

2) BOX OUT 13.5 INCHES FROM PROPOSED FINISH GRADE, PROVIDE NEW LANDSCAPE AREAS. 3) BOX OUT NEW PAVEMENT AREA 13.5 INCHES ON EASTERLY SIDE NEAR GOLFVIEW ROAD 4) PUT DOWN 12 INCHES OF COMPACTED LIMEROCK OR SHELLROCK IN TWO 6 INCH LIFTS.
95% DENSITY, AASHTO T-180 LIMEROACK BEARING RATIO PAVE WITH 1.5 INCHES OF SP-9.5 HOT ASPHALT.

GEO-TECH TESTING REQUIRED IN ACCORDANCE WITH HIGHLANDS COUNTY TECHINICAL STANDARDS MANUAL, SECTION TWO - TRANSPORTATION SYSTEM DESIGN AND CONSTRUCTION STANDARDS - SECTION 02.207 INSPECTIONS, FIELD MEASUREMENTS AND LAB TEST.

GEO-TECH TESTING REQUIRED IN ACCORDANCE WITH HIGHLANDS COUNTY TECHINICAL STANDARDS MANUAL, SECTION TWO - TRANSPORTATION SYSTEM DESIGN AND CONSTRUCTION STANDARDS - SECTION 02.207 INSPECTIONS, FIELD MEASUREMENTS AND LAB TEST.

-SAW CUT AROUND PLANTING AREA REMOVE PLANTINGS AND SOIL, BACK FILL WITH TWO LIFTS OF 6" LIMEROCK OR SHELLROCK AND 1.5" OF SP-9.5 ASPHALT. see paving SPECIFICATIONS.

REMARK DR
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DATE OF PREPARATION: 10-AUGUST-2018 DEVELOPMENT:

OWNER:

ENGINEER:

SURVEYOR:

CITY OF SEBRING PROPOSED PARKING LOT RENOVATION SEC. 36, TWP. 34E, RGE. 28E 368 SOUTH COMMERCE AVE. EBRING, 33870 363-471-5100 POLSTON, P. DISTON ENGINEERING, INC. 25 KENILWORTH BOULEVARD EBRING. FL 33870 (863) 385-5564 (863) 385-2462 FAX

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SCOPE OF WORK ) REMOVE EXISTING CONCRETE PADS AND ASPHALT. (SEE EXEMPTION NOTE BELOW) ) BOX OUT 13.5 INCHES FROM PROPOSED FINISH GRADE. ) CONSTRUCT A NEW PARKING LOT AND OTHER IMPROVEMENTS. 4) REMOVE AND RELOCATE POWER POLES.

ERMAINE SURVEYING, INC 3803 KENILWORTH BLVD. SEBRING, FLORIDA 33870

(863) 385-6856 (863) 382-4531

APPLYING FOR PERMIT EXEMPTION. REMOVAL OF EXISTING PAVEMENT : 3,580 SQUARE FEET OF ASPHALT CREATING 3,580 SQUARE FEET OF NEW LANDSCAPE AREAS. ADDING 2,000 SQUARE FEET OF OF ADDITIONAL ASPHALT FOR GOLF CART PARKING, PAVING AN EXISTING LANDSCAPED AREA TO CREATE TRAFFIC FLOW. THE TWO BUILDINGS WILL BE REPLACING EXISTING BUILDINGS AND EXISTING ASPHALT. THIS YIELDS A NET REDUCTION OF 1,580 SQUARE FEET OF PAVEMENT. (3,580 - 2,000 = 1,580 SQ. FT. OF NEW GREEN AREA)

PARKING

EXISTING PARKING SPACES : 70 + 6 A.D.A. SPACES PROPOSED NEW PARKING SPACES: 75 TOTAL AFTER IMPROVEMENTS: 145 PARKING SPACES + 6 A.D.A. SPACES= 151 TOTAL NO ADDITIONAL A.D.A. SPACES NEEDED.

SPECIAL CONSTRUCTION NOTES: ---THE TOPOGRAPHY SHOWN FOR PROJECT IS A REPRESENTATION OF FIELD CONDITIONS. THE DESIGN OF THIS PROJECT IS BASED ON THE TOPOGRAPHY PROVIDED. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY INFORMATION IN FIELD. ANY DISCREPANCIES BETWEEN THE DESIGN TOPOGRAPHY AND FIELD CONDITIONS DURING CONSTRUCTION SHOULD BE BROUGHT TO THE ENGINEER OF RECORDS ATTENTION IMMEDIATELY SO THAT ALTERATIONS IN THE DESIGN CAN BE MADE TO KEEP THE FINAL SITE CONSTRUCTION IN COMPLIANCE WITH THE GOVERNING AGENCIES REGULATIONS ENCIES REGULATIONS.

--THESE PLANS HAVE BEEN DRAWN TO DEPICT THE REQUIRE CONSTRUCTION WITHIN THE PROJECT AREA. IN CERTAIN CASES THE SIZE AND/OR LOCATION OF PROPOSED CONSTRUCTION HAS BEEN BLOWN-UP TO SHOW ITEMS. THEREFORE, LOCATIONS ON THE DRAWINGS MAY NOT BE EXACT AND SHOULD NOT BE SCALED FOR CONSTRUCTION. ITEMS SUCH AS INLETS, CULVERTS, MITERED END SECTIONS, SIGNS, STRIPING, TURN LANES, SLOPES ETC. WILL NEED TO BE CONSTRUCTED USING EXISTING SITE CONDITIONS AND CURRENT HIGHLANDS COUNTY CONSTRUCTION REQUIREMENTS AND RECULATIONS REGULATIONS.

REGULATIONS. --THE INFORMATION AND DESIGN SHOWN ON THESE DRAWINGS IS BASED ON THE BEST AVAILABLE INFORMATION PROVIDED FOR DESIGN. THE DRAWING IS TO SCALE AS MUCH AS POSSIBLE; HOWEVER NO MEASUREMENTS SHOULD BE MADE BY SCALING FROM THESE DRAWINGS AS SOME ITEMS MAY BE NOT TO SCALE FOR DRAWING CLARITY. ANY QUESTIONS OR CONFLICTS SHOULD BE BROUGHT TO THE ENGINEER OF RECORDS ATTENTION IMMEDIATELY FOR CLARIFICATION OR RESOLUTION. POLSTON ENGINEERING INC. SHALL NOT BE RESPONSIBLE FOR ANY ERRORS MADE BY OTHERS CAUSED BY MAKING ASSUMPTIONS OR ERRORS CAUSED BY SCALING THE PLANS. ALL CONSTRUCTION SHALL FOLLOW THE ACCEPTED SAFETY PROCEDURES AND CONSTRUCTION TECHNIQUES AS REQUIRED BY ANY APPLICABLE GOVERNMENT STANDARDS.

## SITE PREPARATION

WE RECOMMEND NORMAL, GOOD-PRACTICE SITE PREPARATION PROCEDURES. THESE PROCEDURES INCLUDE CLEARING THE SITE OF ABOVE AND BELOW GRADE IMPROVEMENTS, PROOF-ROLLING AND PROOF-COMPACTING THE SUBGRADE, AND FILLING TO GRADE WITH ENGINEERED FILL AS NEEDED. A MORE DETAILED SYNOPSIS OF THIS WORK IS AS FOLLOWS: 1. IF REQUIRED, PERFORM REMEDIAL TEMPORARY DEWATERING PRIOR TO ANY EARTHWORK OPERATIONS TO REDUCE THE LIKELIHOOD OF PUMPING OF THE SHALLOW SUBGRADE SOILS DURING NORMAL CONSTRUCTION OPERATIONS. MAINTAIN GROUNDWATER LEVELS AT LEAST 24 INCHES BELOW THE LOWEST ANTICIPATED CUT AND/OR ALL COMPACTION SURFACES.

STRIP THE PROPOSED CONSTRUCTION LIMITS OF ALL EXISTING PAVEMENT SECTIONS (INCLUDING BASE MATERIAL, WHERE PRESENT), TREES, GRASS, ROOTS, TOPSOIL, CONSTRUCTION DEBRIS, AND OTHER DELETERIOUS MATERIALS WITHIN AND 5 FEET BEYOND THE PERIMETER OF THE PROPOSED BUILDING AREAS, PAVED AREAS, OR OTHER AREAS RECEIVING IMPROVEMENTS. EXPECT CLEARING AND GRUBBING TO DEPTHS OF 6 INCHES, ON AVERAGE.

