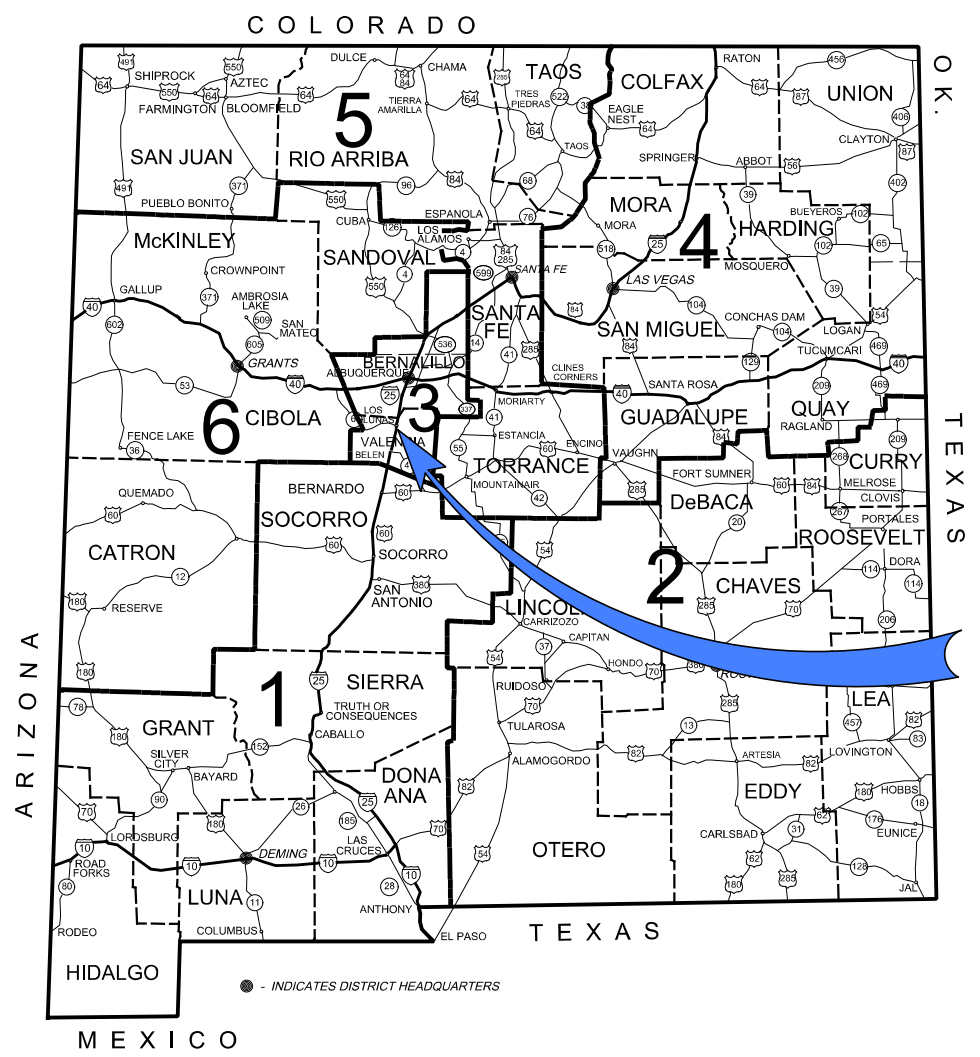
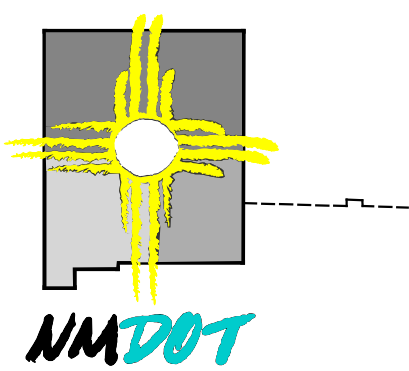
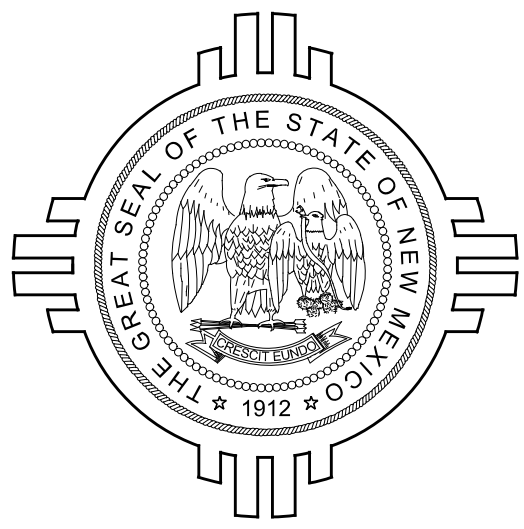
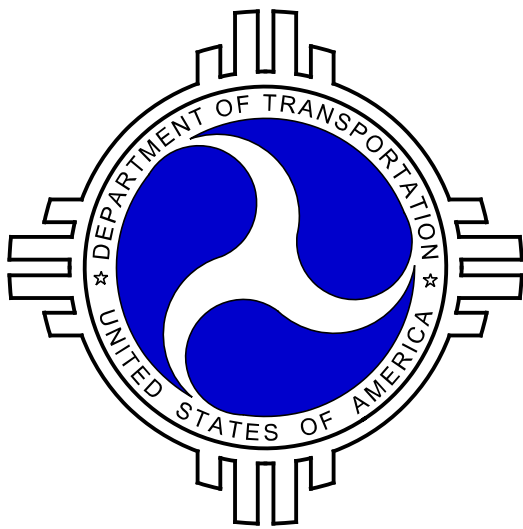


VALENCIA COUNTY, NEW MEXICO CONSTRUCTION PLANS



VALENCIA COUNTY CN: A301430



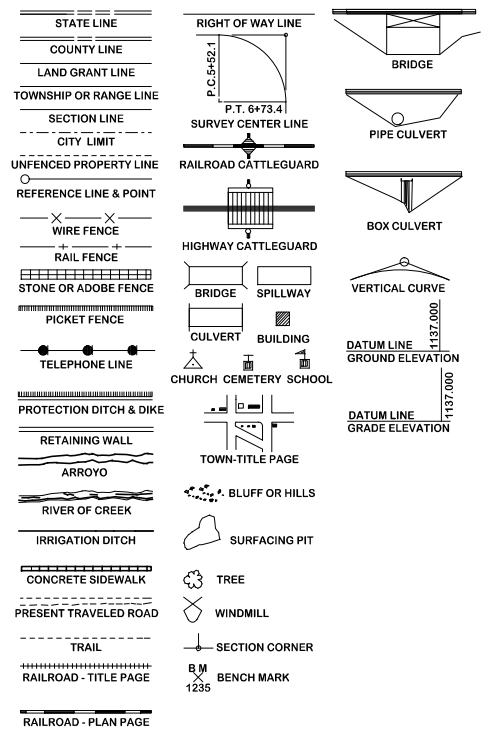
CN: A301430
GABALDON RD.
BEGIN AT EAST ARAGON RD
END AT NM 314 FRONTAGE RD



Length Of Project 2.429 Miles

THIS PROJECT BEGINS IN SECTION 16, T. 5 N., R. 2 E.,
AND ENDS IN SECTION 6, T. 5 N., R. 2 E., N.M.P.M.

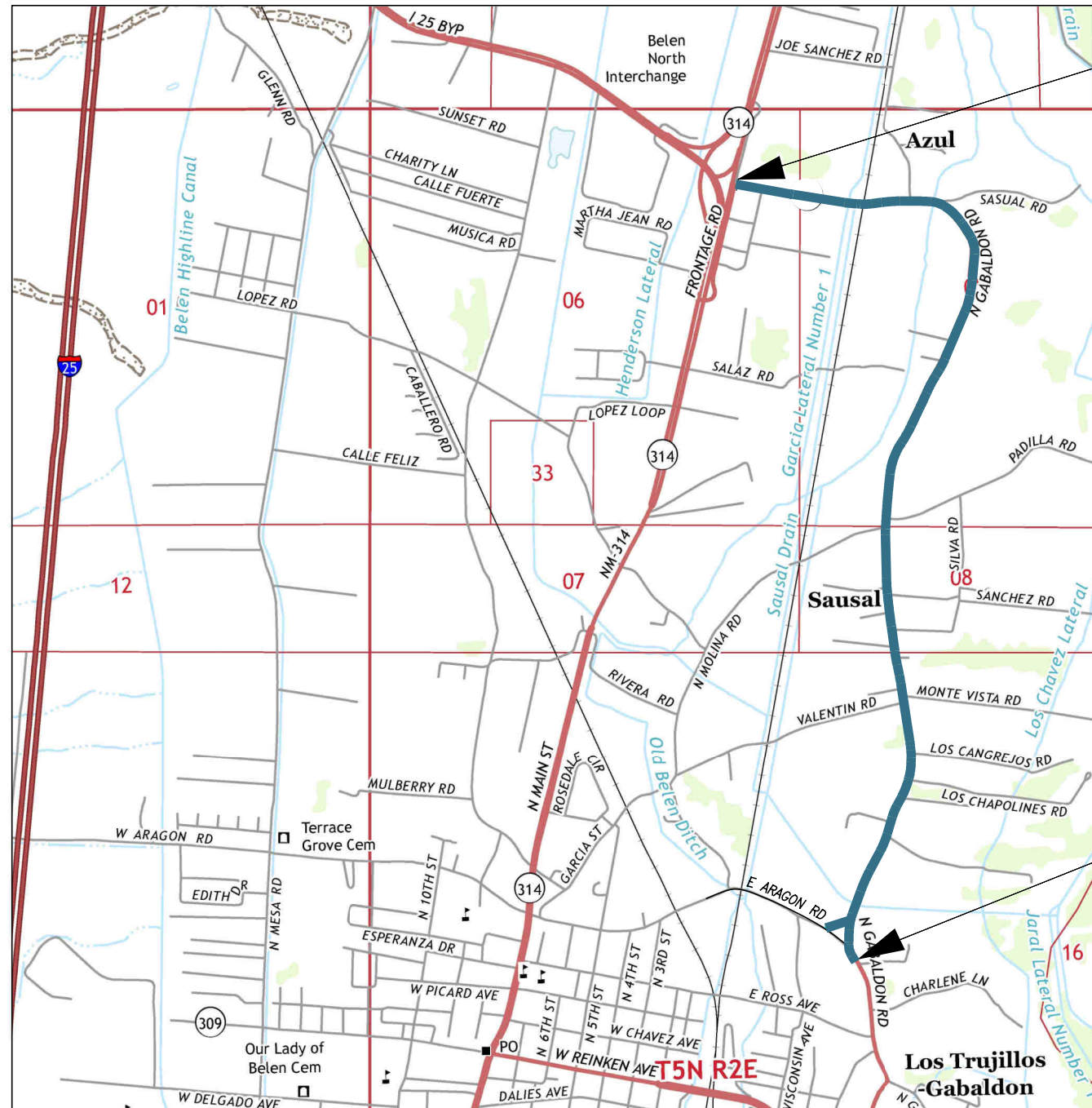
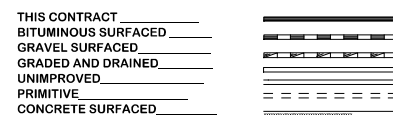
CONVENTIONAL SIGNS ON PLAN ON PROFILE



SHIPPING POINTS BELEN, NEW MEXICO

THE 2014 EDITION OF NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SHALL GOVERN CONSTRUCTION OF THIS PROJECT.

LEGEND



VICINITY MAP

SCALE: NOT TO SCALE

PROJECT INTENT

IMPROVEMENT OF GABALDON ROAD SIGNAGE AND PAVEMENT MARKINGS FROM EAST ARAGON RD TO NM 314 FRONTAGE RD.

CN: A301430
EOP: STA. 129+25.00
MP: 2.980 GABALDON RD



PROJECT CONTACTS

Valencia County Public Works
Lina Benavidez
(505)-866-2475

MRCOG
Ray Gomez
(505)-247-0234

Molzen Corbin
Kevin Eades
(505) 242-5700

CN: A301430
BOP: STA. 1+00.00
MP: 0.580 GABALDON RD

5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

GABALDON ROAD
SIGNING AND STRIPING

PROJECT VICINITY MAP



DRAWING SCALE: 1:NA

NEW MEXICO PROJECT NO. A301430

MOLZENCORBIN

SHEET NO. 1-2

EMAIL:

PHONE:

DESIGNED BY: desimer

INDEX OF SHEETS

SHEET NO.	DESCRIPTION	
1 - SERIES		
1-1	COVER SHEET	
1-2	PROJECT VICINITY MAP	
1-3	INDEX OF SHEETS AND INDEX OF STANDARD DRAWINGS	
1-4	SUMMARY OF QUANTITIES	
1-5	ENVIRONMENTAL REQUIREMENTS	
		SUBTOTAL: 5
2 - SERIES		
	NOT USED	
		SUBTOTAL: 0
3 - SERIES		
	NOT USED	
		SUBTOTAL: 0
4 - SERIES		
	NOT USED	
		SUBTOTAL: 0
5 - SERIES		
	NOT USED	
		SUBTOTAL: 0
6 - SERIES		
6-1	TEMPORARY TRAFFIC CONTROL GENERAL NOTES	
6-2	TEMPORARY TRAFFIC CONTROL GENERAL NOTES	
6-3	CONSTRUCTION & MAINTENANCE SIGN FACE DETAILS	
6-4	CHANNELIZATION DEVICES FOR CONSTRUCTION, MAINTENANCE, UTILITY & INCIDENT MANAGEMENT OPERATIONS	
6-5	TEMPORARY TRAFFIC MARKINGS FOR CONSTRUCTION	
6-6	MOBILE OPERATIONS ON 2 LANE FACILITIES AND SUGGESTED SEQUENCE OF CONSTRUCTION	
		SUBTOTAL: 6
7 - SERIES		
7-1	PERMANENT SIGNING AND STRIPING - GENERAL NOTES AND SUMMARY OF QUANTITIES	
7-2	PERMANENT SIGNING AND STRIPING PLAN - B.O.P. STA. 1+00 TO STA. 25+00	
7-3	PERMANENT SIGNING AND STRIPING PLAN - STA. 25+00 TO STA. 51+00	
7-4	PERMANENT SIGNING AND STRIPING PLAN - STA. 51+00 TO STA. 77+00	
7-5	PERMANENT SIGNING AND STRIPING PLAN - STA. 77+00 TO STA. 102+00	
7-6	PERMANENT SIGNING AND STRIPING PLAN - STA. 102+00 TO STA. 129+25 E.O.P.	
7-7	PERMANENT SIGNING AND STRIPING - NEW SIGN SCHEDULE SHEET 1 OF 2	
7-8	PERMANENT SIGNING AND STRIPING - NEW SIGN SCHEDULE SHEET 2 OF 2	
7-9	PERMANENT SIGNING AND STRIPING - STRIPING SCHEDULE	
		SUBTOTAL: 9
8 - SERIES		
	NOT USED	
		SUBTOTAL: 0
9 - SERIES		
	NOT USED	
		SUBTOTAL: 0
10 - SERIES		
	NOT USED	
		SUBTOTAL: 0
11 - SERIES		
	NOT USED	
		SUBTOTAL: 0
12 - SERIES		
12-1 to 12-13	NMDOT STANDARD SERIAL DRAWINGS (SEE INDEX ON THIS SHEET)	
		SUBTOTAL: 13
13 - SERIES		
	NOT USED	
		SUBTOTAL: 0
14 - SERIES		
	NOT USED	
		SUBTOTAL: 0
15 - SERIES		
	NOT USED	
		SUBTOTAL: 0
		PROJECT TOTAL: 33

INDEX OF NMDOT STANDARD DRAWINGS

SHEET NO.	DESCRIPTION	SERIAL	REV DATE
6-1	Temporary Traffic Control General Notes	702-01-1/5	03/02/17
6-2	Temporary Traffic Control General Notes	702-01-2/5	12/10/15
6-3	Construction & Maintenance Sign Face Details	702-01-3/5	12/10/15
6-4	Channelization Devices For Construction, Maintenance, Utility & Incident Management Operations	702-01-4/5	11/31/15
6-5	Temporary Traffic Markings For Construction	702-01-5/5	11/31/15
12-1	Small Sign Support Installation Details	701-02-1/3	01/11/05
12-2	Small Sign Support Installation Details	701-02-2/3	02/03/05
12-3	Multi-Directional Slip Base Post Details	701-02-3/3	01/11/05
12-4	Aluminum Sign Panel Details	701-03-1/2	01/11/05
12-5	Aluminum Panel Sign Details	701-03-2/2	01/11/05
12-6	Miscellaneous Sign Face Details	701-15-1/2	02/03/05
12-7	Railroad Roadway Crossing Sign Bill of Materials for Sign and Post	701-21-1/5	10/02/15
12-8	Railroad Roadway Crossing Sign Emergency Notification Sign	701-21-4/5	10/02/15
12-9	Road Object Marker Details	703-01-1/3	03/14/05
12-10	Road Delineator - Guide Details	703-01-2/3	01/11/05
12-11	Delineators & Object Markers	703-01-3/3	01/11/05
12-12	Pavement Markings For Typical Railroad Crossings	704-02-1/1	01/11/05
12-13	Pavement Markings And Messages	704-03-1/2	04/17/08

EMAIL:

PHONE:

DESIGNED BY: desimer



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
GABALDON ROAD SIGNING AND STRIPING INDEX OF SHEETS AND INDEX OF STANDARD DRAWINGS			

DRAWING SCALE: 1:

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	UNIT	ROADWAY		CONSTRUCTION ENGINEERING		BRIDGE		CONSTRUCTION SIGNING		PERMANENT SIGNING		TOTAL	
			ESTIMATE	FINAL	ESTIMATE	FINAL	ESTIMATE	FINAL	ESTIMATE	FINAL	ESTIMATE	FINAL	QUANTITY	FINAL
601000	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	L.S.										1		1
618000	TRAFFIC CONTROL MANAGEMENT	L.S.			1									1
621000	MOBILIZATION	L.S.			1									1
701000	PANEL SIGN	SQ.FT.										469		469
701100	STEEL POST AND BASE POST FOR ALUMINUM PANEL SIGNS	LIN.FT.										850		850
702000	CONSTRUCTION SIGNING	SQ.FT.							10					10
702100	STEEL POST AND BASE POST FOR CONSTRUCTION SIGNING	LIN.FT.							10					10
702238	BARRICADE, TYPE III - 8'	SQ.FT.							3					3
702525	CHANNELIZATION DEVICES TYPE DRUM	EACH							10					10
702810	TRAFFIC CONTROL DEVICES FOR CONSTRUCTION	L.S.							1					1
703002	OBJECT MARKER TYPE 2	EACH										2		2
703003	OBJECT MARKER TYPE 3	EACH										14		14
704224	RETROREFLECTORIZED PLASTIC PAVEMENT STRIPE, 24"	LIN.FT.										165		165
704700	HOT THERMOPLASTIC PAVEMENT MARKINGS, 4"	LIN.FT.										51300		51300
704740	HOT THERMOPLASTIC PAVEMENT MARKINGS RAILROAD CROSSING	EACH										2		2
801000	CONSTRUCTION STAKING BY CONTRACTOR	L.S.			1									1

EMAIL:

PHONE:

DESIGNED BY:



GABALDON ROAD
SIGNING AND STRIPING

SUMMARY OF QUANTITIES

DRAWING SCALE: 1: NA



July 14, 2017

J. Don Martinez
 Division Administrator
 Federal Highway Administration
 4001 Office Court Dr., Suite 801
 Santa Fe, NM 87507

District: 3
Control No.: A301430
Project No.: A301430
T/LGA: Valencia County

Susana Martinez
 Governor

Tom Church
 Cabinet Secretary

SUBJECT: Programmatic Categorical Exclusion-Tribal/Local Government Agency

Dear Mr. Martinez:

The New Mexico Department of Transportation (NMDOT) staff has reviewed the following project. The project meets the conditions stipulated in the agreement approved September 25, 2015, for completing the requirements of 23 CFR § 771 and it qualifies for the NMDOT and Federal Highway Administration Programmatic Categorical Exclusion.

Commissioners

Ronald Schmeits
 Chairman
 District 4

Dr. Kenneth White
 Secretary
 District 1

David Sepich
 Commissioner
 District 2

Keith Mortensen
 Commissioner
 District 3

Butch Mathews
 Commissioner
 District 5

Jackson Gibson
 Commissioner
 District 6

District	County	Project Number	Termini Description
3	Valencia	A301430	Gabalton Rd from North Frontage to Aragon Rd.

Project Scope:
 Replacement of signs and pavement striping in existing road prism. No new right-of-way required.
 The scope of work described herein has been supplied by the local government agency. Any subsequent alterations to the project design will require environmental analysis. For this analysis contact Brian Cribbin at (505) 690-2286 or brian.cribbin@state.nm.us .

In addition, a review of the project has been completed in accordance with the requirements of the National Historic Preservation Act of 1966, as amended through 1992 and 36 CFR 800 (August 5, 2004). We have determined that the proposed project does not include any activities or programs that can result in changes in the character or use of historic properties. The undertaking has no potential to cause effects on historic properties as defined in 36 CFR § 800.3(a)(1). There are no biological or natural resource concerns associated with this project.

NMDOT files contain documentation supporting this determination. The files are available for FHWA review. We will continue to monitor project design and development to ensure the applicability of the Programmatic Categorical Exclusion.

The aforementioned project meets all of the conditions stipulated in the Programmatic Categorical Exclusion under 23 CFR § 771(c)(8), 23 CFR § 771(c)(22), and 23 CFR § 771(c)(23)(i). No further coordination is necessary for environmental approval.

Sincerely,

Blake Roxlau, Manager

General Office | P.O. Box 1149 | Santa Fe, NM 87504

5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

**GABALDON ROAD
 SIGNING AND STRIPING**

**ENVIRONMENTAL
 REQUIREMENTS**

DESIGNED BY: PHONE: EMAIL:

TRAFFIC CONTROL GENERAL NOTES:

1. **TRAFFIC CONTROL:** All Temporary Traffic Control (TTC) devices shall be placed in accordance with the NMDOT Standard Specifications for Highway and Bridge Construction (latest edition) and the Manual on Uniform Traffic Control Devices (latest edition) and current revisions with the following constraints:
 - a. No substitutions will be allowed for channelization devices type drum unless otherwise noted in the plans.
 - b. Temporary portable sign stands are an unnecessary hazard when not in use. Unused temporary sign stands shall be removed from the roadway. If temporary sign stands are staged for future use (approved by the project manager) they shall be folded up and stored away from the paved shoulder.
 - c. Use of Type I or II barricades on roadways with speed limit greater than 40 mph is strictly prohibited.
 - d. The work zone shall comply with, but not limited to, NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control.
2. **BOP AND EOP SIGNING:** BOP and EOP signing in accordance with Standard Drawing 702-03-1/1 and/or 702-04-1/1 shall be placed at the project limits prior to construction operations commencing and shall remain in place throughout the duration of the project or as directed by the Project Manager. Advance warning signs shall be placed at all side streets.
3. **FLAGGING:** Flagging shall be provided for safety per the plan or as directed by the project manager and shall conform to the MUTCD latest edition. The flaggers, applicable signs and other related items shall be considered incidental to the completion of the project and no separate measurement or payment will be made.
 - a. All flaggers shall be certified and shall have their certification available for review at all times when on duty.
 - b. Flagging operations shall adhere to NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control; Flaggers shall wear high-visibility safety apparel that meets Performance Class 2 or 3.
4. **INGRESS AND EGRESS:** The Contractor shall provide ingress and egress to local residences and businesses for the duration of the project. If access closure is required, the contractor shall request the closure through the Project Manager. Upon approval, the Contractor shall coordinate such closure with the property owners and the Project Manager at least 48 hours in advance. All work associated with this shall be considered incidental to the completion of the project and no separate payment or measurement will be made.
5. **PORTABLE CHANGEABLE MESSAGE SIGNS:** The Contractor shall supply Portable Changeable Message Signs (PCMS), which will be retained by the contractor. The message PCMS shall be utilized to convey messages, expected delays, and detours to motorists as required. Messages should be determined by the Contractor and approved by the Project Manager. Two (2) Weeks prior to start of construction: name locations, and number of PCMS to be placed.
6. **TRAFFIC CONTROL PLANS:** This traffic control plan (TCP) represents a suggested method for traffic control during construction. Adjustments to the details of this TCP and requirements within the plan may be necessary due to construction activities, or as directed by the Project Manager. If the contractor elects to make any changes to the TCP or sequence of construction, the Contractor shall submit four (4) 11" X 17" copies of the proposed TCP to the Project Manager at least two (2) weeks prior to implementation. The TCP shall conform to the current editions of the MUTCD, NMDOT Standard Specifications and AASHTO Roadside Design Guide. The TCP shall be in computer drafted format and shall be designed, stamped, and revised as necessary by a current New Mexico Licensed professional engineer and submitted to the Project Manager for approval. All costs associated with developing the TCP and any additional devices associated with the TCP shall be incidental to Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.
7. **PUBLIC INFORMATION:** The Contractor / TCP firm shall contact the Project Manager or District Public Information Officer, as preferred by the district through the district office, to confirm the actual start dates of the construction and the contractor's schedule a minimum of 48 hours before any work listed in the TCP is performed.
8. **REMOVAL OF CONSTRUCTION SIGNING:** All temporary traffic control signs, sign posts and post bases installed with the construction project shall be **REMOVED** by the Contractor at the completion of the project. Removal shall consist of complete extraction of the bases from the ground. This work shall be incidental to the completion of the project and no separate measurement or payment will be made.
9. **CONFLICTING SIGNS:** All conflicting signs within or in advance of the work zone shall be covered completely with an opaque non-light transmitting material so as not to damage the sign. The Contractor is to use an approved method of covering existing signs so as not to damage/distort the sign sheeting or markings. The Contractor shall not place tape directly on the face of the sign. Failure to adhere to this requirement will result in the Contractor replacing the sign at no cost to the NMDOT.
10. **TEMPORARY STRIPING:** The use of black paint to cover existing lane lines or symbols is strictly prohibited. All temporary striping shall be placed before opening any work zone or portion of a work zone in accordance with the MUTCD and the approved traffic control plan. This work shall be included in Item No. 704100, "Removable Marking Tape" and no separate measurement or payment will be made, unless otherwise noted in the contract.


11. CONSTRUCTION SIGNING: All construction signing shall meet retroreflectivity requirements listed in section 702.2.1 "Construction Signing" of the NMDOT Standard Specifications.

- a. All construction signing on the interstate and on high speed (greater or equal to 45 MPH) multilane divided facilities shall be double indicated (left and right shoulders).
- b. All signs that are part of work zone that are in place for more than 3 days shall be placed on breakaway posts. If there are physical restrictions at the site that prohibit the sign from being placed on posts, the Contractor shall notify the District Traffic Engineer and obtain a waiver.
- c. All warning and regulatory signs shall meet the following size requirements:

Interstate:	Warning sign 48"x48"	Regulatory 48"x60"
Non-Interstate:	Warning sign 36"x36"	Regulatory 36"x42"
- d. The following reflectivity material shall be used on all construction signing placed on NMDOT roadways.

SIGN	SIGN CODE	COLOR	LETTER SHEETING	BACKGROUND SHEETING
APPROACH SIGNS	W20-XX	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
CHEVRONS	W1-8	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
CURVES	W1-2	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
REVERSE CURVE	W1-4	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
MERGE	W4-1	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
NO PASSING ZONE	W14-3	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI
FLAGGER PADDLE	-----	BLK/FLUORESCENT ORANGE on Side 1 with RED on Side 2	-----	TYPE VIII, IX, XI Type IV White
ALL DRUMS	-----	WHITE/ FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI Type IV white
All Other Const. Signs	-----	BLK/FLUORESCENT ORANGE	-----	TYPE VIII, IX, XI

12. **REMOVAL OF TEMPORARY STRIPING:** All relevant temporary striping shall be removed upon completion of each phase of construction. The only approved method of stripe removal is water blasting. The contractor is to ensure that there is no conflicting striping through the work zone or through detours. This work shall be included in Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.
13. **SATISFACTORY WORKING CONDITION:** All traffic control devices used on this project shall be in satisfactory working condition and shall function equivalently to new equipment in accordance with the MUTCD (latest edition). Traffic Control work zone shall comply with requirements of FHWA 23CFR 630 Subpart K for traffic control devices. At the beginning of the project 100% of signs/devices shall be in acceptable condition (new or like new). After 2 weeks at no time shall less than 75% of devices be in acceptable condition. All traffic devices shall be kept clean throughout the duration of the project. Any sign that is tagged by graffiti shall be cleaned (as long as it does not affect the reflective sheeting) within 24 hours or removed and replaced.
14. **TRAFFIC CONTROL FIELD ADJUSTMENTS:** Location of device spacing shall be field verified to account for existing roadway features which may obstruct placement and/or view of devices. Any changes to the traffic control plan should be approved by the District Traffic Engineer or his/her designee. All field adjustments of signs should be approved by the District Traffic Engineer. This work shall be considered incidental to Item No. 618000 "Traffic Control Management" and no additional measurement or payment will be made, unless noted in the contract.
15. **PEDESTRIANS:** Routes/paths shall not be closed without providing a detour. ADA requirements shall be adhered to; ADA compliant devices shall be used for channelization.
16. **BICYCLES:** Shall be accommodated or rerouted per MUTCD recommendations.

A	3/2/2017	DRB	REVISE NOTE 2
A	3/2/2017	DRB	REVISE NOTE 7
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC CONTROL GENERAL NOTES			
APPROVED	 <small>DESIGN ENGINEER</small>		3/2/17
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
702-01-1/5			

TRAFFIC CONTROL GENERAL NOTES (CONTINUED):

17. **TEMPORARY CONCRETE WALL BARRIER (CWB):** When flaring the leading end of a Temporary Wall Barrier (CWB) within a construction work zone, the flare rate shall be done in accordance with the rates shown in the table below: (NMDOT Standard Drawing 606-20-5/5)

Roadway Speed Limit	Minimum Taper/ Flare Rate	Desirable Taper/ Flare Rate
Less than 45 MPH	8:1	18:1
Between 45 MPH and 55 MPH	10:1	24:1
Greater than 55 MPH	15:1	30:1

- a. When temporary wall barrier is placed in a construction work zone, a 5' clear area is required between the CWB and the work zone to accommodate barrier deflection. When a 5' clear area is not attainable, CWB shall be anchored to the pavement surface.
- b. Temporary CWB shall be provided with reflective barrier delineators as indicated in NMDOT standard drawing 606-21-1/1.

18. **CRASH ATTENUATORS:** The crash cushion attenuators shall be designed as per the District Traffic Engineer's recommendations. The District may elect to either utilize the pre-construction posted speed, or the 85% speed in the layout of the crash cushion attenuators within the work zone.

19. **DROP OFF POLICY:** In the areas of pavement operations or other activities within the traveled way and adjacent to the existing traveled lane, the contractor shall assure that no pavement drop-offs are left exposed during non-working hours. The contractor shall initiate corrective means as per "the New Mexico Department of Transportation Pavement Drop-off Guideline" to achieve a minimum 6:1 slope between traveled lanes and a minimum 3:1 slope adjacent to the existing traveled lane with two 11foot driving lanes as shown in the detail below. (AD241)



20. **Lane Closures:** The Contractor/TCP firm shall not place a lane closure taper along a horizontal curve. The taper shall be placed in advance of the horizontal curve so that it is visible to all oncoming traffic. On crest vertical curves, the Contractor/TCP firm shall place lane closures in advance of, or at the beginning of the curve to enhance visibility of the lane closure to oncoming traffic.

