



SR 44 @ SCOTT ROAD TRAFFIC SIGNAL INSTALLATION

5/10/2021 (updated 5/14/2021)

RESPONSE TO QUESTIONS

BID NUMBER: 21-42001-002

The following are responses **(in red)** to questions received from prospective bidders:

The following questions were received on 5/6/2021 at 5:55 PM:

- Please confirm if the county is going to purchase the mast arm poles. Note #1 on bid form page 4 of 7 states that the “Strain poles and mast arms may be purchased by the county in the base bid”.
- Please provide roadway plans for the concrete and pavement work quantities.

Response to Questions:

- **The ITB provides the county with the option of purchasing the strain poles (mast arm uprights) and mast arms directly from the manufacturer (Base Bid) or including the strain poles/mast arms in the bid package (Alternate Bid). Refer to Note #1 in the Bid Schedule of Items.**

Refer to Addendum #2 which includes the corrected Bid Form (pages 1-7).

Bidders must provide a bid for the BASE BID and the ALTERNATE BID.

- **Refer to Addendum #1 dated 5/10/2021. Plan set has been revised to clarify quantities for pavement work, the areas of pavement widening and a plan sheet for the striping.**

The following questions were received on 5/6/2021 at 10:39 AM:

- We understand it that the county shall purchase the mast arm uprights and arms in the base bid. Page 4 of 7 of the bid forms Note 1 references the base bid price for Signal Installation. Should this state the mast arm pay items should be labor and foundation only. The signal installation price (pay item) should include signal materials

- Can additional detail be provided for the Class B, GAB, and asphalt paving work to be completed? It appears, in the signal drawing provided, that this work will be done on the radii on the NW & SW corners. Is this correct?
- GDOT spec allows for small modifications such as radius cutbacks to use Class B concrete in lieu of asphalt paving. Will the county allow this as well? This would eliminate the need for GAB as well.

Response to Questions:

- **Note #1 within the Base Bid Schedule of Items states this. For clarification purposes, pages 1-7 will be clarified with issuance of Addendum #2**
- **Yes – you are correct. Refer to Addendum #1**
- **Yes – you are correct. Refer to Addendum #1**

The following question was received on 5/3/2021 at 3:20 PM:

- The timeline of this project is 120 days from NTP. Painted mast arms are running 18-22 weeks after submittals approval; I had a set show up 28 weeks after order a few weeks back. Will NTP be held until the poles arrive?

Response to Question:

- **Yes – Under the ALTERNATIVE BID, Putnam County will expect materials to be ordered when the contract is executed by the County and the selected Contractor. The County will need to have verification that materials are ordered the day following the contract execution. A NTP will be issued when the materials are shipped to the Contractor by the manufacturer. The County will need to receive periodic status updates from the Contractor as to when the poles and mast arms are expected to arrive from the manufacturer.**

The following questions were received on 4/28/2021 at 11:20 AM & 2:18 PM:

- I noticed there are only plans for the Traffic Signal work, will you be providing construction/signs and marking plans for the rest of the work needed.
- I would like to clarify there are not any plans that have installation notes similar to that of the Traffic Signal showing where the Asphalt Begins/Ends ETC. I should be able to go off line items and the notes for concrete and striping but detailed plans would be helpful.
- Also could you please clarify the difference between the Base Bid and the Alternate Bid? Thanks

Response to Questions:

- Refer to Addendum #1
 - Addendum #1 provides clarification with a revised plan set that identifies pavement widening and striping details
 - Refer to Note #1 within the BASE BID Schedule of Items and Addendum #2.
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The following question was received 5/14/2021 at 5:12PM:

One of the issues we are having with a lot of project coming out to after April 15. The new GDOT spec went into effect. The new mast arm spec in particular has a major effect on pricing, pole, and foundation sizes. Mast arm prices have doubled and tripled.

I am assuming the Alternate bid is to include cost for the mast arms due to note 1 stating the city “may” purchase the mast arms. Can you specify which GDOT specification will be required should the contractor be responsible for supplying the poles?