3. PROOF-ROLL THE SUBGRADE WITH A HEAVILY LOADED, RUBBER-TIRED VEHICLE. PROOFROLLING WILL HELP LOCATE ANY ZONES OF ESPECIALLY LOOSE OR SOFT SOILS NOT ENCOUNTERED IN THE SOIL TEST BORINGS. THEN UNDERCUT, OR OTHERWISE TREAT THESE ZONES AS RECOMMENDED BY THE ENGINEER. 4. PRIOR TO ANY FILLING OF THE SITE, PROOF-COMPACT THE SUBGRADE FROM THE SURFACE USING SUITABLE COMPACTION EQUIPMENT, UNTIL YOU OBTAIN A MINIMUM DENSITY OF 95% MPMDD TO A DEPTH OF 1 FOOT BELOW STRIPPED GRADE. IN ORDER TO ACHIEVE THE REQUIRED DEGREE OF COMPACTION, THE SOILS MAY NEED TO BE MOISTURE CONDITIONED UNTIL THE IN-SITU WATER CONTENT IS WITHIN ±2% OF THE OPTIMUM MOISTURE CONTENT

5. TEST THE SUBGRADE FOR COMPACTION AT A FREQUENCY OF NOT LESS THAN ONE TEST PER 2,500 SQUARE FEET PER FOOT OF DEPTH IMPROVEMENT IN THE BUILDING AREAS OR AT A MINIMUM OF TWO TEST LOCATIONS, WHICHEVER IS GREATER.

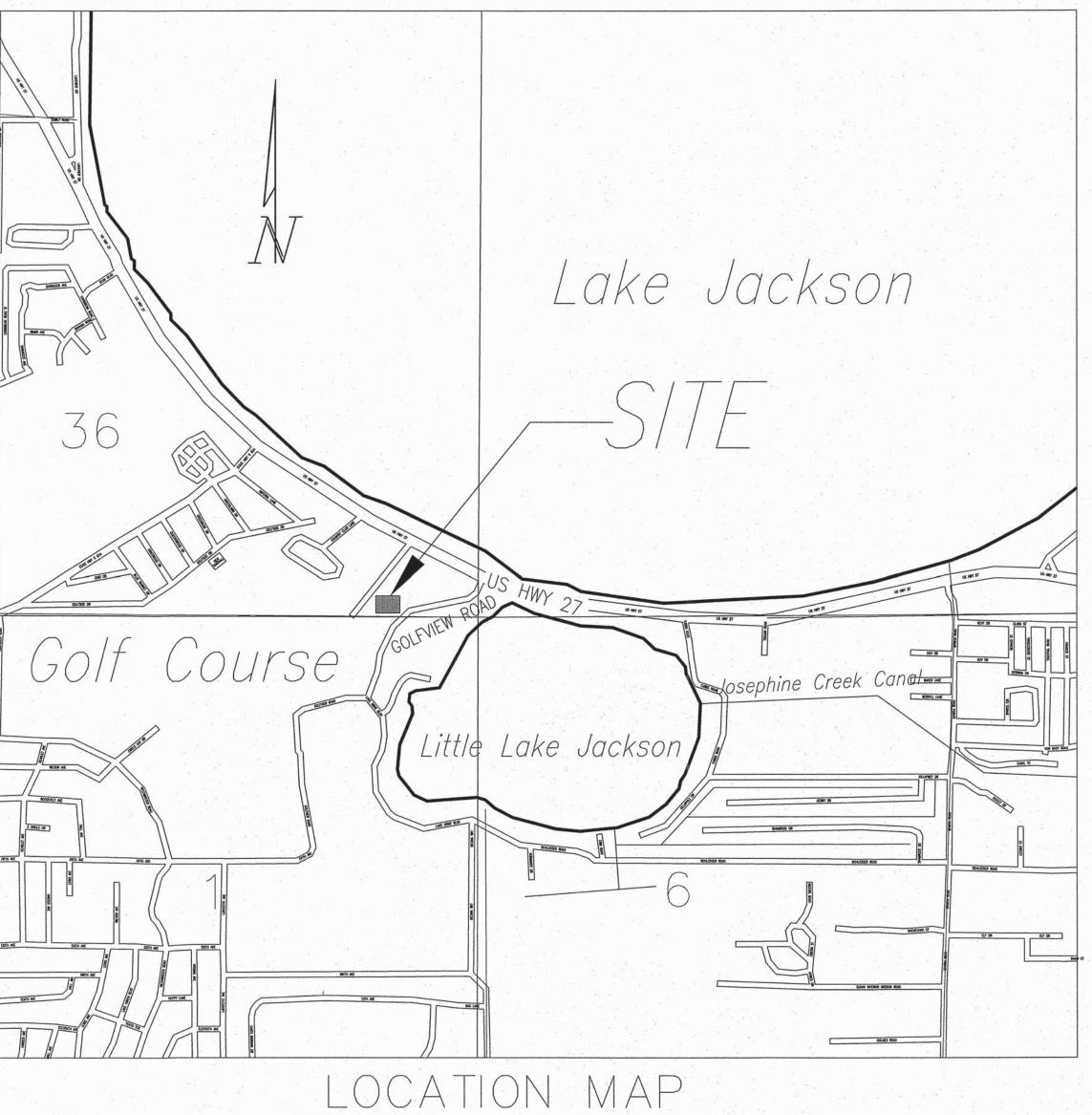
6. PLACE FILL MATERIAL, AS REQUIRED. THE FILL SHOULD CONSIST OF FINE TO MEDIUM SAND WITH LESS THAN 5 PERCENT SOIL FINES. YOU MAY USE FILL MATERIALS WITH SOIL FINES BETWEEN 5 AND 12 PERCENT, BUT STRICT MOISTURE CONTROL MAY BE REQUIRED. PLACE FILL IN UNIFORM 10 TO 12 INCH LOOSE LIFTS AND COMPACT EACH LIFT TO A MINIMUM DENSITY OF 95% MPMDD AT A MOISTURE CONTENT OF ±2% OF OPTIMUM (OMC). 7. PERFORM COMPLIANCE TESTS WITHIN THE FILL AT A FREQUENCY OF NOT LESS THAN ONE TEST PER 2,500 SQUARE FEET PER LIFT IN THE BUILDING AREAS, OR AT A MINIMUM OF TWO TEST LOCATIONS, WHICHEVER IS GREATER.

8. TEST ALL FINAL FOOTING CUTS FOR COMPACTION TO A DEPTH OF 1 FOOT. ADDITIONALLY, WE RECOMMEND TESTING EVERY COLUMN FOOTINGS, AND AT LEAST ONE TEST PER EVERY 50 LINEAL FEET OF WALL FOOTING. USING VIBRATORY COMPACTION EQUIPMENT AT THIS SITE MAY DISTURB NEARBY STRUCTURES. VIBRATORY COMPACTION WITHIN 50 FEET OF EXISTING STRUCTURES IS NOT RECOMMENDED. FURTHERMORE, WE RECOMMEND YOU MONITOR NEARBY STRUCTURES BEFORE AND DURING ANY COMPACTION OPERATIONS ONSITE. IF DISTURBANCE IS NOTED, HALT VIBRATORY COMPACTION AND INFORM THE ENGINEER OF RECORD IMMEDIATELY. WE WILL REVIEW THE COMPACTION PROCEDURES AND EVALUATE IF THE COMPACTIVE EFFORT RESULTS IN A SATISFACTORY SUBGRADE COMPLYING WITH OUR ORIGINAL DESIGN ASSUMPTIONS.