21. **Sequential Arrow Display:** Placement of the sequential arrow shall be at or near the beginning of the lane closure taper. In areas of insufficient pavement width, the sequential arrow may be placed within the taper, but not to exceed 1/2 the taper length. In all cases, the sequential arrow shall be placed behind the channelization devices. The shoulder shall be closed in advance of the merging taper to direct vehicular traffic to remain within the traveled way. (MUTCD 6F.61)

22. **ADDITIONAL SIGNS:** "BUMP", "LOOSE GRAVEL", "LANE DROP-OFF SIGN" sign placement: The contractor shall place WB-1 sign ("BUMP" - B/FO), WB-7 sign ("LOOSE GRAVEL" - B/FO) and/or WB-17 signs ("SHOULDER DROP-OFF" - B/FO) in advance of bridge approaches or other locations during cold milling and overlay operations as needed or as directed by the project manager.

23. **CLEAR ZONE:** All stationary objects within clear zone shall be properly shielded and outlined with drums mounted with Type "A" warning lights. Use of vertically mounted retro-reflective material in lieu of a Type A warning light is strictly prohibited.

- a. Equipment, materials, or vehicles stored within Right -of-way (ROW) shall be outside of clear zone (based on existing posted speed).
- b. Equipment, material or vehicles stored within clear zone shall be properly shielded.
- c. Materials, work activities, equipment, and vehicles shall not be stored within the established buffer space of the project work zone.
- d. All construction equipment, vehicles and materials shall remain behind traffic control devices.

24. **TRAFFIC CONTROL MANAGEMENT:** The contractor or the traffic Control Subcontractor shall provide a Traffic Control Supervisor on site during working hours for response within 1 hour to traffic control issues/concerns.

25. **INCIDENT MANAGEMENT:** Contractor is required to comply with requirements of FHWA CFR 630 Subpart J for Work Zone Safety and Mobility which shall include an Incident Management Plan to be utilized for the entire duration of the project. The Incident Management Plan shall contain a method to address traffic flow through the work zone during incidents. The Incident Management Plan must be reviewed and approved by the District Traffic Engineer. The plan shall contain the following as a minimum:

- a. Contacts for the contractor, local enforcement, safety agencies, municipal agencies, public information officer and NMDOT
- b. Steps to be followed during incidents
- c. Method of recording and reporting incidents

26. **LIST OF INCIDENTALS - No Additional Payment Associated**

LIST OF INCIDENTALS for Temporary Traffic Control/A MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

	LIST OF INCIDENTALS for Temporary Traffic Control
A.	MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC CONTROL GENERAL NOTES			
APPROVED:	<i>Afshin</i>		12/10/15
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
702-01-2/5			






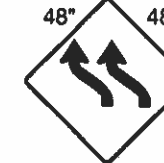

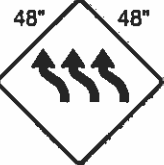
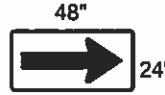


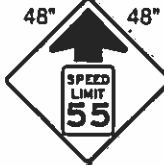

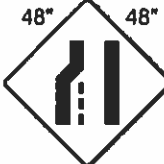

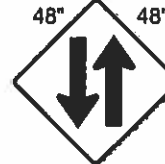








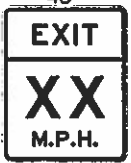
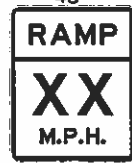






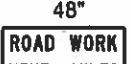
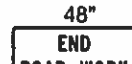
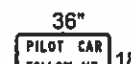
SIGN FACE DETAILS

FOR CONSTRUCTION / MAINTENANCE



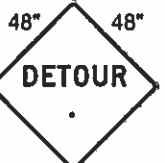




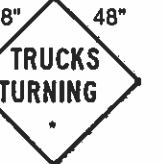
1. SIGNS SHALL MEET SPECIFICATIONS IN THE STANDARD HIGHWAY SIGNS MANUAL AND CURRENT EDITION OF THE MUTCD.
2. SEE CURRENT EDITION OF MUTCD FOR ADDITIONAL SIGNS.
3. ALL SIGNS SHALL COMPLY WITH SHEETING REQUIREMENTS AS SPECIFIED IN STANDARD DRAWING 702-01-3/3.
4. SIGN SIZES MAY BE ADJUSTED PER MUTCD RECOMMENDATIONS.

WARNING SIGNS:







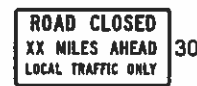

ALL WARNING SIGNS ARE BLACK/FLUORESCENT ORANGE UNLESS OTHERWISE SPECIFIED.

 W1-2L-48	 W1-2R-48	 W1-4L-48	 W1-4R-48	 W1-4bR-48	 W1-4bL-48
 W1-4cR-48	 W1-4cL-48	 W1-6R-48	 W1-6L-48	 W3-4-48	 W3-5-48
 W4-2R-48	 W4-2L-48	 W5-1-48	 W6-3-48	 W8-1-48	 W8-3-48
 W8-7-48	 W11-1-48	 W11-2-48	 W13-1-24-XX	 W13-2-36-XX	 W13-3-36-XX
 W13-2-48-XX	 W13-3-48-XX	 W16-2-30	 W20-7a-48	 M4-9bL-30	 M4-9bR-30
 M4-9R-30	 M4-9L-30	 G20-1-48-24	 G20-2-48	 G20-4-36	



* THESE SIGNS REQUIRE APPROPRIATE DISTANCE INDICATION
(1/2 MILE, 1 MILE, 1500 FT., 750 FT., 500 FT., 350 FT.)

 W8-6-48-XX	 W20-1-48-XX	 W20-2-48-XX	 W20-4-48-XX
 W20-5L-48-XX	 W20-5R-48-XX	 W21-5d-48-XX	 W24-NM-17-48-XX

REGULATORY SIGNS:





 R1-1-36 W/R, RL-RB	 R1-2-36 W/R	 R2-1-36-XX B/W	 R2-1-48-XX B/W
 R11-NM-2-48 B/W	 R11-2-48 B/W	 R11-3a-60 B/W	 R11-4-60 B/W

SPECIAL SIGNS:

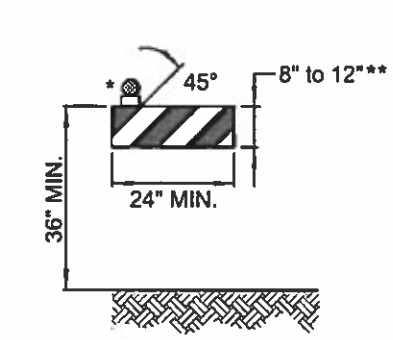
 SP-10R-48 W/B, RB/RL	 SP-10L-48 W/B, RB/RL
--	--

8 FT. TYPE III BARRICADES

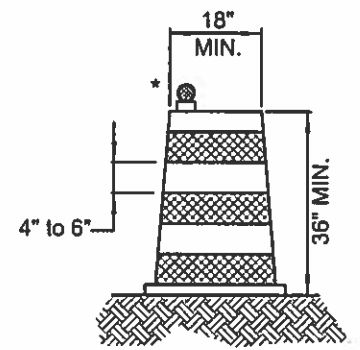
BARRICADE SYMBOL

 R11-2-48 ROAD CLOSED W1-6-48 OR M4-10R-48	 R11-2-48 ROAD CLOSED W1-6-48 OR M4-10L-48
 R11-2-48 ROAD CLOSED DEAD END B1-1 (RL)-96 (NO TURNS AVAILABLE)	 R11-2-48 ROAD CLOSED DEAD END B1-1 (RL)-96 (LEFT & RIGHT TURNS AVAILABLE)

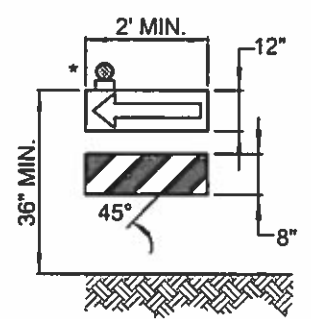
NO.	DATE	REV BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
CONSTRUCTION & MAINTENANCE SIGN FACE DETAILS			
APPROVED: <i>Afshin</i>			12/10/15
DESIGNED BY: _____			DRAWN BY: _____
			CHECKED BY: _____
702-01-3/5			



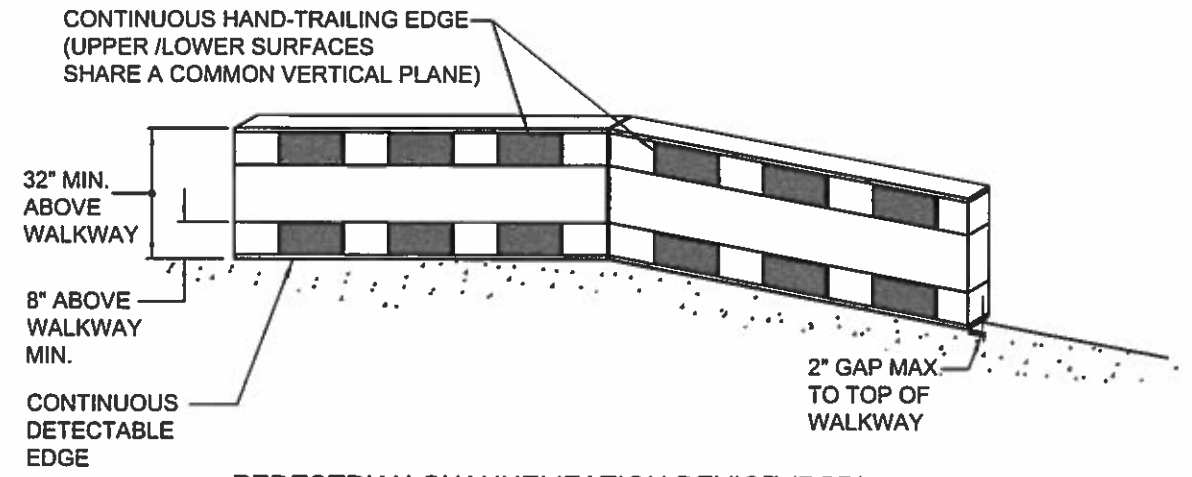
TYPE I BARRICADE ::
LOW SPEED ROADWAY (≤ 40 MPH)



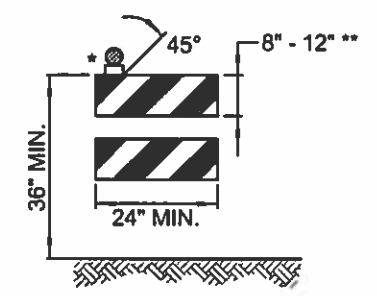
DRUM*



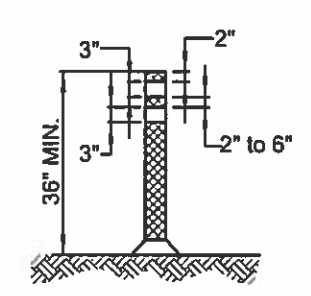
DIRECTION INDICATOR BARRICADE ::



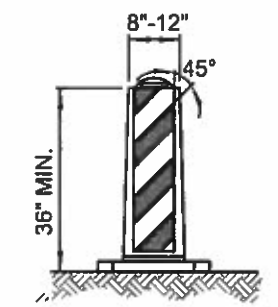
PEDESTRIAN CHANNELIZATION DEVICE (PCD)



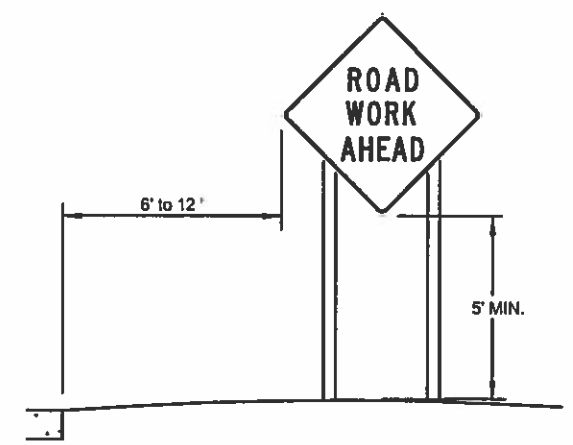
TYPE II BARRICADE ***
LOW SPEED ROADWAY (≤ 40 MPH)



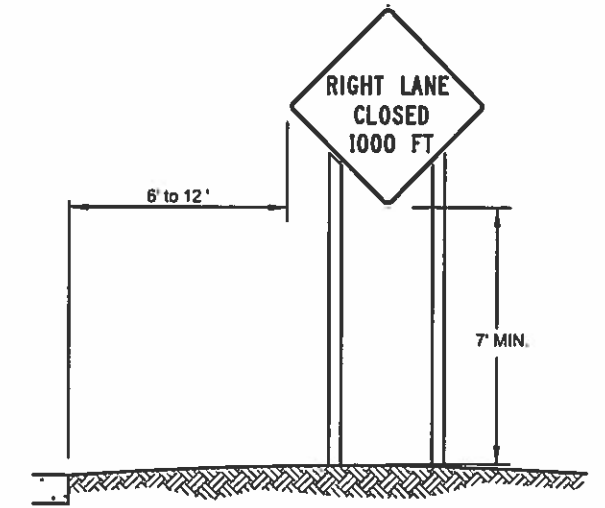
TUBULAR MARKERS



VERTICAL PANEL ***

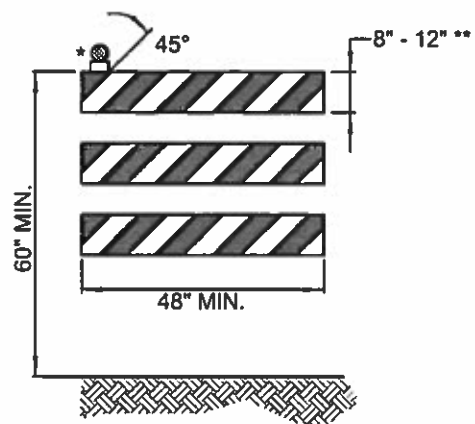


RURAL AREA

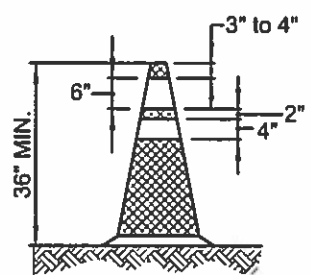


BUSINESS, COMMERCIAL, OR RESIDENTIAL AREA (WITHOUT CURB)

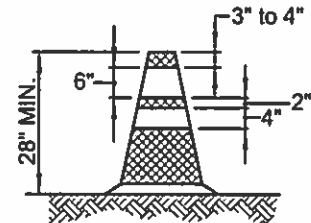
HEIGHT AND LATERAL LOCATIONS OF SIGNS - TYPICAL INSTALLATIONS



TYPE III BARRICADE ::



NIGHT AND/OR FREEWAY HIGH SPEED ROADWAY (≥ 45 MPH)



DAY AND LOW SPEED ROADWAY (≤ 40 MPH)

CONES

NOTE:
ALL DEVICES USED ON NMDOT ROADWAYS SHALL BE ON THE APPROVED PRODUCTS LIST.

- * WARNING LIGHT (OPTIONAL)
- ** NOMINAL LUMBER DIMENSIONS ARE SATISFACTORY FOR BARRICADE RAIL WIDTH DIMENSIONS.
- *** RAIL STRIPE WIDTHS SHALL BE 6 INCHES. EXCEPTION: WHERE RAIL LENGTHS ARE LESS THAN 36 INCHES, THEN 4 INCH WIDE STRIPES MAY BE USED.

THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES. VERTICAL PANELS AND BARRICADES USED ON HIGH-SPEED ROADWAYS, EXPRESSWAYS, AND FREEWAYS SHALL HAVE A MINIMUM OF 270 SQUARE INCHES OF RETROREFLECTIVE AREA FACING TRAFFIC.

DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING		
CHANNELIZATION DEVICES FOR CONSTRUCTION, MAINTENANCE, UTILITY & INCIDENT MANAGEMENT OPERATIONS		
APPROVED:	<i>A. E. Hines</i>	11/2/15
DESIGNED BY:	DRAWN BY:	CHECKED BY:
702-01-4/5		

FIGURE 1

SHORT TERM WORK ZONE INTERIM MARKINGS (IN PLACE FOR LESS THAN 14 CALENDAR DAYS) (MINIMUM OF 2 COATS OR AS DIRECTED BY THE PROJECT MANAGER)

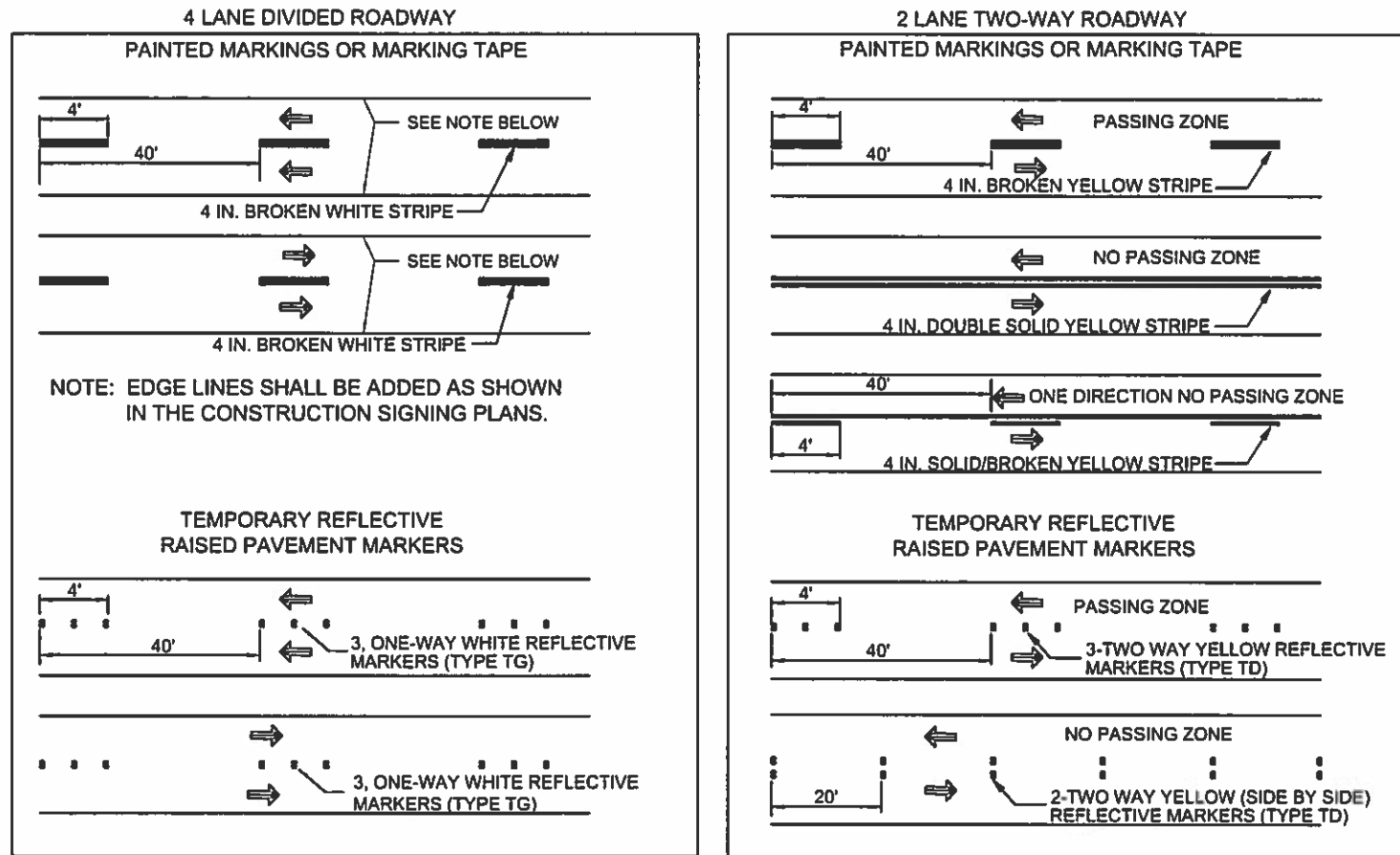
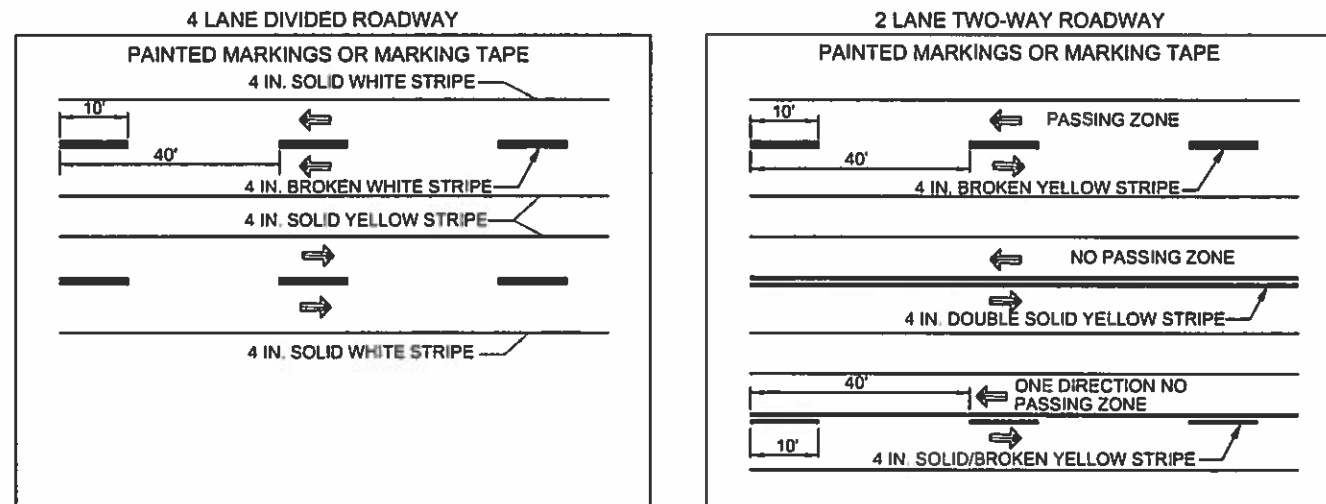


FIGURE 1A

STANDARD WORK ZONE INTERIM MARKINGS (IN PLACE FOR 14 CALENDAR DAYS OR MORE) (MINIMUM OF 2 COATS OR AS DIRECTED BY THE PROJECT MANAGER)



GENERAL NOTES

WORK ZONE INTERIM MARKINGS:

- The contractor shall place reflectorized painted markings on each lane line on each intermediate lift of surfacing or milled surface at the end of the daily surfacing or milling operation. These markings shall be placed in accordance with Figure 1 or Figure 1 A on this sheet, or as directed by the Project Manager.
- In the event the painted markings cannot be placed as described above, the contractor shall, with the approval of the project manager, place marking tape or temporary reflective raised pavement markers. The contractor will be responsible for maintaining the temporary raised pavement markers when requested by the project manager, District Traffic Engineer or their designees. maintenance of the tabs will be considered incidental to the completion of the project.
- The contractor shall place removable marking tape or temporary reflective raised pavement markers after placement of the final lift of surfacing if permanent markings are not placed during the same working day. These markings shall be placed in accordance with Figure 1 or Figure 1 A on this sheet, or as directed by the project manager.
- On roadways with severe curvature, broken-line interim markings with half-cycle lengths and a minimum of two foot (2') stripes or a group of two temporary reflective pavement markings spaced 2 feet apart may be used where passing is allowed. Interim edge lines or channelization lines for delineation may be used as needed or as directed by the project manager. Passing/ no passing zone signing to supplement interim markings for delineation may be used as needed or as directed by the project manager. All interim markings shall be placed in accordance with the current edition of the MUTCD.
- Shoulder and gore area delineation will be required on each intermediate lift of surfacing or milled surface at the end of the day's pavement operation. Payment for marking tape or temporary pavement markings will be paid for under the unit priced of reflectorized painted markings, unless otherwise specified within the contract or Traffic Control Notes. Contractor may substitute edge line pavement marking with traffic control devices such as drums or vertical panels for a maximum 13 day period.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC MARKINGS FOR CONSTRUCTION			
APPROVED	A. E. ...		11/3/15
DESIGNED BY		DRAWN BY	CHECKED BY
702-01-5/5			

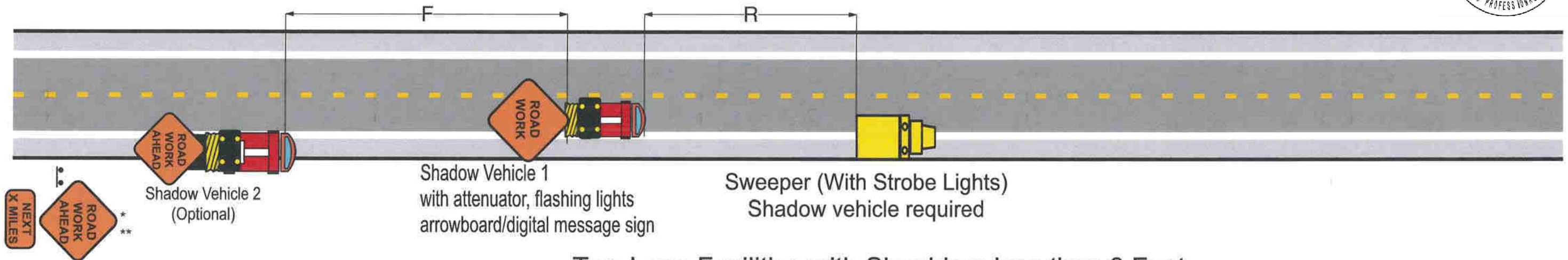
ADDITIONAL NOTES

A. DRAWING IS NOT INTENDED TO SHOW ALL SEQUENCE OF ADVANCED SIGNING AND IS NOT INTENDED TO BE A COMPLETE CONSTRUCTION SIGNING PLAN. SIGNS MAY BE COMBINED WITH OTHER WORK ZONE SIGNING THAT MAY INCLUDE BUT IS NOT LIMITED TO ADVANCE WARNING SIGNS, BOP/EOP SIGNING, SPEED REDUCTION SIGNS, LANE CLOSURES, ETC. SPACING SHALL CONFORM TO THE RECOMMENDATIONS OF THE MOST CURRENT EDITION OF THE MUTCD.

B. DOUBLE FINE SIGNS (R52-NM-4, R52-NM-5, R6-2) SHALL NOT BE USED WHEN WORK ACTIVITIES ARE OUTSIDE THE CLEAR ZONE, FOR SHORT DURATION OPERATIONS (WORK OCCUPYING A LOCATION FOR UP TO 1 HOUR) AND MOBILE OPERATIONS (WORK MOVING INTERMITTENTLY OR CONTINUOUSLY). SEE THE MUTCD (CURRENT EDITION) FOR MORE INFORMATION.

SUGGESTED SEQUENCE OF CONSTRUCTION

1. COMPLETE PANEL SIGN INSTALLATION FROM B.O.P. TO E.O.P. TEMPORARY TRAFFIC CONTROL WILL BE ESTABLISHED UTILIZING PROCEDURES AND DEVICES SUITABLE FOR SHORT-TERM STATIONARY OPERATIONS FOR WORK ZONES OUTSIDE OF THE ROADWAY SHOULDER.
2. COMPLETE ROADWAY STRIPING OPERATIONS FROM B.O.P. TO E.O.P. TEMPORARY TRAFFIC CONTROL SET UP WILL BE SIMILAR TO THAT REQUIRED FOR MOBILE OPERATIONS ON 2 LANE FACILITIES.



Two Lane Facilities with Shoulders less than 8 Feet

Notes

- 1) Driver shall turn on flashing, oscillating, or strobe lights before starting the sweeping operation
- 2) Driver shall deploy the Variable Message Board and use the warning display mode or the dancing diamond display mode
- 3) Sweeping and other mobile operations on the interstate or the state roadway shall not be done during the following periods:
 - a) 6:30 AM and 8:30 AM
 - b) 4:00 PM and 6:30 PM

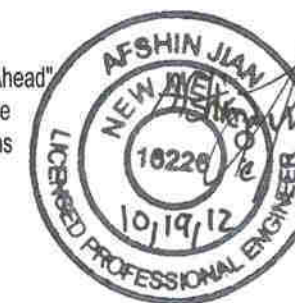
Sweeping, or other mobile operations, during peak periods may be authorized by written notice from the Maintenance Engineer, Traffic Engineer or their designees.

- 4) On roadways with shoulders equal to or less than 6 feet, the sweeper shall have a shadow vehicle that is equipped with an Attenuator, warning sign, and an arrowboard
- 5) If a warning sign is not placed on the shadow vehicle, an advance warning sign (Road work ahead or sweeping ahead) shall be placed in advance of the area that is being swept. Supplemental signs will be required as follows:
 - a) Every 2 Miles,
 - b) At every major intersection.
- 6) During sweeping operations, The signs "Road Work Ahead" or "Shoulder Work Ahead" may be substituted with the "Sweeping Ahead" or "Sweeping Next XX Miles" Signs

- 7) Along curved roadway sections, the distance R should be increased to ensure that the Shadow Vehicle can be seen by oncoming traffic.
- 8) All signs shall be 36" x 36" where feasible. 30"x30" signs may be used in area where is it not feasible to use 36"x36" signs.
- 9) Vehicle mounted attenuator on Shadow Vehicles 2 is optional.
- 10) Shadow and work vehicles shall display high intensity rotating, flashing, oscillating, or strobe lights.

