



CITY OF CALLAWAY
CHERRY STREET RESURFACING, SIDEWALKS
& DRAINAGE - BID NO.: PW2022-02

ADDENDUM #4

Date Issued: March 22, 2022

This addendum is being released to address the following question.

1. We have concerns with the response for question #20. The designer has stated that barrels will be ok for the drop off of 18” overnight. With live traffic directly adjacent to the excavated area there will not be enough setback for barrels in this location. These cuts should be shielded with barrier wall. When doing so, there will need to be enough room for the proper install, according to FDOT standards. Has the designer looked at this option? Whom will the liability fall on, should an accident occur and it is not per the FDOT Standards we are being told to follow? If liability shall fall on the Contractor, please provide what type of barrier wall and enough quantity for bidding purposes to provide 2 full runs and 2 runs of rest to encompass all phases of MOT.

A. Note 7 on Sheet 83 of the plan set refers to FDOT Index 102-000 (see below) which discusses drop-off scenarios related to temporary traffic control. Based on the 18” drop off situation during excavation, Condition 3 would apply (shown in Table 1 on Sheet 2 of 7 of the index). Condition 3 states the device required is a Temporary Barrier, therefore Question 20 re: barrels were unnecessary, and our response assumed the contractor would use the barrels under their own liability. The contractor will modify the MOT plan as needed and should have a certified MOT person on staff or under contract for those services. We suggest using FDOT Index 102-120, Low Profile Barriers. The contractor would price them per general note #9, Sheet 1 of 5 of the index.

This is an approved FDOT product, and the contractor will assume full liability for work within the work zone.

The basis of bid identifies a Maintenance of Traffic (lump sum) line item. The contractor is to provide a proposed bid cost for all required MOT materials (including the number of barriers required to complete the work).

ATTACHMENTS:

- FDOT Standard Plans - Temporary Traffic Control Index 102-000 7 Pages
- FDOT Standard Plans - Temporary Barrier Index 102-100 4 Pages
- FDOT Standard Plans - Low Profile Barrier Index 102-120 5 Pages

Janice L. Peters, City Clerk

This Addendum must be acknowledged and included with the bid packet submission.

Signature

Company Name

Date

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3	Drop-offs
4	Miscellaneous Details
5	Post-mounted Work Zone Sign Supports
6	Work Zone Pavement Markings
7	Temporary Raised Rumble Strips
8	Temporary Traffic Control Devices

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102-000	Temporary Traffic Control General Information and Devices	N/A
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DESCRIPTION:
 Deleted project information sign details and renumbered.

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 01/28/20

GENERAL NOTES:
 1. Use this Index for all work zones.
 2. See the Plans for Work Zone Speed.

CLEAR ZONE WIDTHS FOR WORK ZONES	
Work Zone Speed (mph)	Taper Lengths & Auxiliary Lanes & Single Lane Ramps (feet)
≥ 60	30, 24, 18
55	24, 18, 14
45-50	18, 14, 10
All Speeds with Curb & Gutter	14, 10
	d' Behind Face of Curb

CHANNELIZING DEVICE SPACING	
Work Zone Speed (mph)	Max. Spacing (feet)
≤ 45	25, 50, 25, 50
≥ 50	25, 50, 50, 100

TAPER LENGTH "L"	
Work Zone Speed (mph)	Min. Length (feet)
≤ 40	WS'
≥ 45	L = WS
	L = WS
	Where: W = width of offset in feet
	S = speed in mph

COMMON TAPER LENGTHS "L" (FEET)	
Work Zone Speed (mph)	Width of Offset (feet)
25	5, 8, 10, 11, 12
30	55, 85, 105, 115, 125
35	75, 120, 150, 165, 180
40	105, 165, 205, 225, 245
45	135, 215, 270, 295, 320
50	225, 360, 450, 495, 540
55	250, 400, 500, 550, 600
60	275, 440, 550, 605, 660
65	300, 480, 600, 660, 720
70	325, 520, 650, 715, 780
75	350, 560, 700, 770, 840

BUFFER LENGTH "U"	
Work Zone Speed (mph)	Min. Length (feet)
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	720
Note: When Buffer Length "U" cannot be attained due to geometric constraints, use the minimum buffer length available, but not less than 155 feet.	

WORK ZONE SIGN DISTANCE "X"	
Road Type	Min. Spacing (feet)
Arterials and Collectors with Work Zone Speed ≤ 40 mph	200
Arterials and Collectors with Work Zone Speed ≥ 45 mph	500
Limited Access Roadways	1,500
Note: When using MUTCD Typical Applications, use the appropriate "A", "B", and "C" distances between signs.	

TEMPORARY TRAFFIC CONTROL TABLES

TEMPORARY TRAFFIC CONTROL
 GENERAL INFORMATION AND DEVICES

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 102-000

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DROP-OFF NOTES:

- When drop-offs occur within the clear zone due to construction or maintenance activities, protection devices are required (See Table 1). A drop-off is defined as a drop in elevation, parallel to the adjacent travel lanes, greater than 3" with slope (A/B) steeper than 1:4. In super/elevated sections, the algebraic difference in slopes should not exceed 0.25 (See Drop-off Condition Detail).
- Optionally, mitigate drop-offs by placing slopes of optional base material per Specifications Section 285. Slopes shallower than 1:4 may be required to avoid algebraic difference in slopes greater than 0.25. Include the cost for the placement and removal of the material in Maintenance of Traffic (MOT). Use of this treatment in lieu of a temporary barrier is not eligible for CSIP consideration. Conduct daily inspections for deficiencies related to erosion, excessive slopes, rutting or other adverse conditions. Repair any deficiencies immediately.
- For Setback Distance, refer to the Index or Approved Products List (APL) drawing of the selected barrier.
- For Conditions 1 and 3 provided in Table 1, any drop-off condition that is created and restored within the same work period will not be subject to the use of temporary barriers. However, channelizing devices will be required.
- When permanent curb heights are $\geq 6"$, no channelizing device will be required. For curb heights $\leq 6"$, see Table 1.

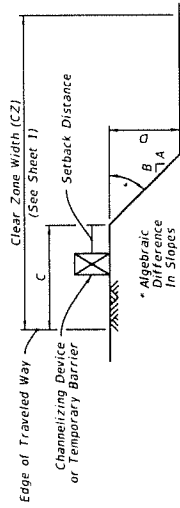


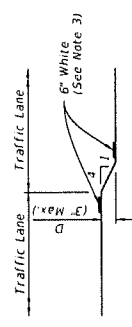
Table 1
Drop-off Protection Requirements

Condition	D (inches)	C (feet)	Device Required
1	>3	2 - 12	Temporary Barrier
2	>3 to ≤ 5	12 - CZ	Channelizing Device
3	>5	2 - CZ	Temporary Barrier
4	Removal of Bridge or Retaining Wall Barrier		Temporary Barrier
5	Removal of portions of Bridge Deck		Temporary Barrier

Note: Do not allow any drop-off conditions greater than 3 inches within two feet of the edge of traveled way.

MILLING & RESURFACING NOTES:

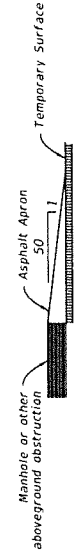
- Whenever there is a difference in elevation between adjacent travel lanes, place "Uneven Lanes" signs (WB-11) at intervals of 0.5 miles or less.
- If D is 1.5" or less, no drop-off treatment is required.
- If the slope of the drop-off is greater than 1:4 (not to exceed 1:1), place a 6" white solid line on each side of the drop-off. Additionally, place "Stay In Your Lane" signs (MOT-1-06) as a supplement to the "Uneven Lanes" signs (WB-11). This condition may be used for distances of three miles or less.



MILLING & RESURFACING LANE DROP-OFF TREATMENT DETAIL

MANHOLE/CROSSWALK/JOINT DROP-OFF NOTES:

- Construct temporary asphalt apron for manholes extending 1" or more above the travel lane and crosswalks having an uneven surface greater than $\frac{1}{8}"$.
- Construct temporary asphalt apron for all transverse joints that have a difference in elevation of 1" or more.
- Remove aprons prior to constructing the next lift of asphalt.