# PLANS OF PROPOSED **CITY OF SEBRING PARKING LOT RESTORATION** CADDY SHACK / SEBRING MUNICIPAL GOLF COURSE **HIGHLANDS COUNTY, FLORIDA**

THE EXISTING CONCRETE, PARKING LOT TO SERVICE ASPHALT AND DEBRIS TO CONSTRUCT THE CADDY SHACK RESTAURANT AND THE SEBRING MUNICIPAL GOLF COURSE. LOCATED IN SECTION 36, TOWNSHIP 34S, RANGE 28E

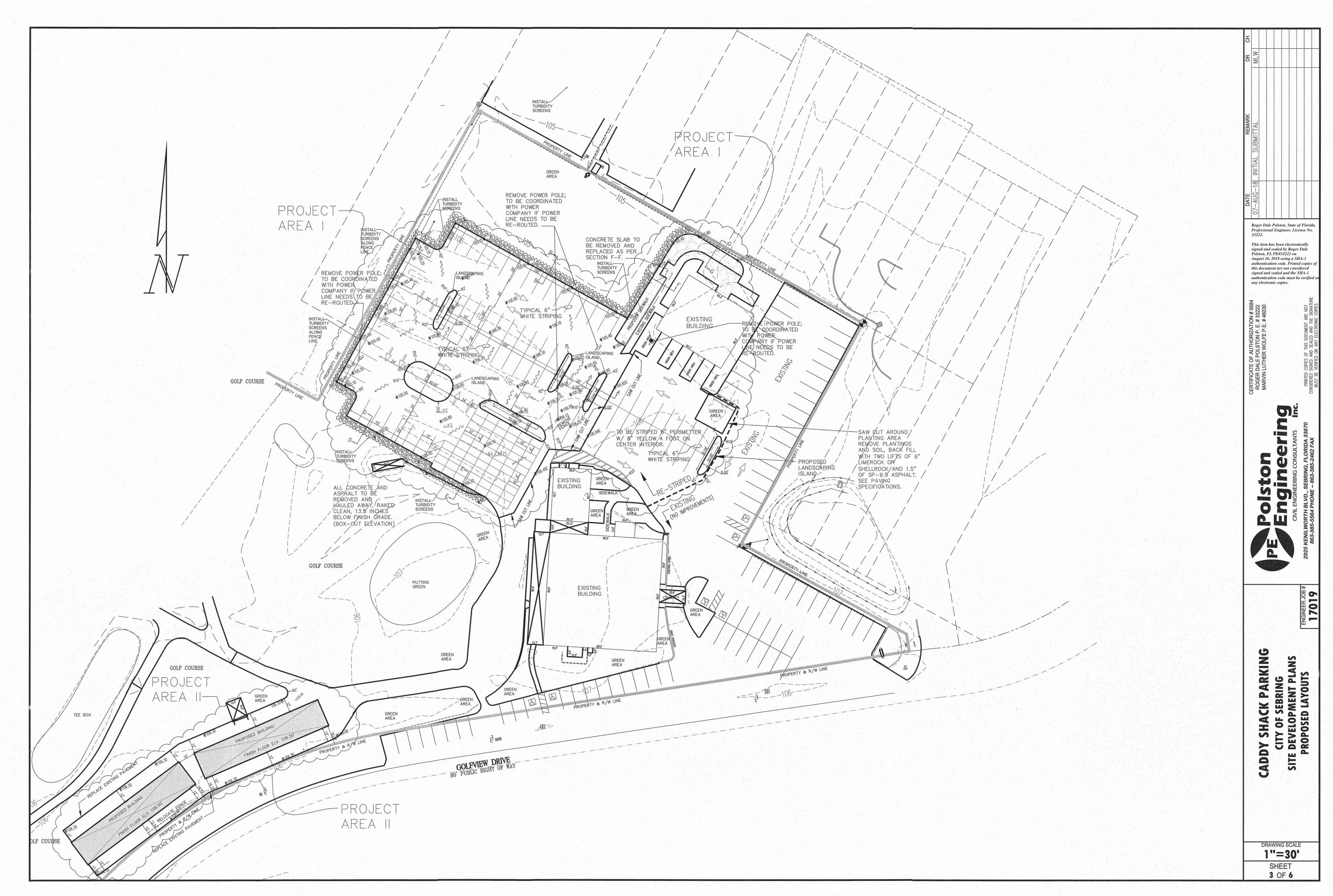


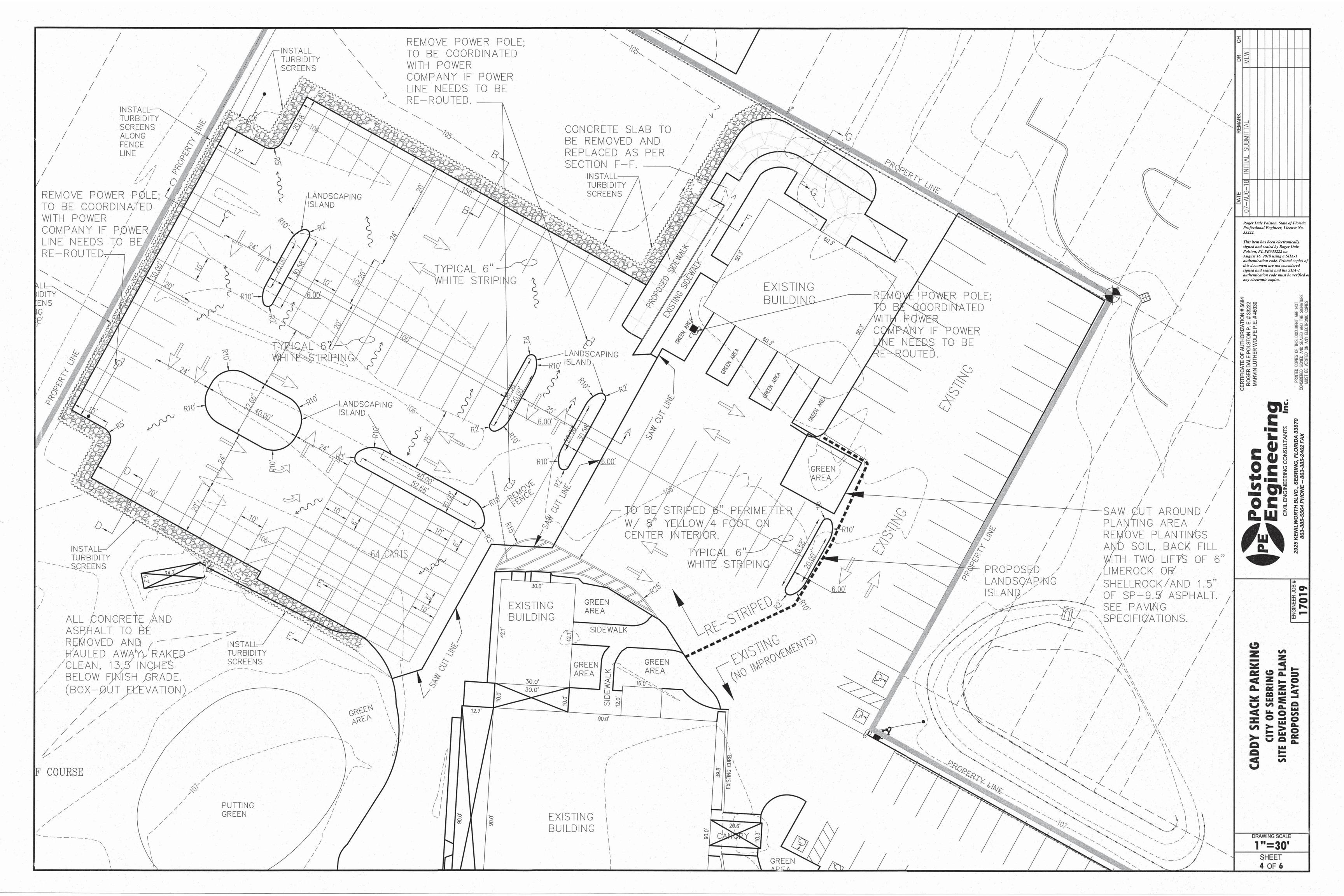
THE CITY OF SEBRING SCOTT NOETHLICH, CITY ADMINISTRATOR 368 SOUTH COMMERCE AVE. SEBRING, FLORIDA 33870 (863) 471-5100