* "Mobile Operations" signs may be used in lieu of "Shoulder Work" or Road Work" warning signs
 ** Advance Warning sign optional if sign is located on the sweeping vehicle and/or the shadow vehicle
 (see note number 5 on this sheet)

Posted Speed Limit Prior to Work Starting (mph)	Protection Vehicle Roll-Ahead Buffer Distance (with or without TMA) (R)		Shadow Vehicle Following Distance (F)	
	Moving (15 mph max)			
	Feet	Stopped Feet	Feet	Feet
0 - 30	100	100	250	550
35 - 40	100	100	325	700
45 - 50	175	125	600	900
55	175	125	750	1200
60 - 65	225	175	1000	1400
70 - 75	225	175	1200	1600



New Mexico Department of Transportation Standard Drawing
 Maintenance Traffic Control Plans
Mobile Operations On 2 lane Facilities AND SUGGESTED SEQUENCE OF CONSTRUCTION

PERMANENT SIGNING & STRIPING NOTES

1. ALL REGULATORY, WARNING, SPECIAL, AND GUIDE SIGNS SHALL COMPLY WITH IDD-2014-03 – SIGN SHEETING REQUIREMENTS. LEGEND AND BACKGROUND TO BE SAME SHEETING TYPE.
2. ONLY 0.125 INCHES THICK ALUMINUM PANEL SIGNS ARE PERMITTED FOR WARNING AND REGULATORY SIGNS.
3. ANTI GRAFFITI COATING, WITH UV INHIBITOR, SHALL BE APPLIED ON ALL REGULATORY, WARNING, GUIDE, AND SPECIAL SIGNS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO ITEM NO. 701000 PANEL SIGN, AND NO SEPARATE PAYMENT SHALL BE MADE.
4. QUANTITIES MAY VARY AS FIELD CONDITIONS DICTATE. THE CONTRACTOR WILL BE PAID FOR ACTUAL QUANTITIES USED.
5. ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH N.M. STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION (LATEST EDITION) AND ANY APPLICABLE SPECIAL PROVISION AND/OR SUPPLEMENTAL SPECIFICATION. ALSO THE DEVICES SHALL COMPLY WITH CURRENT EDITION, WITH REVISIONS, OF THE MUTCD.
6. EACH SIGN FACE SHOWN ON PLANS SHALL MEET THE SPECIFICATIONS IN THE STANDARD HIGHWAY SIGNS MANUAL (CURRENT EDITION) FOR PROPER ARRANGEMENT, SPACING OF LETTERS, LETTER HEIGHT, SYMBOLS AND BORDERS FOR THE SPECIFIED SIZE AND MESSAGE AS SHOWN ON PLANS. ALL SPECIAL SIGN FACE DETAILS SHALL BE SUBMITTED TO THE NMDOT FOR REVIEW AND APPROVAL.
7. POST LENGTHS ARE BASED ON A MINIMUM OF 5 FT FOR RURAL ROADWAY SECTIONS TO A MINIMUM OF 7 FT FOR URBAN AND INTERSTATE ROADWAY SECTIONS. THE LENGTHS ARE MEASURED FROM THE BOTTOM OF THE SIGN TO NEAR EDGE OF THE DRIVING LANE.
8. ALL SIGN AND TRAFFIC MARKER HARDWARE, INCLUDING, BUT NOT LIMITED TO, BRACKETS, BANDING, BUCKLES, FASTENERS, SCREWS, AND WASHERS, ETC. SHALL BE CONSIDERED INCIDENTAL TO INSTALLATION OF THE MAIN ITEMS. THEREFORE, NO PAYMENT WILL BE MADE.
9. THREE 4 LB/FT SIGN POST INSTALLATION APPROVED ONLY IF THE SPAN BETWEEN THE OUTER POSTS EXCEEDS 84" (SIGN WIDTH OF 96" OR GREATER).
10. 3'- 6" BASE POSTS ARE REQUIRED FOR ALL SMALL SIGN POSTS. SEE STANDARD DRAWING 701-02-1/3 FOR DETAILS. BASE POSTS SHALL BE 2.5" X 2.5" SQUARE.
11. SIGN POSTS SHALL BE 2.25"X2.25" SQUARE TUBING OR AS APPROVED BY THE PROJECT MANAGER, THE DISTRICT TRAFFIC ENGINEER OR HIS/HER DESIGNEE.
12. THE LATERAL CLEARANCE OF SIGNS SHALL BE NO CLOSER THAN 6 FT FROM THE EDGE OF SHOULDER OR 12 FT FROM THE EDGE OF TRAVELED WAY FOR RURAL AREAS AND NO CLOSER THAN 2 FT FROM FACE OF CURB FOR URBAN AREAS.
13. CONTRACTOR SHALL FIELD VERIFY ALL SIGNING, TEXT, AND LOCATIONS PRIOR TO FABRICATION AND INSTALLATION. NEW SIGN LOCATIONS SHALL BE APPROVED BY THE PROJECT MANAGER OR THE DISTRICT TRAFFIC ENGINEER.
14. HOT THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON ALL STRIPING (LONG LINES, LEGENDS, SYMBOLS, ETC.). THE THERMOPLASTIC PAVEMENT MARKING SHALL BE APPLIED AT A THICKNESS OF 90 MIL. THE APPROVED THERMOPLASTIC MARKING APPLICATION METHOD SHALL BE EITHER GRAVITY EXTRUSION OR RIBBON APPLICATION.
15. THE CONTRACTOR SHALL MARK ALL LOCATIONS IN THE FIELD IN WHICH PAVEMENT MARKINGS, SUCH AS LEGENDS, ARROWS, CROSSWALKS, AND STOP BARS, ETC. ARE TO BE PLACED. THE PAVEMENT MARKING LAYOUT SHALL BE CONSIDERED AS INCLUDED IN PAYMENT FOR THE MAIN ITEMS. BEFORE THE FINAL MARKINGS ARE APPLIED, THE CONTRACTOR SHALL OBTAIN CONCURRENCE FROM DISTRICT TRAFFIC ENGINEER DESIGNEE KEVIN EADES (505)242-5700.
16. WATER BLASTING IS THE ONLY APPROVED METHOD OF EXISTING STRIPE REMOVAL WITHIN DISTRICT THREE. THE USE OF BLACK PAINT AS A METHOD OF REMOVING OR COVERING EXISTING STRIPING IS STRICTLY PROHIBITED. WHEN WATER BLASTING ON FINAL PAVEMENT SURFACE, THE CONTRACTOR SHALL APPLY A SEAL TO THE WATER BLASTED AREA. WATER BLASTING AND SEAL SHALL BE CONSIDERED INCIDENTAL TO ITEM NO. 618000 TRAFFIC CONTROL MANAGEMENT..
17. CONTRACTOR SHALL REMOVE EXISTING PANEL SIGN(S), FOUNDATION, BASE POSTS AND HARDWARE FOR ANY EXISTING SIGN OR TRAFFIC MARKER SHOWN IN THE PLANS TO BE REMOVED, AND AT LOCATIONS SHOWN IN THE NEW SIGN SCHEDULE. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN PAYMENT FOR ITEM NO. 601000 REMOVAL OF STRUCTURES AND OBSTRUCTIONS, AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.

18. **RAILROAD COORDINATION** THE CONTRACTOR IS HEREBY ADVISED THAT THE SCOPE OF WORK OR A PORTION THEREOF REQUIRED UNDER THIS PROJECT WILL OCCUR WITHIN RAILROAD RIGHT OF WAY OWNED BY NEW MEXICO DEPARTMENT OF TRANSPORTATION (NMDOT). THE CONTRACTOR IS FURTHER ADVISED OF THE SAFETY-SENSITIVE NATURE OF WORKING WITHIN RAILROAD RIGHT OF WAY AND SHALL ENTER INTO AN AGREEMENT WITH NMDOT FOR RIGHT-OF-WAY ENTRY AND SHALL FOLLOW ALL REQUIREMENTS THEREIN. CONTRACTOR SHALL COORDINATE ALL OPERATION WITH NMDOT TO ASSURE SAFETY AND THAT RAILROAD ACTIVITIES ARE NOT INTERRUPTED. THE CONTRACTOR SHALL NOT BEGIN ANY WORK WITHIN THE RAILROAD RIGHT-OF-WAY WITHOUT PRIOR WRITTEN APPROVAL FROM NMDOT. CONTACT PERSONS ARE AS FOLLOWS:

TO OBTAIN AND EXECUTE AN AGREEMENT TO ENTER RAILROAD RIGHT OF WAY: NMDOT-ROBERT FINE; (505) 827-5133; ROBERT.FINE@STATE.NM.US

TO OBTAIN RAILROAD FLAGGING PROTECTION FOR CONTRACTOR WORKERS AND EQUIPMENT: RMRTD - STEPHANIE PAIZ (505) 414-1308; SPAIZ@MRCOG-NM.GOV

FOR RAILROAD CONSTRUCTION COORDINATION AND INSPECTION: RMRTD – MATT WYLIE (505) 264-5105; MWYLIE@MRCOG-NM.GOV

CONSULT THE RAILROAD "NOTICE TO CONTRACTOR" AND RAILROAD RIGHT OF WAY ENTRY AGREEMENT SAMPLE AVAILABLE FROM NMDOT FOR FURTHER GUIDANCE.

19. **RAILROAD FLAGGING** THE CONTRACTOR SHALL COORDINATE WITH RIO METRO REGIONAL TRANSIT DISTRICT (RMRTD), OPERATOR OF NMDOT'S RAILROAD, FOR RAILROAD FLAGGING PROTECTION REQUIREMENTS. COST FOR THIS FLAGGING IS ESTIMATED IN THE AMOUNT OF \$1200.00 PER DAY. THE CONTRACTOR SHALL PAY THE FLAGGING INVOICES AS RECEIVED FROM RMRTD.

THE CONTRACTOR SHALL SCHEDULE ITS WORK IN BLOCKS SO AS TO MINIMIZE THE NUMBER OF WORKDAYS WHERE ITS OPERATIONS OR EQUIPMENT ARE LOCATED WITHIN FIFTY (50) FEET FROM TRACKS REQUIRING RAILROAD FLAGGING. ACCORDINGLY THE CONTRACTOR SHALL SUBMIT A WORK PLAN TO THE PROJECT MANAGER FOR REVIEW AT LEAST 2 WEEKS PRIOR TO ENTERING THE RAILROAD RIGHT OF WAY FOR EACH PHASE OF CONSTRUCTION. THIS WORK PLAN SHALL DETAIL THE NUMBER OF WORKING DAYS WHERE CONTRACTOR WILL REQUIRE RAILROAD FLAGGING. THE CONTRACTOR SHALL NOT BEGIN THIS WORK UNTIL WRITTEN APPROVAL OF THIS WORK PLAN IS RECEIVED. THE CONTRACTOR SHALL TRACK THE RAILROAD FLAGGING DATES AND TIMES.

THE CONTRACTOR IS HEREBY ADVISED THAT WHEN A TRAIN PASSES THE WORK SITE, RAILROAD FLAGGING PERSONNEL WILL REQUIRE ALL WORK WITHIN 50 FEET OF TRACK CENTERLINE TO STOP AND ALL PERSONNEL TO CLEAR THE AREA WITHIN 25 FEET OF THE TRACK UNLESS AN APPROVED TEMPORARY CONSTRUCTION FENCE IS PRESENT. ALL EQUIPMENT MUST BE SECURED AND SHUT DOWN UNTIL THE TRAIN HAS FULLY CLEARED THE WORK AREA. NO CLAIM FOR DELAY WILL BE ALLOWED AS A RESULT OF RAILROAD FLAGGING DELAYS AND THE CONTRACTOR MUST PROVIDE FOR THESE CONTINGENCIES WHEN BIDDING THE PROJECT.

20. **RAILROAD REQUIREMENTS** THE CONTRACTOR SHALL ASSURE THAT ALL NMDOT REQUIREMENTS AS PROVIDED IN THE CONTRACTOR RIGHT-OF-ENTRY AGREEMENT WITH THE RAILROAD ARE STRICTLY ADHERED TO. ALL COSTS ASSOCIATED WITH ADHERING TO ALL NMRX REQUIREMENTS AND GUIDELINES AS SHOWN IN THE CONTRACT DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE COMPLETION OF THE PROJECT.

21. **TRAIN PROTECTION** THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING TRAIN TRAFFIC AND RAILS FROM CONSTRUCTION OPERATIONS. TIME EXTENSIONS WILL NOT BE GRANTED DUE TO NORMAL DELAYS IN OBTAINING RAILROAD APPROVAL. THE CONTRACTOR IS TO PROVIDE RMRTD TEN (10) WORKING DAYS NOTICE AND REQUEST FOR FLAGGING PROTECTION PRIOR TO SCHEDULED TIME TO WORK WITHIN RAILROAD RIGHT-OF-WAY.

LIST OF INCIDENTALS (NO ADDITIONAL PAYMENT ASSOCIATED):

LIST OF INCIDENTALS FOR PERMANENT SIGNING AND STRIPING	
A.	ALL SIGNING HARDWARE, INCLUDING, BUT NOT LIMITED TO, BRACKETS, BANDING, BUCKLES, FASTENERS, SCREWS, WASHERS, ETC.
B.	PAVEMENT MARKING LAYOUT
C.	SEAL APPLIED TO PAVEMENT AFTER STRIPE REMOVAL

22. **UTILITY COORDINATION:** PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS.

IF ANY UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES ARE SHOWN ON THESE DRAWINGS, THEY ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. IF ANY SUCH EXISTING LINES ARE SHOWN, THE LOCATION IS BASED UPON INFORMATION PROVIDED BY THE OWNER OF SAID UTILITY, AND THE INFORMATION MAY BE INCOMPLETE, OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES. THE ENGINEER HAS UNDERTAKEN NO FIELD VERIFICATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY HIS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. IN PLANNING AND CONDUCTING EXCAVATION, THE CONTRACTOR SHALL COMPLY WITH STATE STATUTES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING IN ADVANCE OF CONSTRUCTION OPERATIONS, IF OVERHEAD UTILITY LINES SUPPORT STRUCTURES, POLES, GUYS, ETC. ARE AN OBSTRUCTION TO CONSTRUCTION OPERATIONS. IF ANY OBSTRUCTIONS TO CONSTRUCTION OPERATIONS IS EVIDENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE APPROPRIATE UTILITY OWNER TO REMOVE OR SUPPORT THE UTILITY OBSTRUCTION. ANY COST ASSOCIATED WITH THIS EFFORT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO SEPERATE PAYMENT WILL BE MADE.

CONTRACTOR IS TO SUPPORT AND MAINTAIN THE INTEGRITY OF ALL UNDERGROUND TELEPHONE, ELECTRIC CABLES, AND CABLE TELEVISION UTILITIES AT NO ADDITIONAL COST TO THE OWNER. CABLE IS TO BE SUPPORTED EVERY 15' (MINIMUM). CONTRACTOR SHALL COORDINATE WITH AND MAKE NECESSARY PAYMENT (IF ANY) TO UTILITY OWNER FOR DE-ENERGIZATION OF CABLES OR SUPPORT OF CABLES BY THE UTILITY OWNER.

SUMMARY OF PERMANENT SIGNING AND STRIPING QUANTITIES

ITEM NO.	ITEM	UNIT	QUANTITY
701000	PANEL SIGN	SQ.FT.	469
701100	STEEL POST AND BASE POST FOR ALUMINUM PANEL SIGNS	LIN.FT.	850
703002	OBJECT MARKER TYPE 2	EACH	2
703003	OBJECT MARKER TYPE 3	EACH	14
704224	RETROREFLECTORIZED PLASTIC PAVEMENT STRIPE 24"	LIN.FT.	165
704700	HOT THERMOPLASTIC PAVEMENT MARKINGS 4"	LIN.FT.	51,300
704740	HOT THERMOPLASTIC PAVEMENT MARKING RAILROAD CROSSING	EACH	2
	* PORTABLE CHANGEABLE MESSAGE SIGN	EACH	2

* PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE INCIDENTAL TO ITEM NO. 618000 AND RETAINED BY THE CONTRACTOR

EMAIL:

PHONE:

DESIGNED BY:



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
GABALDON ROAD SIGNING AND STRIPING PERMANENT SIGNING AND STRIPING GENERAL NOTES & SUMMARY OF QUANTITIES			

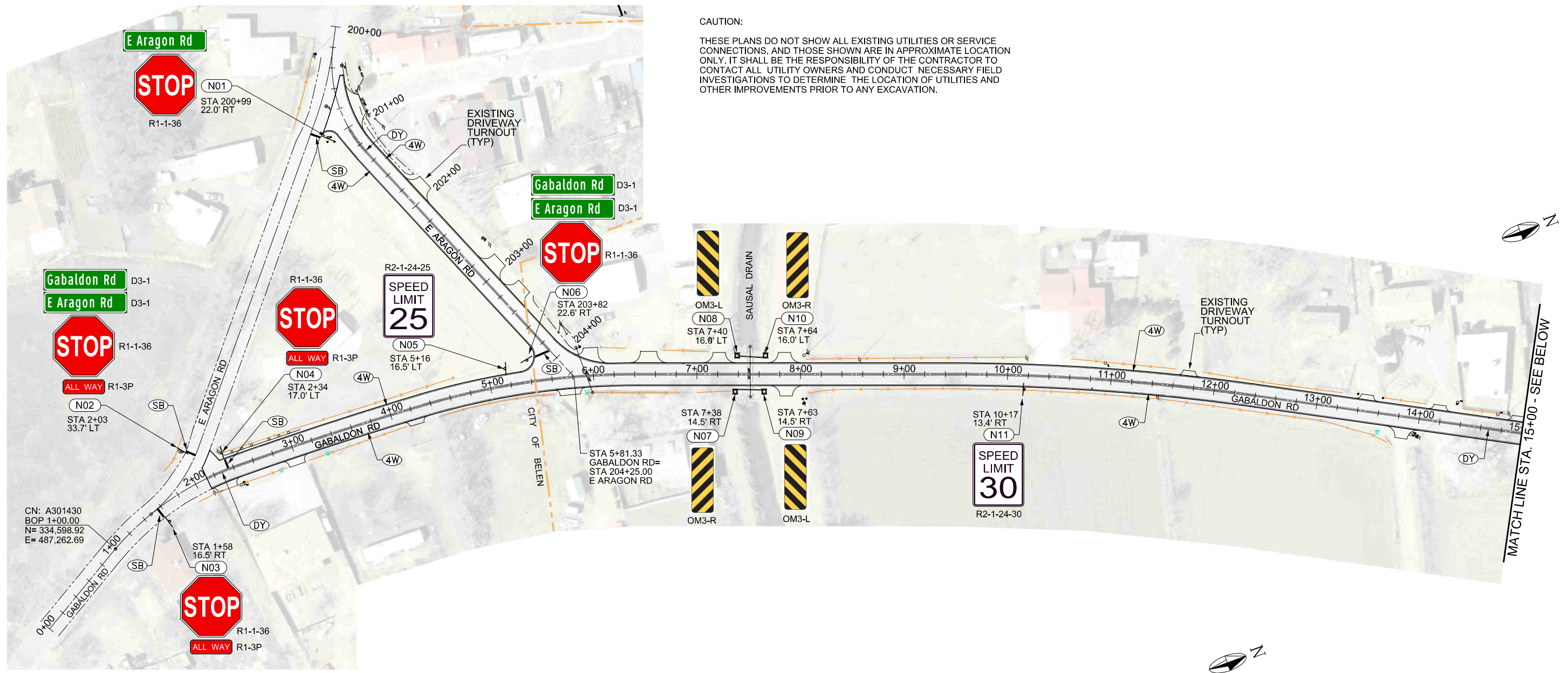
DRAWING SCALE: 1:NA

NEW MEXICO PROJECT NO. A301430

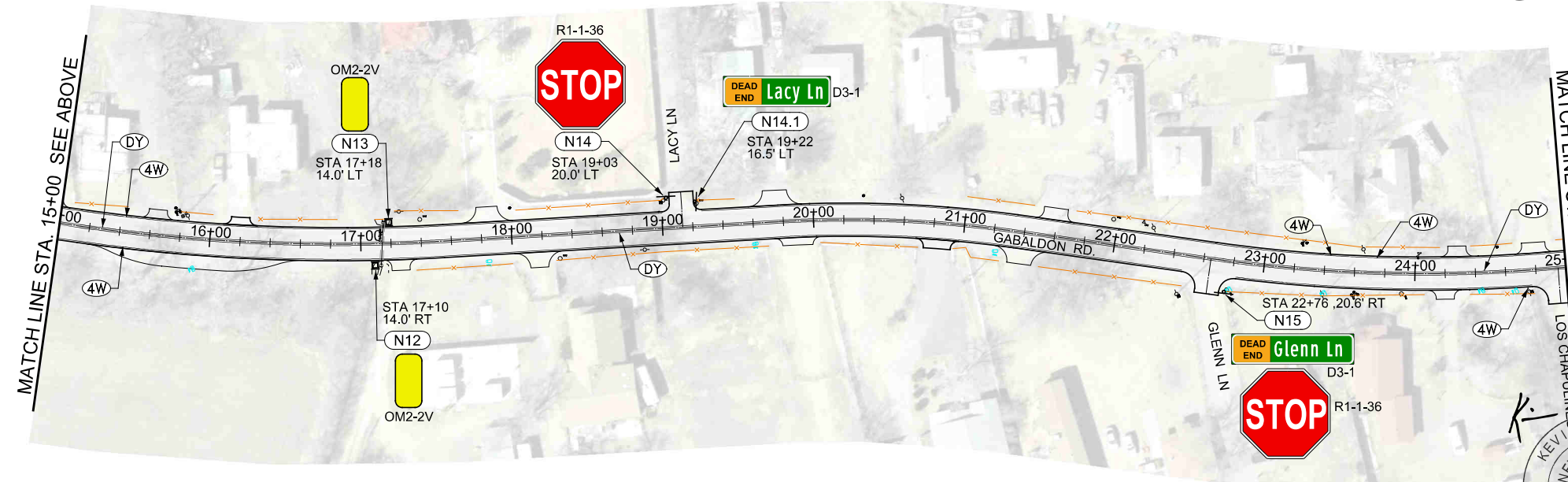
MOLZENCORBIN

SHEET NO. 7 - 1

CAUTION:
 THESE PLANS DO NOT SHOW ALL EXISTING UTILITIES OR SERVICE CONNECTIONS, AND THOSE SHOWN ARE IN APPROXIMATE LOCATION ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ALL UTILITY OWNERS AND CONDUCT NECESSARY FIELD INVESTIGATIONS TO DETERMINE THE LOCATION OF UTILITIES AND OTHER IMPROVEMENTS PRIOR TO ANY EXCAVATION.

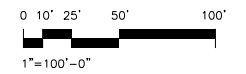


CN: A301430
 BOP 1+00.00
 N= 334,598.92
 E= 487,262.69



STRIPING KEY:

- (4W) 4" SOLID WHITE
- (DY) 4" DOUBLE SOLID YELLOW
- (SB) 24" SOLID WHITE STOP BAR
- (RR) RAILROAD CROSSING SYMBOL



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

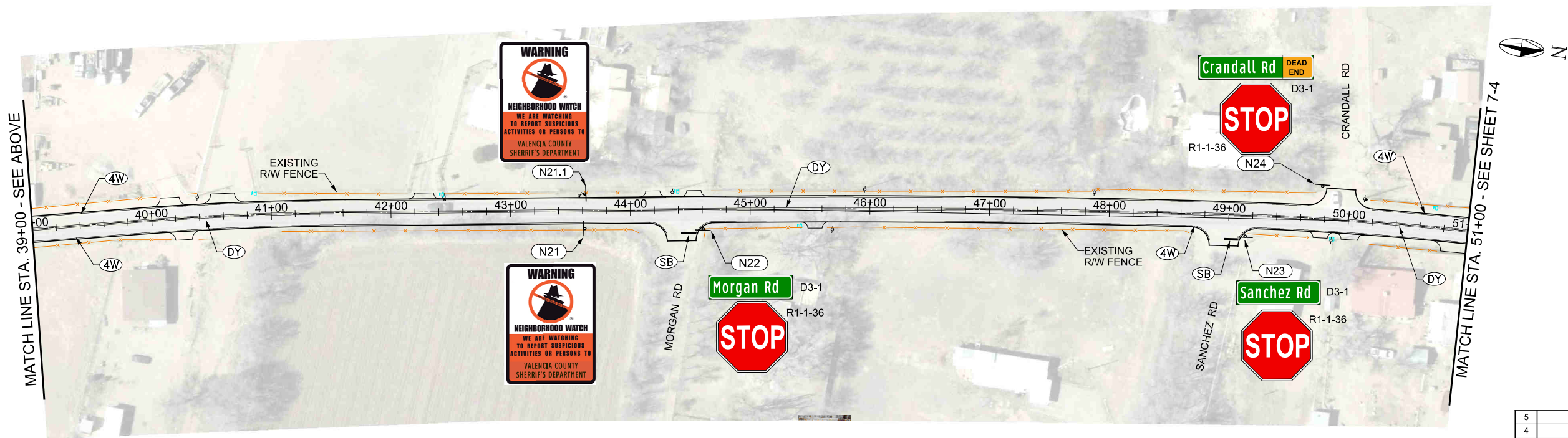
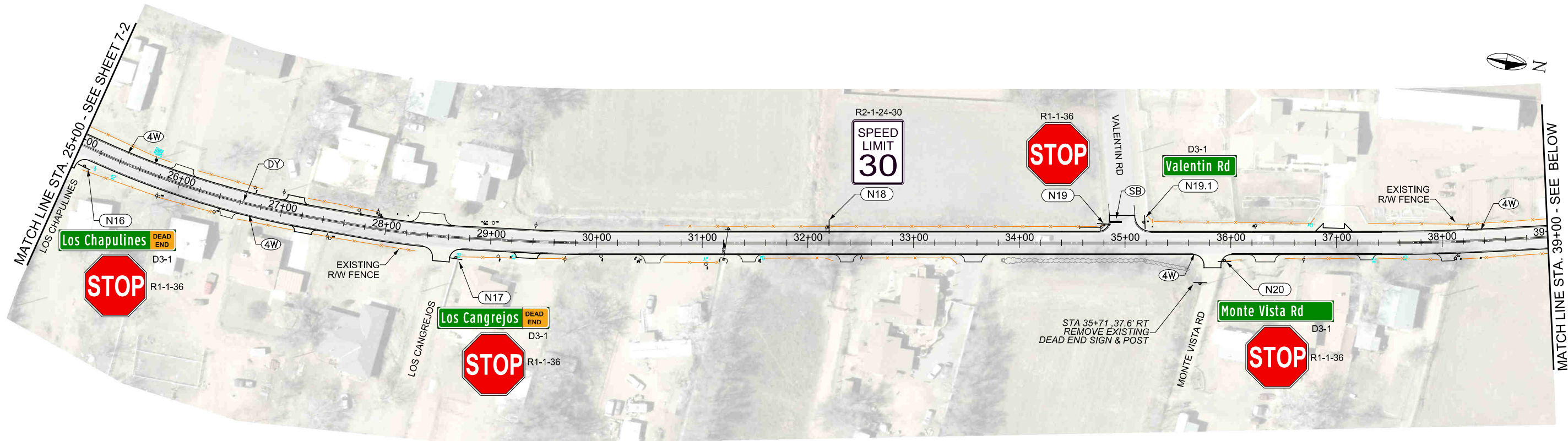
GABALDON ROAD SIGNING AND STRIPING

PERMANENT SIGNING AND STRIPING PLAN
 B.O.P TO STA. 25+00



DRAWING SCALE: 1:100

DESIGNED BY: PHONE: EMAIL:



STRIPING KEY:

- (4W)** 4" SOLID WHITE
- (DY)** 4" DOUBLE SOLID YELLOW
- (SB)** 24" SOLID WHITE STOP BAR
- (RR)** RAILROAD CROSSING SYMBOL

NO.	DESCRIPTION	DATE	BY
5			
4			
3			
2			
1			

**GABALDON ROAD
SIGNING AND STRIPING**

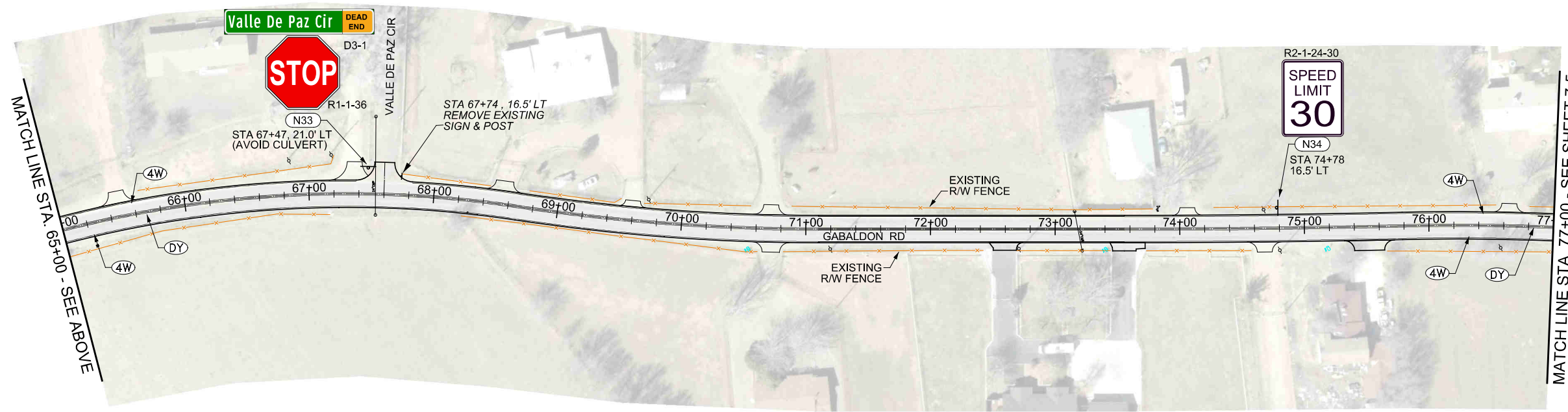
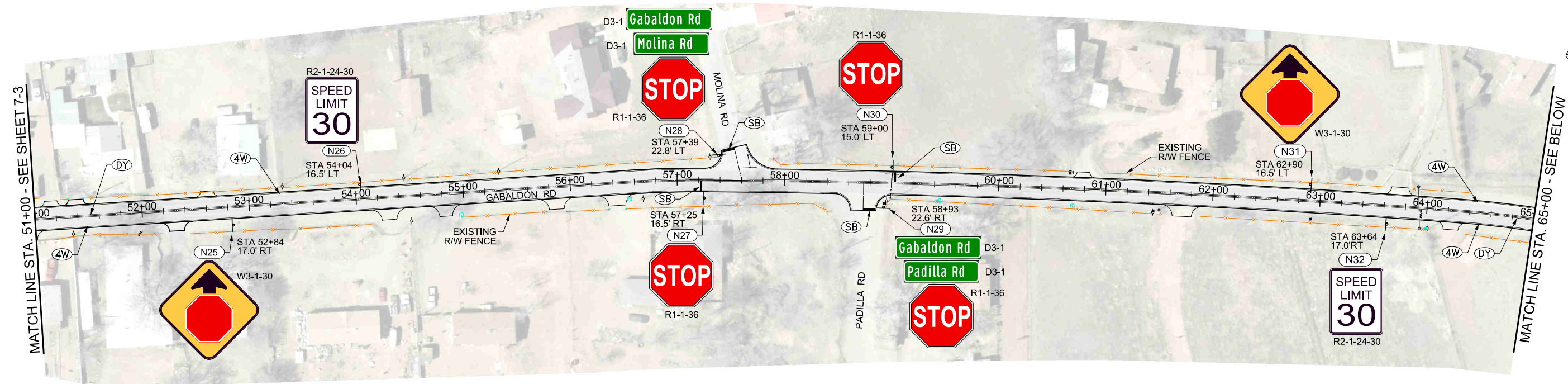
**PERMANENT SIGNING
AND STRIPING PLAN
STA. 25+00 TO STA. 51+00**



DRAWING SCALE: 1:100

NEW MEXICO PROJECT NO. A301430

DESIGNED BY: PHONE: EMAIL:



STRIPING KEY:

- (4W) 4" SOLID WHITE
- (DY) 4" DOUBLE SOLID YELLOW
- (SB) 24" SOLID WHITE STOP BAR
- (RR) RAILROAD CROSSING SYMBOL

NO.	DESCRIPTION	DATE	BY
5			
4			
3			
2			
1			

REVISIONS (OR CHANGE NOTICES)

