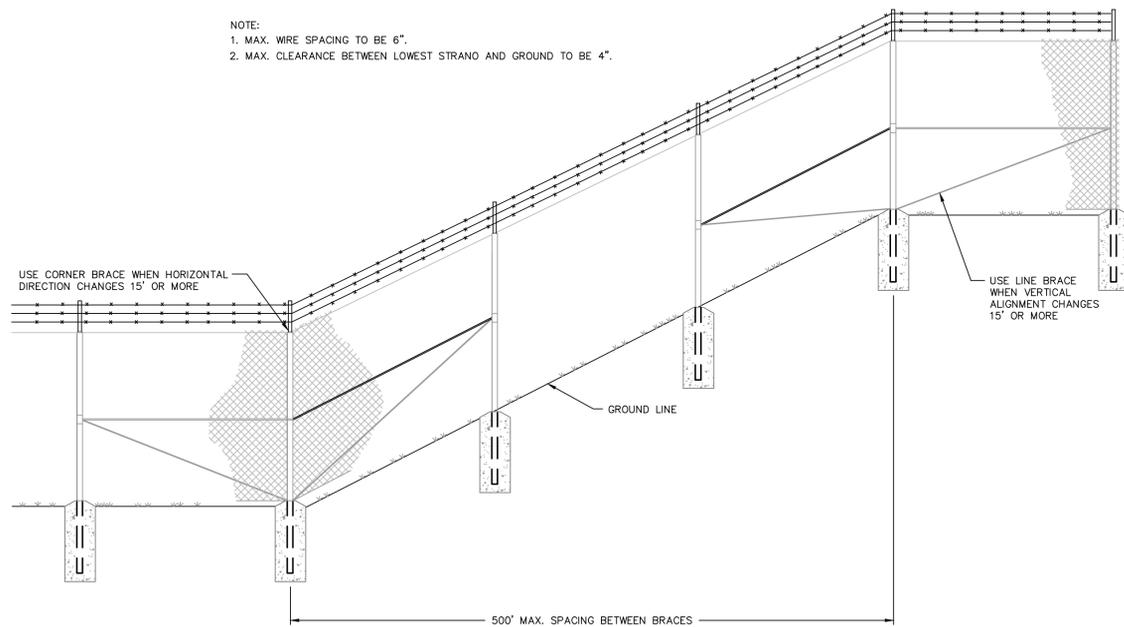
**WIRE FENCE DETAIL**  
NOT TO SCALE

- NOTE:
1. A MOISTURE-EXCLUDING CAP IS REQUIRED ON LINE POSTS, TUBULAR METAL MATERIAL FOR CAP SHALL CONFORM TO THE ALLOWABLE TYPES FOR OTHER LISTED FITTINGS.
  2. PROPOSED FENCE, FABRIC, POSTS, POST CAPS, AND RAILS SHALL BE BLACK VINYL COATED.

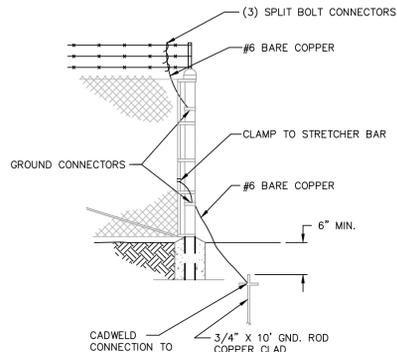


**DETAIL OF DITCH CROSSING**  
NOT TO SCALE

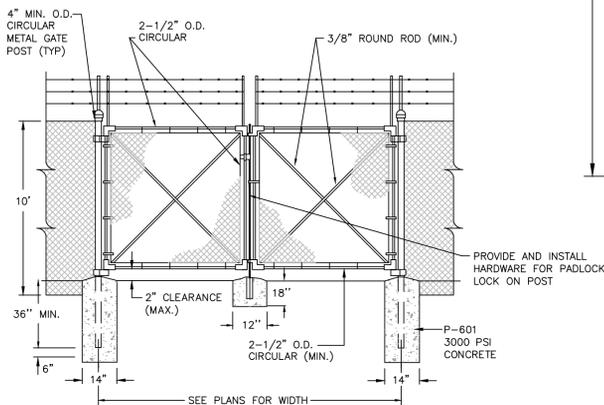
- NOTE:
1. MAX. WIRE SPACING TO BE 6".
  2. MAX. CLEARANCE BETWEEN LOWEST STRAND AND GROUND TO BE 4".



**CHANGE IN FENCE ALIGNMENT DETAIL**  
NOT TO SCALE



**FENCE GROUNDING DETAIL**  
NOT TO SCALE



**DOUBLE SWING GATE**  
NOT TO SCALE



- NOTES:
1. THE FULL SIGN (BOTH BACKGROUND AND TEXT) SHALL BE RETROREFLECTIVE.
  2. THE CONTRACTOR SHALL FIELD VERIFY EXISTING SIGNS AND ATTEMPT TO MATCH.

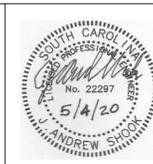
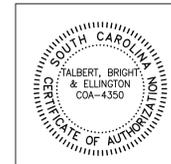
**SECURITY FENCE SIGN DETAIL**  
NOT TO SCALE

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MISCELLANEOUS DETAILS  
COMMERCIAL APRON EXPANSION  
SCALE: N.T.S.  
FILE NAME: 1906-MISCDET



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SHT.

**650**