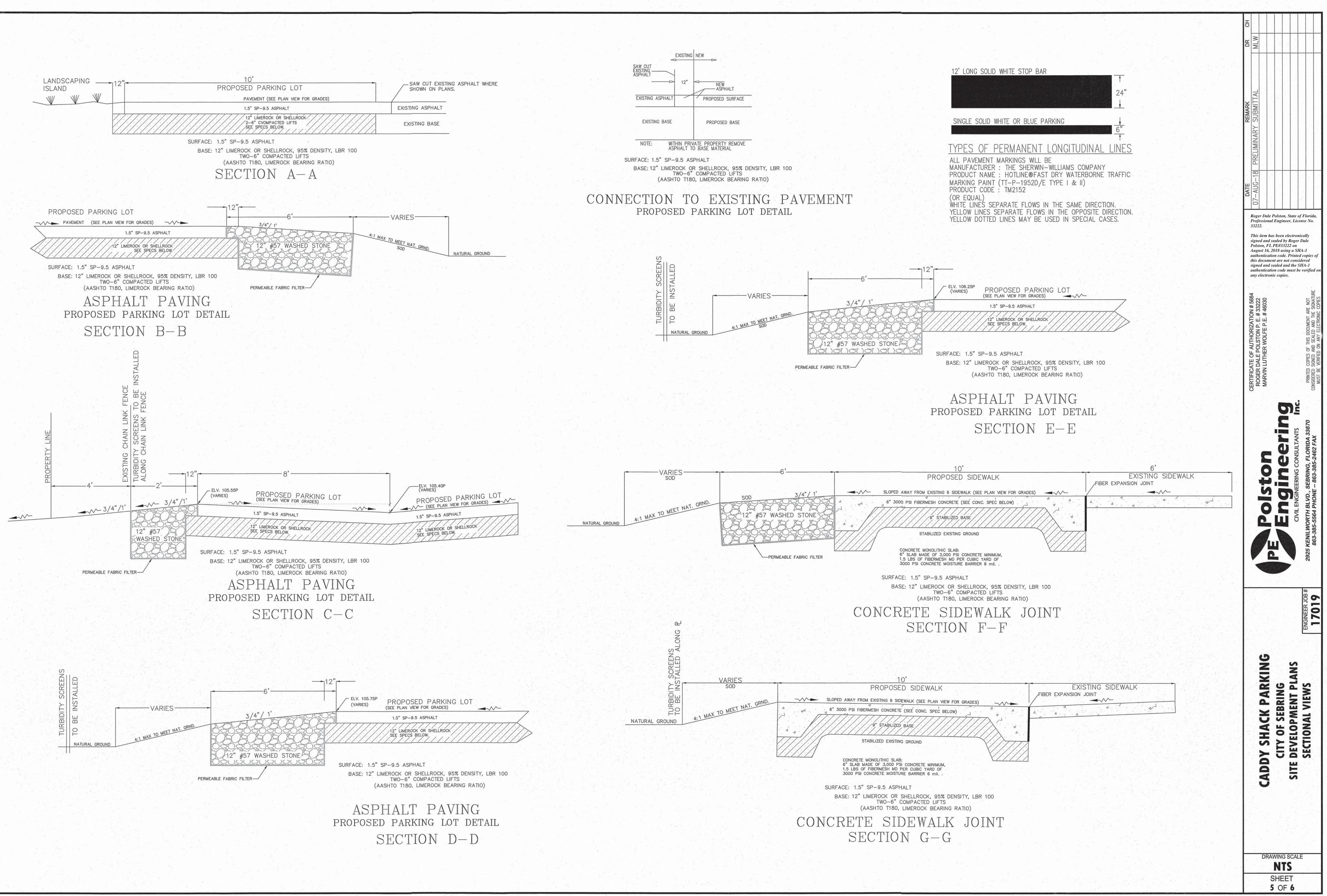
No. 33222 his item has been electronically signed a ealed by Roger Dale Polston, PE 33222 usir SHA-1 authentication code. Printed copie f this document are not considered signed ind sealed and the SHA-1 authentication ode must be verified on any electronic 018.08.16 14:44:17 -04'00' 30 22 56 ERTIFICATE OF AUTHORIZATION # ROGER DALE POLSTON P. E. # 332 MARVIN LUTHER WOLFE P.E. # 460 DĔ gine СШ **O** 701 CITY OF SEBRING SITE DEVELOPMENT PLANS COVER SHEET / GENERAL INFORMATION ARKING 0 SHACK CADDY CALL 48 HOURS BEFORE YOU DIG IN FLORIDA COUNTY NOTE: ALL CONSTRUCTION WITHIN THE COUNTY RIGHT-OF-WAY IS TO BE IN ACCORDANCE WITH THE CURRENT HIGHLANDS COUNTY STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION AND THE ROADWAY AND DRAWING SCALE NTS IT'S THE LAW THE F.D.O.T. TRAFFIC DESIGN STANDARD 811 INDEX. SHEET SUNSHINE STATE ONE CALL OF FLORIDA, INC 1 OF 6



8 Roger Dale Polston, State of Florida, Professional Engineer, License No. 33222. This item has been electronically signed and sealed by Roger Dale Polston, FL PE#33222 on August 16, 2018 using a SHA-1 authentication code. Printed copies of this document are not considered signed and sealed and the SHA-1 authentication code must be verified on any electronic copies. ERTIFICATE OF AUTHORIZATION # 568 ROGER DALE POLSTON P. E. # 33222 MARVIN LUTHER WOLFE P.E. # 46030 SEA AND SIGNED O **Polston** Engine BL TH BA ШC ENGINEER JOB # 1 7019 CADDY SHACK PARKING CITY OF SEBRING SITE DEVELOPMENT PLANS EXISTING CONDITIONS DRAWING SCALE SHEET 2 OF 6







02.200. GENERAL: THE SPECIFICATIONS AND DRAWINGS ARE AN INTEGRATED PART OF THE CONTRACT DOCUMENTS AND AS SUCH WILL NOT STAND ALONE F USED INDEPENDENTLY AS INDIVIDUAL PARTS, PARAGRAPHS, OR DRAWING SHEETS. THE DRAWINGS AND SPECIFICATIONS ESTABLISH MINIMUM STANDARDS OF QUALITY FOR A PROJECT. THEY DO NOT PURPORT TO COVER ALL DETAILS ENTERING INTO ITS DESIGN AND CONSTRUCTION OR OF ALL MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THE WORK. 02.201. MAINTENANCE OF TRAFFIC:

A. CONTROL DEVICES: THE CONTRACTOR SHALL BE REQUIRED TO KEEP THE ENTIRE WORK SITE IN FULL COMPLIANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION ROADWAY AND TRAFFIC DESIGN STANDARDS, CURRENT EDITION AND THE USDOT, FHWA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION B. DETOURS: DETOURS SHALL REQUIRE APPROVAL BY THE COUNTY ENGINEER. ANY DETOURS APPROVED AS A PART OF THE TRAFFIC CIRCULATION PLAN, SHALL

BE REQUIRED TO BE PROPERLY POSTED AND A MINIMUM OF 48-HOURS ADVANCE NOTICE SHALL BE GIVEN TO THE COUNTY ENGINEER'S OFFICE, LAW ENFORCEMENT AGENCIES, FIRE DEPARTMENT, SCHOOL BOARD AND EMERGENCY SERVICES. ADVANCE NOTICE SHALL ALSO BE PLACED AT THE LAST INTERSECTION BEFORE THE DETOUR. DRIVEWAYS KEPT OPEN: NO BUSINESS WILL HAVE VEHICULAR ACCESS TOTALLY BLOCKED AT ANY TIME, DRIVEWAY ACCESS TO PROPERTY WILL NOT B

BLOCKED FOR MORE THAN 8 HOURS ON ANY DAY PROPERTY OWNERS WILL BE NOTIFIED IN WRITTEN FORM BY THE CONTRACTOR 24 HOURS PRIOR TO THE BLOCKING OF ANY DRIVEWAY, BUSINESS, OR PROPERTY ACCESS. BLOCKING OF DRIVEWAYS WILL REQUIRE ADVANCE APPROVAL BY THE COUNTY ENGINEER. D. MAINTENANCE OF TRAFFIC VIOLATIONS: THE COUNTY WILL REPORT ANY KNOWN VIOLATION OF THE REQUIRED MAINTENANCE OF TRAFFIC TO THE OWNER, PROJECT ENGINEER, OR CONTRACTOR. THE CONTRACTOR WILL HAVE 4 HOURS OF REGULARLY SCHEDULED WORK TIME TO BRING THE SITE INTO FULL COMPLIANCE. 'HIS IS NOT DONE, THE COUNTY WILL HAVE THE OPTION TO TAKE ANY CORRECTIVE MEASURES IT FEELS NECESSARY AND TO BILL THE OWNER FOR THE COS' OF THESE MEASURES