MANHOLE/CROSSWALK/JOINT DROP-OFF DETAIL

PEDESTRIAN WAY DROP-OFF NOTES:

- A pedestrian way drop-off is defined as either:
 - a drop in elevation greater than 10" that is closer than 2' from the edge of the pedestrian way;
 - a slope steeper than 1:2 that begins closer than 2' from the edge of the pedestrian way when the total drop-off is greater than 60".
- Protect any drop-off adjacent to a pedestrian way with pedestrian longitudinal channelizing devices, temporary barrier wall, or approved handrail.

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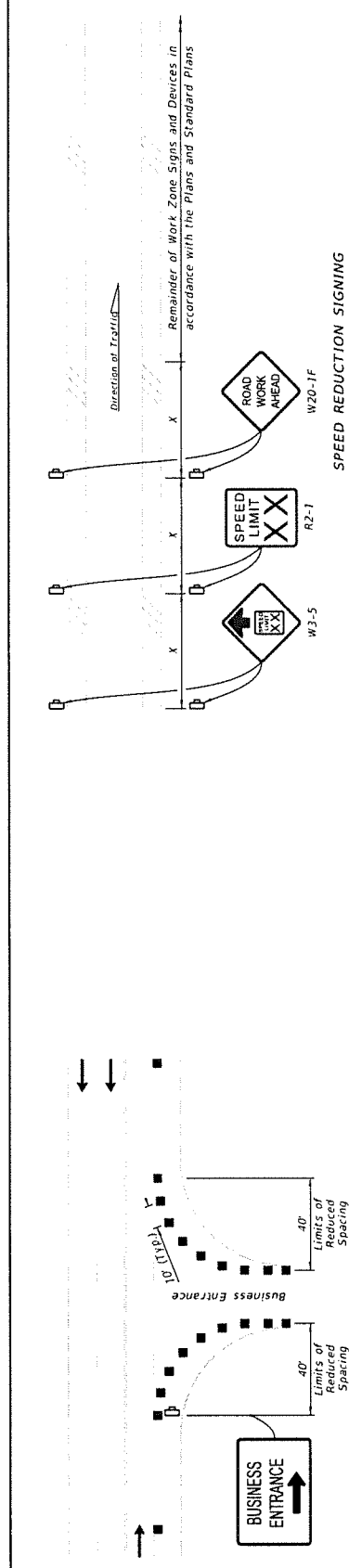
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TEMPORARY TRAFFIC CONTROL
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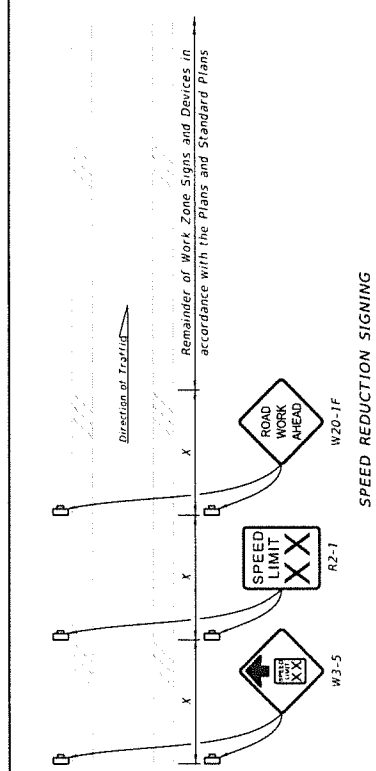
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DROP-OFFS



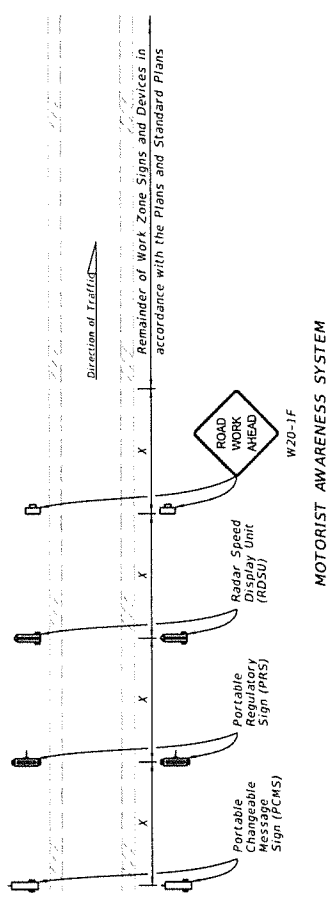
BUSINESS SIGN AND CHANNELIZING DEVICE PLACEMENT AT BUSINESS ENTRANCE

- NOTES:**
- Use this detail when work disrupts a business entrance for greater than one entire calendar day.
 - For single business entrances, place one 24"x36" business sign with the business name at each affected driveway entrance. Logos may be provided by the business owners. Alternatively, a sign with the message "Business Entrance" (see Index 700-102) may be used when approved by the Engineer.
 - When two or more businesses share a common driveway entrance, place a 24"x36" with the message "Business Entrance" (see Index 700-102) at the common driveway entrance.



SPEED REDUCTION SIGNING

- NOTES:**
- X = Work Zone Spacing
 - When called for in the Plans, use this detail in accordance with the Plans and Standard Plans. Place the speed reduction signs (W3-5 and R2-1) in advance of the "Road Work Ahead" sign (W20-1F) as shown.
 - Do not use this detail in conjunction with the Motorist Awareness System.
 - For speed reductions greater than 10 MPH, reduce speed in 10 MPH increments of 'X'.
 - Place additional "Speed Limit" signs (R2-1) at intervals of no less than one mile for rural conditions and 1,000 feet for urban conditions.
 - For undivided roadways, omit the signs shown in the median.



MOTORIST AWARENESS SYSTEM

- NOTES:**
- X = Work Zone Spacing
 - When called for in the Plans, use the Motorist Awareness System (MAS) in accordance with the Plans and Standard Plans. Indexes 102-045, 102-050, 102-055, and 102-060. When using this detail with the indexes, place the MAS devices (i.e., PCMS, PRS, and RDSU) in advance of the "Road Work Ahead" sign (W20-1F) as shown.
 - For a posted speed of 65 mph or greater, display speed with a ten mph reduction. For a posted speed of 60 mph, display a reduced speed of 55 mph. Use posted speed as the work zone speed.
 - Omit the PCMS in the median for roadways with three lanes or less in the same direction of traffic.

- TYPICAL PCMS DISPLAY:**
- With speed reduction:
 Message 1: WORKERS PRESENT AHEAD
 Message 2: SPEED REDUCED NEXT XX MI
- Without speed reduction:
 Message 1: WORKERS PRESENT AHEAD
 Message 2: NEXT XX MILES

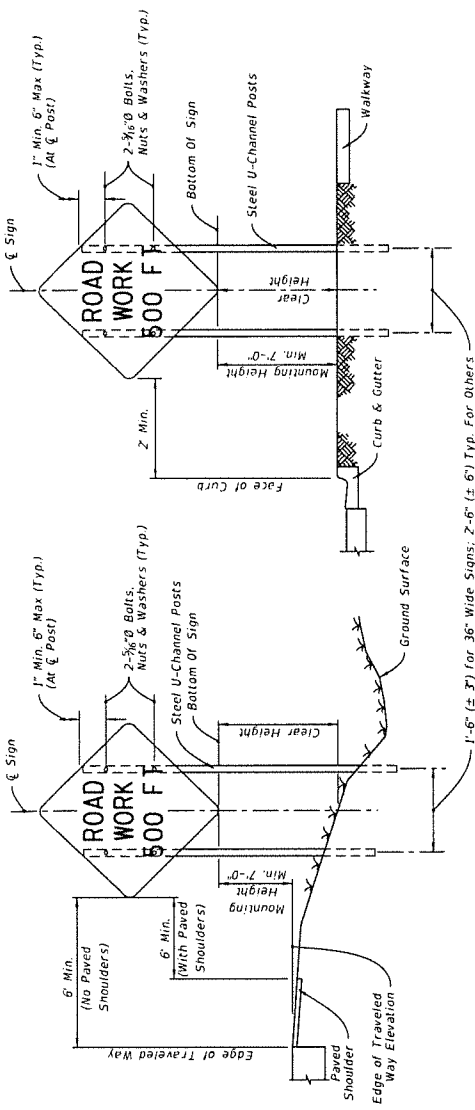
- SYMBOLS:**
- Channelizing Device (See Sheet 8)
 - Lane Identification and Direction of Traffic
 - ▭ Work Zone Sign

