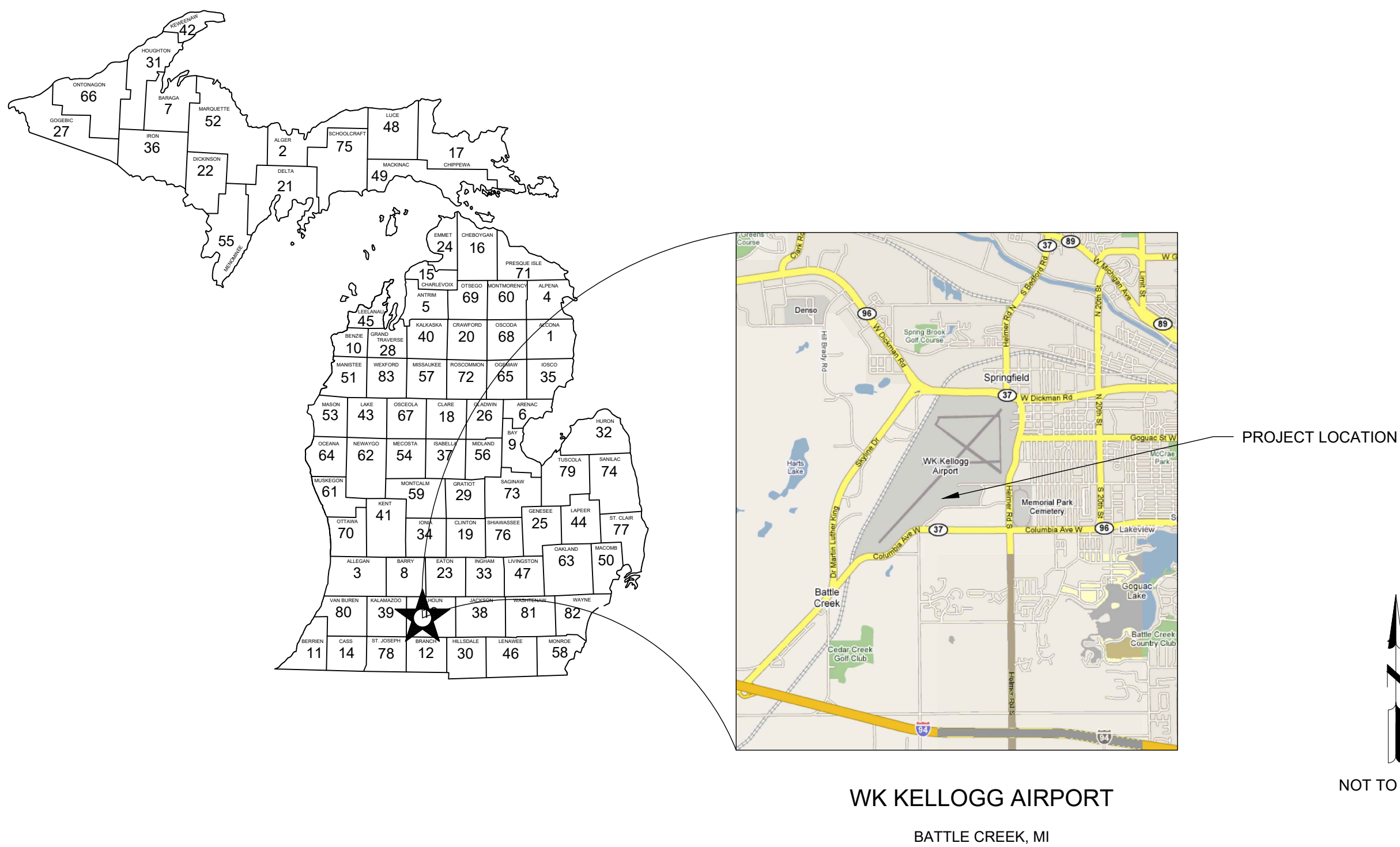


WK KELLOGG AIRPORT

CALHOUN COUNTY
SPONSOR: CITY OF BATTLE CREEK
AIP NO.: B-26-0008-4518

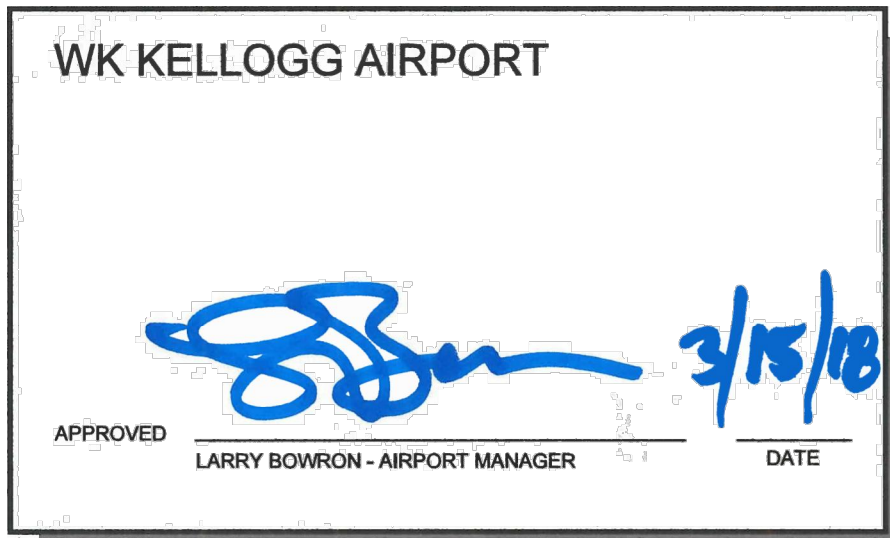
STATE CONTRACT NO.: FM 13-01-C85 & FM 13-01-C86

RECONSTRUCT TAXIWAY C PAVEMENT & LIGHTING PHASE 2



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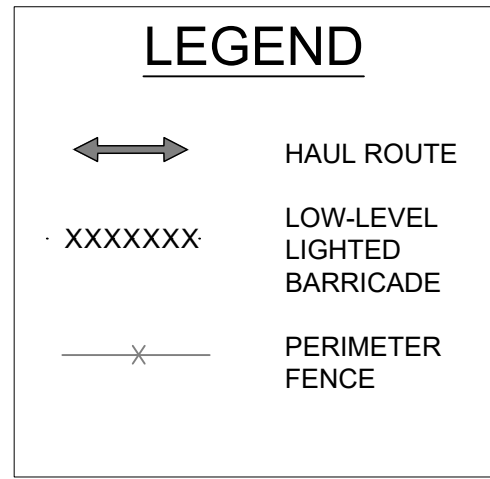


**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI**

AIP NO. B-26-0008-4518
M&H NO.: 0228800-161097.0
DATE: 03.29.2018
DESIGNED BY: ALP
DRAWN BY: ALP
CHECKED BY: JET
DO NOT SCALE DRAWINGS

SHEET NO. 3 of 76

G-050



* WORK IN PHASE 2 AREA PERMITTED FROM STATION 10+50 TO 32+24

1. ALL COST ASSOCIATED WITH WORK SHOWN ON G-050-G-056 SHALL BE INCLUDED IN ITEM NO. 100001 MOBILIZATION AND GENERAL CONDITIONS OR NO. 100002 SAFETY AND SECURITY.
2. ACCESS SHALL BE THROUGH THE ACCESS GATE SHOWN ABOVE ONLY, UNLESS GIVEN WRITTEN PERMISSION FROM THE AIRPORT OR ENGINEER. ANY WORK NECESSARY TO IMPROVE THE STAGING AREA SITE FOR THE CONTRACTOR OPERATIONS SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