E. TRAFFIC CONTROL PLAN APPROVAL: PRIOR TO THE COMMENCEMENT OF WORK AT THE JOB SITE, THE PROJECT ENGINEER SHALL RECEIVE APPROVAL OF HIS TRAFFIC CONTROL PLAN FROM THE COUNTY ENGINEER ACCESS FOR LOCAL TRAFFIC SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD OF THE 02.202. DRIVEWAY INGRESS AND EGRESS MAINTENANCE: THIS SPECIFICATION SHALL ONLY APPLY WHERE RESIDENCES, BUSINESSES, AND OTHER TYPES

OF PROPERTY FRONT ON THE ROAD TO BE CONSTRUCTED OR RECONSTRUCTED, AND DRIVEWAY CONNECTIONS ARE TO BE DISTURBED A. CONTRACTOR WILL BE REQUIRED TO PLACE COMMERCIAL BASE MATERIAL IN DRIVEWAYS AND/OR ACCESS POINTS AFFECTED BY THE PROJECT, WHERE CONSIDERED NECESSARY BY THE PROJECT ENGINEER TO PROVIDE SAFE, STABLE AND REASONABLE ACCESS TO RESIDENCES, BUSINESSES, AND PROPERTY. B. THE MATERIALS TO BE USED FOR DRIVEWAY MAINTENANCE SHALL BE LIMEROCK, STONE OR OYSTER SHELL. THE GRADE AND QUALITY OF THE MATERIAL SHALL BE THAT OFFERED FOR COMMERCIAL SUPPLY IN THE AREA. COMMERCIAL MATÉRIALS USED IN LOCATIONS WHICH HAVE INADEQUATE DRAINAGE OR ARE PRONE TO BE WET, SHALL BE OF A STABLE CHARACTER, UNAFFECTED BY WET CONDITIONS. C. THE MATERIAL SHALL BE PLACED IN THE DRIVEWAY AS DIRECTED BY THE PROJECT ENGINEER. THE MATERIAL SHALL BE LEVELED, MANIPULATED, COMPACTED AND MAINTAINED, TO THE EXTENT APPROPRIATE FOR THE INTENDED USE OF THE PARTICULAR DRIVEWAY. ). AS PERMANENT DRIVEWAY CONSTRUCTION IS ACCOMPLISHED AT A PARTICULAR LOCATION, PREVIOUSLY PLACED COMMERCIAL MATERIALS WHICH ARE SUITABLE

FOR REUSE MAY BE SALVAGED AND REUSED ON OTHER DRIVEWAYS AS DIRECTED. 2 20.3 CLEARING AND GRUBBING A. CLEARING AND GRUBBING SHALL CONSIST OF THE COMPLETE REMOVAL AND DISPOSAL OF ALL TIMBER, BRUSH, VEGETATION, STUMPS, ROOTS, BOULDERS, PAVEMENT, RUBBISH AND DEBRIS AND ALL OTHER MATERIALS AND OBSTRUCTIONS RESTING ON OR PROTRUDING THROUGH THE SURFACE OF THE EXISTING GROUND

AND THE SURFACE OF WORK AREAS. MATERIAL RESULTING FROM CLEARING AND GRUBBING SHALL BE DISPOSED OF BY THE CONTRACTOR IN A PROPER PLACE. B. AS AN EXCEPTION TO THE ABOVE PROVISIONS, WHERE SO DIRECTED BY THE PROJECT ENGINEER AND APPROVED BY THE COUNTY ENGINEER, DESIRABLE TREES WITHIN THE ROADWAY SHALL BE TRIMMED, PROTECTED AND LEFT STANDING. BRANCHES OF TREES EXTENDING OVER THE AREA OCCUPIED BY THE ROADWAY SHALL BE TRIMMED AS DIRECTED, TO GIVE A CLEAR HEIGHT OF 16 FEET ABOVE THE ROADWAY. C. WITHIN THE RIGHT-OF-WAY AND WITHIN ALL SWALES AND DITCHES, ALL STUMPS, ROOTS, ETC., PROTRUDING THROUGH OR APPEARING ON THE SURFACE OF THE COMPLETED EXCAVATION SHALL BE REMOVED OR CUT OFF BELOW THE FINISHED EXCAVATION SURFACE. WITHIN ALL OTHER AREAS WHERE CLEARING AND GRUBBING IS TO BE DONE, ROOTS AND OTHER DEBRIS, PROJECTING THROUGH OR APPEARING ON THE SURFACE OF THE ONE DONE, ROOTS AND OTHER DEBRIS, PROJECTING THROUGH OR APPEARING ON THE SURFACE OF THE ONE DONE, ROOTS AND OTHER DEBRIS, PROJECTING THROUGH OR APPEARING ON THE SUBGRADE. D. PLIEDING OF SUCH MATERIALS WILL ONLY PE ALLOWED WHEN A PROPER PLUEN DEDRUIT CAN BE ORTAINED AND ALL SUCH PLUENING SHALL BE REMOVED TO A DEPTH OF ONE FOOT BELOW THE SUBGRADE.

D. BURNING OF SUCH MATERIALS WILL ONLY BE ALLOWED WHEN A PROPER BURN PERMIT CAN BE OBTAINED AND ALL SUCH BURNING SHALL BE SUBJECT TO APPLICABLE LAWS, ORDINANCES AND REGULATIONS AND SHALL BE DONE AT LOCATIONS WHERE TREES AND SHRUBS ADJACENT TO THE CLEARED AREA WILL NOT BE HARMED. BURNING MAY BE REQUIRED TO CEASE IMMEDIATELY IF COMPLAINTS ARE RECEIVED BY THE PROJECT ENGINEER OR THE COUNTY ENGINEER. 02.204. EARTHWORK: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND ALL ACTIVITIES PERFORMED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF FRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. SEC. 02.205. RIGID DITCH CHECKS: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION.

SEC. 02.206. RIPRAP (SAND-CEMENT): ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF RANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. 02.207. INSPECTIONS, FIELD MEASUREMENTS AND LABORATORY A. GENERAL: THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN ADVANCE OF THE TIME AND DATE WHEN ANY TESTS CAN BE CONDUCTED, SO THA

THE PROJECT ENGINEER MAY SCHEDULE THE REQUIRED TESTING WITH THE INDEPENDENT TESTING LABORATORY. THE TEST SAMPLES SHALL BE TAKEN BY EITHER AN EMPLOYEE OF THE INDEPENDENT TESTING LABORATORY OR THE PROJECT ENGINEER OR HIS REPRESENTATIVE. IN NO CASE SHALL THE CONTRACTOR TAKE THE SAMPLES OR TRANSPORT THE SAMPLES TO THE LABORATORY. THE PROJECT ENGINEER SHALL INSPECT ALL CONSTRUCTION AND IS AUTHORIZED TO CALL TO THE ATTENTION OF THE CONTRACTOR ANY FAILURE OF WORK OR MATERIALS TO CONFORM WITH THE PLANS AND SPECIFICATIONS. THE FOLLOWING LABORATORY TESTS OR FIELD MEASUREMENTS AND FREQUENCY OF SUCH SHALL BE MADE IN ACCORDANCE WITH THE PROJECT ENGINEER'S DIRECTION BY THE INDEPENDENT TESTING LABORATORY, AT THE PROJECT EXPENSE, AND IN KEEPING WITH GOOD ENGINEERING PRACTICES. THE CONTRACTOR IS REQUIRED TO CONDUCT AND/OR STOP HIS WORK SO THAT THE APPROPRIATE TESTS, SAMPLES AND MEASUREMENTS CAN BE MADE INA SAFE AND PROPER MANNER. THE CONTRACTOR SHALL RECEIVE COPIES OF THE TEST REPORTS FROM THE PROJECT ENGINEER. THE INDEPENDENT TESTING LABORATORY SHALL MAIL OR HAND DELIVER COPIES OF ALL TESTS DIRECTLY TO THE OFFICE OF THE COUNTY ENGINEER

B. SUB-BASE AND SHOULDER: SUB-BASE AND SHOULDER TESTS SHALL BE MADE AS FOLLOWS: PROCTOR: ONE PER MILE UNLESS THE NATIVE SOILS ARE SIGNIFICANTLY DIFFERENT. IN THAT CASE, ONE PER EVERY MAJOR SOILS TYPE. SAMPLE SHALL BE TAKEN FROM THE ROADWAY AFTER SCARIFYING AND MIXING. WIDTH: EVERY 200 FEET AFTER SUB-BASE AND SHOULDERS HAVE BEEN MIXED, AND COMPACTED AND PRIOR TO ANY "BOXING OUT" OPERATION.