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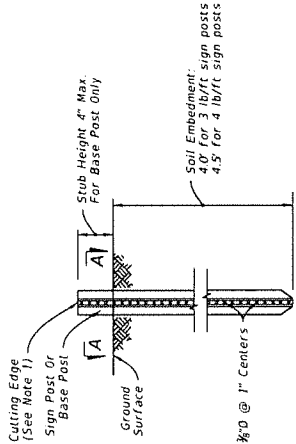
MISCELLANEOUS DETAILS

WORK ZONE SIGN POST TABLE:

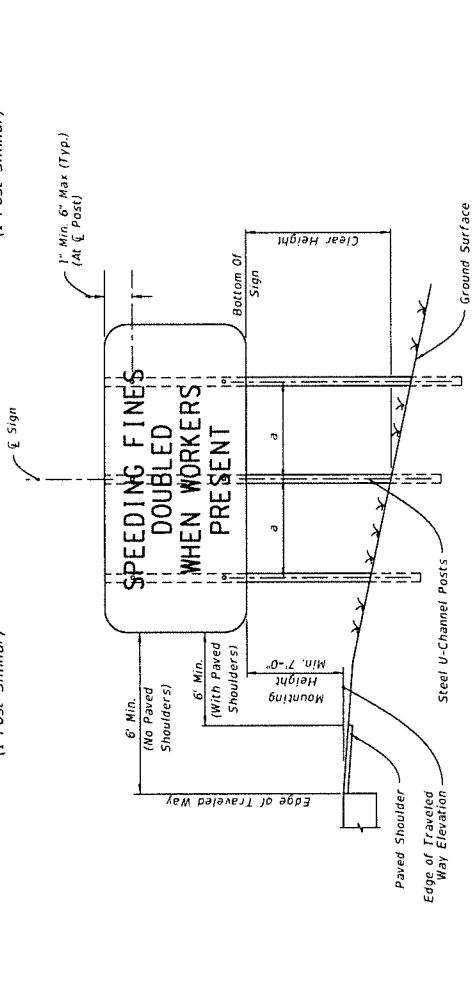
SIGN AREA (ft ²)	NUMBER OF SIGN POSTS
9.10 < 20	1
20 to < 30	2
> 30	3



SECTION A-A (Schematic)

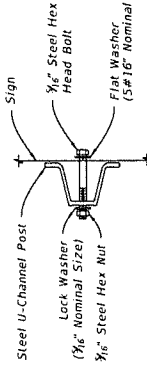


TYPICAL FOUNDATION DETAIL



NOTES:

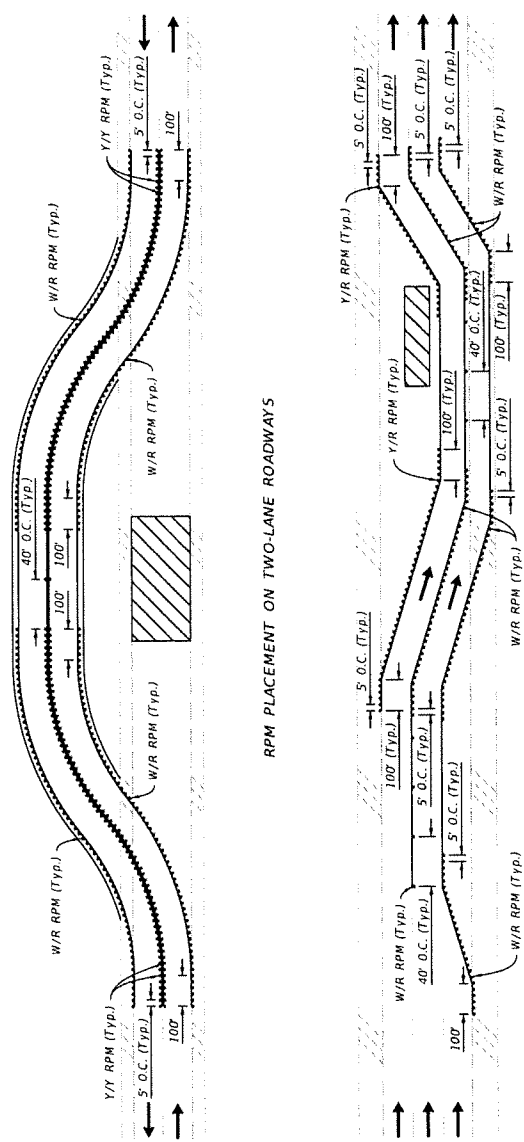
- Do not install bolts closer than 1" to cutting edge of Base Post.
- Soil plates are not required for posts installed in asphalt pavement, shoulder pavement, sidewalk, or existing rock with a minimum cumulative depth of 2'.
- Use 3 lb/ft posts for Clear Height up to 10' and 4 lb/ft posts for Clear Height up to 12'.
- For diamond warning signs with supplement plaque (up to 5 ft² in area), use 4 lb/ft posts for Clear Height up to 10'.



SIGN ATTACHMENT DETAILS

3 POSTS - FLUSH SHOULDER ROADWAY

Where: $W = 48''$; $a = 1' - 4\frac{1}{2}'' (\pm 1'')$
 $W = 60''$; $a = 1' - 9'' (\pm 1'')$
 $W = 72''$; $a = 2' - 1'' (\pm 1'')$



RPM PLACEMENT ON TWO-LANE ROADWAYS

RPM PLACEMENT ON MULTILANE ROADWAYS
(Lane Shift Shown, Other Multilane Typical Applications Similar)

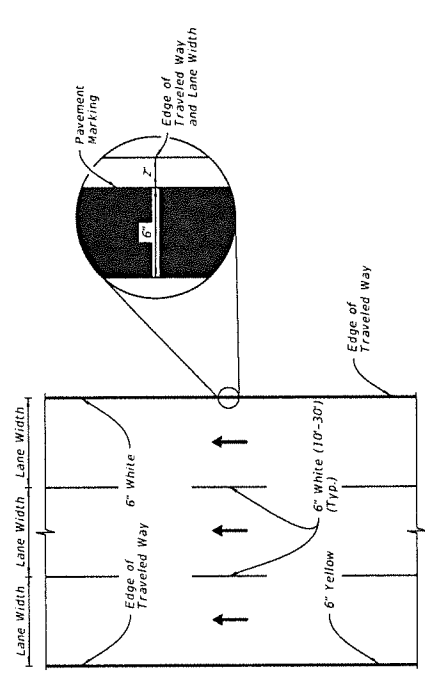
NOTES:

1. Install RPMs as a supplement to:
 - a. All lane lines
 - b. Edge lines in transitions (e.g., merges, diversions, lane shifts)
 - c. Edge lines of gore areas
2. Extend pavement marking and 5' RPM spacing by 100' in each direction for all transitions regardless of the line type.
3. Place RPMs in accordance with this detail and Index 706-001.