**GABALDON ROAD
SIGNING AND STRIPING**

**PERMANENT SIGNING
AND STRIPING PLAN
STA. 51+00 TO STA. 77+00**

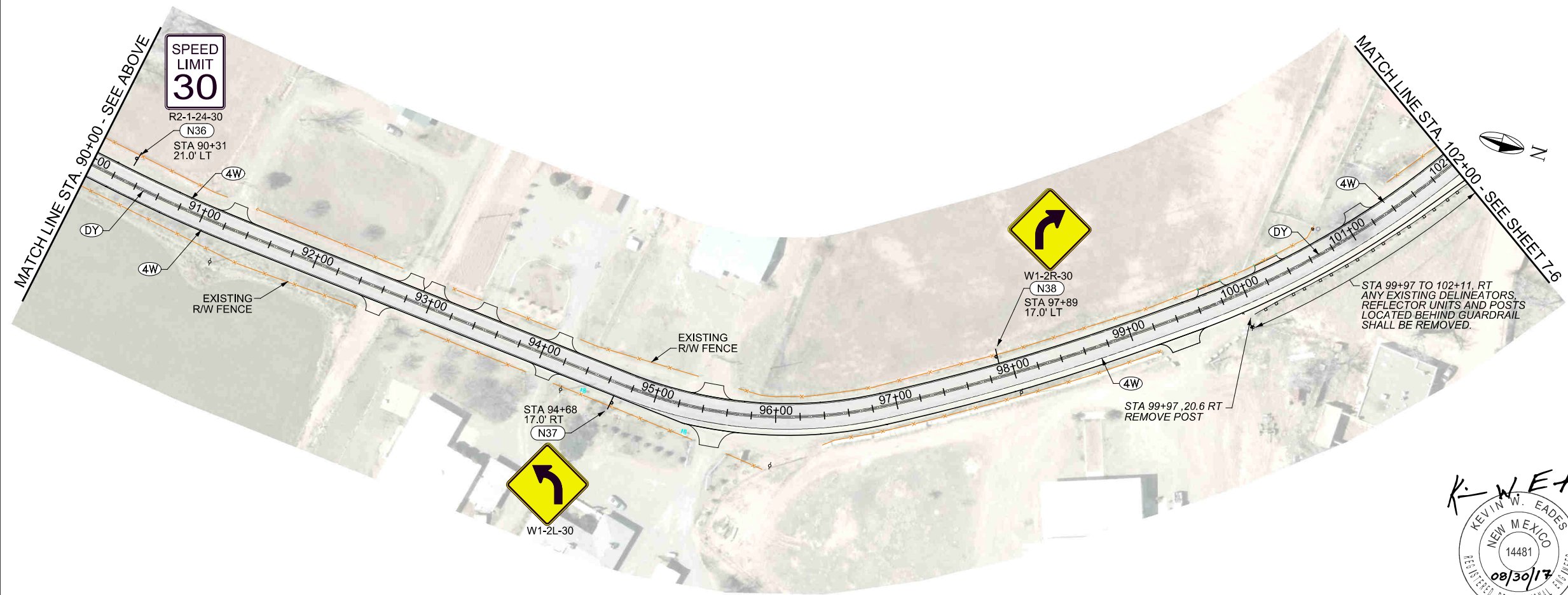
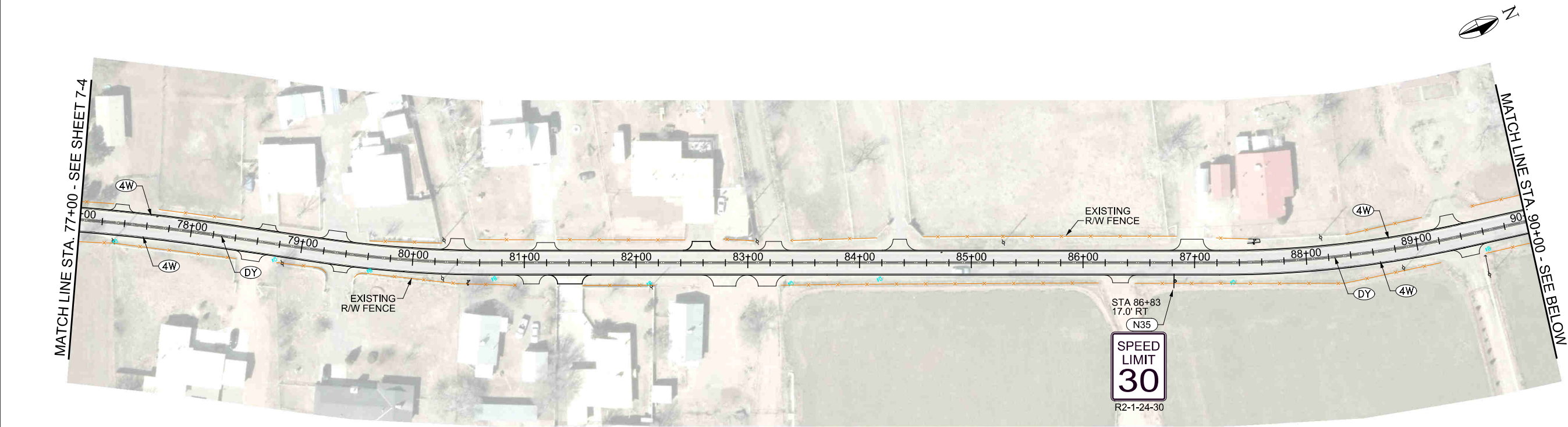


DRAWING SCALE: 1:100

NEW MEXICO PROJECT NO. A301430

MOLZENCORBIN SHEET NO. 7 - 4

DESIGNED BY: PHONE: EMAIL:



STRIPING KEY:

- (4W) 4" SOLID WHITE
- (DY) 4" DOUBLE SOLID YELLOW
- (SB) 24" SOLID WHITE STOP BAR
- (RR) RAILROAD CROSSING SYMBOL

NO.	DESCRIPTION	DATE	BY
5			
4			
3			
2			
1			

REVISIONS (OR CHANGE NOTICES)

**GABALDON ROAD
SIGNING AND STRIPING**

**PERMANENT SIGNING
AND STRIPING PLAN
STA. 77+00 TO STA. 102+00**

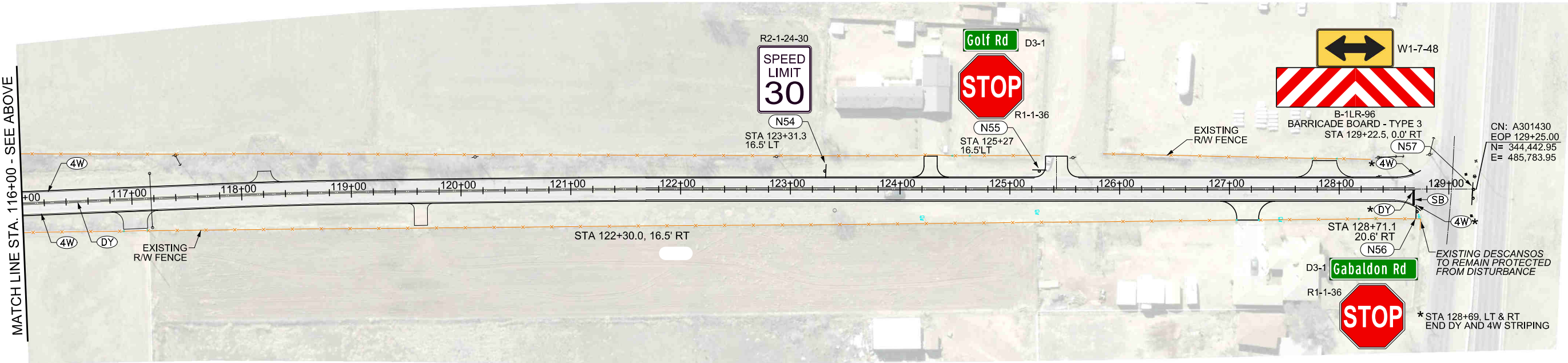
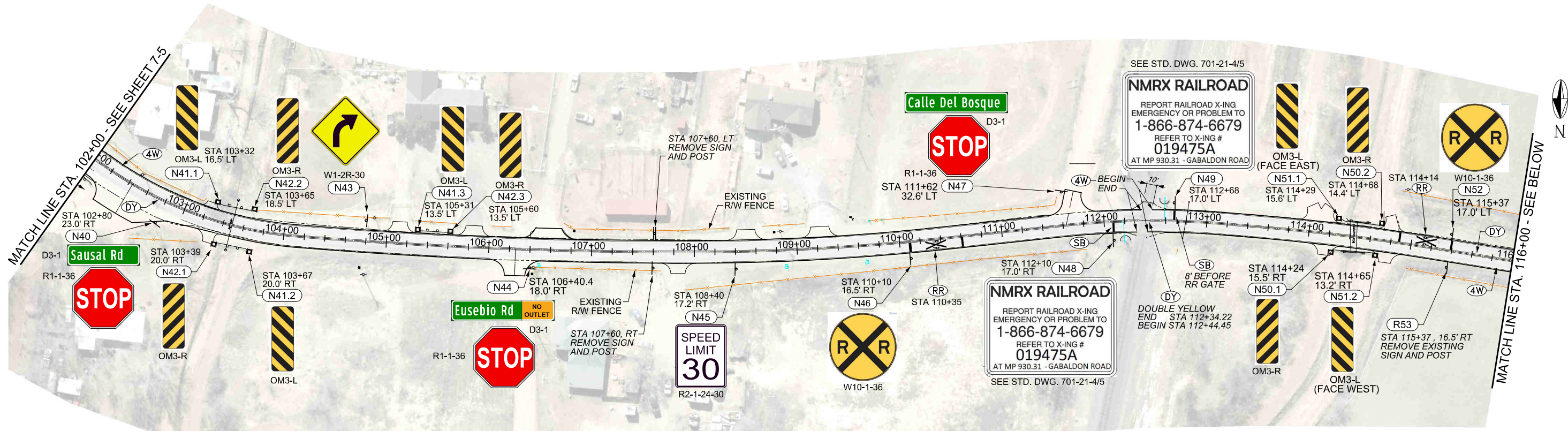


DRAWING SCALE: 1:100

NEW MEXICO PROJECT NO. A301430

MOLZENCORBIN SHEET NO. 7-5

DESIGNED BY: PHONE: EMAIL:



STRIPING KEY:

- 4" SOLID WHITE
- 4" DOUBLE SOLID YELLOW
- 24" SOLID WHITE STOP BAR
- RAILROAD CROSSING SYMBOL

5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

**GABALDON ROAD
SIGNING AND STRIPING**

**PERMANENT SIGNING
AND STRIPING PLAN
STA. 102+00 TO E.O.P.**



DRAWING SCALE: 1:100

NEW MEXICO PROJECT NO. A301430

NEW SIGN SCHEDULE

SHEET	INSTALL NO.	SIGN CODE	DESCRIPTION	HORIZ. WIDTH OF SIGN (IN)	VERT. LENGTH OF SIGN (IN)	NO. OF SIGNS	TOTAL SIGN AREA (SQ FT)	POST LENGTH				MOUNTING REQUIREMENTS					BASE POST	
								LT	CENTER	RT	TOTAL (FT)	U-CHANNEL		SQUARE TUBING (12 GAUGE)			NO.	TOTAL LENGTH (FT)
												1.12 LB/FT	1.75 X	2.00 X	2.25 X	2.5 X		
GABALDON RD																		
7-2	N 01	R1-1-36	STOP	36	36	1	9		11		11				X		1	3
		D3-1	E ARAGON RD	42	9	1	2.63											0
7-2	N 02	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	GABALDON RD	42	9	1	2.63											0
		D3-1	E ARAGON RD	42	9	1	2.63											0
		R1-3P	ALL WAY (PLAQUE)	18	6	1	0.75											0
7-2	N 03	R1-1-36	STOP	36	36	1	9		10		10				X		1	3
		R1-3P	ALL WAY (PLAQUE)	18	6	1	0.75											0
7-2	N 04	R1-1-36	STOP	36	36	1	9		10		10				X		1	3
		R1-3P	ALL WAY (PLAQUE)	18	6	1	0.75											0
7-2	N 05	R2-1-24-25	SPEED LIMIT 25	24	30	1	5		10		10				X		1	3
7-2	N 06	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	E ARAGON RD	42	9	1	2.63											0
		D3-1	GABALDON RD	42	9	1	2.63											0
7-2	N 07	OM3-R	OBJECT MARKER - TYPE 3	12	36	1	3		5		5				X		1	3
7-2	N 08	OM3-L	OBJECT MARKER - TYPE 3	12	36	1	3		5		5				X		1	3
7-2	N 09	OM3-L	OBJECT MARKER - TYPE 3	12	36	1	3		5		5				X		1	3
7-2	N 10	OM3-R	OBJECT MARKER - TYPE 3	12	36	1	3		5		5				X		1	3
7-2	N 11	R2-1-24-30	SPEED LIMIT 30	24	30	1	5		10		10				X		1	3
7-2	N 12	OM2-2V	OBJECT MARKER - TYPE 2	6	12	1	0.5		5		5	X						0
7-2	N 13	OM2-2V	OBJECT MARKER - TYPE 2	6	12	1	0.5		5		5	X						0
7-2	N 14	R1-1-36	STOP	36	36	1	9		10		10				X		1	3
7-2	N 14.1	D3-1	LACY LN W/ DEAD END	30	9	1	1.88		11		11				X		1	3
7-2	N 15	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	GLENN LN W/ DEAD END	36	9	1	2.25											0
7-3	N 16	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	LOS CHAPULINES W/ DEAD END	42	9	1	2.63											0
7-3	N 17	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	LOS CANGREJOS W/ DEAD END	42	9	1	2.63											0
7-3	N 18	R2-1-24-30	SPEED LIMIT 30	24	30	1	5		10		10				X		1	3
7-3	N 19	R1-1-36	STOP	36	36	1	9		10		10				X		1	3
7-3	N 19.1	D3-1	VALENTIN RD	42	9	1	2.63		10		10				X		1	3
7-3	N 20	R1-1-36	STOP	36	36	1	9		12		12				X		1	3
		D3-1	MONTE VISTA RD W/ DEAD END	42	9	1	2.63											0
7-3	N 21	SPECIAL	NEIGHBORHOOD WATCH	12	18	1	1.5		10		10				X		1	3
7-3	N 21.1	SPECIAL	NEIGHBORHOOD WATCH	12	18	1	1.5		10		10				X		1	3
7-3	N 22	R1-1-36	STOP	36	36	1	9		11		11				X		1	3
		D3-1	MORGAN RD	42	12	1	3.5											0

CONTINUED ON SHEET 7-7



TYPICAL STREET SIGN FACE DETAIL



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
GABALDON ROAD SIGNING AND STRIPING PERMANENT SIGNING AND STRIPING NEW SIGN SCHEDULE SHEET 1 OF 2			

DESIGNED BY: designer EMAIL: PHONE:

NEW SIGN SCHEDULE

SHEET	INSTALL NO.	SIGN CODE	DESCRIPTION	HORIZ. WIDTH OF SIGN (IN)	VERT. LENGTH OF SIGN (IN)	NO. OF SIGNS	TOTAL SIGN AREA (SQ FT)	POST LENGTH				MOUNTING REQUIREMENTS					BASE POST		
								LT	CENTER	RT	TOTAL (FT)	U-CHANNEL					NO.	TOTAL LENGTH (FT)	
												SQUARE TUBING (12 GAUGE)							
					1.12 LB/FT	1.75 X	2.00 X	2.25 X	2.5 X										
					1.75	2.00	2.25	2.5											
GABALDON RD																			
7-3	N 23	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	SANCHEZ RD	42	12	1	3.5											0	
7-3	N 24	R1-1-36	STOP	36	36	1	9			12					X		1	3	
		D3-1	CRANDALL RD W/ DEAD END	42	9	1	2.63								X		1	3	
7-4	N 25	W3-1-36	STOP AHEAD	36	36	1	9			10					X		1	3	
7-4	N 26	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-4	N 27	R1-1-36	STOP	36	36	1	9			10					X		1	3	
7-4	N 28	R1-1-36	STOP	36	36	1	9			12					X		1	3	
		D3-1	MOLINA RD	42	12	1	3.5											0	
		D3-1	GABALDON RD	42	9	1	2.63											0	
7-4	N 29	R1-1-36	STOP	36	36	1	9			12					X		1	3	
		D3-1	PADILLA RD	42	12	1	3.5											0	
		D3-1	GABALDON RD	42	9	1	2.63											0	
7-4	N 30	R1-1-36	STOP	36	36	1	9			10					X		1	3	
7-4	N 31	W3-1-36	STOP AHEAD	36	36	1	9			10					X		1	3	
7-4	N 32	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-4	N 33	R1-1-36	STOP	36	36	1	9			10					X		1	3	
7-4	N 33.1	D3-1	VALLE DE PAZ CIR W/ DEAD END	42	9	1	2.63			11					X		1	3	
7-4	N 34	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-5	N 35	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-5	N 36	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-5	N 37	W1-2L-30	CURVE LEFT	30	30	1	6.25			10					X		1	3	
7-5	N 38	W1-2R-30	CURVE RIGHT	30	30	1	6.25			10					X		1	3	
7-5	N 39	NOT USED	REFLECTORS ON EX. GUARDRAIL															0	
7-6	N 40	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	SAUSAL RD	42	12	1	3.5											0	
7-6	N 41	OM3-L	OBJECT MARKER - TYPE 3	12	36	3	9			5					X		3	9	
7-6	N 42	OM3-R	OBJECT MARKER - TYPE 3	12	36	3	9			5					X		3	9	
7-6	N 43	W1-2R-30	CURVE RIGHT	30	30	1	6.25			10					X		1	3	
7-6	N 44	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	EUSEBIO RD W/ NO OUTLET	42	12	1	3.5											0	
7-6	N 45	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-6	N 46	W10-1-36	RAILROAD ADVANCE WARNING	36	36	1	7.06			10					X		1	3	
7-6	N 47	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	CALLE DEL BOSQUE	42	9	1	2.63											0	
7-6	N 48	701-21-4/5	EMERGENCY NOTIFICATION SIGN	24	18	2	6			10					X		2	6	
7-6	N 49	701-21-4/5	EMERGENCY NOTIFICATION SIGN	24	18	2	6			10					X		2	6	
7-6	N 50	OM3-L	OBJECT MARKER - TYPE 3	12	36	2	6			5					X		2	6	
7-6	N 51	OM3-R	OBJECT MARKER - TYPE 3	12	36	2	6			5					X		2	6	
7-6	N 52	W10-1-36	RAILROAD ADVANCE WARNING	36	36	1	7.06			10					X		1	3	
7-6	N 53		NOT USED															0	
7-6	N 54	R2-1-24-30	SPEED LIMIT 30	24	30	1	5			10					X		1	3	
7-6	N 55	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	GOLF RD	30	12	1	2.5											0	
7-6	N 56	R1-1-36	STOP	36	36	1	9			11					X		1	3	
		D3-1	GABALDON RD	42	9	1	2.63											0	
7-6	N 57	B-1LR-96	BARRICADE BOARD - TYPE 3	96	8	3	16	12	12	12	36				X		3	9	
		W1-7-48	TWO-DIRECTION LARGE ARROW	48	24	1	8			2					X			0	
							PROJECT TOTAL	468.12					646						204
							PROJECT USE	469					646						204



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

**GABALDON ROAD
SIGNING AND STRIPING**

**PERMANENT SIGNING AND STRIPING
NEW SIGN SCHEDULE
SHEET 2 OF 2**

DESIGNED BY: designer PHONE: EMAIL:

DRAWING SCALE: 1:NA

STRIPING SCHEDULE

SHEET NO.	STATION TO STATION	LENGTH (LIN. FT.)			**	**	
			703002	703003	704224	704700	704740
			OBJECT MARKER TYPE 2 (EA)	OBJECT MARKER TYPE 3 (EA)	24" PLASTIC PAVEMENT STRIPE (FT.)	4" PAVEMENT MARKINGS (FT.)	RAILROAD CROSSING EA
7-2	01+00.00 - 25+00.00	2,400	2	4	55	9,600	
7-3	25+00.00 - 51+00.00	2,600			33	10,400	
7-4	51+00.00 - 77+00.00	2,600			44	10,400	
7-5	77+00.00 - 102+00.00	2,500				10,000	
7-6	102+00.00 - 129+25.00	2,725		10	33	10,900	2
PROJECT TOTAL			2	14	165	51,300	2
PROJECT USE			2	14	165	51300	2

* FOR CONTRACTOR INFORMATION ONLY
 ** HOT THERMOPLASTIC PAVEMENT MARKINGS

EMAIL:

PHONE:

DESIGNED BY: desimer



5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

**GABALDON ROAD
SIGNING AND STRIPING**

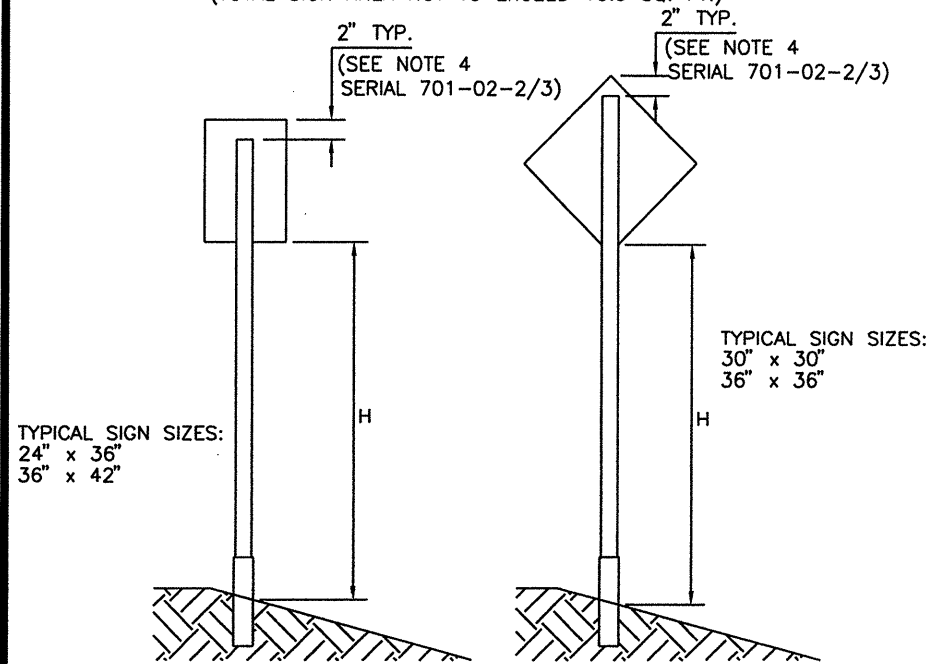
**PERMANENT SIGNING AND STRIPING
STRIPING SCHEDULE**

DRAWING SCALE: 1: N/A

NEW MEXICO PROJECT NO. A301430

SINGLE POST INSTALLATION

(TOTAL SIGN AREA NOT TO EXCEED 10.5 SQ. FT.)



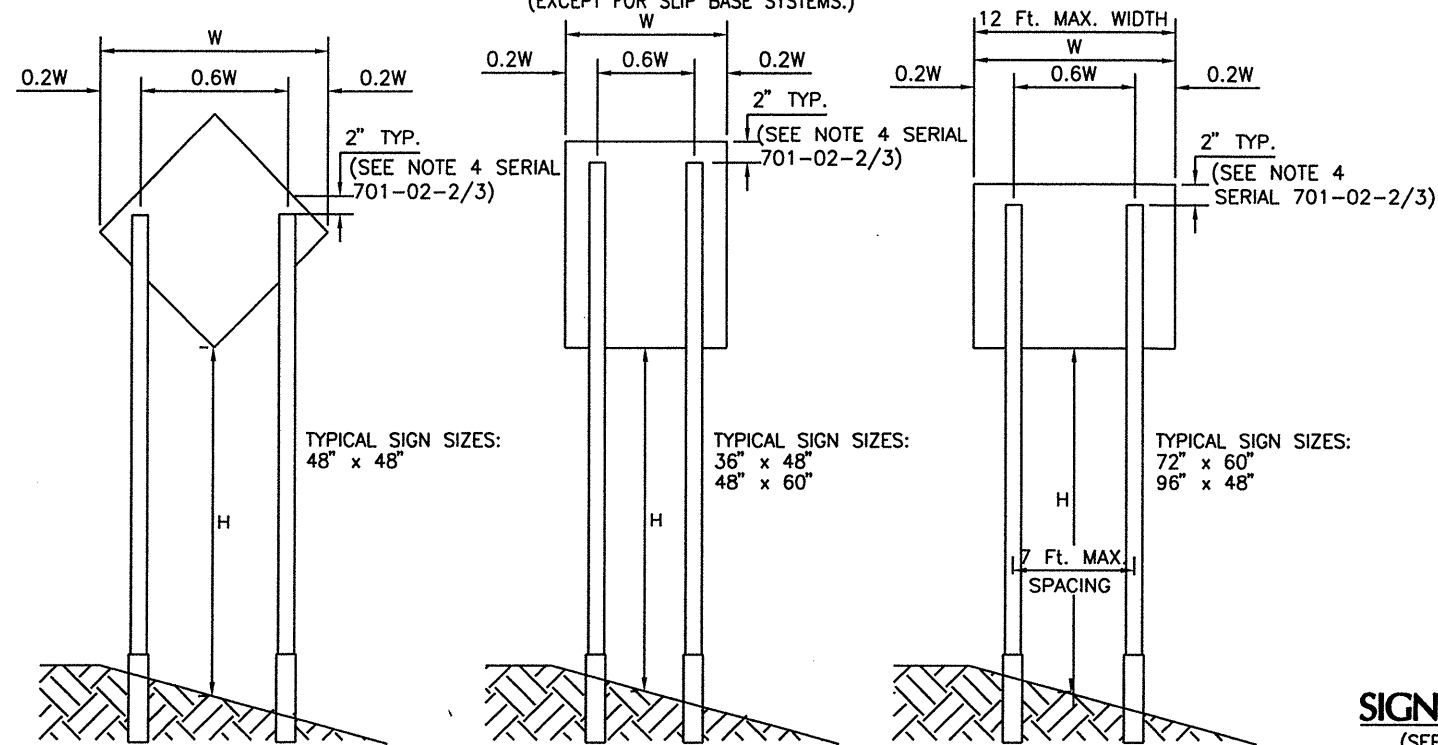
SIGN POST REQUIREMENTS

(SEE NOTES 1 & 3, SHT. 701-02-2/3)

POST TYPE	POST SIZE	MAX. CLEAR HEIGHT, H (FT.)	MAX. SIGN AREA (SQ. FT.)
SQUARE TUBING	1.75" X 1.75" (12 GA.)	9	5
SQUARE TUBING	1.75" X 1.75" (12 GA.)	8	6
SQUARE TUBING	1.75" X 1.75" (12 GA.)	7	7
SQUARE TUBING	2.00" X 2.00" (12 GA.)	9	8
SQUARE TUBING	2.00" X 2.00" (12 GA.)	8	9
SQUARE TUBING	2.00" X 2.00" (12 GA.)	7	10
SQUARE TUBING	2.25" X 2.25" (12 GA.)	9	10.5
SQUARE TUBING	2.25" X 2.25" (12 GA.)	10	10.5

DOUBLE POST INSTALLATION

(TOTAL SIGN AREA NOT TO EXCEED 30 SQ. FT.)
(EXCEPT FOR SLIP BASE SYSTEMS.)



SIGN POST REQUIREMENTS

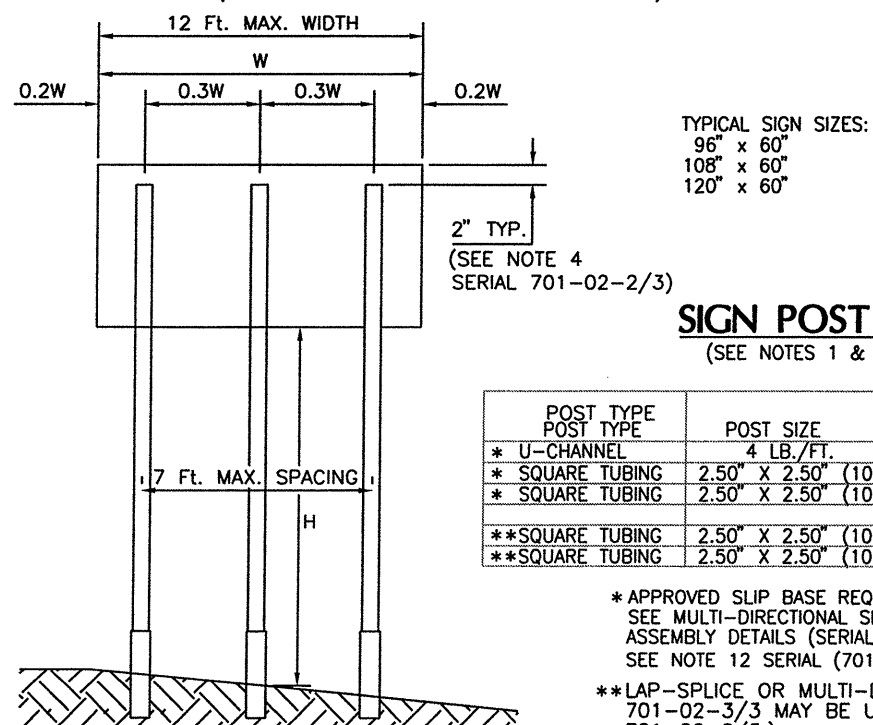
(SEE NOTES 1 & 2, SERIAL 701-02-2/3)

POST TYPE	POST SIZE	MAX. CLEAR HEIGHT, H (FT.)	MAX. SIGN AREA (SQ. FT.)
SQUARE TUBING	2.00" X 2.00" (12 GA.)	11	13
SQUARE TUBING	2.00" X 2.00" (12 GA.)	9	15
SQUARE TUBING	2.00" X 2.00" (12 GA.)	8	16
SQUARE TUBING	2.25" X 2.25" (12 GA.)	7	20
* U-CHANNEL	4 LB./FT.	11	25
* U-CHANNEL	4 LB./FT.	9	30
* SQUARE TUBING	2.50" X 2.50" (10 GA.)	7	36

* APPROVED SLIP BASE REQUIRED WITH THIS INSTALLATION
SEE MULTI-DIRECTIONAL SLIP BASE
ASSEMBLY DETAILS (SERIAL 701-02-3/3)
SEE NOTE 12 SERIAL (701-02-2/3)

TRIPLE POST INSTALLATION

(TOTAL SIGN AREA NOT TO EXCEED 50 SQ. FT.)