. DEPTH A. SUB-BASE: EVERY 200 FEET WITHIN THE AREA TO BE COVERED BY THE BASE MATERIAL AFTER FINAL GRADING AND COMPACTION; JUST PRIOR TO THE PLACEMENT OF THE BASE MATERIAL. TESTS WILL BE CONDUCTED IN A ZIG-ZAG PATTERN COVERING THE ENTIRE AREA DESCRIBED ABOVE. B. SHOULDER: EVERY 400 FEET, EACH SIDE, WITHIN THE SHOULDER AREAS PRIOR TO ANY "BOXING OUT" OPERATION, BUT AFTER MIXING AND COMPACTION.

1. FLORIDA BEARING VALUE (FBV): AT 200 FOOT INTERVALS TAKE THREE SAMPLES. SAMPLES SHALL BE TAKEN FROM ONE FOOT IN FROM EACH OUTSIDE EDGE OF THE SHOULDER AND ONE FROM WITHIN THE TRAFFIC LANE (AREAS). COMBINE THE TOP 1/2 OF THREE CONSECUTIVE SAMPLES INTO ONE COMPOSITE SAMPLE AND COMBINE THE BOTTOM 1/2 OF THE SAME THREE SAMPLES INTO ANOTHER SINGLE COMPOSITE SAMPLE MINIMUM ACCEPTABLE FBV IS 60 PSI. (NO TOLERANCE ACCEPTABLE.) 2. DENSITY:

A. SUB-BASE: EVERY 200 FEET, IN A ZIG-ZAG PATTERN, AND JUST PRIOR TO THE PLACEMENT OF THE BASE. EVERY OTHER TEST WILL BE MADE AT THE PROPOSED EDGE OF THE PAVEMENT. MINIMUM ACCEPTABLE VALUE: 95% DENSITY AS PER AASHTO T-180. . SHOULDER: EVERY 400 FEET, ONE TO TWO FEET IN FROM THE OUTSIDE EDGE OF THE SHOULDER, ON EACH SIDE OF THE ROAD. DENSITY SAMPLES SHALL BE TAKEN JUST PRIOR TO THE "BOXING OUT" OPERATION FOR THE BASE. MINIMUM ACCEPTABLE VALUE: 95% DENSITY AS PER AASHTO T-180. 3. FAILURES: ANY FAILURE REVEALED BY THE REQUIRED FIELD MEASUREMENTS AND LABORATORY TESTS REQUIRING ADDITIONAL MATERIAL SHALL REQUIRE THE CONTRACTOR TO SCARIFY THE EXISTING MATERIAL, PLACE THE ADDITIONAL MATERIAL AND THEN RE-SHAPE AND RE-COMPACT THE SUB-BASE FOR A MINIMUM DISTANCE OF 50 FEET EACH SIDE OF THE FAILURE. DEFICIENT DENSITY WILL REQUIRE ADDITIONAL COMPACTION A MINIMUM OF 50 FEET EACH SIDE OF THE FAILURE.

4. PLACEMENT OF THE BASE: THE BASE SHALL BE PLACED ON THE SUB-BASE ONLY AFTER COPIES OF THE RESULTS OF THE REQUIRED FIELD MEASUREMENTS AND LABORATORY TESTS FOR THE SUB-BASE HAVE BEEN RECEIVED AND APPROVED BY THE PROJECT ENGINEER. C. BASE (LIMEROCK OR SHELLROCK): TESTS FOR THE BASE SHALL BE MADE AS FOLLOWS: 1. MATERIAL: FOR MATERIAL WHOSE SOURCE IS AN FOOT APPROVED AND CERTIFIED MINING PIT, SUBMITTAL OF COPIES OF THE PIT CERTIFICATION

SHALL BE REQUIRED; FOR MATERIAL FROM ANY OTHER SOURCE, SUBMITTAL OF TEST RESULTS FROM AN APPROVED TESTING LABORATORY IN ACCORDANCE WITH A BASE MATERIAL TESTING PLAN, APPROVED IN ADVANCE BY THE COUNTY ENGINEER, SHALL BE REQUIRED. PROCTOR: ONE PER MILE UNLESS THE BASE MATERIAL CHANGES IN QUALITY; SAMPLE MUST BE TAKEN FROM AN ON-SITE STOCKPILE

WIDTH, DEPTH, CROWN: EVERY 200 FEET AS SHOWN ON PLANS. SEE TYPICAL SECTIONS IN SECTION SIX: ILLUSTRATIONS FOR MINIMUM REQUIREMENTS. . DENSITY: EVERY 200 FEET IN A ZIG-ZAG PATTERN WITHIN THE AREAS TO BE COVERED BY PAVEMENT. MINIMUM ACCEPTABLE VALUE: 95% DENSITY AS PER AASHTO T-180 5. BASE FAILURES: ANY FAILURES OF THE BASE REVEALED BY THE REQUIRED FIELD MEASUREMENT AND LABORATORY TESTS REQUIRING ADDITIONAL

BASE MATERIAL SHALL REQUIRE THE CONTRACTOR TO SCARIFY THE EXISTING BASE MATERIAL, PLACE THE ADDITIONAL MATERIAL AND THEN RE-SHAPE AND RE-COMPACT THE BASE FOR A MINIMUM DISTANCE OF 50 FEET EACH SIDE OF THE FAILURE. DEFICIENT DENSITY OF THE BASE WILL REQUIRE ADDITIONAL COMPACTION A MINIMUM OF 50 FEET EACH SIDE OF FAILURE.

## INLETS AND STRUCTURES NOTE:

FOR ADDITIONAL INFORMATION FOR STRUCTURE BOTTOMS, INLETS, CURB INLETS, AND MANHOLES REFER TO THE CURRENT FDOT DESIGN STANDARDS, INDEXES 200, 201, 210 & 232

### SPECIAL NOTES:

--ALL DISTURBED AREAS WITHIN THE CITY, COUNTY OR F.D.O.T. R/W WILL BE RESTORED AND SODDED. --ALL SAFETY PRECAUTIONS DURING CONSTRUCTION MUST MEET F.D.O.T. SPECIFICATIONS. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND IMPLEMENTING A TRAFFIC CONTROL PLAN DURING THE DURATION OF CONSTRUCTION WITHIN THE RIGHT OF WAYS. --ALL CONCRETE USED WILL BE 3000 PSI (MINIMUM).

#### CONSTRUCTION NOTES:

--THE TOPOGRAPHY SHOWN FOR PROJECT IS A REPRESENTATION OF FIELD CONDITIONS. THE DESIGN OF THIS PROJECT IS BASED ON THE TOPOGRAPHY PROVIDED. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY INFORMATION IN FIELD. ANY DISCREPANCIES BETWEEN THE DESIGN TOPOGRAPHY AND FIELD CONDITIONS DURING CONSTRUCTION SHOULD BE BROUGHT TO THE ENGINEER OF RECORDS ATTENTION IMMEDIATELY SO THAT ALTERATIONS IN THE DESIGN CAN BE MADE TO KEEP THE FINAL SITE CONSTRUCTION IN COMPLIANCE WITH THE GOVERNING AGENCIES REGULATIONS. --THE INTENT FOR PROPOSED DRIVEWAY CONSTRUCTION IS TO MATCH THE EXISTING SLOPE OF THE ROADWAY AND SHOULDERS (UNLESS SHOWN OTHERWISE). --THESE PLANS HAVE BEEN DRAWN TO DEPICT THE REQUIRED CONSTRUCTION WITHIN THE PROJECT AREA. IN CERTAIN CASES THE SIZE AND/OR LOCATION OF PROPOSED CONSTRUCTION HAS BEEN BLOWN-UP TO SHOW ITEMS. THEREFORE, LOCATIONS ON THE DRAWINGS MAY NOT BE EXACT AND SHOULD NOT BE SCALED FOR CONSTRUCTION. ITEMS SUCH AS INLETS, CULVERTS, MITERED END SECTIONS, SIGNS, STRIPING, TURN LANES, SLOPES ETC. WILL NEED TO BE CONSTRUCTED USING EXISTING SITE CONDITIONS AND CURRENT F.D.O.T. CONSTRUCTION REQUIREMENTS AND REGULATIONS.