3. ALL VEHICLES ENTERING OR EXITING THE SITE SHALL BE DRIVEN BY EMPLOYEES THAT HAVE OBTAINED AIRFIELD DRIVING PRIVILEGES THROUGH THE AIRPORTS BADGING PROCESS OR SHALL BE ESCORTED TO THE CONSTRUCTION SITE BY A VEHICLE DRIVEN BY AN EMPLOYEE THAT HAS OBTAINED AIRFIELD DRIVING PRIVILEGES THROUGH THE AIRPORTS BADGING PROCESS. AT NO POINT SHALL THE CONTRACTOR RELY ON THE AIRPORT OR ENGINEER TO PROVIDE ESCORTING SERVICES. ALL PERSONNEL WORKING ON THE PROJECT SHALL EITHER OBTAIN A VALID BADGE FROM THE AIRPORT OR BE UNDER DIRECT SUPERVISION OF A BADGED EMPLOYEE AT ALL TIMES. DIRECT SUPERVISION REQUIRES LINE OF SIGHT AT A DISTANCE NO GREATER THAN CAN BE REASONABLY EXPECTED TO HEAR VOCAL COMMANDS FROM THE BADGED EMPLOYEE.
4. AIRFIELD LIGHTS AND SIGNS IN CLOSED AREAS OR LEADING TO CLOSED AREAS SHALL BE DISABLED. THE CONTRACTOR SHALL INSTALL TEMPORARY JUMPERS AS NECESSARY TO PROVIDE POWER TO CIRCUITS AND PORTIONS OF CIRCUITS THAT ARE NOT UNDER CONSTRUCTION. SEE SHEET G-156 FOR TEMPORARY JUMPER PLAN.