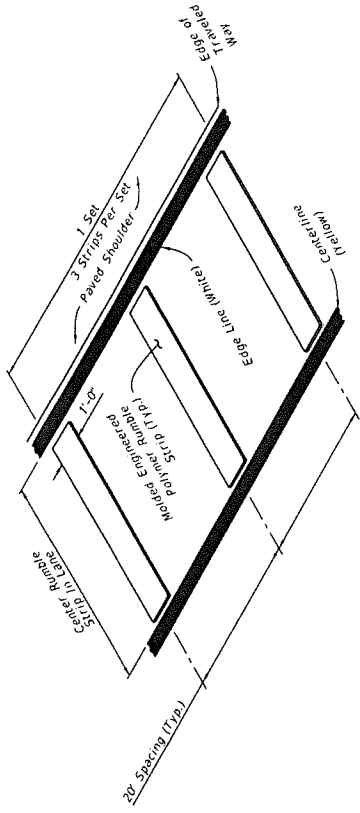
SYMBOLS:

- Work Space
- Lane Identification and Direction of Traffic

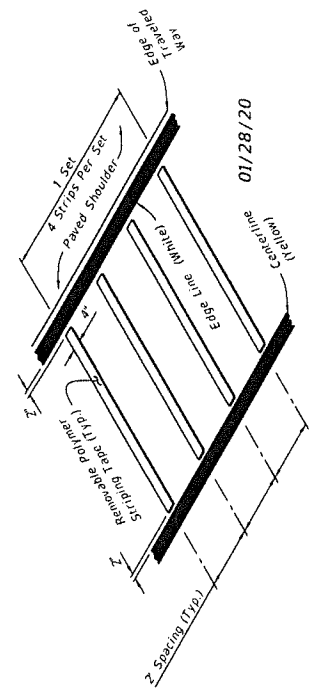
PLACEMENT OF PAVEMENT MARKINGS



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					WORK ZONE PAVEMENT MARKINGS			



MOLDED ENGINEERED POLYMER MATERIAL

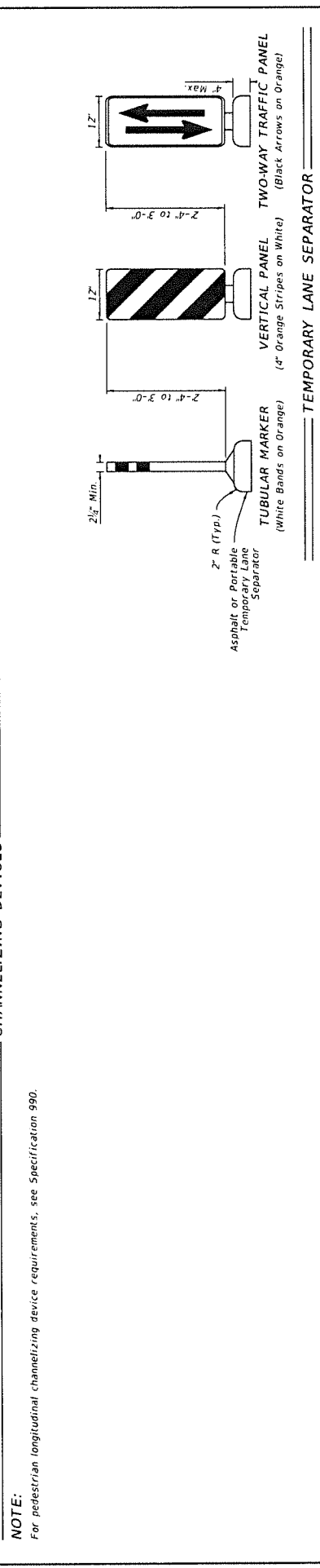
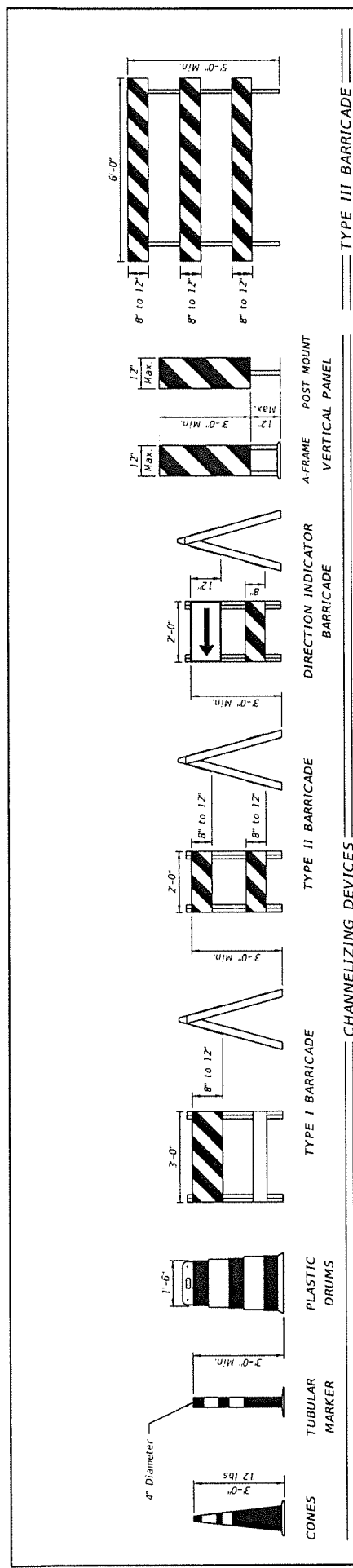


REMOVABLE POLYMER STRIPING TAPE

TEMPORARY RAISED RUMBLE STRIP SETS

NOTE:
Use Temporary Raised Rumble Strips in accordance with the Plans and Specification 102.

TEMPORARY RAISED RUMBLE STRIPS	
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NOTE:
 For pedestrian longitudinal channelizing device requirements, see Specification 990.

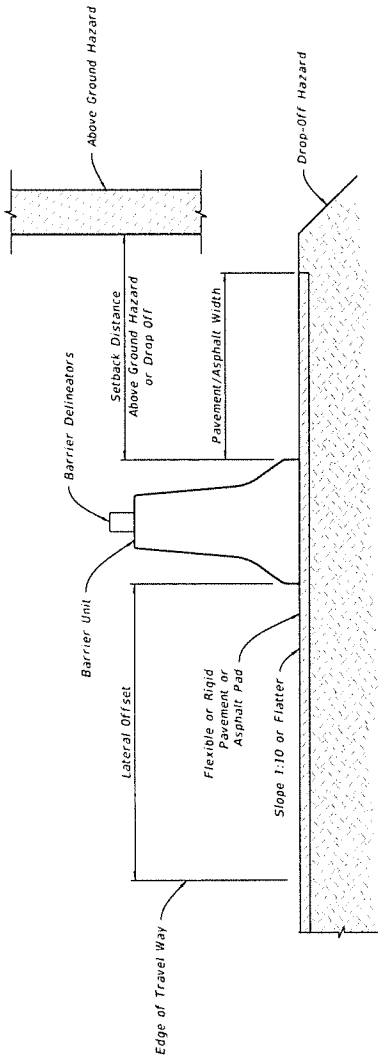
TEMPORARY TRAFFIC CONTROL DEVICES	
TEMPORARY TRAFFIC CONTROL GENERAL INFORMATION AND DEVICES	INDEX 102-000 SHEET 7 of 7
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GENERAL NOTES:

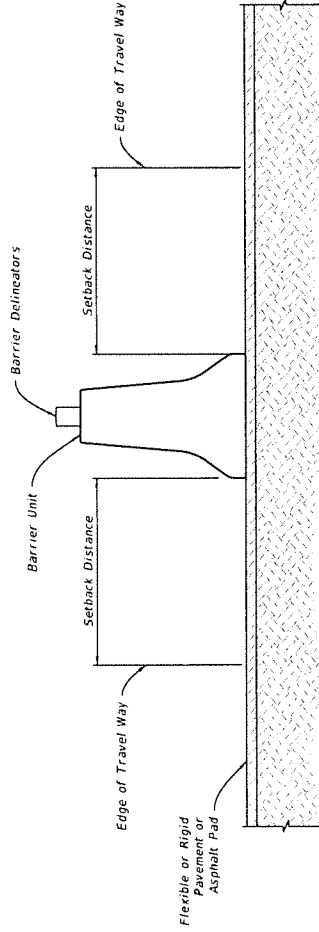
1. Temporary barrier systems may be any of the following.
 - A. Type K Temporary Concrete Barrier System (Index 102-110) installed as either Free-Standing or Anchored.
 - B. Proprietary Temporary Barrier Systems on the Approved Product List (APL).
 - a. Concrete Barrier (Free-Standing or Anchored)
 - b. Steel Barrier (Anchored)
 - c. Water Filled Barrier (Free-Standing)
2. Where existing flexible pavement is not present, construct a minimum 2" thick temporary Asphalt Pad using Miscellaneous Asphalt Pavement in accordance with Specification 339 with the exception that the use of a pre-emergent herbicide is not required.
3. For Barrier Delineators, see Specification 102. Mount on top of temporary barriers. Color must match adjacent longitudinal pavement marking.
4. Remove all grass debris, loose dirt, and sand for the pavement, bridge deck, or asphalt pad surface within the barrier footprint just prior to placement of the temporary barrier.
5. Ensure the setback distance is clear of any grass, construction debris, stockpiled materials, equipment, and objects.
6. Transitions are required between Type K Barrier and Free-standing, anchored, back-filled or other types of temporary barrier. See Index 102-110 for transitions between Type K Barrier and permanent bridge or traffic railing. Refer to the APL for transitions allowed for Proprietary Temporary Barrier Systems.
7. Anchoring (Bolting) of temporary barrier or crash cushions is not permitted on bridge superstructures that contain post-tensioned tendons within the concrete deck (top flange of concrete box girders) or on bridge superstructures consisting of longitudinally prestressed, transversely post-tensioned, solid or voided concrete slab units.
8. Anchor abutting segments of temporary barrier terminated with a Crash Cushion as shown in Index 102-110 or the APL.
9. The requirements of this Index do not apply to Temporary Low Profile Barrier. See Index 102-120.
10. Setback requirements below cover most Temporary Barrier options. Provide additional setback distance for APL products that require additional setback (deflection) space.