SIGN POST REQUIREMENTS

(SEE NOTES 1 & 2, SERIAL 701-02-2/3)

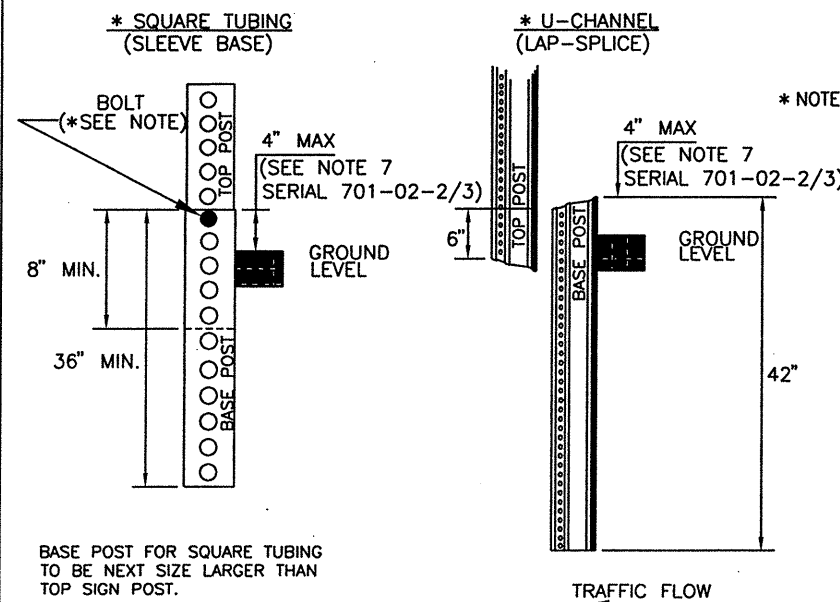
POST TYPE	POST SIZE	MAX. CLEAR HEIGHT, H (FT.)	MAX. SIGN AREA (SQ. FT.)
* U-CHANNEL	4 LB./FT.	11	36
* SQUARE TUBING	2.50" X 2.50" (10 GA.)	9	45
* SQUARE TUBING	2.50" X 2.50" (10 GA.)	7	50
**SQUARE TUBING	2.50" X 2.50" (10 GA.)	11	36
**SQUARE TUBING	2.50" X 2.50" (10 GA.)	9	45

* APPROVED SLIP BASE REQUIRED WITH THIS INSTALLATION
SEE MULTI-DIRECTIONAL SLIP BASE
ASSEMBLY DETAILS (SERIAL 701-02-3/3)
SEE NOTE 12 SERIAL (701-02-2/3)

**LAP-SPLICE OR MULTI-DIRECTIONAL SLIP BASE (SERIAL 701-02-3/3) MAY BE USED. SEE NOTE 12 (SERIAL 701-02-2/3.)

BASE POST INSTALLATION DETAILS FOR SQUARE TUBING AND U-CHANNEL SYSTEMS

(SEE SERIAL. 701-02-3/3 FOR MULTI-DIRECTIONAL SLIP BASE SYSTEMS)



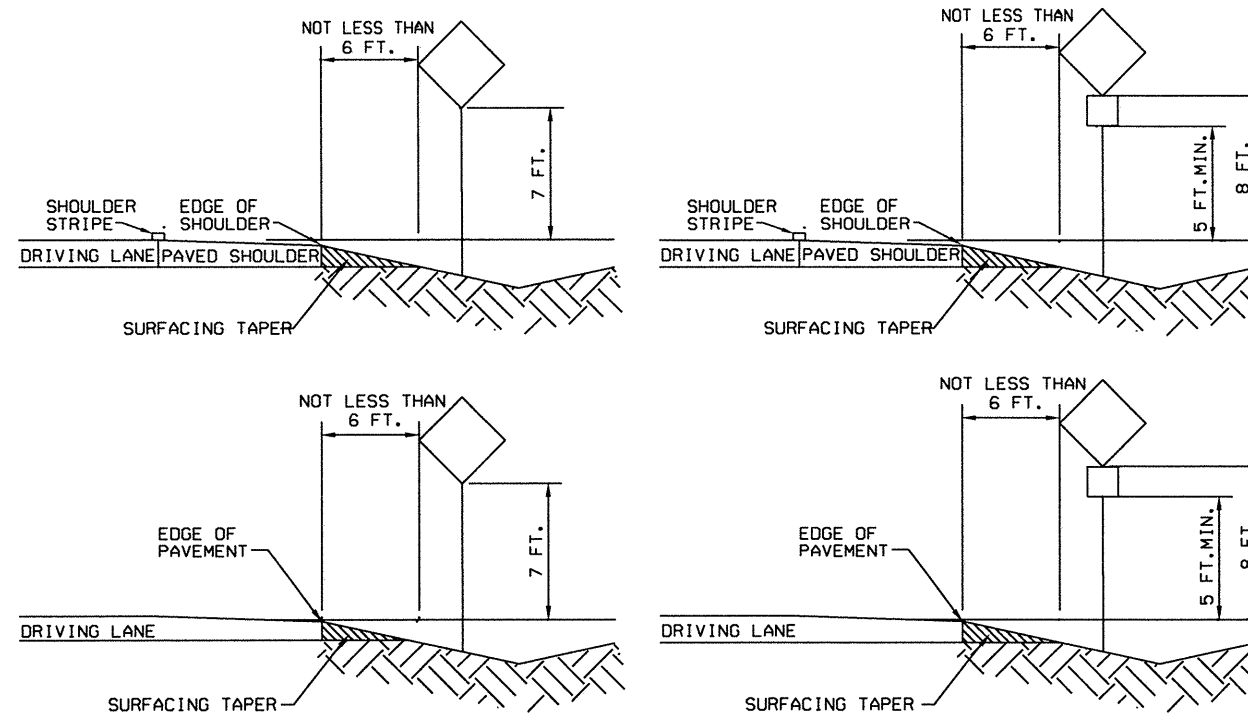
* NOTE: SEE MANUFACTURER'S DRAWINGS FOR SPECIFIC ASSEMBLY INFORMATION (POST TO BASE POST OVERLAP), INCLUDING TYPES OF NUTS, BOLTS, WASHERS, AND OTHER PARTS REQUIRED FOR PRODUCT USE.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO			
DEPARTMENT OF TRANSPORTATION			
STANDARD DRAWING			
SMALL SIGN SUPPORT			
INSTALLATION DETAILS			
APPROVED	DESIGN ENGINEER		DATE
	DESIGNED BY	DRAWN BY	CHECKED BY
701-02-1/3		SHEET 1 OF 3	

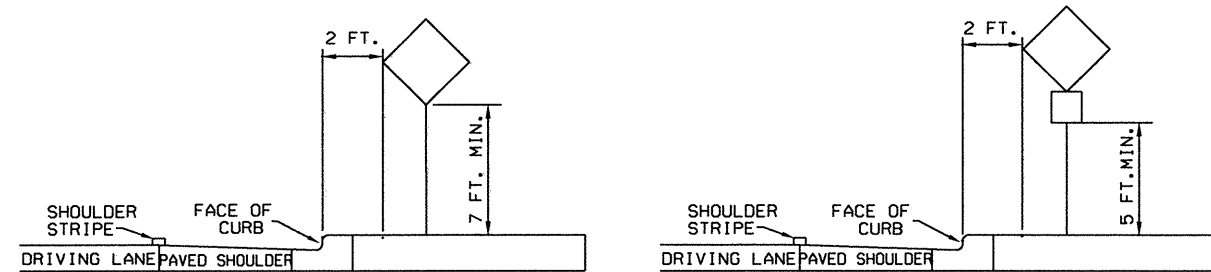
HORIZONTAL AND VERTICAL CLEARANCES

(SEE NOTE 9)

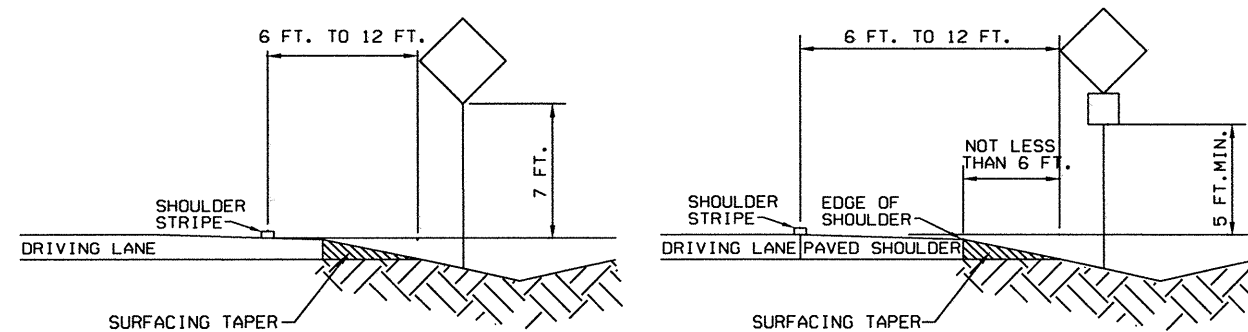
FREEWAYS/EXPRESSWAYS & RURAL AREAS



URBAN (BUSINESS, COMMERCIAL, & RESIDENTIAL AREAS) CONSTRUCTION ZONES IN URBAN AREAS



CONSTRUCTION ZONES IN FREEWAYS/EXPRESSWAYS AND RURAL AREAS



GENERAL NOTES:

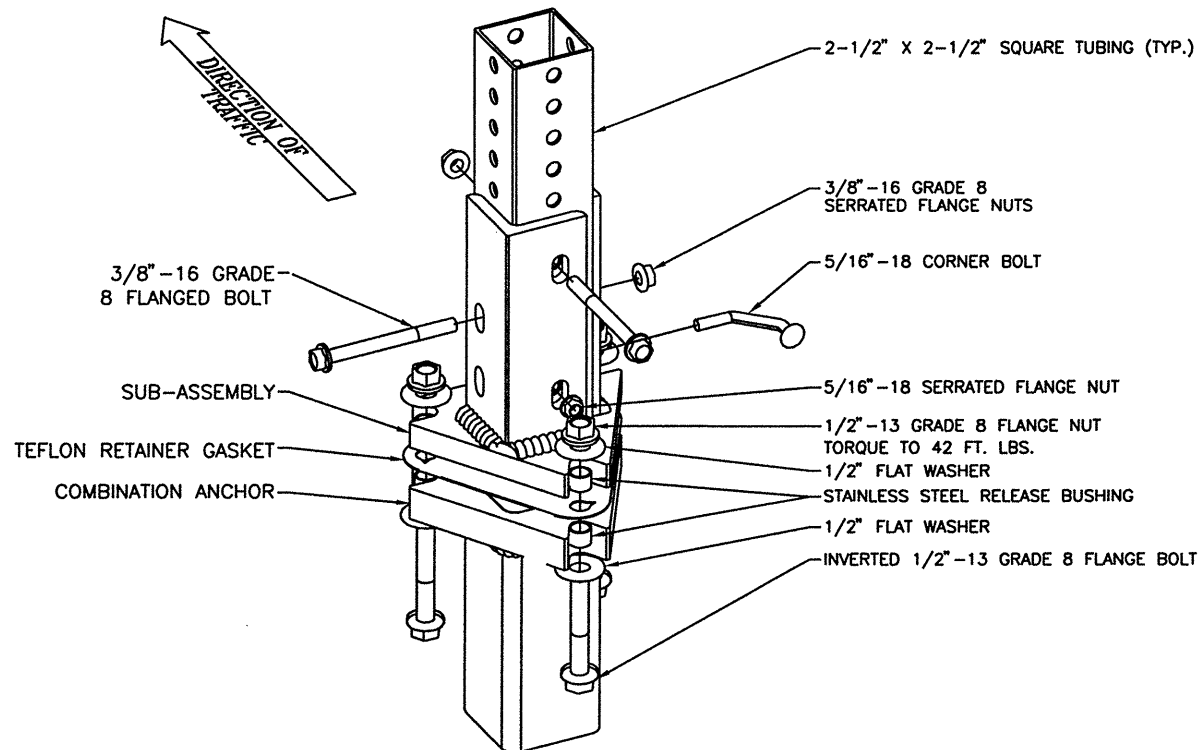
1. ALL SQUARE TUBING SIGN POST REQUIREMENTS ARE BASED ON A 10 OR 12 GAUGE THICKNESS, ASTM A570 GRADE 50 STEEL, A MINIMUM YIELD STRENGTH OF 60,000 PSI AND A 70 MPH WIND LOAD. ALL U-CHANNEL SIGN POSTS REQUIREMENTS ARE BASED ON A MINIMUM YIELD STRENGTH OF 80,000 PSI AND 85 MPH WIND LOAD. SEE THE MUTCD & STANDARD HIGHWAY SIGNS MANUAL (CURRENT EDITION) FOR FURTHER GUIDANCE.
2. FOR CONSTRUCTION SIGNING & PERMANENT SINGLE AND TRIPLE POST INSTALLATIONS, SMALLER POST CROSS SECTIONS MAY BE USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND THE RECOMMENDATIONS DETAILED IN NOTE 1.
3. TOP EDGE OF POSTS SHALL NOT EXTEND PAST TOP EDGE OF SIGN.
4. STEEL POSTS, BASE POSTS, AND SLIP BASES FOR ALUMINUM PANEL SIGNS SHALL BE SELECTED FROM THE DEPARTMENT'S APPROVED PRODUCT LIST. ALL SIGNS MOUNTED WITHIN THE CLEAR ZONE SHALL BE MOUNTED ON A NCHRP REPORT 350 APPROVED SIGN POST/BASE POST BREAKAWAY SYSTEM UNLESS INSTALLATION IS LOCATED BEHIND A NON-GATING LONGITUDINAL BARRIER. OTHER INSTALLATIONS, CONFIGURATIONS OR SYSTEMS NOT SHOWN MAY BE USED AS RECOMMENDED BY THE MANUFACTURER WITH APPROVAL OF THE DISTRICT TRAFFIC ENGINEER.
5. FOR INSTALLATIONS ON WEAK (SOFT) SOIL, SOIL PLATES SHALL BE USED AS RECOMMENDED BY THE MANUFACTURER. PAYMENT FOR SOIL PLATES SHALL BE INCIDENTAL TO THE SIGN INSTALLATION.
6. BASE POSTS SHALL NOT EXTEND MORE THAN 4' ABOVE GROUND LEVEL AND SHALL BE OF THE SAME WEIGHT/GAUGE AND TYPE AS THE SIGN POST.
7. INTERMIXING OF U-CHANNEL AND SQUARE TUBING POSTS, POSTS OF DIFFERENT WEIGHTS/GAUGES OR PRODUCT BRANDS IS NOT ALLOWED EXCEPT WHERE RECOMMENDED BY THE MANUFACTURER.
8. HORIZONTAL CLEARANCES APPLY TO INSTALLATIONS ON LEFT AND RIGHT SIDE OF ROADWAY.
9. SUPPLEMENTAL SIGNS SHALL NOT BE ATTACHED DIRECTLY TO PRIMARY PANELS ON EITHER PERMANENT OR CONSTRUCTION SIGNING INSTALLATIONS.
10. SPACING BETWEEN SUPPLEMENTAL PANELS AND PRIMARY PANELS SHALL NOT EXCEED 6'.
11. SIGN PANELS PLACED PARALLEL TO TRAFFIC SHALL BE MOUNTED ON A MULTI-DIRECTIONAL BREAKAWAY SYSTEM. (SEE SERIAL 701-02-3/3)

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
SMALL SIGN SUPPORT INSTALLATION DETAILS			
APPROVED		DATE	
<i>[Signature]</i>		2-3-02	
DESIGNED BY		DRAWN BY	
CHECKED BY		DATE	
701-02-2/3 SHEET 2 OF 3			

MULTI-DIRECTIONAL SLIP BASE DETAILS

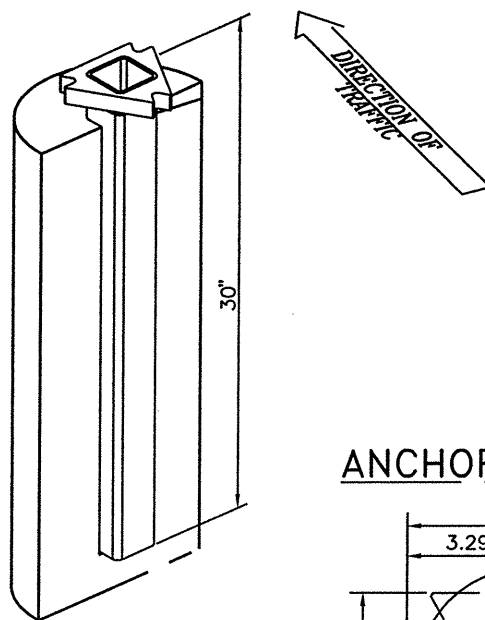
SEE APPROVED PRODUCT LIST FOR APPROVED EQUALS.

SQUARE TUBING SLIP BASE



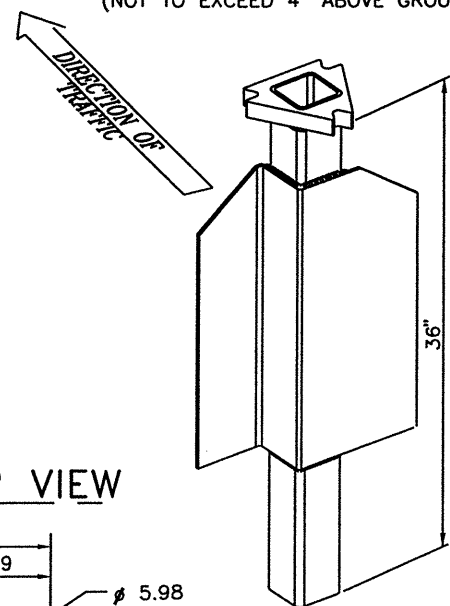
DETAIL "A" - CONCRETE ANCHOR

(NOT TO EXCEED 4" ABOVE GROUND LEVEL)

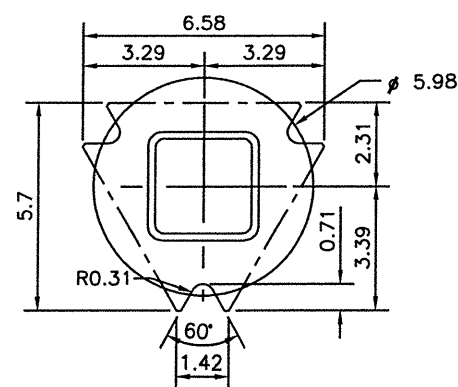


DETAIL "B" - SOIL ANCHOR

(NOT TO EXCEED 4" ABOVE GROUND LEVEL)



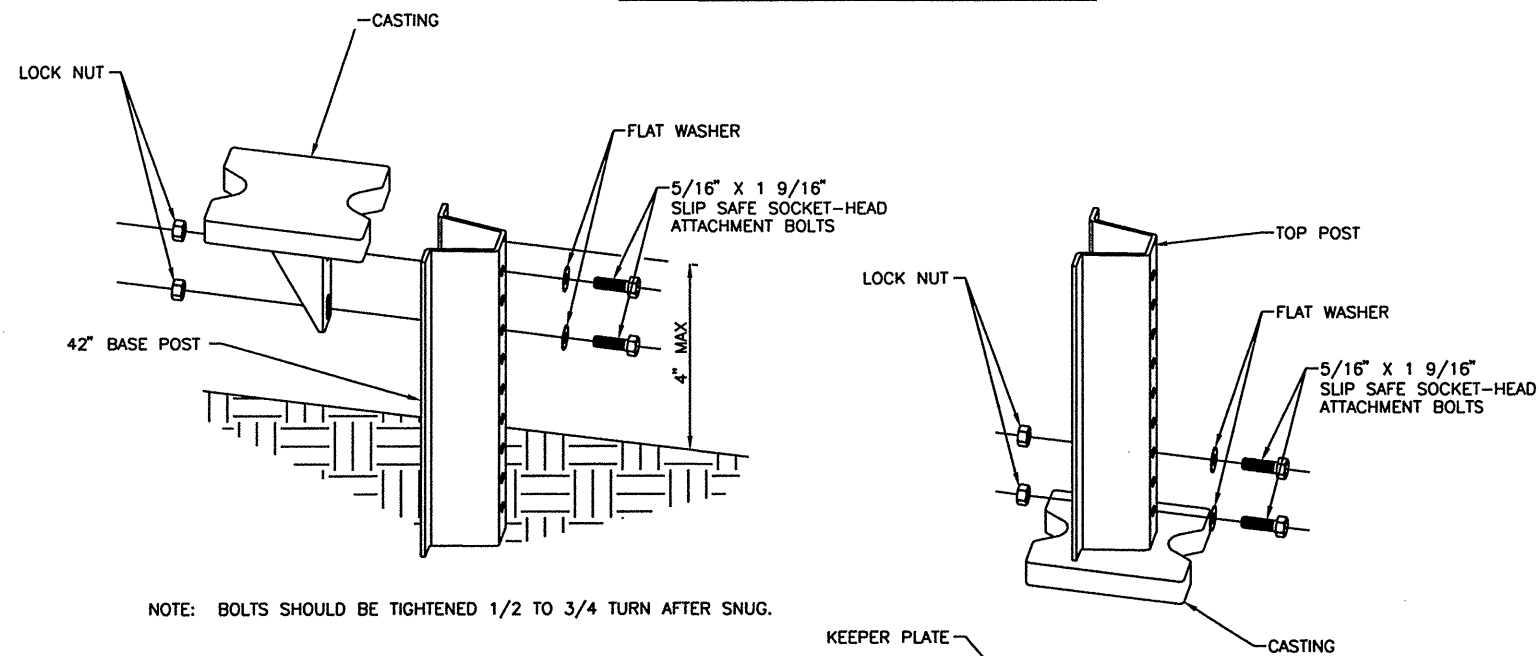
ANCHOR TOP VIEW



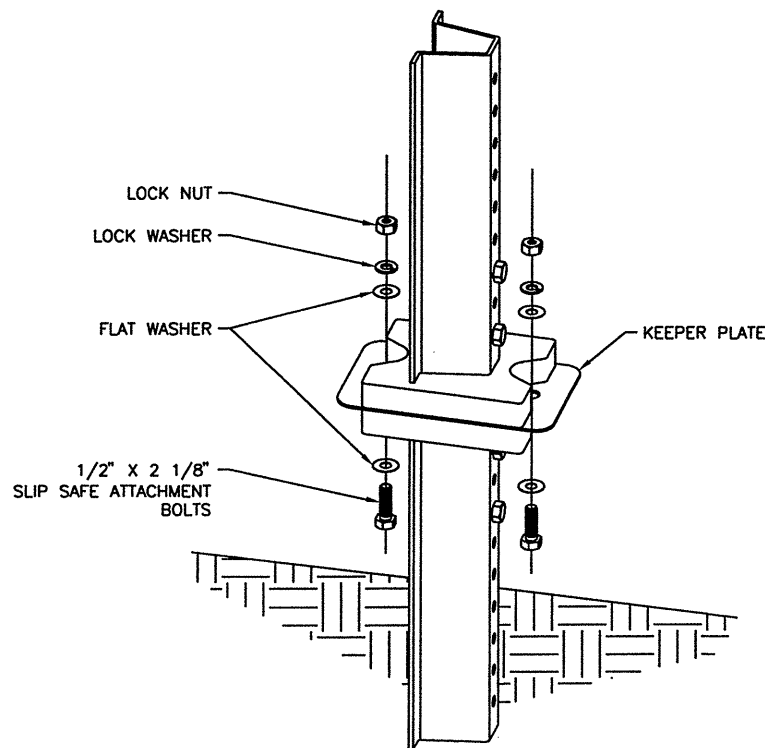
NOTES:

1. CONCRETE SHOULD BE A MINIMUM OF 12" IN DIAMETER AND 30" DEEP.
2. CLASS "A" CONCRETE SHALL BE USED. THE COST SHALL BE INCIDENTAL TO THE PRICE OF STEEL POSTS & BASE POSTS FOR ALUMINUM PANEL SIGNS.

U-CHANNEL SLIP BASE



NOTE: BOLTS SHOULD BE TIGHTENED 1/2 TO 3/4 TURN AFTER SNUG.

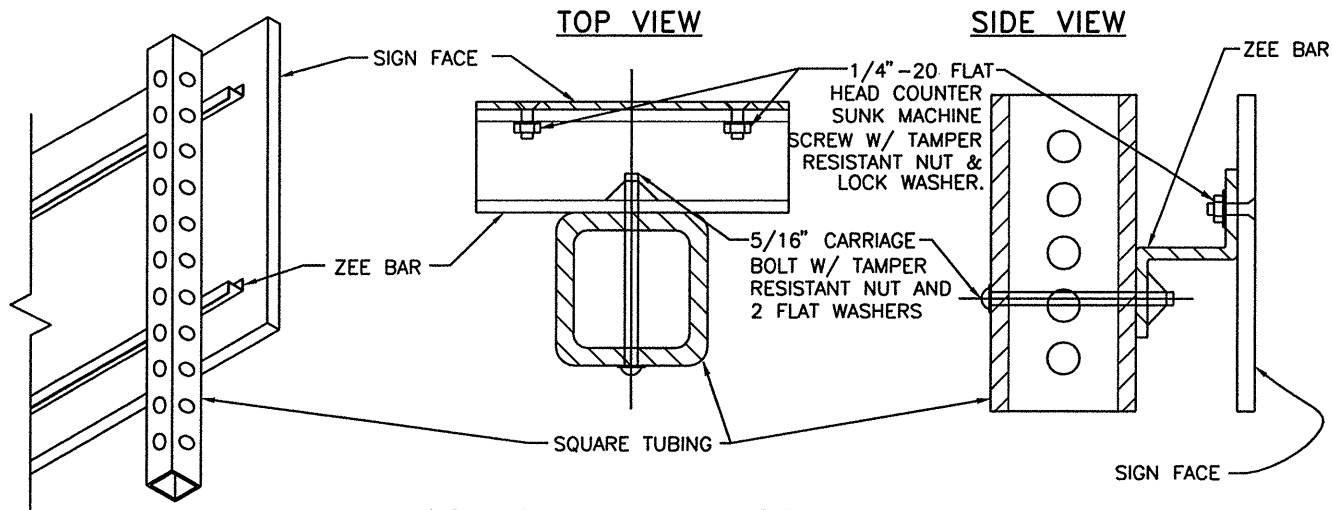


NOTE: TIGHTEN NUT 1/3 TO 1/2 TURN AFTER SNUG.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO			
DEPARTMENT OF TRANSPORTATION			
STANDARD DRAWING			
MULTI-DIRECTIONAL SLIP			
BASE POST DETAILS			
APPROVED	DESIGN ENGINEER		DATE
DESIGNED BY		DRAWN BY	CHECKED BY
701-02-3/3		SHEET 3 OF 3	

TYPICAL

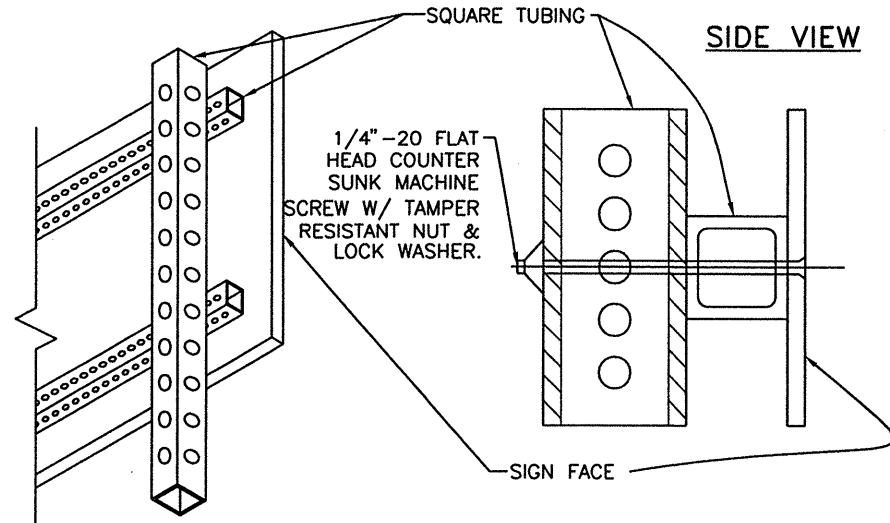
(SEE NOTES)



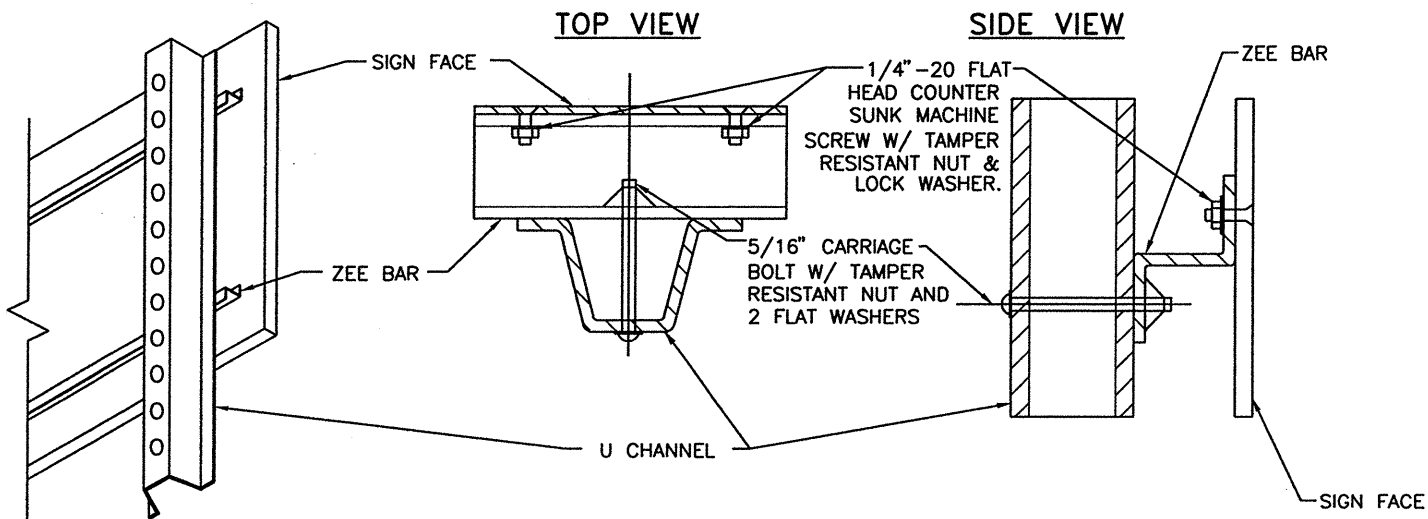
SQUARE TUBING POSTS

ALTERNATE

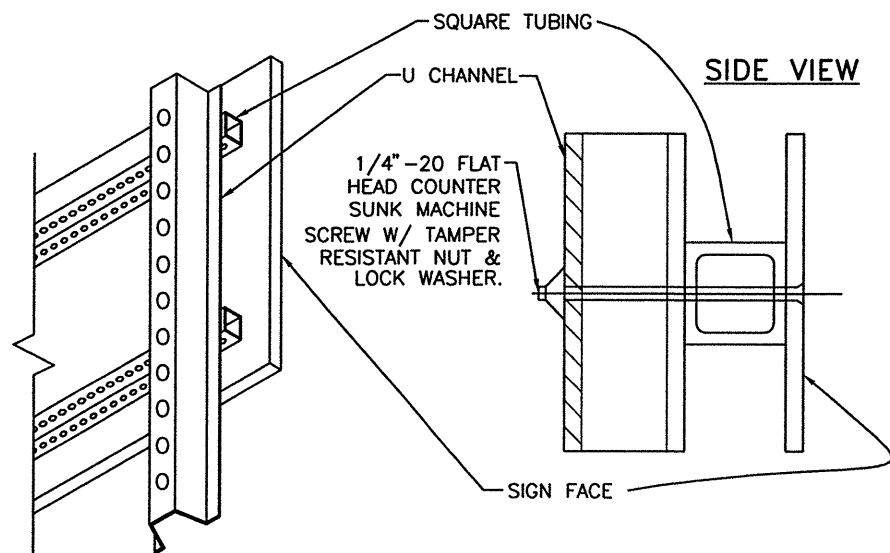
(SEE NOTES)