Response to Question:

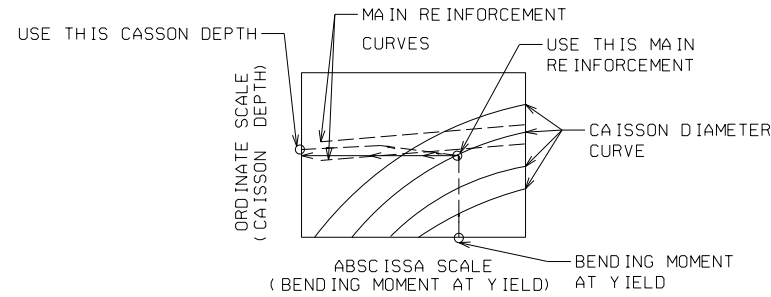
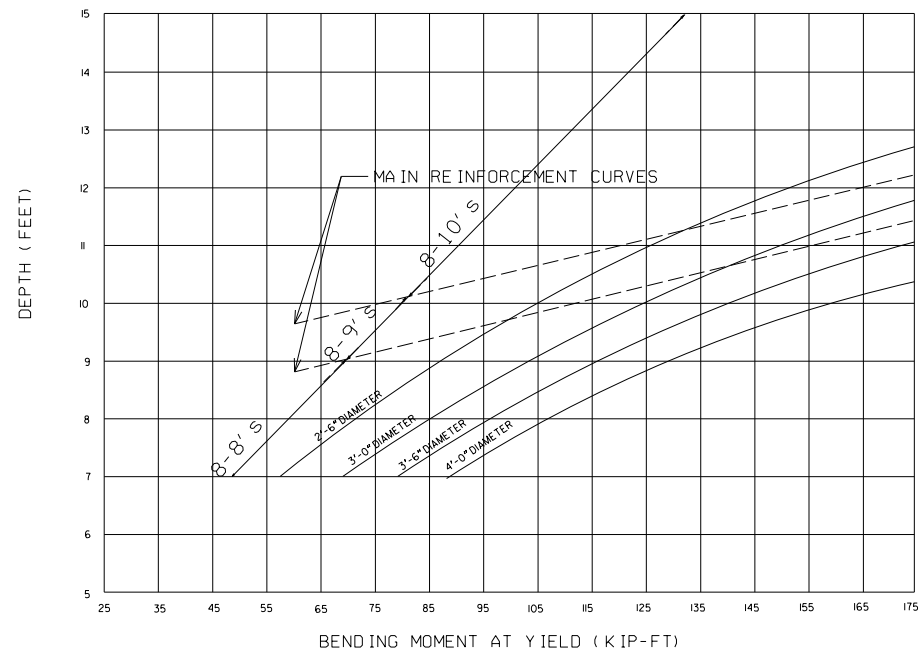
Please note that the BASE BID is where the county is purchasing the strain poles/mast arms. Refer to Note 1.

Please refer to Addendum #4 and the Response to Questions dated 5/10/2021 for clarifications.

The county is aware that beginning on 4/16/2021, the following GDOT 2021 Specification for Strain Pole and Mast Arm Foundations (TS-05), which was adopted by GDOT on March 19th, 2021, shall be applicable to all projects on state right-of-way. This new specification will be applicable to this ITB.