#### PAVEMENT AND CURB REMOVAL

--SAW CUT AND REMOVE THE PORTIONS OF THE ASPHALT AND CURB WHICH ARE TO BE REMOVED. DEBRIS SHALL BE HAULED TO A SITE APPROVED FOR RECEIVING CONSTRUCTION DEBRIS.

THE PROJECT ENGINEER.

AND RESET BY A LAND SURVEYOR QUALIFIED UNDER THE LAWS OF THE STATE OF FLORIDA.

#### D. SAMPLE OF TAG FOR WRITTEN NOTIFICATION OF OWNERS; AND E. TRAFFIC PLAN (A DRAWING TO SCALE OF EACH PHASE SHOWING ALL BARRICADES, SIGNS AND FLAGMEN IS REQUIRED)

COMPOUND, AND JOINT MATERIAL

A. INFORMATION ON THE PIPE AND CULVERTS, INDICATING THE TYPE, CLASS, SIZE, AND OTHER RELEVANT INFORMATION: 3. ASPHALT MIX DESIGN; . DOCUMENTATION ON ALL OTHER MATERIALS USED INCLUDING, BUT NOT LIMITED TO, FILTER FABRIC, GUARDRAILS, CONCRETE, STEEL POSTS, CURING

SUPPORT POLE IS WOOD AND LESS THAN 4 INCHES IN DIAMETER OR IS A BREAKAWAY TYPE POLE. IF THE ORIGINAL SUPPORT POLE IS DAMAGED OR UNSATISFACTORY, THE CONTRACTOR WILL NOT BE REQUIRED TO REPLACE OR RELOCATE THE MAILBOX. EC. 02.223. SHOP DRAWING SUBMITTALS: THE FOLLOWING INFORMATION AND/OR DRAWINGS SHALL BE SUBMITTED TO THE PROJECT ENGINEER PRIOR TO **BEGINNING WORK:** 

02.222. OBSTRUCTIONS IN RIGHT-OF-WAY: A. HEADWALLS, FENCES, MAIL BOXES, STATUES, WALKWAYS, AND OTHER OBSTRUCTIONS PLACED IN THE RIGHT-OF-WAY WILL BE REMOVED FROM THE WORK AREA, AS REQUIRED TO KEEP THE WORK PROGRESSING, BY THE CONTRACTOR. IN THE EVENT THAT THE OWNER CANNOT BE IDENTIFIED OR IS UNABLE OR UNWILLING TO REMOVE SAID OBSTRUCTIONS THEMSELVES, THE OBSTRUCTION WILL BE POSTED WITH A NOTICE, IN WRITING, BY THE CONTRACTOR 24 HOURS PRIOR TO ANY REMOVAL, THAT SAID OBJECT WILL BE REMOVED. THE CONTRACTOR WILL NOT BE RESPONSIBLE FOR ANY DAMAGE TO SAID OBJECTS THAT OCCURS WHILE BEING MOVED BY THE CONTRACTOR, AFTER PROPER NOTIFICATION AND THE APPROVAL OF THE PROJECT ENGINEER. ANY OBJECTS THAT ARE REMOVED FROM WITHIN THE RIGHT-OF-WAY, EXCEPT MAILBOXES, WILL BE HAULED AWAY AND DISPOSED OF BY THE CONTRACTOR IN A PROPER LANDFILL. B. MAILBOXES MOVED DURING CONSTRUCTION, WILL BE REPLACED AT A LOCATION 3 FEET FROM THE EDGE OF PAVEMENT BY THE CONTRACTOR, PROVIDING THE

INFORMATION AND MEASUREMENTS USED IN ESTABLISHING LOCATIONS AND GRADES SHALL BE KEPT IN NOTEBOOKS AND COPIES FURNISHED TO PROJECT ENGINEER AND THE COUNTY ENGINEER. AS A MINIMUM THE FOLLOWING ITEMS WILL BE STAKED RIGHT-OF-WAY, STAKED AT EACH STATION OR WHEREVER THE RIGHT-OF-WAY CHANGES WIDTH OR DIRECTION, AT ANY OFFSET DESIREI CUT OR FILL TO CENTERLINE GRADE AND SWALE GRADE AT EACH STATION OR WHEREVER CHANGES OCCUR AT POINTS OF VERTICAL INTERSECTION: AND SET PROPOSED CENTERLINE ELEVATION OF ALL INTERSECTING ROADS ONE TIME AND NOTE THOSE ELEVATIONS IN FIELDBOOK; COMPARE THOSE ELEVATIONS AND BRING ANY DISCREPANCIES TO THE ATTENTION OF THE PROJECT ENGINEER. ALL THE ABOVE MENTIONED SURVEY WORK WILL BE DONE BY A SURVEYOR LICENSED TO PRACTICE IN THE STATE OF FLORIDA

. CONTRACTOR SHALL LAY OUT THE WORK AT THE LOCATION AND TO THE LINES AND GRADES SHOWN ON THE DRAWINGS. SURVEY NOTES INDICATING THE

A. FIELD ENGINEERING AND SURVEYING SERVICES SHALL INCLUDE SURVEY WORK TO ESTABLISH RIGHT-OF-WAY LINES AND LEVELS AND TO LOCATE AND LAY OUT SITE IMPROVEMENTS, STRUCTURES, AND CONTROLLING LINES AND LEVELS REQUIRED FOR THE CONSTRUCTION OF THE WORK. ALSO INCLUDED ARE SUCH ENGINEERING SERVICES AS ARE SPECIFIED OR REQUIRED TO EXECUTE CONTRACTOR'S CONSTRUCTION METHODS. ENGINEERS AND SURVEYORS SHALL BE LICENSED PROFESSIONALS IN THE STATE OF FLORIDA. B. EXISTING BASIC HORIZONTAL AND VERTICAL CONTROL POINTS FOR THE PROJECT ARE THOSE DESIGNATED ON THE DRAWINGS. CONTRACTOR SHALL LOCATE AND PROTECT CONTROL POINTS PRIOR TO STARTING SITE WORK AND SHALL PRESERVE ALL PERMANENT REFERENCE POINTS DURING CONSTRUCTION. IN WORKING NEAR ANY PERMANENT PROPERTY CORNERS OR REFERENCE MARKERS, CONTRACTOR SHALL USE CARE NOT TO REMOVE OR DISTURB ANY SUCH MARKERS. IN THE EVENT THAT MARKERS MUST BE REMOVED OR ARE DISTURBED DUE TO PROXIMITY OF THE CONSTRUCTION WORK, CONTRACTOR SHALL HAVE THEM REFERENCED

02.218. FENCING: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. 02.219. GUARDRAIL: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. SEC. 02.220. CONCRETE: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. EC. 02.221. FIELD ENGINEERING, SURVEYING AND RIGHT-OF-WAY STAKING:

SOD IS TO BE PLACED SHALL CONTAIN SUFFICIENT MOISTURE FOR OPTIMUM RESULTS AFTER BEING PLACED. THE SOD SHALL BE WATERED AND KEPT IN A MOIST CONDITION FOR NO LESS THAN TWO WEEKS (MINIMUM) OR UNTIL THE ENTIRE PROJECT IS ACCEPTED BY THE PROJECT ENGINEER AND THE COUNTY ENGINEER THE MOISTENED CONDITION SHALL EXTEND AT LEAST TO THE FULL DEPTH OF THE ROOTING ZONE. WATER SHALL NOT BE APPLIED, HOWEVER, WHEN THERE IS DANGER OF A FREEZING CONDITION. 02.216. STAKED SILT FENCES: ALL MATERIALS AND INSTALLATION PROCEDURES SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. SEC. 02.217. TEMPORARY PREVENTION, CONTROL, AND ABATEMENT OF EROSION AND WATER POLLUTION: ALL MATERIALS AND INSTALLATIONS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION.