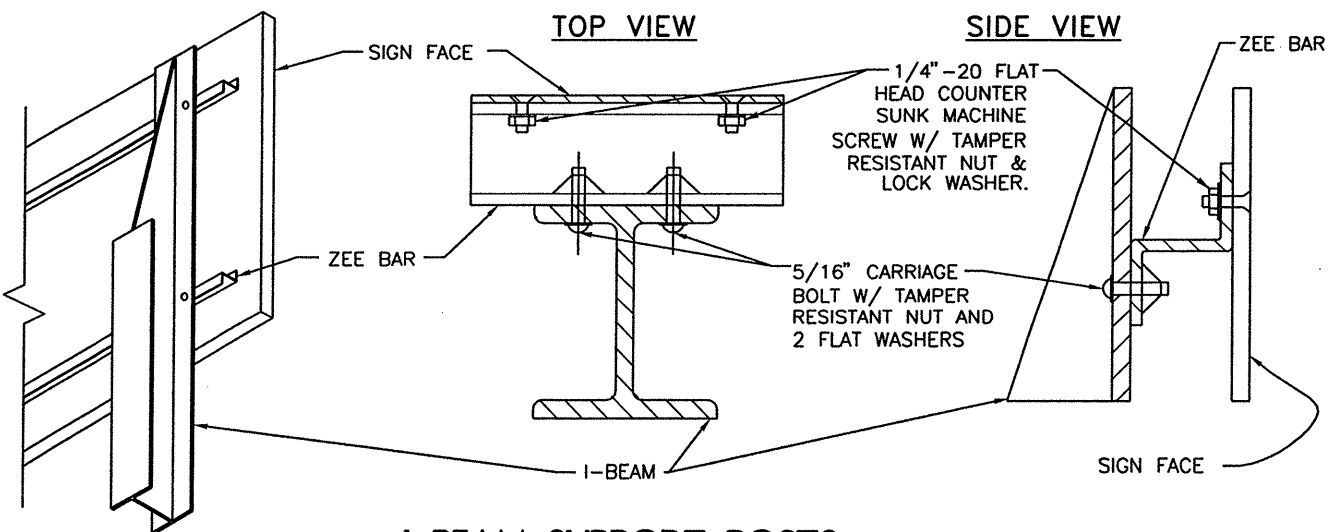
SQUARE TUBING POSTS



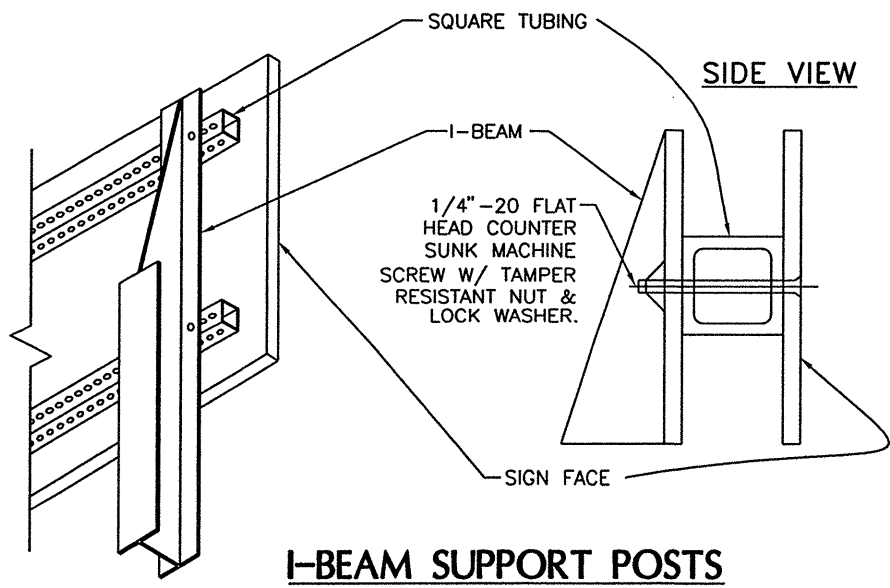
U CHANNEL SUPPORT POSTS



U CHANNEL SUPPORT POSTS



I-BEAM SUPPORT POSTS

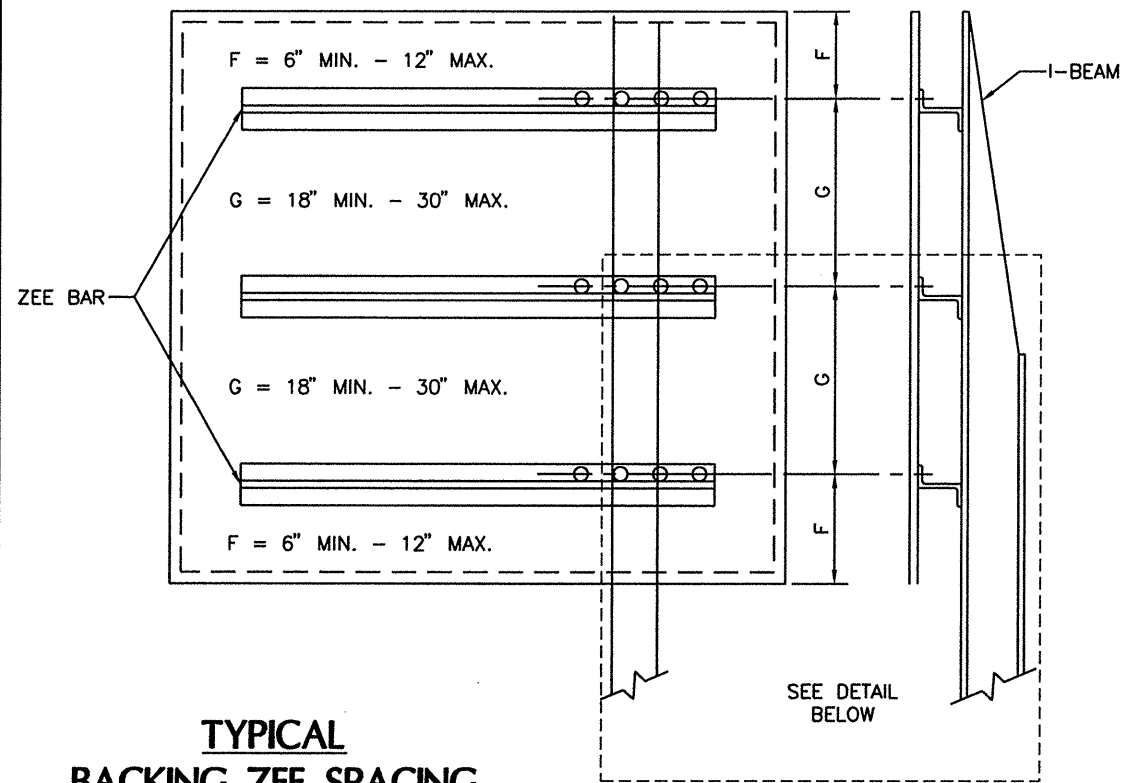


I-BEAM SUPPORT POSTS

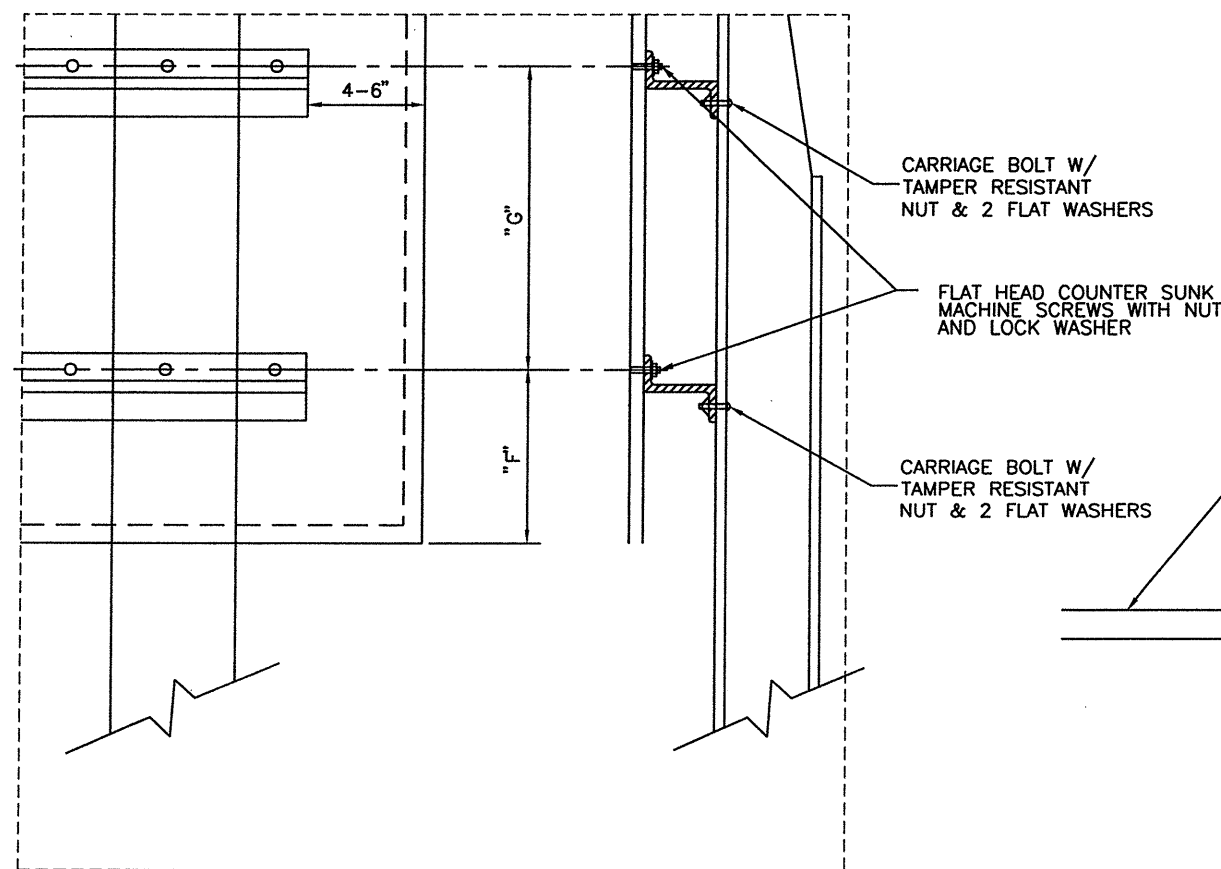
GENERAL NOTES:

1. BACKING ZEE SHALL BE ALUMINUM ALLOY 6061-T6. EACH ZEE SHALL BE PROVIDED WITH A 9/16" X 2" HORIZONTAL SLOT FOR EACH POST MOUNTING BOLT.
2. BACKING ZEE SHALL BE FASTENED TO THE SIGN BLANK WITH 1/4" - 20 X 3/4" FLAT HEAD COUNTER SUNK MACHINE SCREWS WITH NUT AND LOCK WASHER (CADMIUM OR GALVANIZED OR 2024-T4 ALUMINUM-ALLOY) FASTENERS AT 9" +/- 1/2" INTERVALS.
3. BACKING ZEE WITH SIGN FACES SHALL BE FASTENED TO THE SIGN SUPPORT POST WITH 5/16" CARRIAGE BOLT, TAMPER RESISTANT NUT & TWO WASHERS GALVANIZED OR CADMIUM PLATED TO ASTM B-766 SPECIFICATION.
4. BACKING ZEE IS NOT NEEDED WHERE SIGN IS TO BE MOUNTED ON ONE POST AND IS APPROXIMATELY TEN AND A HALF (10.5) SQUARE FEET OR LESS.
5. ALTERNATE BACKING MAY BE 1-1/2" PERFORATED SQUARE TUBING AND ASSOCIATED HARDWARE, DESCRIBED IN SERIAL 701-02-1/2 AND 701-02-2/2
6. ALUMINUM PANEL SIGNS UNDER 24" IN WIDTH SHALL BE 0.08" MINIMUM THICKNESS 6061-T6 OR 5052-H38 ALUMINUM ALLOY. ALUMINUM PANEL SIGNS 24" AND OVER IN WIDTH SHALL BE SINGLE SHEET 6061-T6 OR 5052-H38 ALUMINUM ALLOY 1/8" MINIMUM THICKNESS.
7. BACKING ZEES SHALL BE 3" X 2-3/4" X 1/4" AT 2.33 LBS. PER FT. FOR 6061-T6 ALUMINUM.
8. EXPOSED BOLT HEADS ON THE FACE OF THE SIGN SHALL BE COLORED AND REFLECTORIZED TO MATCH THE SURROUNDING COLOR.

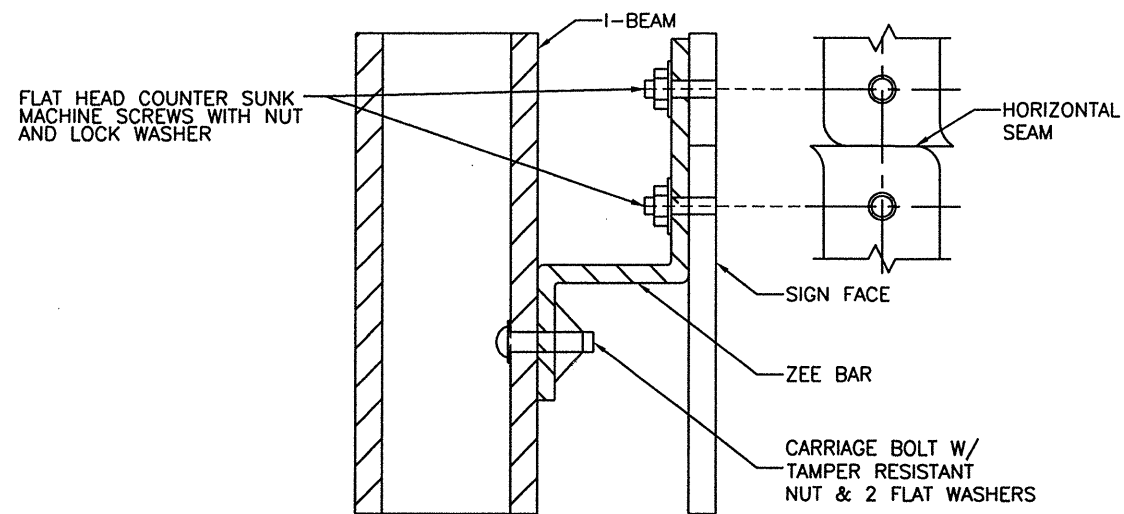
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
ALUMINUM SIGN PANEL DETAILS			
APPROVED	<i>[Signature]</i> DESIGN ENGINEER		DATE 7/1/05
DESIGNED BY	DRAWN BY	CHECKED BY	
701-03-1/2			SHEET 1 OF 2



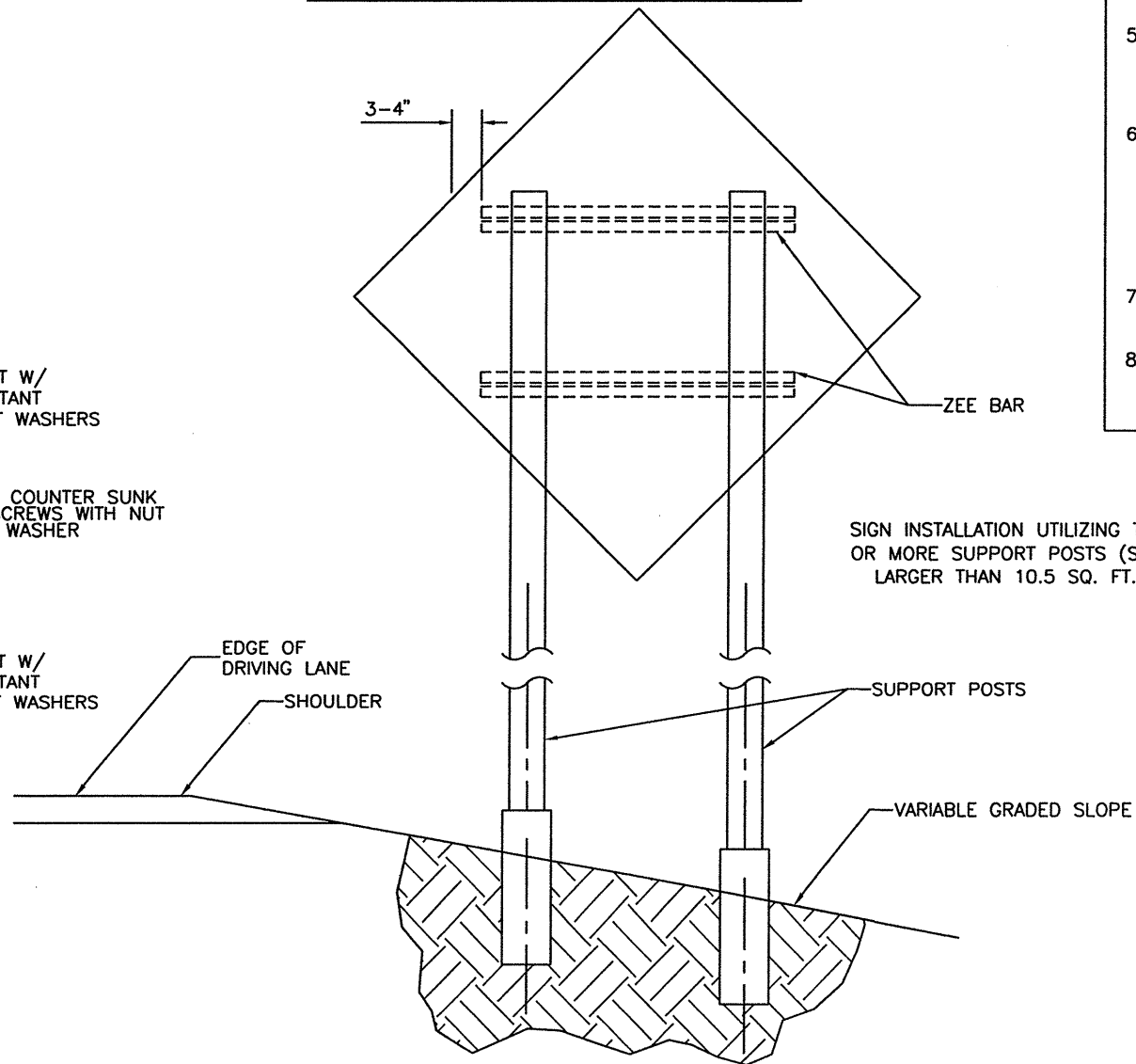
**TYPICAL
BACKING ZEE SPACING**



**TYPICAL
MOUNTING DETAIL**



**TYPICAL
HORIZONTAL SEAM CLOSURE**



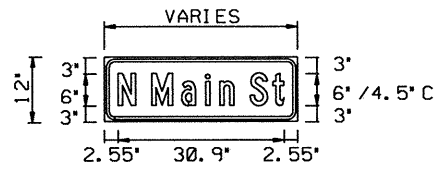
**TYPICAL
ZEE BACKING FOR DIAMOND SHAPED SIGN**

GENERAL NOTES:

1. BACKING ZEE SHALL BE ALUMINUM ALLOY 6061-T6. EACH ZEE SHALL BE PROVIDED WITH A 9/16" X 2" HORIZONTAL SLOT FOR EACH POST MOUNTING BOLT.
2. BACKING ZEE SHALL BE FASTENED TO THE SIGN BLANK WITH 1/4" - 20 X 3/4" FLAT HEAD COUNTER SUNK MACHINE SCREWS WITH NUT AND LOCK WASHER (CADMIUM OR GALVANIZED OR 2024-T4 ALUMINUM-ALLOY) FASTENERS AT 9" ± 1/2" INTERVALS.
3. BACKING ZEE WITH SIGN FACES SHALL BE FASTENED TO THE SIGN SUPPORT POST WITH 5/16" CARRIAGE BOLT, TAMPER RESISTANT NUT & TWO WASHERS GALVANIZED OR CADMIUM PLATED TO ASTM B-766 SPECIFICATION.
4. BACKING ZEE IS NOT NEEDED WHERE SIGN IS TO BE MOUNTED ON ONE POST AND IS APPROXIMATELY TEN AND A HALF (10.5) SQUARE FEET OR LESS.
5. ALTERNATE BACKING MAY BE 1-1/2" PERFORATED SQUARE TUBING AND ASSOCIATED HARDWARE, DESCRIBED IN SERIAL 701-02-1/2 AND 701-02-2/2
6. ALUMINUM PANEL SIGNS UNDER 24" IN WIDTH SHALL BE 0.08" MINIMUM THICKNESS 6061-T6 OR 5052-H38 ALUMINUM ALLOY. ALUMINUM PANEL SIGNS 24" AND OVER IN WIDTH SHALL BE 1/8" MINIMUM THICKNESS 6061-T6 OR 5052-H38 ALUMINUM ALLOY.
7. BACKING ZEES SHALL BE 3" X 2-3/4" X 1/4" AT 2.33 LBS. PER FT. FOR 6061-T6 ALUMINUM.
8. EXPOSED BOLT HEADS ON THE FACE OF THE SIGN SHALL BE COLORED AND REFLECTORIZED TO MATCH THE SURROUNDING COLOR.

SIGN INSTALLATION UTILIZING TWO OR MORE SUPPORT POSTS (SIGN LARGER THAN 10.5 SQ. FT.)

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
ALUMINUM PANEL SIGN DETAILS			
APPROVED			1-11-05 DATE
DESIGNED BY _____ DRAWN BY _____ CHECKED BY _____			
701-03-2/2			SHEET 2 OF 2

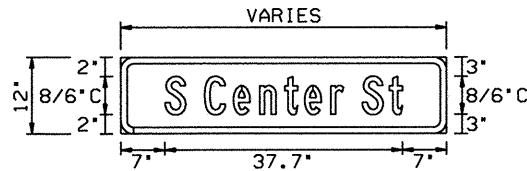


BORDER
R=1.5"
TH=0.5"

**D3-1-XX
STREET NAME SIGN**

W/G

LETTERING ON GROUND-MOUNTED STREET NAME SIGNS SHOULD BE AT LEAST 6" HIGH IN CAPITAL LETTERS OR 6" UPPER-CASE WITH 4.5 LOWER CASE LETTERS.

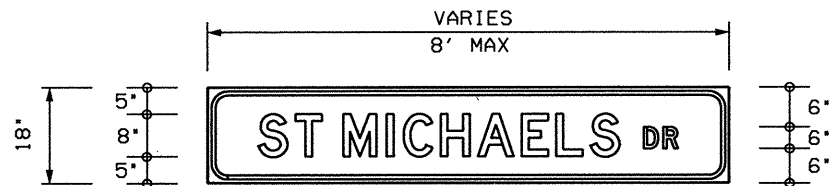


BORDER
R=1.5"
TH=0.5"

**D3-1-XX
STREET NAME SIGN**

W/G

FOR USE ON MULTI-LANE ROADWAYS WITH SPEEDS GREATER THAN 40 MPH
LETTERING ON GROUND-MOUNTED STREET NAME SIGNS SHOULD BE AT LEAST 8" HIGH IN CAPITAL LETTERS OR 8" UPPER-CASE WITH 6" LOWER CASE LETTERS

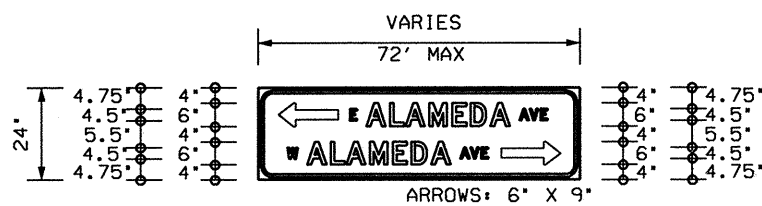


BORDER
R=2"
TH=0.75"
IN=0"

**D3-3-XX
MASTARM SIGN**

MAX. TOTAL SIGN AREA = 12FT.

(IF UPPER AND LOWER CASE LETTERS ARE USED, THEY SHALL BE 12" AND 9" RESPECTIVELY, HOWEVER OVERALL SIGN DIMENSIONS CAN NOT BE CHANGED)
NOTE: LETTER TYPE AS APPROPRIATE (TYPE C OR D)
W/G, RL - RB

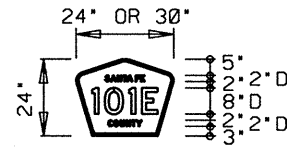


BORDER
R=2"
TH=0.75"
IN=0"

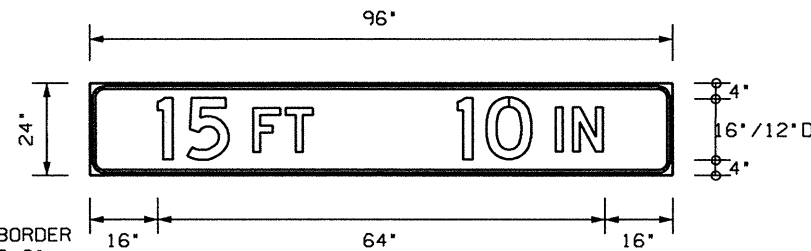
**D3-3A-XX
MASTARM SIGN**

MAX. TOTAL SIGN AREA = 12FT.

NOTE: LETTER TYPE AS APPROPRIATE (TYPE C OR D)
W/G, RL - RB



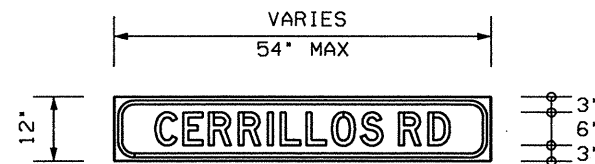
**M1-6
COUNTY ROAD**
1 TO 2 DIGITS 24" X 24" SIZE
3 TO 4 DIGITS 30" X 24" SIZE
Y/BL, RL-RB



BORDER
R=3"
TH=0.75"
IN=0.75"

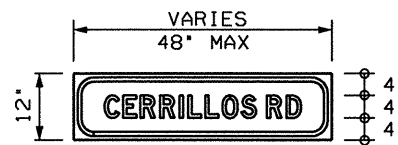
**W12-2A-96
BRIDGE CLEARANCE SIGN**

FOR STRUCTURES WITH CLEARANCES LESS THAN 16'-0" - B/FY, RB
FOR STRUCTURES WITH CLEARANCES EQUAL TO OR GREATER THAN 16'-0" - W/G, RL-RB



BORDER
R=2"
TH=0.75"
IN=0.75"

***W16-8-XX
MOUNTED BELOW 48" W2
OR W3 WARNING SIGN**
NOTE: LETTER TYPE AS APPROPRIATE (TYPE C OR D)
*SEE SHS MANUAL FOR W16-8A SIGN
B/FY, RB



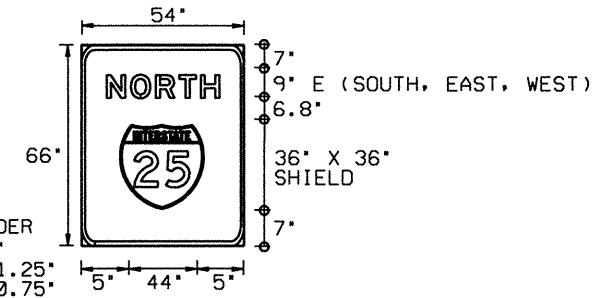
BORDER
R=2"
TH=0.75"
IN=0.75"

**W16-8B-XX
MOUNTED BELOW 36" W2
OR W3 WARNING SIGN**

NOTE: LETTER TYPE AS APPROPRIATE (TYPE C OR D)
B/FY, RB

NOTES:

1. SEE THE MUTCD & STANDARD HIGHWAY SIGNS MANUAL (CURRENT EDITIONS) FOR FURTHER GUIDANCE.
2. SIGNS TO BE SIZED IN 6" INCREMENTS.
3. PERIODS, COMMAS, ETC. SHOULD NOT BE USED.
4. CONVENTIONAL ABBREVIATIONS SHOULD BE USED EXCEPT FOR STREET & PLACE NAMES.
5. WHEN GIVEN THE OPTION FOR LETTER TYPE C OR D, D IS PREFERRED AS SPACE PERMITS.
6. THE DETAILS SHOWN ARE FOR INFORMATION ONLY. SEE PERMANENT SIGNING PLANS FOR PROJECT SPECIFIC SIGN FACE DETAILS.
7. SEE SHEET 701-15-2/2 FOR ABBREVIATION LEGEND.



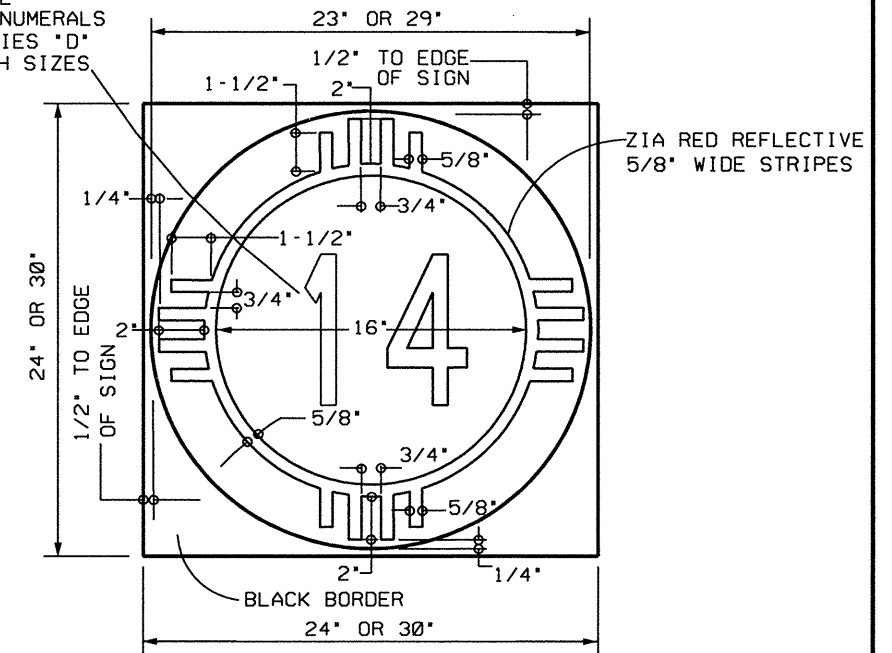
BORDER
R=3"
TH=1.25"
IN=0.75"

**M1-1A-54-XX
INTERSTATE ROUTE MARKER**

W/G, RL - RB

SHIELD: W/BL - ROUTE NUMBER
W/R - "INTERSTATE"

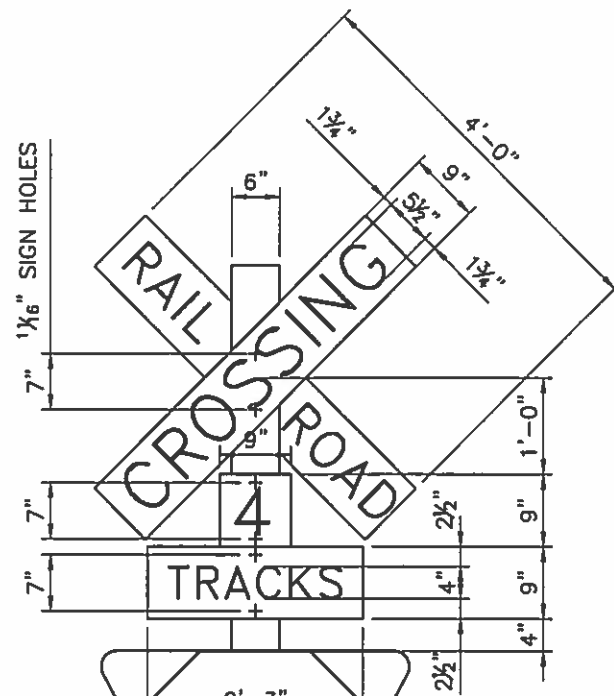
TYPICAL
BLACK NUMERALS
8" SERIES "D"
ON BOTH SIZES



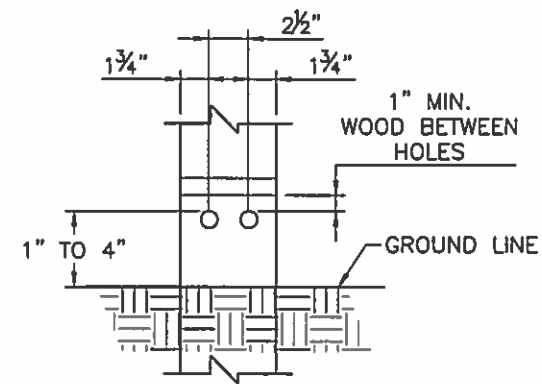
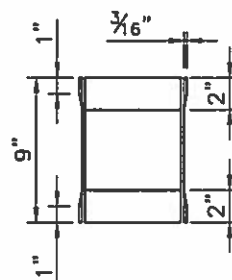
**M1-5-XX
STATE ROUTE MARKER**

1 TO 2 DIGITS 24" X 24" SIZE
3 DIGITS 30" X 30" SIZE
B/W

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
MISCELLANEOUS SIGN FACE DETAILS			
APPROVED		DATE	
<i>[Signature]</i>		2-03-05	
DESIGNED BY		CHECKED BY	
701-15-1/2		SHEET 1 OF 2	



BLADE CROSS SECTION



DETAIL A

"BREAK AWAY" SIGN POST MUST BE DRILLED PER DETAIL "A" THROUGH BOTH SIDES OF POST, 4'-6" FROM ONE END.

NOTES

1. ALL SIGNS LISTED IN THIS PLAN ARE 9"x9", 9"x27", AND 9"x48" SIZED. R15-2P SIGN WITH WHITE BACKGROUND AND BLACK LETTERS, R1-1 SIGN WITH WHITE BACKGROUND ON BOTH SIDES WITH BLACK LETTERS, EXTRUDED ALUMINUM CONSTRUCTION, DOG BONE DESIGN, DIAMOND GRADE SHEETING.
2. NO OTHER SIGNS MAY BE MOUNTED TO CROSS BUCK POST UNLESS SHOWN IN THIS STANDARD PLAN.
3. ALL SIGNS ARE TO BE PLACED NO CLOSER THAN 14 FEET FROM CENTER LINE OF TRACK.
4. WHERE THE DISTANCE BETWEEN CENTER LINE TO CENTER LINE OF TRACKS. MEASURED ALONG THE HIGHWAY EXCEEDS 100 FEET, ADDITIONAL CROSS BUCKS (AND OTHER APPROPRIATE) SIGNS ARE TO BE INSTALLED.
5. SCOTCHLITE OR EQUIVALENT DIAMOND GRADE LDP-80CPL.
6. SEE WOOD SIGN POST DETAILS.
7. NYLON FLAT WASHERS TO BE INSTALLED BETWEEN FACE OF SIGN AND BOLT. THIS IS NECESSARY TO COMPLY WITH 3M WARRANTY.
8. RAILROAD CROSSING SIGNING WILL BE PAID AS ONE ITEM.