PIEDMONT

$\gamma = 120$ PCF
 $\phi = 25$ DEGREES
 $C = 300$ PCF

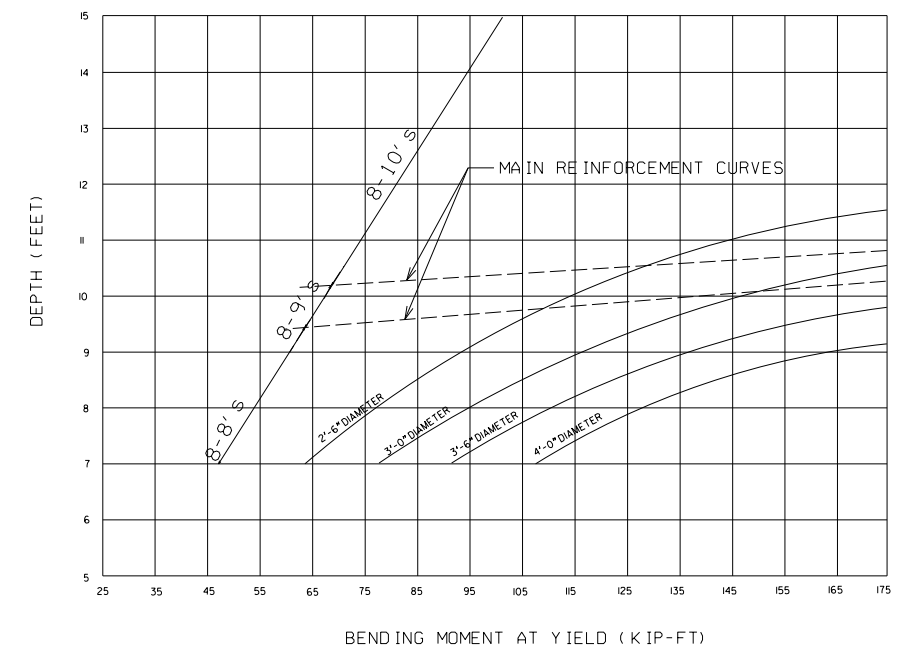


PROCEDURE TO FIND FOOTING SIZE

1. DETERMINE "BENDING MOMENT AT YIELD" FROM APPROVED SHOP DRAWINGS.
2. SELECT DIAMETER OF CAISSON.
3. READ "BENDING MOMENT AT YIELD" ON ABSCISSA SCALE, PROJECT A VERTICAL LINE UPWARD UNTIL THE DESIRED "CAISSON DIAMETER CURVE" IS INTERSECTED, TURN 90 DEGREES AND PROJECT A HORIZONTAL LINE UNTIL THE ORDINATE SCALE IS INTERSECTED.
4. READ THE REQUIRED "CAISSON DEPTH" FROM THE INTERSECTION POINT ON THE ORDINATE SCALE DEPTH SHALL BE INTERPOLATED TO THE NEAREST 3 INCH INCREMENT.
5. READ THE REQUIRED "MAIN REINFORCEMENT SIZE" FROM THE INTERSECTION POINT ON THE CAISSON DIAMETER CURVE.