INSTALLATION DATA			
CONDITION	LATERAL OFFSET	SETBACK DISTANCE	PAVEMENT/ ASPHALT WIDTH
Anchored	Z Min.	Z Min. (See Note)	1' Min.
Free-standing	Z Min.	4' Min.	4' Min.

NOTE: For Bridge Decks see Index 102-110 or APL.



ROADSIDE INSTALLATION



MEDIAN INSTALLATION

LAST REVISION
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DESCRIPTION:

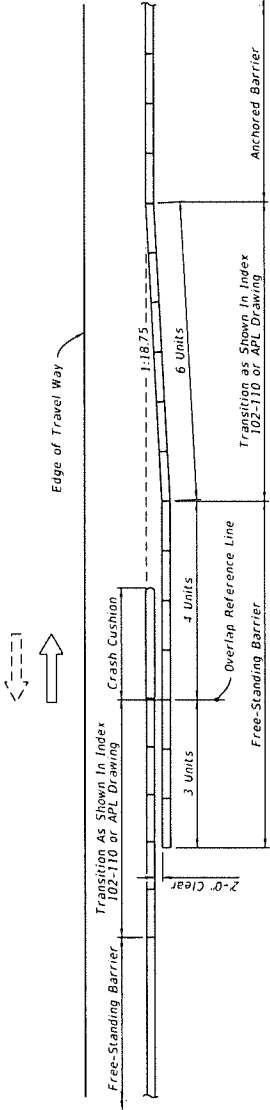


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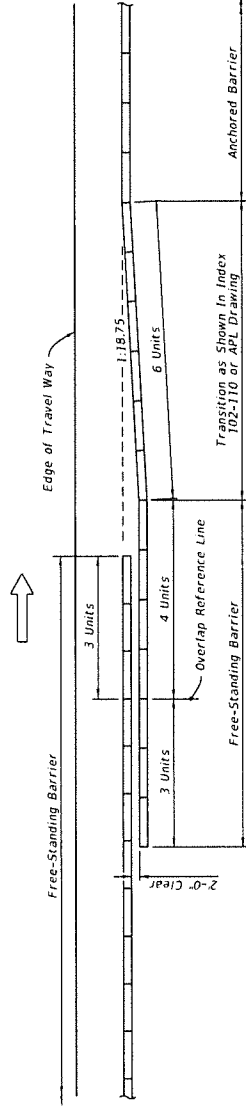
TEMPORARY BARRIER

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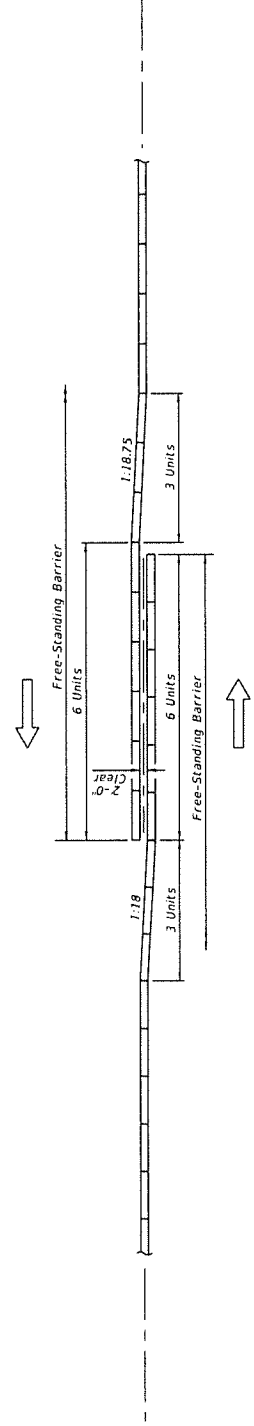
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APPROACH SHOULDER BARRIER TRANSITION ON UNDIVIDED FACILITIES

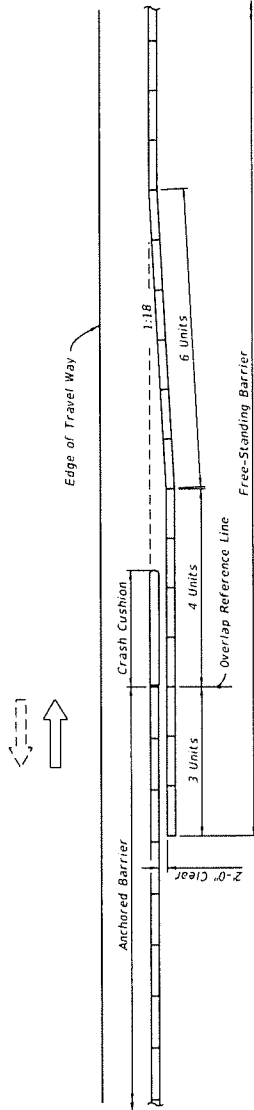


APPROACH SHOULDER BARRIER TRANSITION ON DIVIDED FACILITIES

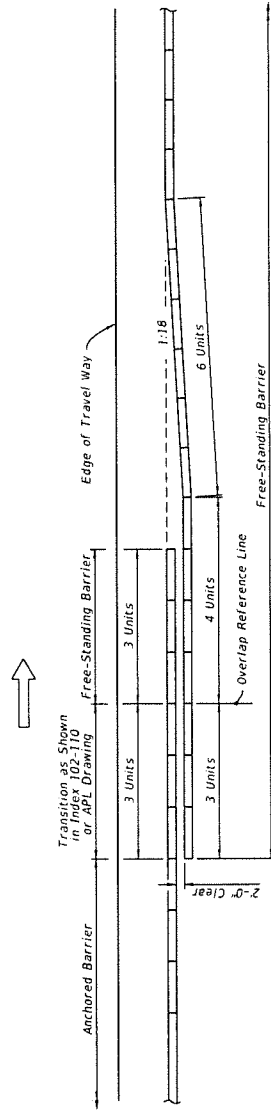


SYMBOL:
 Direction of Traffic

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DEPARTURE SHOULDER BARRIER TRANSITION ON UNDIVIDED FACILITIES



DEPARTURE (TRAILING) SHOULDER BARRIER TRANSITION ON DIVIDED FACILITIES

SYMBOL:

⇨ Direction of Traffic

LAST REVISION	DESCRIPTION:
11/01/20	

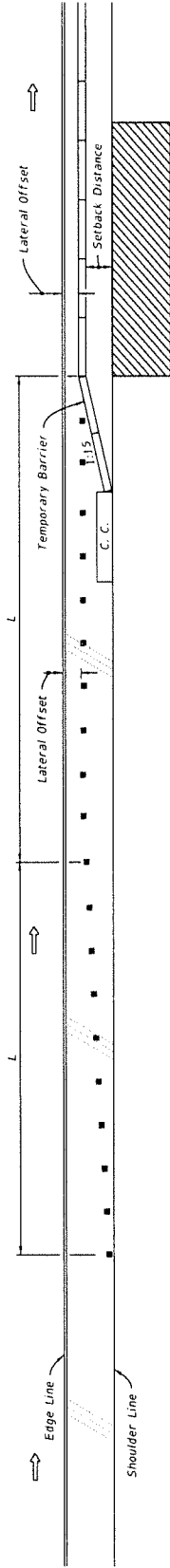


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TEMPORARY BARRIER

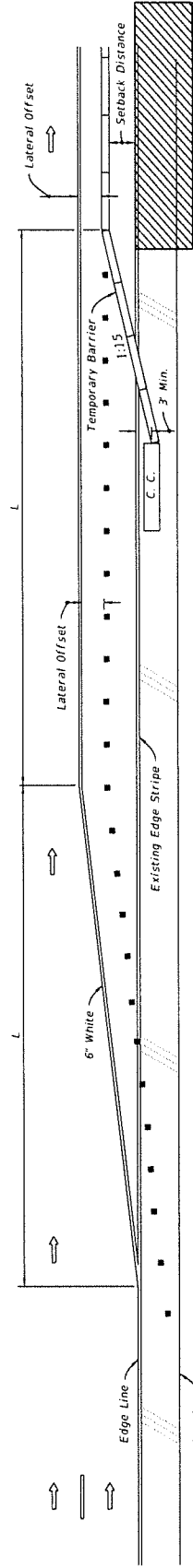
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BARRIER AND TRANSITION LOCATED ON SHOULDERS

(Right Lane Shown - Inverted For Left Lane)



BARRIER AND TRANSITION WITH LANE DROP ON MULTILANE FACILITIES

(Right Lane Merge Left Shown - Inverted For Left Lane Merge Right)

NOTE:
 L = Taper Length, See Index 102-600 for "L"
 and Channelizing device spacing values.

SYMBOLS:

- Work Area
- Channelizing Device (See Index 102-600)
- Crash Cushion
- Lane Identification and Direction of Traffic

DESCRIPTION:

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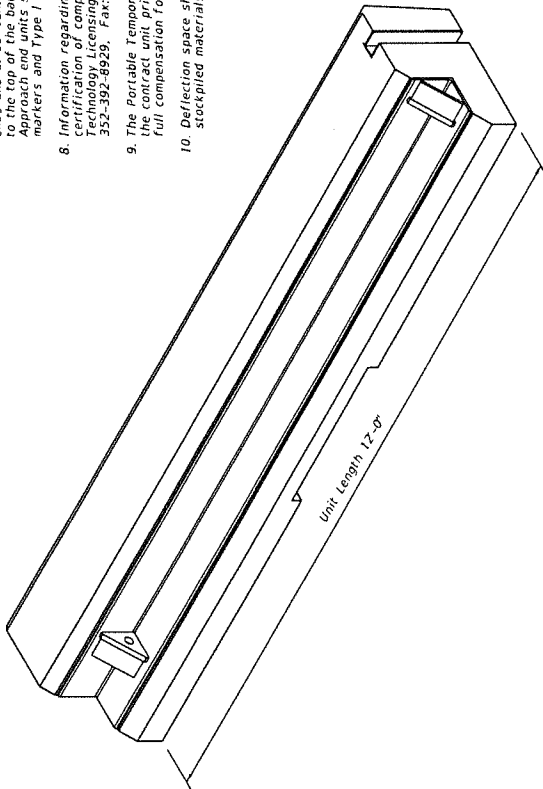
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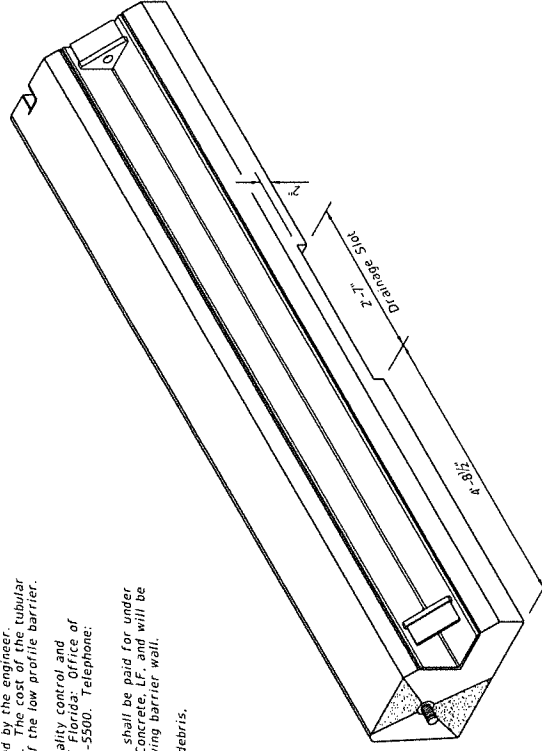
GENERAL NOTES

1. Pursuant to 35 United States Code, Chapter 18, also known as the Bayh Dole Act of 1980, the non-mountable curb was developed through Federal funding. The 'Portable Temporary Low Profile Barrier For Roadside Safety' is a licensed design by the University Of Florida. Any infringement on the rights of the designer shall be the sole responsibility of the user.
2. This Index is provided by the Florida Department Of Transportation solely for use by the Department of Transportation. The purpose for this Index is to indicate the approval of use of the barrier on the State Highway System; to provide sufficient pictorial for identifying the barrier unit; and, to provide general installation geometry for the barrier.
3. This legally mandated relationship is unique to Federally funded University patents that Department contractors use on Contracts. Pursuant to Federal law, the University may pursue intellectual property rights in the barrier design. The University of Florida will be allowed for installation on the State Highway System in Florida. Barrier wall units shall conform to Section 521 of the Standard Specification and shall be produced in Department-approved plants with quality control plans for pre-casting concrete barrier walls. Each barrier wall unit shall be permanently marked with an identification that is traceable to the manufacturer. The producing pre-cast concrete barrier wall units shall be marked with the manufacturer's name and address as certification that the unit has been manufactured in accordance with University of Florida drawings and specifications, and the approved quality control program.
4. The low profile barrier is to be installed only with hardware and accessories furnished by the licensed barrier producer and shall be used in accordance with the approved installation instructions. Low profile barrier wall units shall maintain firm contact with adjoining units. Nuts on tensioning rods shall be installed snug tight.
5. The low profile barrier is applicable for work zone speeds of 45 mph or less.
6. If the plans specify Low Profile Barrier then substitution with other barrier types is not permitted.
7. Tubular markers shall be orange in color and installed along the run of barrier at the ends and at 50' centers on tangents and 25' centers on radii. The markers shall be fixed to the top of the barrier by an adhesive or other method approved by the engineer. Approach and units shall be marked with a Type 1 object marker. The low profile barrier markers and Type 1 object marker shall be included in the cost of the low profile barrier.
8. Information regarding licensing, shop drawings, specifications, quality control and certification of compliance can be obtained from the University Of Florida: Office of Technology Licensing, P.O. Box 115500, Gainesville, Florida, 32611-5500. Telephone: 352-392-8929. Fax: 352-392-6600. Reference UF# 11052.
9. The Portable Temporary Low Profile Barrier For Roadside Safety shall be paid for under the contract unit price for Barrier Wall (Temporary) Low Profile Concrete, LF, and will be full compensation for furnishing, installing, maintaining and removing barrier wall.
10. Deflection space shall be kept clear of any grass, construction debris, stockpiled materials, equipment, and objects.