BILL OF MATERIALS

QUANTITY	SIGN PANEL
1 SET	SIGN NO. R15-1 RAILROAD CROSSING
1 EACH	SIGN NO. R15-2P TRACKS
3 EACH	REFLECTIVE STRIP (4" x 48" STRIPS)
1 EACH	REFLECTIVE STRIP (4" x 24" STRIPS)
1 EACH	SIGN NO. R1-2 YIELD (36" OR 48")
1 EACH	SIGN NO. 64 STOP (36")
QUANTITY	POST AND HARDWARE
1 EACH	4" OR (6") x 6" x 18'-0" TREATED TIMBER POST (HEIGHT MAY BE INCREASED TO MATCH FIELD CONDITIONS)
2 EACH	5/16" x 7" GALVANIZED MACHINE BOLTS
2 EACH	5/16" GALVANIZED HEX NUTS, TAMPER RESISTANT AND STEEL FLAT WASHERS
2 EACH	5/16" NYLON FLAT WASHERS AND LOCK WASHERS (FOR PROTECTION OF FACE OF SIGN, SEE NOTE 6.)
4 EACH	1/4" x 1/2" HEX HEAD LAG SCREW AND 1/4" NYLON FLAT WASHERS (FOR SIGNS NO R15-2P "TRACKS" AND R15-2P "NUMBERS" WHEN USED. SEE NOTE 6 FOR INSTALLATION)

SIGNS

R15-1 SIGN "RAILROAD CROSSING"
PLACE SIGN ON THE RIGHT HAND SIDE OF EACH ROADWAY APPROACH TO THE CROSSING NO LESS THAN 14 FEET FROM EDGE OF TRAVELED ROADWAY TO THE CENTER OF THE POST.

EACH SIGN SHALL BE NO CLOSER THAN 14 FEET FROM CENTERLINE OF TRACK.

R15-2P SIGN NUMBER "2", "3"....."8"
MULTIPLE TRACK SIGN TO BE USED ONLY WHEN MORE THAN ONE TRACK IS TO BE PROTECTED, LETTERED AND MOUNTED AS IN EXAMPLE 1.

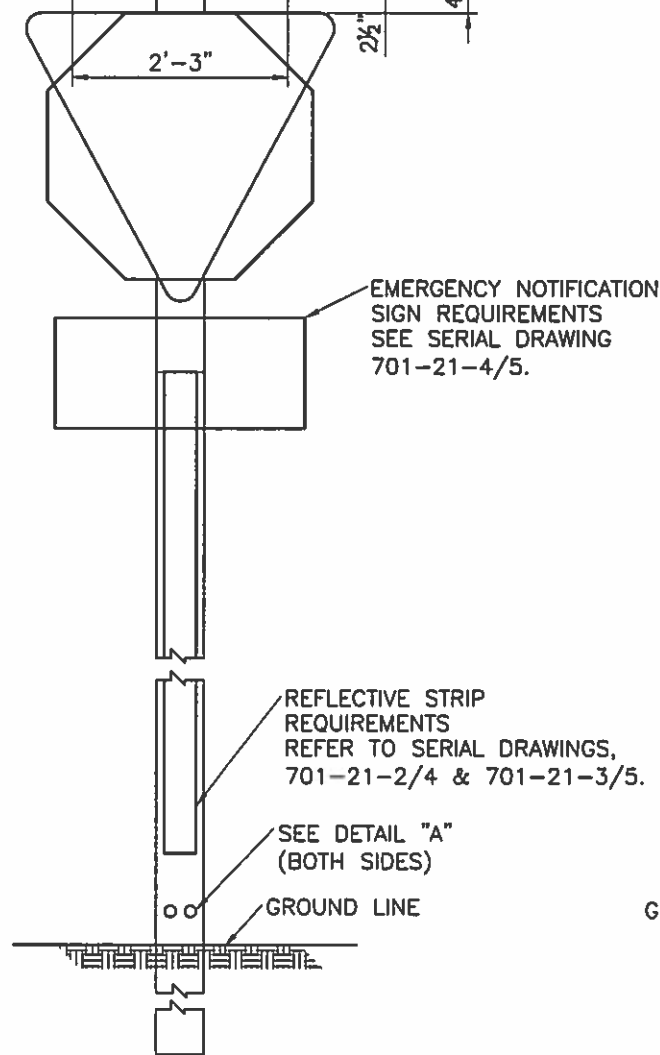
R15-2P SIGN "NUMBER OF TRACKS"
MULTIPLE TRACK SIGN TO BE USED ONLY WHEN MORE THAN ONE TRACK IS TO BE PROTECTED AND USED WITH SIGN R15-2P "NUMBER OF TRACKS", LETTERED AND MOUNTED AS IN EXAMPLE 1.

WOOD POSTS

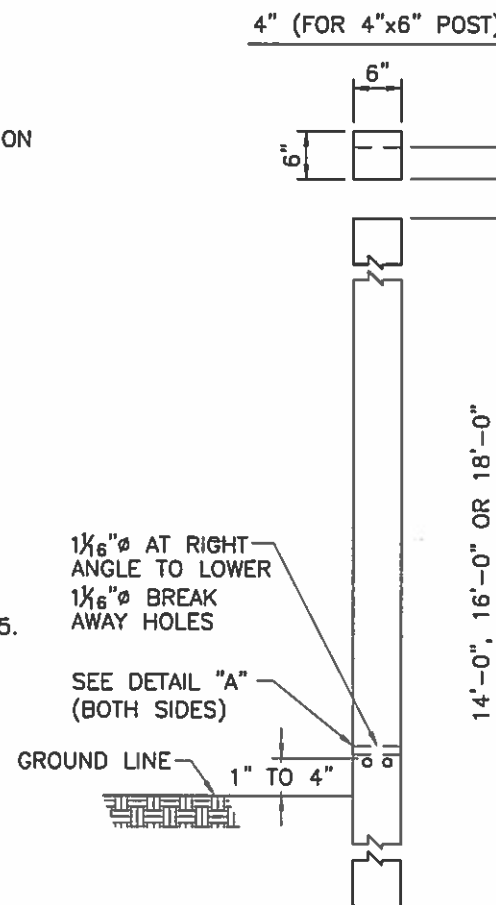
POSTS TO BE CCA TREATED.

USE 18'-6"x6" OR 18'-4"x6" WOOD FOR HIGHWAY GRADE CROSSING SIGN.

BREAK AWAY HOLES FOR 4"x6" POST SHALL BE DRILLED 3'-6" FROM END SIMILAR TO DETAIL "A" EXCEPT, ON 4" SIDE, ONLY 1-1 1/8" DIAMETER HOLE.



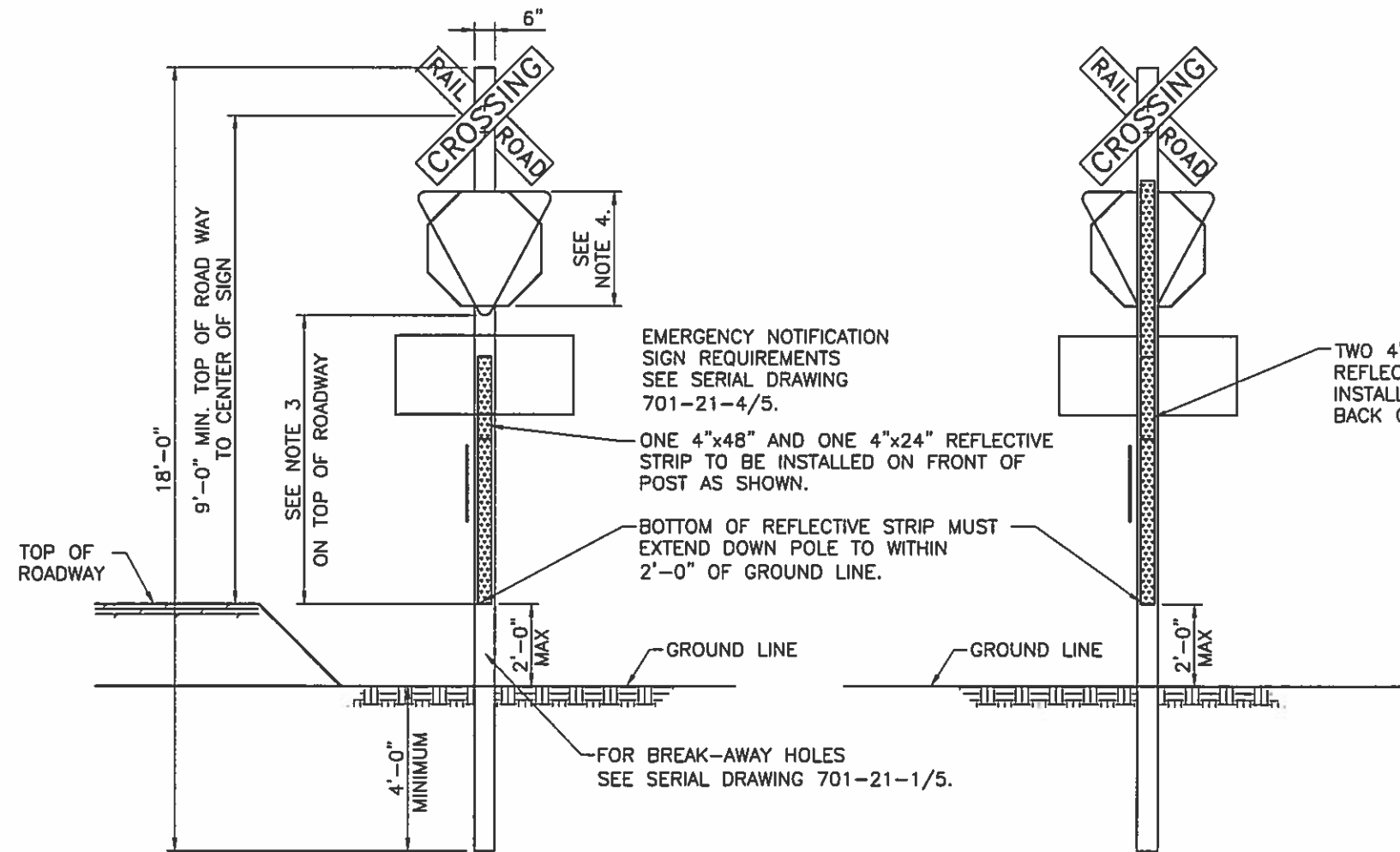
RAILROAD SIGN EXAMPLE 1



WOOD SIGN POST DETAILS

FOR USE ON NMDOT-OWNED RAILROAD TRACK

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
RAILROAD ROADWAY CROSSING SIGN BILL OF MATERIALS FOR SIGN AND POST			
APPROVED: <i>A. E. Shin</i>			10/24/15
DESIGNED BY: RF			CHECKED BY: RF
701-21-1/5			SHEET 1 OF 5



HIGHWAY APPROACH
FRONTAL VIEW OF
CROSSBUCK POST
SINGLE TRACK

HIGHWAY APPROACH
BACK SIDE VIEW OF
CROSSBUCK POST
SINGLE TRACK

- NOTES:**
1. YIELD SIGN TO BE USED UNLESS USE OF STOP SIGN IS DETERMINED BY ENGINEERING STUDY.
 2. MOUNT REFLECTIVE STRIPS AS FOLLOWS, PLACE ONE 4"x48" ON BOTTOM OF POST WITHIN 2 FEET ABOVE ROADWAY. PLACE 4"x24" REFLECTIVE STRIP ABOVE THE FIRST REFLECTIVE STRIP. PLACE ANOTHER 4"x48" REFLECTIVE STRIP ABOVE SECOND STRIP.
 3. MOUNTING HEIGHT SHALL BE AT LEAST 7 FEET FROM TOP OF ROADWAY FOR BUSINESS OR RESIDENTIAL AREAS WITH PEDESTRIANS OR PARKING. OTHERWISE MOUNTING LINE SHALL BE AT LEAST 5 FEET FROM TOP OF ROADWAY.
 4. YIELD SIGN SHALL BE 36" FOR SINGLE LANE. STOP SIGN SHALL BE 36".
 5. POST SHALL BE 4"x6"x18' OR 6"x6"x18' TREATED TIMBER. HEIGHT MAY BE INCREASED TO MATCH FIELD CONDITIONS.

SEE PLAN 701-21-1/5 FOR CROSSBUCK AND TRACK NUMBER SIGN DETAILS.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
RAILROAD ROADWAY CROSSING SIGN AND REFLECTIVE STRIP PLACEMENT REQUIREMENTS FOR SINGLE TRACK WITH STOP OR YIELD SIGN			
APPROVED: <i>Ashin Singh</i>			DATE: 10/21/15
DESIGNED BY: _____			DRAWN BY: SKL
			CHECKED BY: RF



NOTES

1. 18"x24" HORIZONTAL SIGN, METAL.
2. WHITE LETTERS OR NUMBERS ON M.U.T.C.D. BLUE BACKGROUND.
3. SEVEN CHARACTER USDOT NUMBER IS UNIQUE AND ASSIGNED TO EACH RAILROAD CROSSING.
4. ROAD NAME IS UNIQUE TO EACH CROSSING.
5. UNIQUE MILEPOST NUMBER SHALL BE USED IF THERE IS NO ROAD NAME.
6. SIGN TO BE MOUNTED UNDER STOP OR YIELD SIGN ON CROSS BUCK ASSEMBLY FACING ONCOMING TRAFFIC, EACH SIDE OF TRACK, PER SERIAL DRAWING 701-21-2/5 AND 701-21-3/5.
7. REFLECTIVE DIAMOND GRADE SHEETING, ONE SIDE ONLY.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
RAILROAD ROADWAY CROSSING SIGN EMERGENCY NOTIFICATION SIGN			
APPROVED: <i>A. Shin</i>		DESIGNED BY: <i>RF</i>	DATE: 10/2/13
DESIGNED BY: RF		DRAWN BY: SKL	CHECKED BY: RF
701-21-4/5			SHEET 4 OF 5

FOR USE ON NMDOT-OWNED RAILROAD TRACK

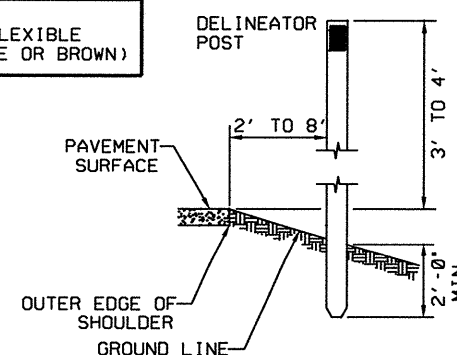
TRAFFIC MARKERS - OBJECT MARKERS

	TYPE 1		TYPE 2		TYPE 3			END OF ROAD MARKER		
	OM1-1	OM1-2	A3 OR A3/A3	OM2-1V	OM2-1H	OM-3L	OM-3C	OM-3R	OM4-1	OM4-2
POST	2 LB/FT STEEL U-CHANNEL W/ BREAKAWAY BASE POST		1.12 LB/FT STEEL U-CHANNEL		2 LB/FT STEEL U-CHANNEL OR 1.5" X 1.5", 12 GA. SQUARE TUBING W/ BREAKAWAY BASE POST			2 LB/FT STEEL U-CHANNEL W/BREAKAWAY BAST POST		

TRAFFIC MARKERS - DELINEATORS

	SINGLE		DOUBLE		1/10 MILE DELINEATOR			
	A1, C1, A1/A1, A1/C1, A1/R1, C1/C1, C1/R1		A2, C2, A2/A2, A2/C2, A2/R2, C2/C2, C2/R2					
POST	1.12 LB/FT STEEL U-CHANNEL	FLEXIBLE (YELLOW OR WHITE)	TUBULAR FLEXIBLE (Y OR W)	1.12 LB/FT STEEL U-CHANNEL	FLEXIBLE (YELLOW OR WHITE)	TUBULAR FLEXIBLE (Y OR W)	1.12 LB/FT STEEL U-CHANNEL	FLEXIBLE (WHITE OR BROWN)

1. ALL HARDWARE SHALL MEET FHWA CRASHWORTHINESS REQUIREMENTS AS PER NCHRP 350 GUIDELINES AND SHALL BE ON THE DEPARTMENT'S APPROVED PRODUCT LIST.
2. SEE DEPARTMENT'S APPROVED PRODUCTS LIST FOR APPROVED U-CHANNEL & SQUARE TUBING, FLEXIBLE & TUBULAR TRAFFIC MARKER REFLECTOR MANUFACTURERS.
3. DELINEATOR POST & REFLECTOR UNIT COLOR SHALL CONFORM TO THE COLOR OF EDGE LINES.
4. BREAKAWAY BASE POST SYSTEMS FOR TYPE 1, TYPE 3 & END OF ROAD OBJECT MARKERS REQUIRED. SEE APPROVED PRODUCT LIST FOR APPROVED SYSTEMS & MANUFACTURER'S RECOMMENDATIONS.
5. STANDARD DELINEATORS ARE NOT TO CONFLICT WITH 1/10 MILE DELINEATORS. WHEN THE TWO COINCIDE, 1/10 MILE DELINEATORS WILL BE USED.
6. SEE SECTION 703 OF THE NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION FOR ADDITIONAL INFORMATION.
7. FOR SQUARE TUBING WITH BREAKAWAY BASE POST SEE STANDARDS 701-02-1/3, 701-02-2/3 AND 701-02-3/3.
8. 1/10 MILE DELINEATORS SHALL BE USED ON INTERSTATE ROADWAYS AND MAY BE USED ON HIGH SPEED 4-LANE DIVIDED ROADWAYS.

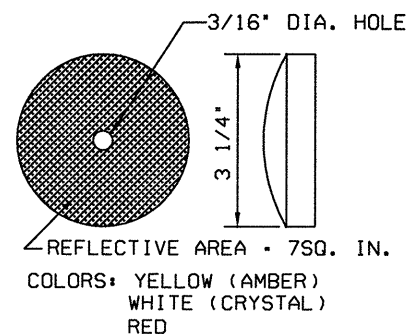


**TYPICAL 1/10 MILE
DELINEATOR INSTALLATION**

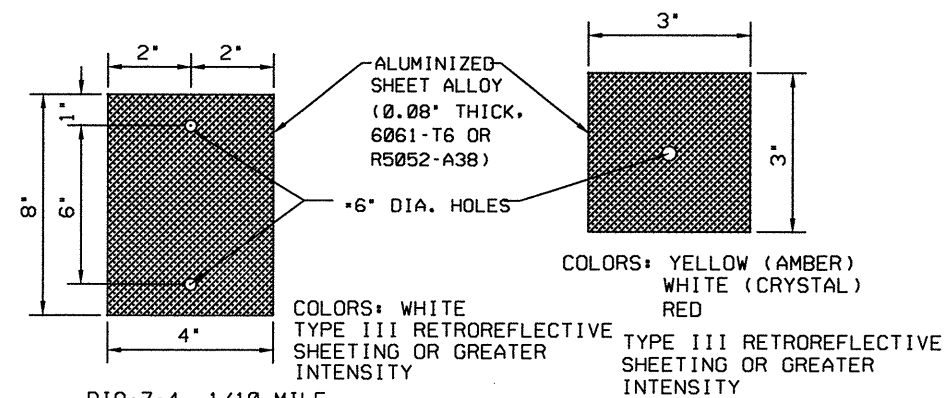
REFLECTOR UNIT TYPES

	SINGLE SIDE MOUNT	REFLECTOR UNIT
	A1	SINGLE YELLOW REFLECTOR
	A2	DOUBLE YELLOW REFLECTOR
	A3	TRIPLE YELLOW REFLECTOR (TYPE 2 OBJECT MARKER)
	C1	SINGLE WHITE REFLECTOR
	C2	DOUBLE WHITE REFLECTOR
	DOUBLE SIDE MOUNT (BACK TO BACK)	REFLECTOR UNIT
	A1/A1	1 EACH
	A1/C1	1 EACH
	A1/R1*	1 EACH
	C1/C1	1 EACH
	C1/R1*	1 EACH
	A2/A2	2 EACH
	A2/C2	2 EACH
	A2/R2*	2 EACH
	C2/C2	2 EACH
	C2/R2*	2 EACH
	A3/A3	3 EACH (TYPE 2 OBJECT MARKER)

*BIDIRECTIONAL MOUNTING BRACKET REQUIRED TO ORIENT RED REFLECTOR



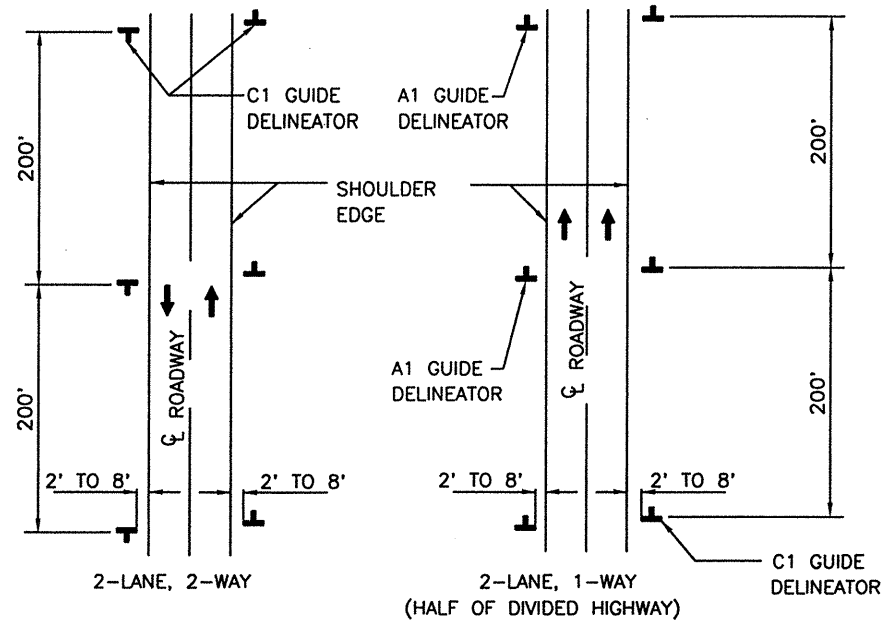
PRISMATIC REFLECTOR



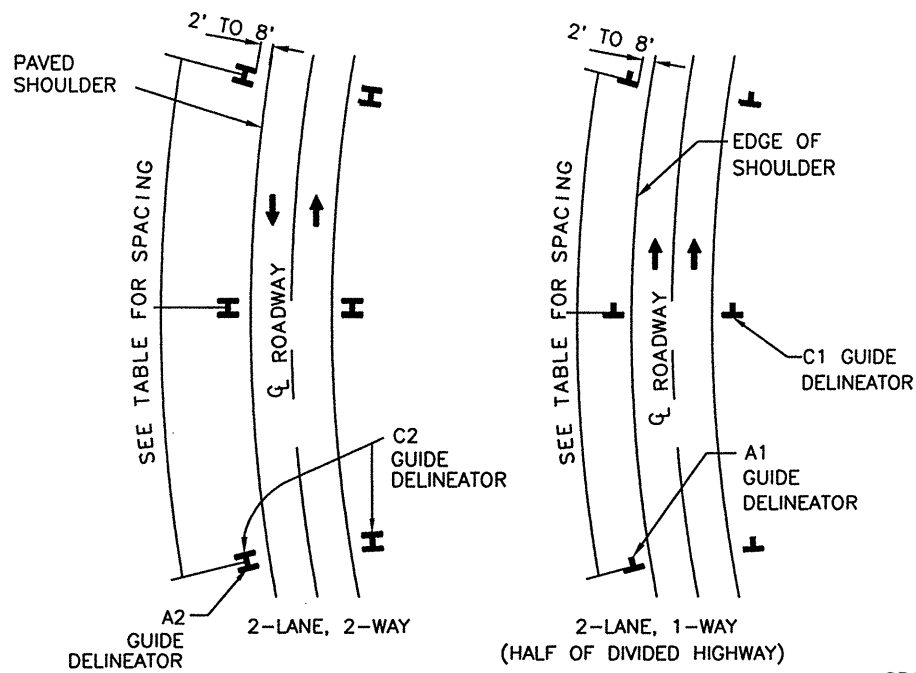
PANEL REFLECTOR

PANEL REFLECTOR

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
ROAD OBJECT MARKER DETAILS			
APPROVED		DATE 3-14-05	
DESIGNED BY		DRAWN BY	
		CHECKED BY	
703-01-1/3		SHEET 1 OF 3	



PLAN ON HIGH FILLS

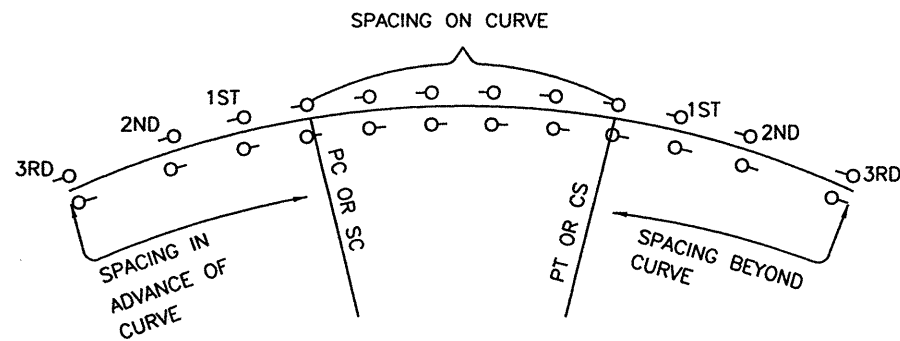


PLAN ON CURVES

DELINEATOR TO BE PLACED ON OUTSIDE OF CURVES ON HIGH FILLS ONLY. SPACE SAME AS HIGH FILLS ON TANGENT

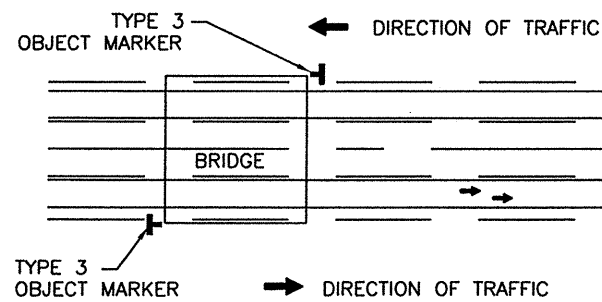
SPACING ON CURVES				
DISTANCE IN FEET ROUNDED TO THE NEAREST 5 FEET				
RADIUS OF CURVES (IN FEET)	SPACING OF CURVES (IN FEET)	SPACING IN ADVANCE AND BEYOND CURVE (IN FEET)		
		1ST	2ND	3RD
50'	20'	40'	60'	120'
115'	25'	50'	75'	150'
180'	35'	70'	105'	210'
250'	40'	80'	120'	240'
300'	50'	100'	150'	300'
400'	55'	110'	165'	300'
500'	65'	130'	195'	300'
600'	70'	140'	210'	300'
700'	75'	150'	225'	300'
800'	80'	160'	240'	300'
900'	85'	170'	255'	300'
1000'	90'	180'	270'	300'
1200'	100'	200'	300'	300'
1400'	110'	220'	300'	300'
1700'	120'	240'	300'	300'
2000'	130'	260'	300'	300'
2500'	145'	280'	300'	300'
3000'	160'	300'	300'	300'

SPACING FOR SPECIFIC RADII MAY BE INTERPOLATED FROM TABLE. THE MINIMUM SPACING SHOULD BE 20ft. THE SPACING ON CURVES SHOULD NOT EXCEED (300ft.) IN ADVANCE OF OR BEYOND A CURVE, AND PROCEEDING AWAY FROM THE END OF THE CURVE, THE SPACING OF THE FIRST DELINEATOR IS 2S, THE SECOND 3S, AND THE THIRD 6S BUT NOT TO EXCEED 300ft. S REFERS TO THE DELINEATOR SPACING FOR SPECIFIC RADII COMPUTED FROM THE FORMULA $S=3\sqrt{R-50}$.



PLACEMENT ON CURVES

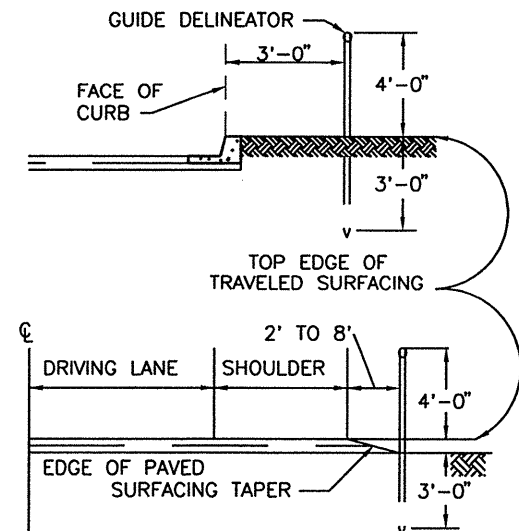
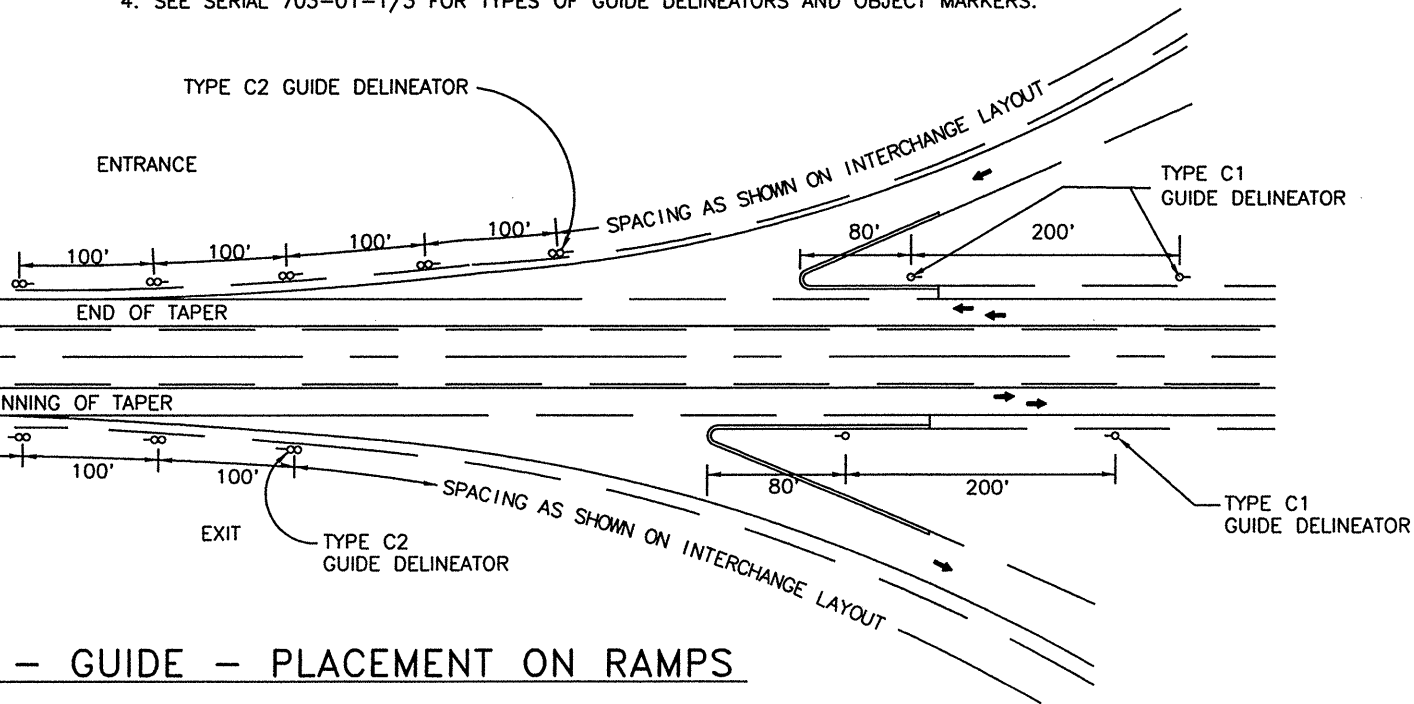
SEE TABLE "SPACING ON CURVES" FOR DISTANCES BETWEEN DELINEATORS.