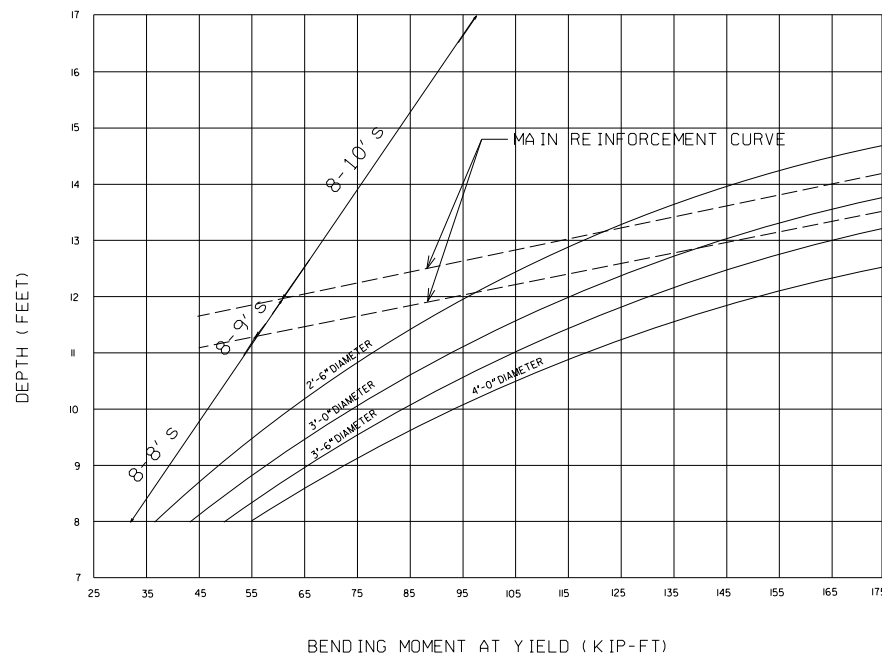
VALLEY & RIDGE

$\gamma = 115$ PCF
 $\phi = 18$ DEGREES
 $C = 750$ PSF



COASTAL PLAIN

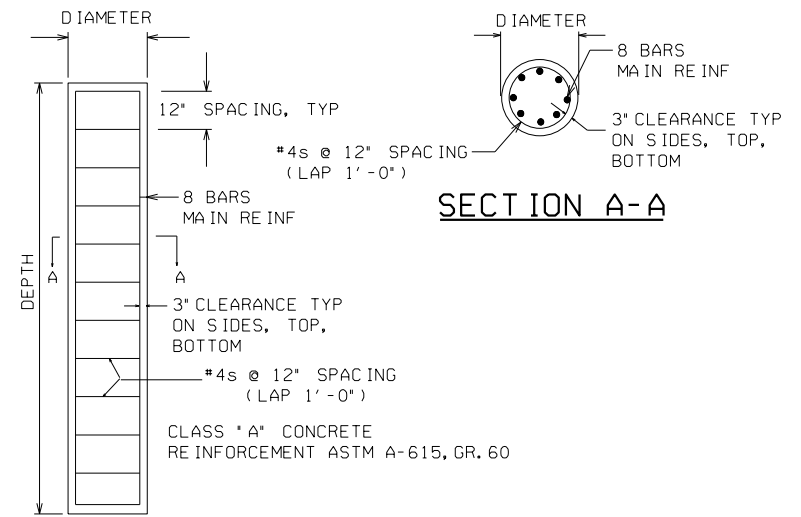
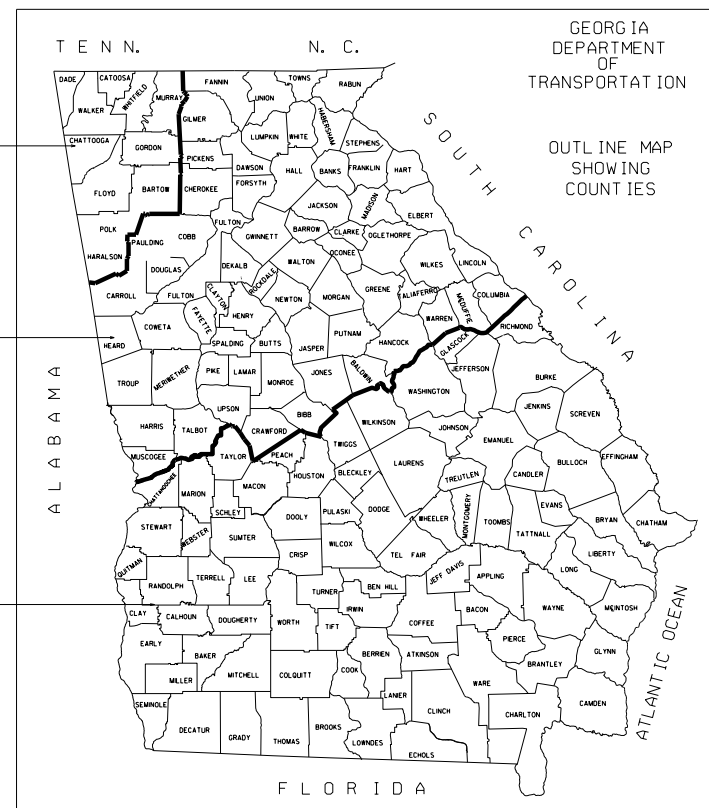
$\gamma = 120$ PCF
 $\phi = 32$ DEGREES
 $C = 0$ PCF



VALLEY AND RIDGE REGION

PIEDMONT REGION

COASTAL PLAIN REGION



SECTION A-A

NOTE:
 ALL REINFORCEMENT SHALL BE PLACED AND TIED IN ACCORDANCE WITH THE STANDARD SPECS AND SPECIAL PROVISIONS. WELDING OF BAR REINFORCEMENT SHALL NOT BE PERMITTED.

CAISSON DETAIL

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAIL	
STRAIN POLE AND MAST ARM FOUNDATIONS	
NOVEMBER 2020 NO SCALE	NUMBER TS-05