BACKSIDE AND END PICTORIAL VIEWS

PORTABLE TEMPORARY LOW PROFILE BARRIER FOR ROADSIDE SAFETY



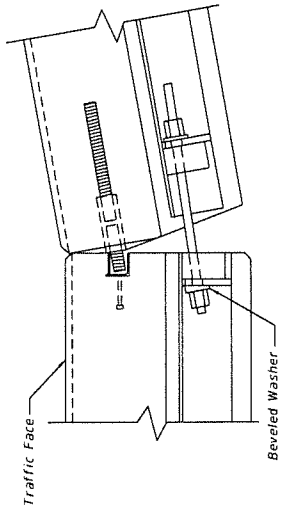
LAST REVISION	11/01/17
DESCRIPTION:	

FDOT
 FY 2019-20
 STANDARD PLANS

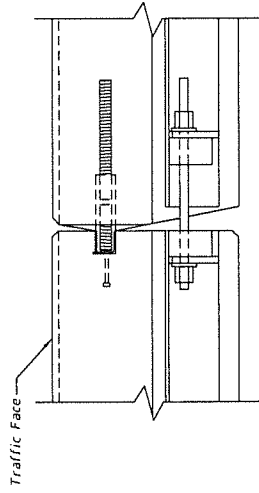
LOW PROFILE BARRIER

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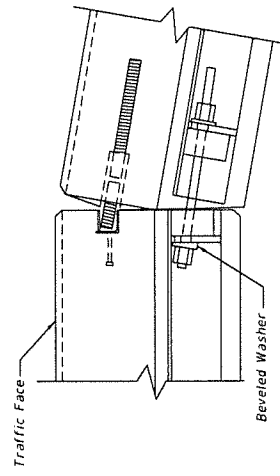
SHEET
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CONCAVE CONNECTION

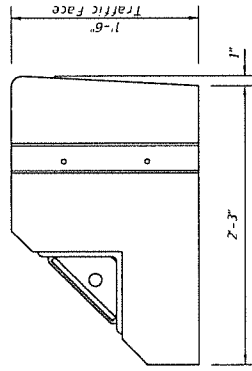


PARALLEL CONNECTION

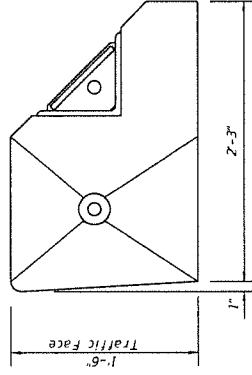


CONVEX CONNECTION

PLAN VIEWS OF CONNECTIONS



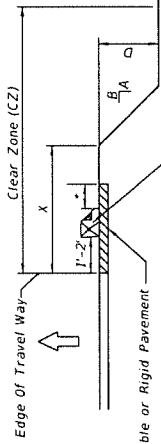
FLAT FACE FEMALE END



BEVELED FACE MALE END

END VIEWS

WORK ZONE SPEED	OFFSET TO TRAVELWAY	DEFLECTION SPACE
45 MPH OR LESS	1' MIN Z PREFERRED	9"

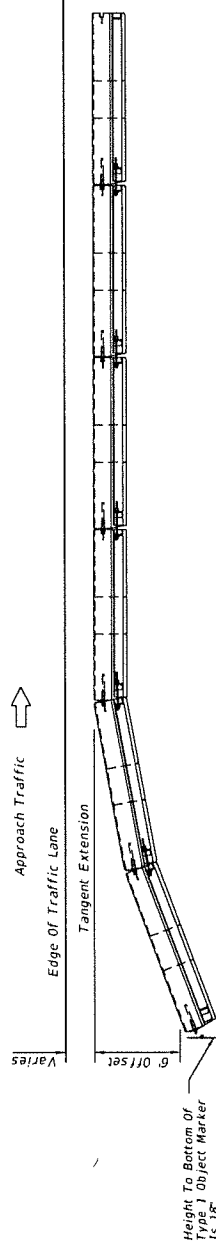


- Minimum σ on 1:10 or flatter slopes for Portable Temporary Low Profile Barrier For Roadside Safety. For values A, B, D and X see Index 102-600.

Notes:

LIMITATION OF USE: This installation technique can only be used on flexible or rigid pavement. ASPHALT PAD: Where existing pavement is not suitable for the installation of the barrier, miscellaneous asphalt pavement in accordance with Specification Section 339 with the exception that the use of a pre-emergent herbicide is not required. Payment for asphalt pad will be included in the cost of the barrier.

DEFLECTION SPACE AT DROP-OFFS



PLAN VIEW OF APPROACH END OFFSET

PORTABLE TEMPORARY LOW PROFILE BARRIER FOR ROADSIDE SAFETY

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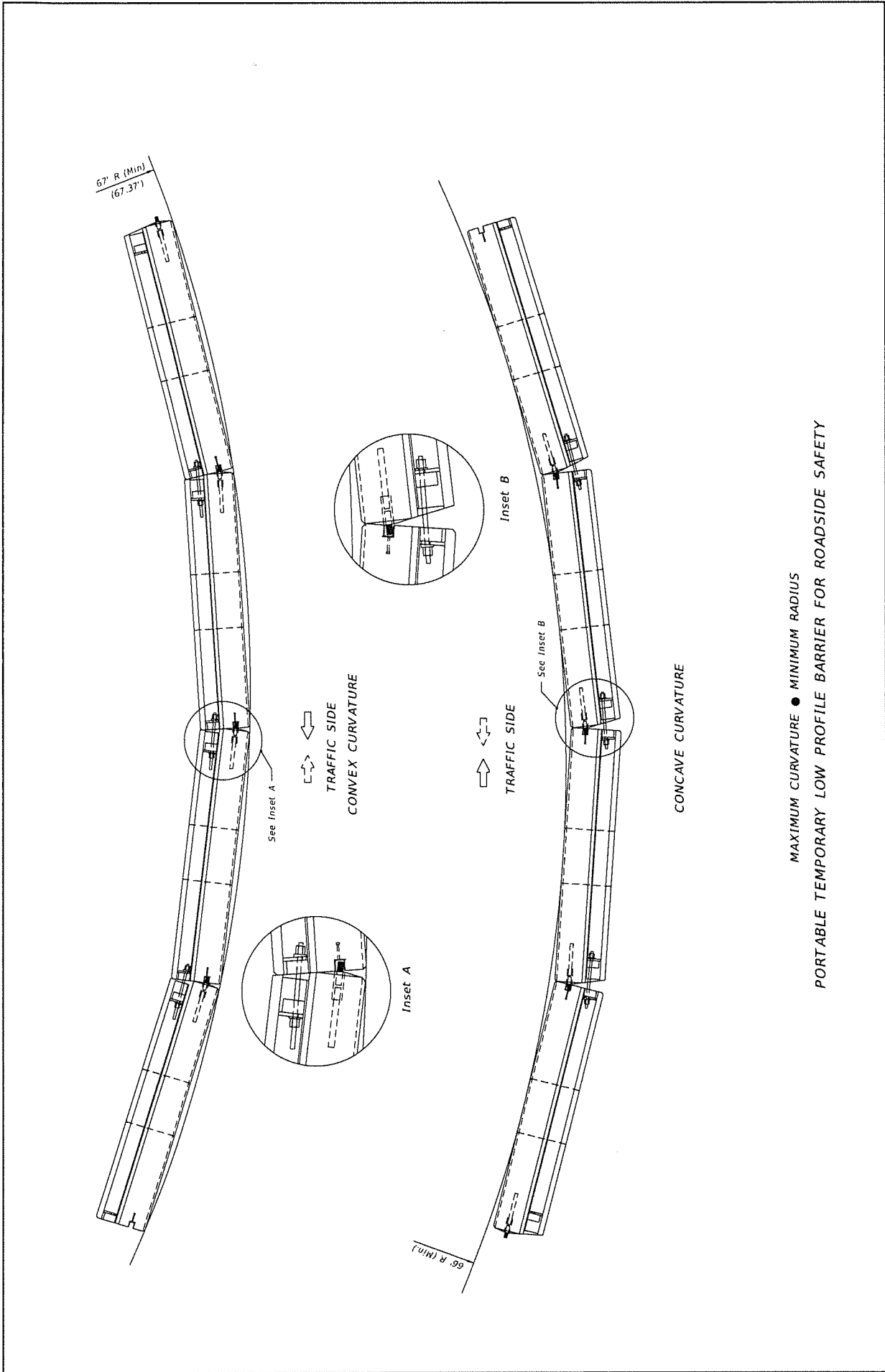
LOW PROFILE BARRIER