ROAD DELINEATOR - GUIDE - PLACEMENT ON RAMPS

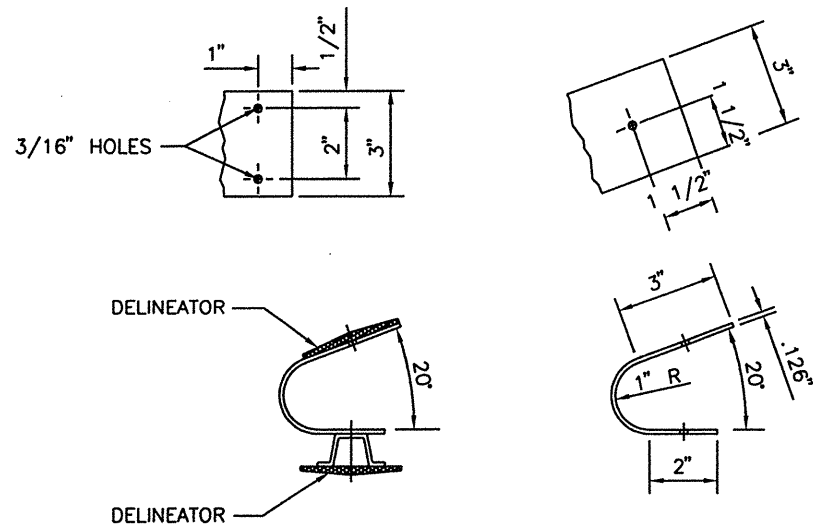
GENERAL NOTES

- WHERE THE SHOULDER AND PAVED SURFACING TAPER CONSTITUTES THE USEABLE SHOULDER, GUIDE MARKERS SHALL BE PLACED AT THE OUTSIDE EDGE OF THE SURFACE TAPER OR 8'-0" FROM THE EDGE OF DRIVING LANE.
- DELINEATORS SHOULD BE PLACED IN LINE WITH OR INSIDE FACE AT GUARDRAIL BUT SHOULD NOT BE PLACED CLOSER THAN 2'-0" FROM EDGE OF SHOULDER.
- STANDARD DELINEATORS ARE NOT TO CONFLICT WITH 1/10 MILE DELINEATORS. WHEN THE TWO COINCIDE, 1/10 MILE DELINEATORS WILL BE USED.
- SEE SERIAL 703-01-1/3 FOR TYPES OF GUIDE DELINEATORS AND OBJECT MARKERS.

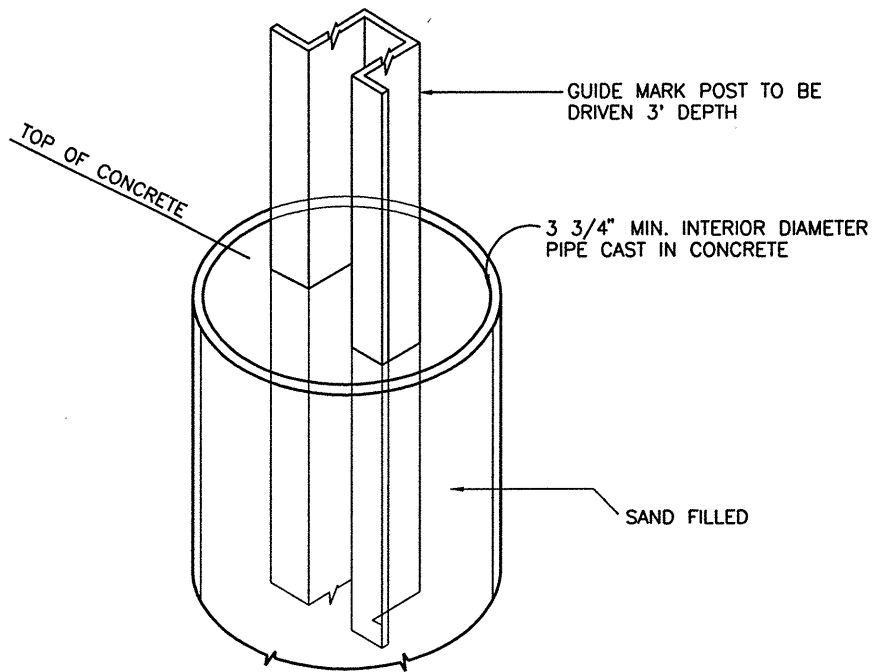


TYPICAL DELINEATOR INSTALLATION

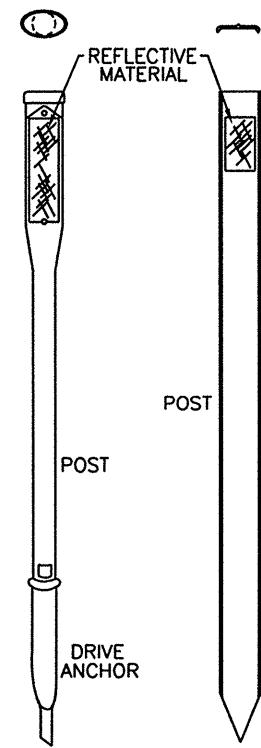
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
ROAD DELINEATOR - GUIDE DETAILS			
APPROVED			1-11-05 DATE
DESIGNED BY	DRAWN BY	CHECKED BY	
703-01-2/3			SHEET 2 OF 3



BIDIRECTIONAL DELINEATOR MOUNTING BRACKET

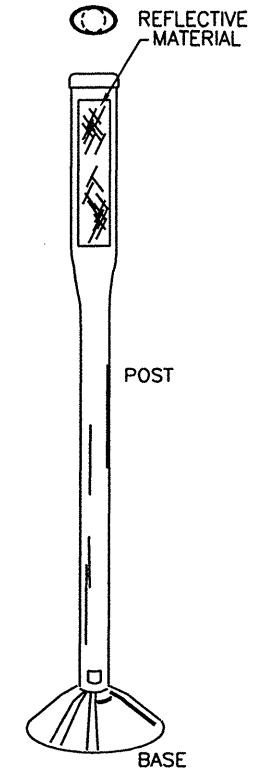


PLACEMENT IN CONCRETE MEDIAN PAVEMENT



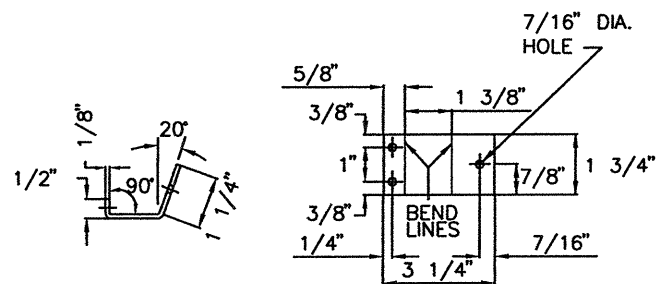
DRIVEABLE OR SEMI-DRIVEABLE DELINEATOR POSTS

INSTALL TO MANUFACTURER RECOMMENDATIONS

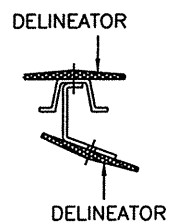


PAVEMENT SURFACE MOUNT

SURFACE MOUNTED DELINEATOR POSTS MAY USE EPOXY, OR HOT BITUMINOUS ADHESIVE TO MOUNT POST TO FLAT SURFACE.

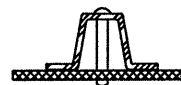


ALUMINUM PULL-THRU OR "POP" RIVET MOUNTING DETAIL

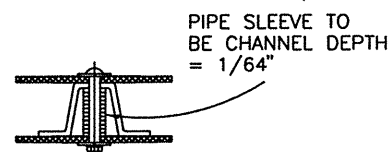


BIDIRECTIONAL DELINEATOR MOUNTING BRACKET

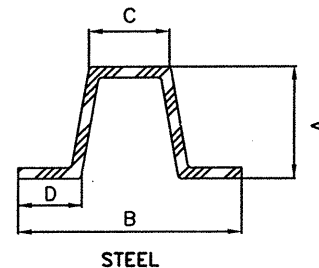
BRACKET MATERIAL: 8" x 3" x 1/8" OR 3 1/4" x 1 3/4" x 1/8" FLAT-MILL FINISH SHEET ALUMINUM 6061-T6 OR EQUIVALENT



TYPE A1, A2, A3, C1, C2



ALL BACK-TO-BACK



2 LB/FT U-CHANNEL POST DETAIL CROSS SECTION

NOTE: FOR ANY CONTRACT OR PURCHASE, ALL POSTS TO BE SAME TYPE MATERIAL AND OF SAME SECTION.

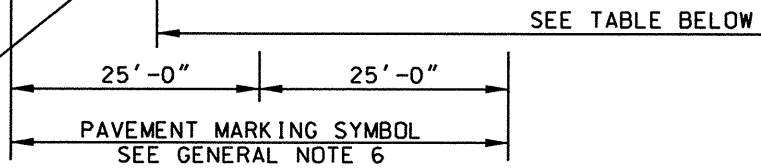
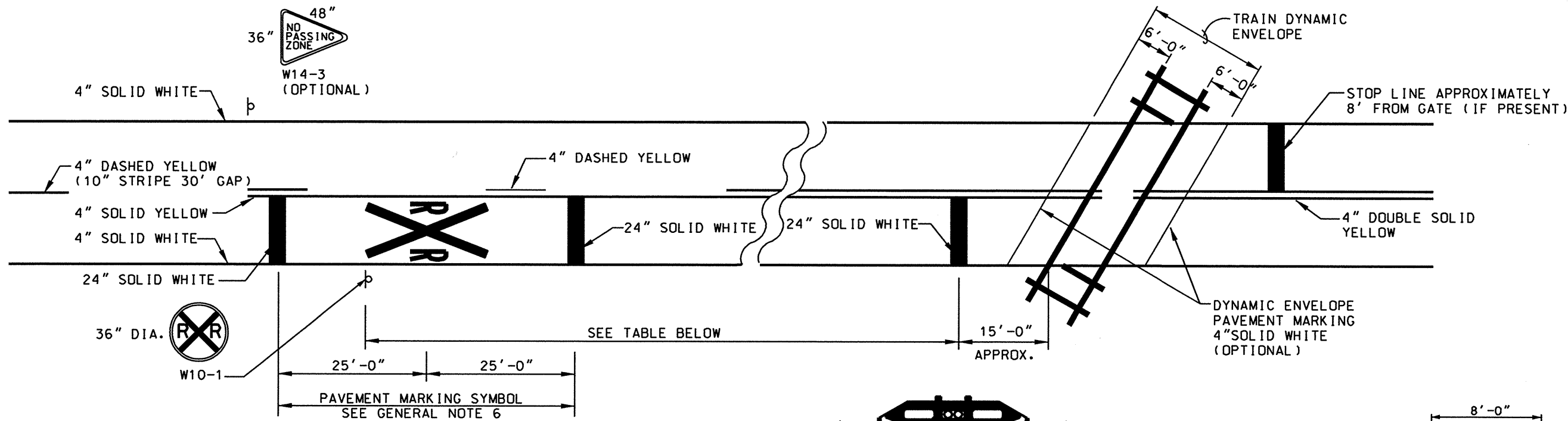
U-CHANNEL FRANKLIN

LB/FT	"A"	"B"	"C"	"D"
2.00	1.516	3.125	1.250	0.625

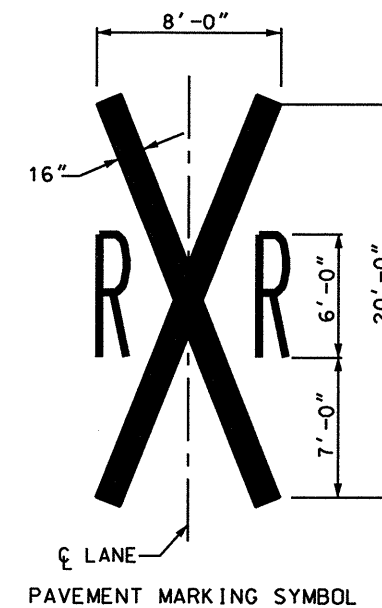
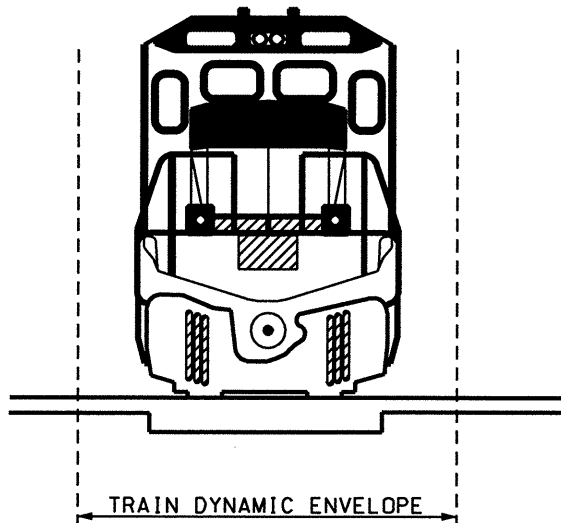
U-CHANNEL MARION

LB/FT	"A"	"B"	"C"	"D"
2.00	1.462	3.062	1.278	

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
DELINEATORS & OBJECT MARKERS			
APPROVED	DESIGN ENGINEER		DATE
DESIGNED BY	DRAWN BY	CHECKED BY	
703-01-3/3		SHEET 3 OF 3	



POSTED OR 85TH PERCENTILE SPEED	ADVANCE PLACEMENT DISTANCE ¹								
	CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC ²	CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION.							
		0 ³	10	20	30	40	50	60	70
20 mph	225 ft.	N/A ⁵	N/A ⁵	---	---	---	---	---	---
25 mph	325 ft.	N/A ⁵	N/A ⁵	N/A ⁵	---	---	---	---	---
30 mph	450 ft.	N/A ⁵	N/A ⁵	N/A ⁵	---	---	---	---	---
35 mph	550 ft.	N/A ⁵	N/A ⁵	N/A ⁵	N/A ⁵	---	---	---	---
40 mph	650 ft.	125 ft.	N/A ⁵	N/A ⁵	N/A ⁵	---	---	---	---
45 mph	750 ft.	175 ft.	125 ft.	N/A ⁵	N/A ⁵	N/A ⁵	---	---	---
50 mph	850 ft.	250 ft.	200 ft.	150 ft.	100 ft.	N/A ⁵	---	---	---
55 mph	950 ft.	325 ft.	275 ft.	225 ft.	175 ft.	100 ft.	N/A ⁵	---	---
60 mph	1100 ft.	400 ft.	350 ft.	300 ft.	250 ft.	175 ft.	N/A ⁵	---	---
65 mph	1200 ft.	475 ft.	425 ft.	400 ft.	350 ft.	275 ft.	175 ft.	N/A ⁵	---
70 mph	1250 ft.	550 ft.	525 ft.	500 ft.	425 ft.	350 ft.	250 ft.	150 ft.	---
75 mph	1350 ft.	650 ft.	625 ft.	600 ft.	525 ft.	450 ft.	350 ft.	250 ft.	100 ft.

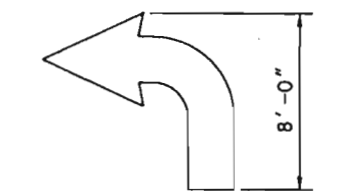


GENERAL NOTES:

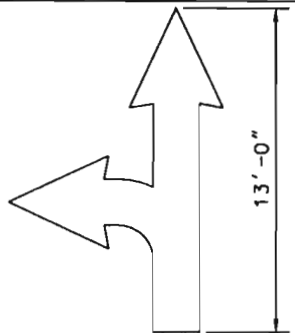
1. THE CONTRACTOR SHALL CONTACT AND COORDINATE WITH THE RAILROADS AND UTILITIES SECTION MANAGER AT THE NEW MEXICO DEPARTMENT OF TRANSPORTATION, AND WITH THE RAILROAD OWNERS PRIOR TO ANY WORK DONE AT RAILROAD CROSSINGS.
2. A THREE-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR TWO-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING.
3. ON MULTI-LANE ROADS, THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL RRR SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
4. THE 24" SOLID WHITE STOP LINE SHALL BE REFLECTIVE 60 MIL. PLASTIC (3 LIN. FT. PER LANE).
5. IN AN EFFORT TO SIMPLIFY THE FIGURE TO SHOW WARNING SIGN AND PAVEMENT MARKING PLACEMENT, NOT ALL REQUIRED TRAFFIC CONTROL DEVICES ARE SHOWN.
6. WHEN THE RAILROAD PAVEMENT MARKING SYMBOL, IS USED A PORTION OF THE SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCED WARNING SIGN (W10-1). IF NEEDED, SUPPLEMENTAL PAVEMENT MARKING SYMBOLS MAY BE PLACED BETWEEN THE ADVANCED WARNING SIGN AND THE CROSSING, BUT SHOULD BE AT LEAST 50 FT. FROM THE STOP LINE.

- NOTES:**
- 1 THE DISTANCES ARE ADJUSTED FOR A SIGN LEGIBILITY DISTANCE OF 175 FT. FOR CONDITION A. THE DISTANCES FOR CONDITION B HAVE BEEN ADJUSTED FOR A SIGN LEGIBILITY DISTANCE OF 250 FT. WHICH IS APPROPRIATE FOR AN ALIGNMENT WARNING SYMBOL SIGN.
 - 2 TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LANE ENDS. THE DISTANCES ARE DETERMINED BY PROVIDING THE DRIVER A PIEV TIME OF 14.0 TO 14.5 SECONDS FOR VEHICLE MANEUVERS (2001 AASHTO POLICY, EXHIBIT 3-3, DECISION SIGHT DISTANCE, AVOIDANCE MANEUVER E) MINUS THE LEGIBILITY DISTANCE OF 175 FT FOR THE APPROPRIATE SIGN.
 - 3 TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD, SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS. THE DISTANCES ARE BASED ON THE 2001 AASHTO POLICY, STOPPING SIGHT DISTANCE, EXHIBIT 3-1, PROVIDING A PIEV TIME OF 2.5 SECONDS, A DECELERATION RATE OF 11.2 FT/SECOND², MINUS THE SIGN LEGIBILITY DISTANCE OF 175 FT.
 - 4 TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANUEVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCE IS DETERMINED BY PROVIDING A 2.5 SECOND PIEV TIME, A VEHICLE DECELERATION RATE OF 10 FT / SECOND² MINUS THE SIGN LEGIBILITY DISTANCE OF 250 FT.
 - 5 NO SUGGESTED DISTANCES ARE PROVIDED FOR THESE SPEEDS. AS THE PLACEMENT LOCATION IS DEPENDANT ON SIGHT CONDITIONS AND OTHER SIGNING TO PROVIDE AN ADEQUATE ADVANCE WARNING FOR THE DRIVER.

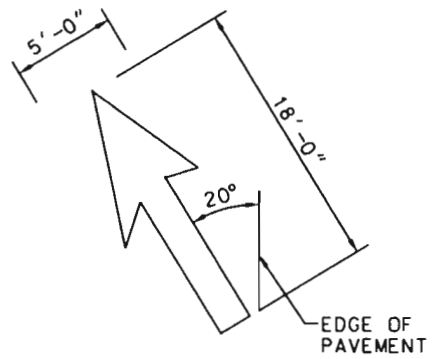
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
PAVEMENT MARKINGS FOR TYPICAL RAILROAD CROSSINGS			
APPROVED	DESIGN ENGINEER		DATE
DESIGNED BY	DRAWN BY	CHECKED BY	
704-02-1/1			



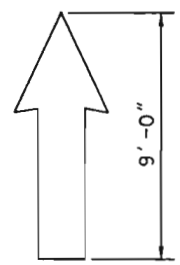
LEFT OR RIGHT TURN ONLY ARROW PAVEMENT MARKER
12' MIN. FROM STOP BAR



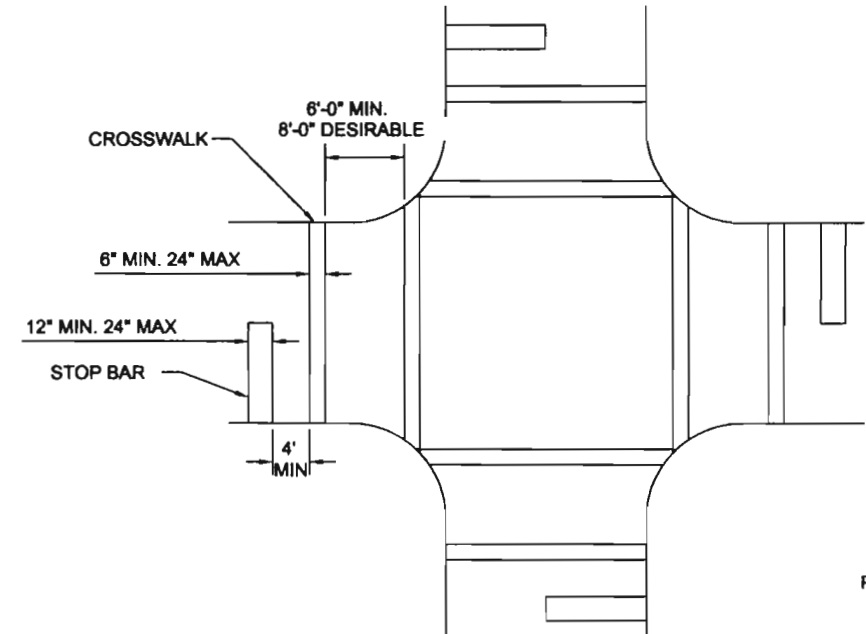
TURN AND THROUGH LANE-USE ARROW PAVEMENT MARKER



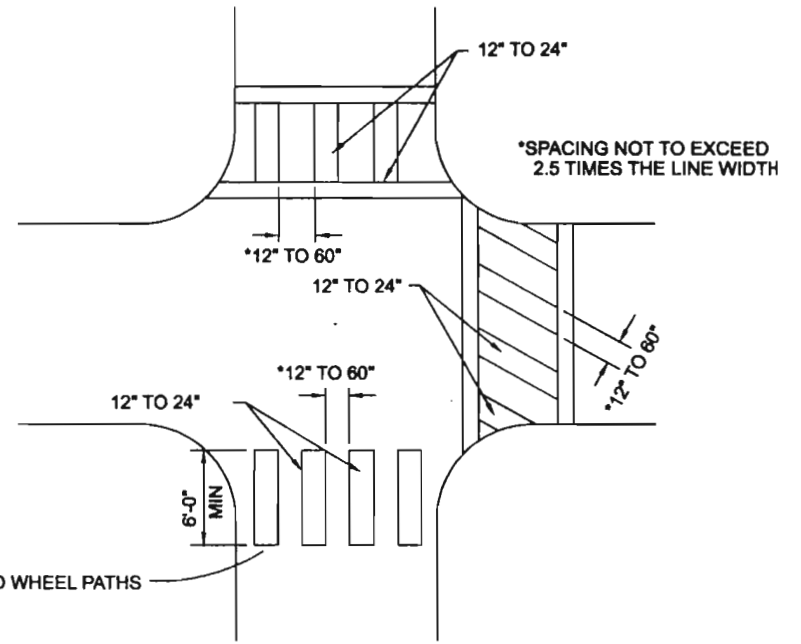
LANE-REDUCTION ARROW PAVEMENT MARKING
RECOMMENDED SPACING OF 72' TO 180'
(4 TO 10 TIMES HEIGHT OF THE ARROW)



THROUGH LANE-USE ARROW PAVEMENT MARKER



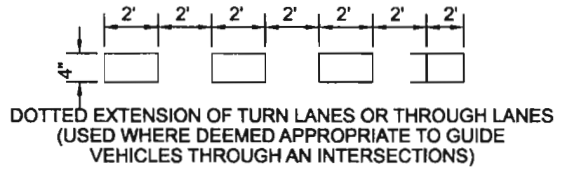
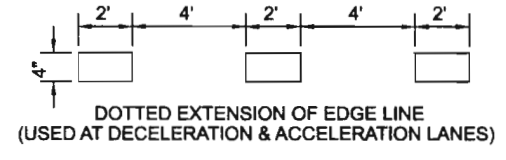
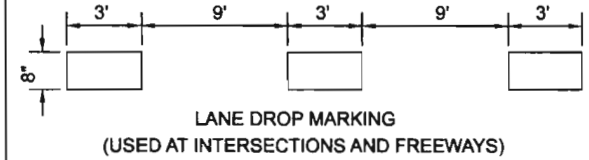
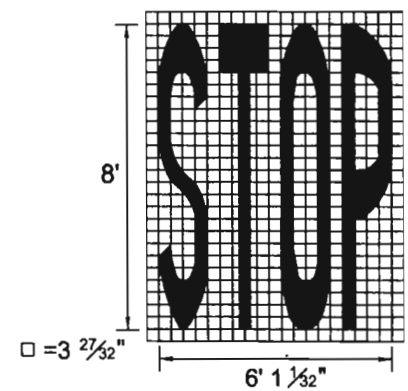
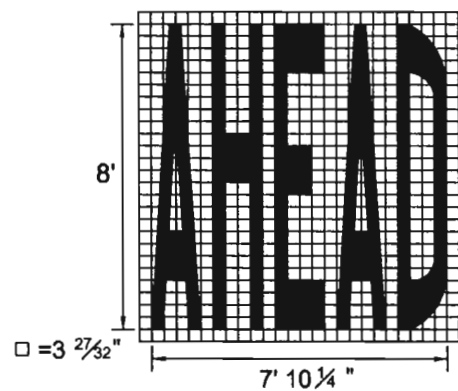
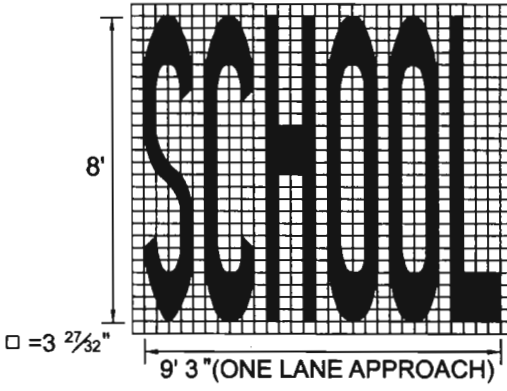
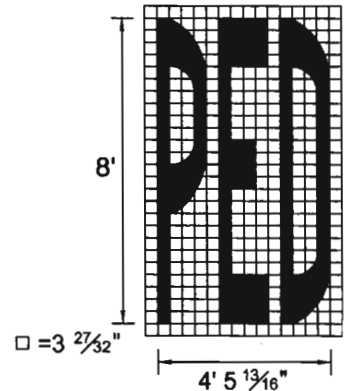
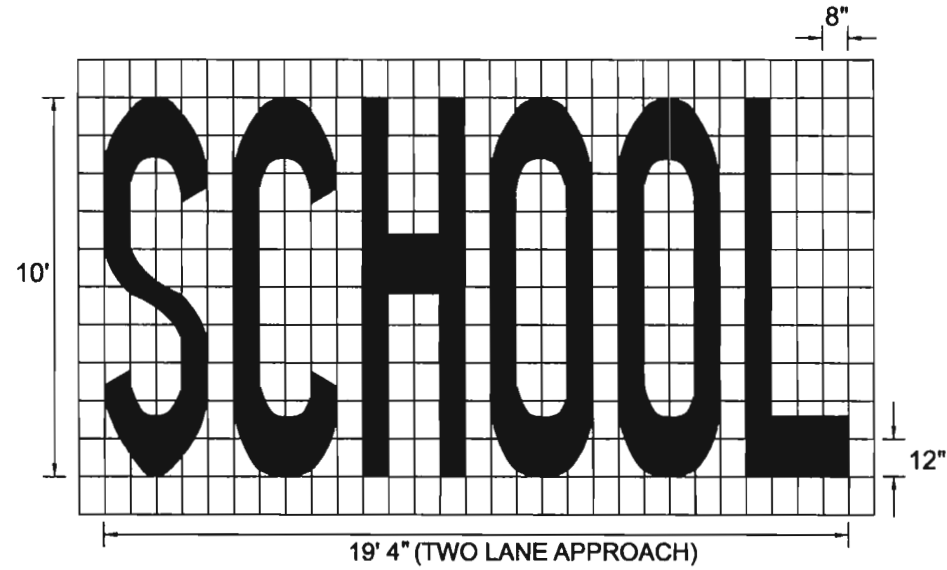
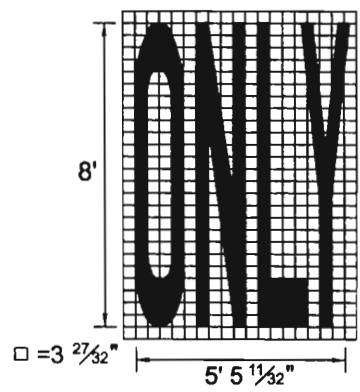
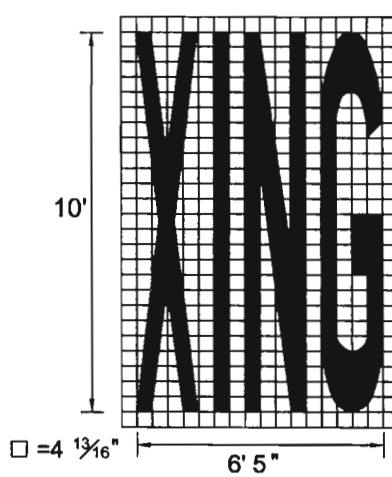
TYPICAL CROSSWALK AND STOP BAR PAVEMENT MARKINGS AT STOP CONTROLLED OR SIGNALIZED INTERSECTIONS.



TYPICAL CROSSWALK PAVEMENT MARKINGS

PLACE STRIPES TO AVOID WHEEL PATHS

NOTE: 12" TO 24" STOP BAR IS TO BE LOCATED IN COMPLIANCE WITH STOP SIGN OR TRAFFIC SIGNAL. IF CROSSWALKS ARE TO BE USED THE STOP LINE SHOULD BE PLACED 4' IN ADVANCE OF THE NEAREST CROSSWALK.



PAVEMENT MESSAGE NOTES:

1. ALL WORD MESSAGES SHALL USE STANDARD WORDING AND LETTERS FROM THE CURRENT MUTCD, THE STANDARD HIGHWAY SIGNS BOOK AND THE STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS.
2. THE LONGITUDINAL SPACE BETWEEN WORD OR SYMBOL MESSAGE MARKINGS, INCLUDING ARROW MARKINGS, SHOULD BE AT LEAST FOUR TIMES THE HEIGHT OF THE CHARACTERS FOR LOW SPEED ROADS, BUT NOT MORE THAN TEN TIMES THE HEIGHT OF THE CHARACTERS UNDER ANY CONDITIONS. THE SPACE BETWEEN THE LINES MAY BE REDUCED APPROPRIATELY WHERE THERE IS LIMITED SPACE BECAUSE OF LOCAL CONDITIONS.
3. PAVEMENT MESSAGES SHOULD GENERALLY BE NO MORE THAN ONE LANE IN WIDTH, EXCEPT THE "SCHOOL" MESSAGES WHICH MAY EXTEND TO THE WIDTH OF TWO LANES.
4. PAVEMENT MARKINGS AND PAVEMENT MESSAGES SHALL BE AS SPECIFIED IN THE CONSTRUCTION PLANS.



NO.	DATE	REV. BY	DESCRIPTION
1	4/16/08	BLJ	ADDED DIMENSIONS FOR WORD MESSAGES
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
PAVEMENT MARKINGS AND MESSAGES			
APPROVED <i>Kathleen Garcia</i>		4-17-08 DATE	
DESIGNED BY _____		DRAWN BY _____ CHECKED BY _____	
704-03-1/2		SHEET 1 OF 2	