SEC. 02.214. CULVERTS/STORM SEWERS: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. 02.215. GRASSING: ALL MATERIALS AND INSTALLATION PROCEDURES SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. THE AREAS ON WHICH THE

2. 02.211. ASPHALT: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, TYPE S ASPHALT CONCRETE SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF, TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2000 EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION SEC. 02.212. PAVEMENT MARKING: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION. SEC. 02.213. SIGNS: ALL MATERIALS AND INSTALLATION METHODS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, USDOT, FHWA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION.

TO 98% OF MAXIMUM DENSITY AS PER AASHTO T-180; AND B. IN OTHER AREAS NOT UNDER PROPOSED OR EXISTING PAVING OR IN AREAS NOT SUBJECT TO VEHICULAR TRAFFIC, THE BACKFILL SHALL BE COMPACTED TO 90% DENSITY AS PER AASHTO T-180. SEC. 02.208. TOLERANCES: THE REQUIRED THICKNESSES AND WIDTHS SHALL BE THE ABSOLUTE MINIMUM ALLOWABLE. NO ALLOWANCE WILL BE MADE FOR FAILURE IN A WIDTH OR DEPTH DIMENSION. FLORIDA BEARING VALUE AND DENSITY REQUIREMENTS SHOWN ON THE PLANS AND SPECIFICATIONS ARE THE ABSOLUTE MINIMUM ALLOWABLE AND NO VALUES LESS THAN THOSE SPECIFIED WILL BE ACCEPTED. GRADES ON ROADWAY CENTERLINE AND DITCH INVERTS SHALL BE PLUS OR MINUS 0.05 OF A FOOT FROM PLAN DIMENSIONS. SEC. 02.209. STABILIZED SUBBASE AND SHOULDERS: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION.

SEC. 02.210. LIMEROCK OR SHELLROCK BASE: ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, AND CONSTRUCTED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF

TRANSPORTATION DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, CURRENT EDITION.

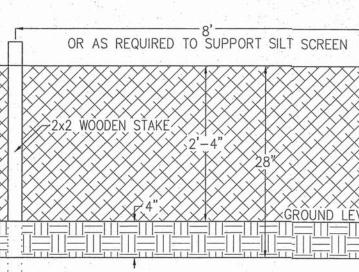
OF THE SHOULDER, HE MAY REQUIRE THAT STABILIZING MATERIAL BE ADDED AND MIXED TO PRODUCE A FBV OF 60 PSI, AND BE COMPACTED TO A MINIMUM DENSITY OF 95% AS PER AASHTO T-180. TRENCHES FOR UNDERGROUND PIPES OR STRUCTURES: THE FOLLOWING TESTS SHALL BE MADE: 1. PROCTOR: ONE PER MILE OF TRENCH UNLESS THE MATERIAL CHANGES; IF MATERIAL CHANGES ONE PER EACH DIFFERENT SOIL OR MATERIAL ALLOWED FOR BACKFILL 2. DENSITY: EVERY 200 FEET OUTSIDE AREAS OF VEHICULAR TRAFFIC AND EVERY 10 FEET WHERE THE TRENCH CROSSES AN AREA OF VEHICULAR TRAVEL, INCLUDING DRIVEWAYS. FREQUENCY OF TESTING WILL BE THE SAME FOR EACH LIFT. EACH COMPACTED ONE FOOT DEPTH OF BACKFILL IS A LIFT. TESTING OF DENSITY WILL START WHEN BACKFILL IS 12 INCHES OVER THE TOP OF THE PIPE. TESTS WILL BE DONE WITHIN THE WIDTH OF THE TRENCH AS INDICATED BY

DENSITY REQUIREMENTS: THE DENSITY REQUIREMENTS ARE THAT:

6. BASE PRIMING: ONCE THE BASE IS APPROVED BY THE TESTING LABORATORY, THE CONTRACTOR SHALL AS SOON AS POSSIBLE PRIME AND SAND-SEAL THE BASE. APPROVAL MUST BE OBTAINED FROM THE PROJECT ENGINEER. THIS APPROVAL CAN BE GRANTED ONLY AFTER THE PROJECT ENGINEER RECEIVES THE RESULTS OF THE FIELD MEASUREMENTS AND LABORATORY TESTS DIRECTLY FROM THE INDEPENDENT TESTING LABORATORY. D. SHOULDER: THE FINISHED SHOULDER AREA IS INTENDED TO BE COMPACTED TO A SMOOTH, FIRM CONDITION THAT CAN ACCOMMODATE VEHICLES WITHOUT RUTS BEING CREATED. IF IN THE OPINION OF THE PROJECT ENGINEER, EXCESS UNSUITABLE MATERIAL HAS BEEN INCORPORATED INTO THE TOP 6 INCHES

A. IN AREAS OF PROPOSED OR EXISTING PAVEMENT OR VEHICULAR TRAFFIC ALL BACKFILL, SUB-BASE, AND BASE MATERIAL SHALL BE COMPACTED

FRONT VIEW SILT FENCES TRANSPORTATION ROADWAY AND TRAFFIC DESIGN STANDARDS.



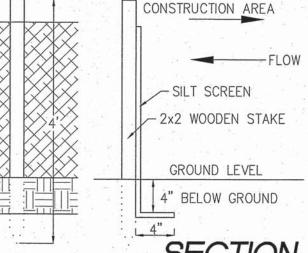
## SILT/TURBIDITY SCREENS

THE CONTRACTOR SHALL, AT HIS EXPENSE, PROVIDE ROUTINE MAINTENANCE OF PERMANENT AND TEMPORARY EROSION CONTROL FEATURES UNTIL THE PROJECT IS COMPLETED AND ACCEPTED. IF SUCH EROSION CONTROL FEATURES MUST BE RECONSTRUCTED DUE TO CONTRACTOR'S NEGLIGENCE OR CARELESSNESS OR, IN THE CASE OF TEMPORARY EROSION CONTROL FEATURES, FAILURE BY CONTRACTOR TO INSTALL PERMANENT EROSION CONTROL FEATURES AS SCHEDULED, SUCH REPLACEMENT SHALL BE AT CONTRACTOR'S EXPENSE. SILT FENCES MUST BE INSTALLED PRIOR TO ANY CONSTRUCTION AND MUST BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT.

STANDARDS, INDEX 102, WITH FILTER FABRIC CONFORMING TO SECTION 985, FDOT STANDARD SPECIFICATIONS. IN ALL CASES THE FILTER FABRIC WILL BE SECURELY ANCHORED TO THE GROUND OR BURIED IN THE GROUND SO THAT IT WILL NOT BE PUSHED UP BY THE EXPECTED RUNOFF. THE ATTACHMENT TO EXISTING TREES WILL NOT BE PERMITTED.

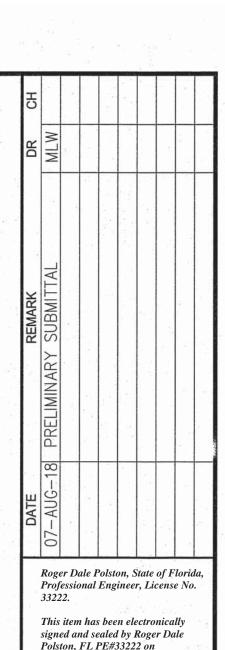
MANUFACTURER'S DIRECTIONS, THESE SPECIFICATIONS, THE DETAILS AS SHOWN ON THE DRAWINGS AND THE FLORIDA DEPARTMENT OF SILT FENCES WILL BE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) TYPE III AS DESCRIBED IN FDOT ROADWAY AND TRAFFIC DESIGN

SECTION THIS WORK SHALL CONSIST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING TEMPORARY SILT FENCES, IN ACCORDANCE WITH THE



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