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102-120

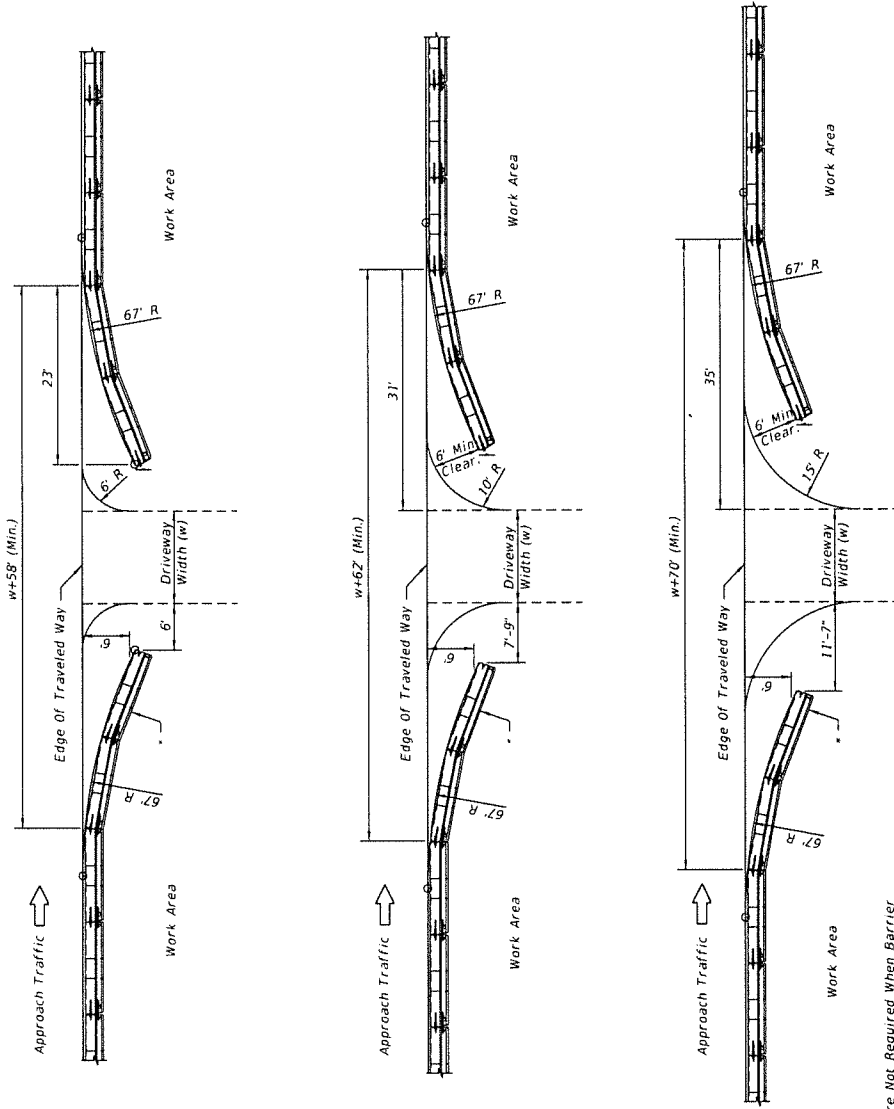
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DESCRIPTION:
REVISION

LAST REVISION
11/01/17



LAST REVISION 11/01/17	DESCRIPTION: REVISION	FDOT FY 2019-20 STANDARD PLANS	LOW PROFILE BARRIER	INDEX 102-120 SHEET 3 of 5
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* Trailing End Flares Are Not Required When Barrier Located Outside The Clear Zone Of Opposing Traffic
 Type I Object Marker To Be Installed When Trailing End Flare Falls Within The Clear Zone Of Opposing Traffic

LEGEND
 | Type I Object Marker

BARRIER OPENINGS AT DRIVEWAYS

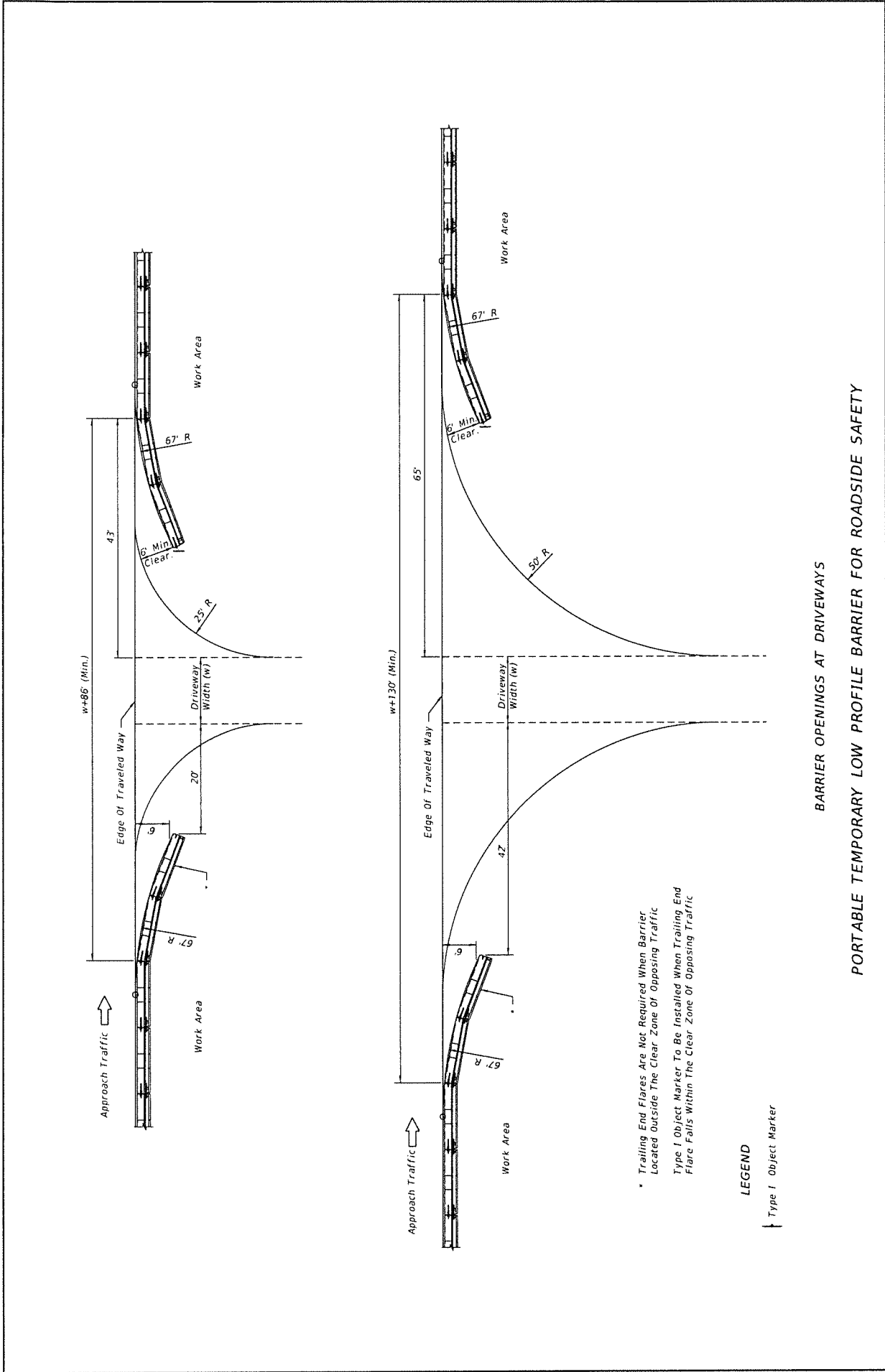
PORTABLE TEMPORARY LOW PROFILE BARRIER FOR ROADSIDE SAFETY

LAST REVISION	DESCRIPTION:
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LOW PROFILE BARRIER

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* Trailing End Flares Are Not Required When Barrier Located Outside The Clear Zone Of Opposing Traffic
 Type I Object Marker To Be Installed When Trailing End Flare Falls Within The Clear Zone Of Opposing Traffic

LEGEND

| Type I Object Marker

BARRIER OPENINGS AT DRIVEWAYS

PORTABLE TEMPORARY LOW PROFILE BARRIER FOR ROADSIDE SAFETY

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LOW PROFILE BARRIER