<u>RUNWAY END COORDINATES</u>			
RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION
5L	N42° 17' 44.6330"	W85° 15' 57.5229"	951.8'
23R	N42° 18' 57.3726"	W85° 14' 27.4058"	928.4'
13	N42° 18' 51.0068"	W85° 15' 16.5345"	919.8'
31	N42° 18' 22.8575"	W85° 14' 24.5162"	929.4'
5R	N42° 17' 57.9332"	W85° 15' 00.7071"	925.0'
23L	N42° 18' 27.7449"	W85° 14' 23.7712"	930.0'

<u>CONTRACTOR STAGING AREA</u> <u>COORDINATES</u>		
CORNER	LATITUDE	LONGITUDE
W	N42° 17' 48.59"	W85° 15' 42.24"
X	N42° 17' 50.41"	W85° 15' 39.99"
Y	N42° 17' 49.41"	W85° 15' 38.52"
Z	N42° 17' 47.59"	W85° 15' 40.77"

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

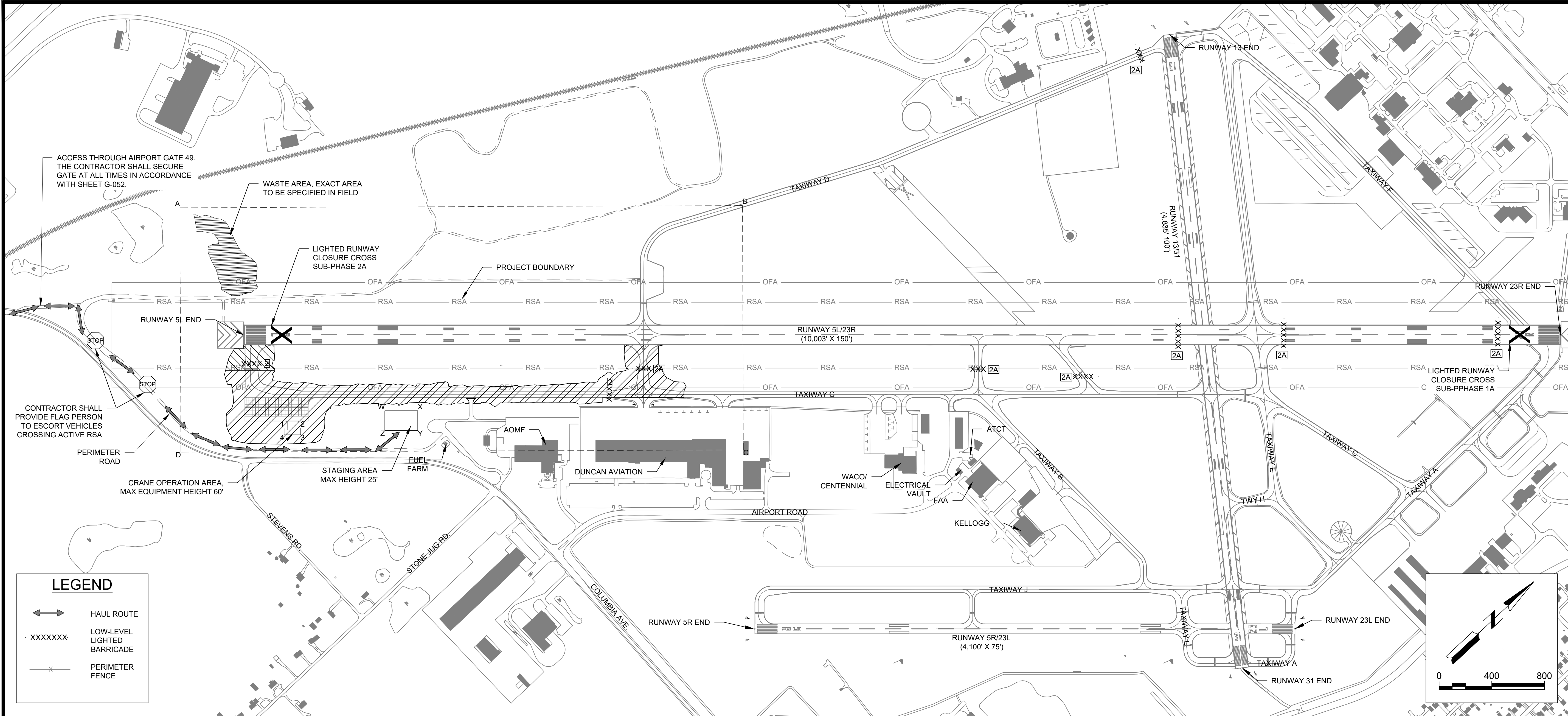
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AIP NO: B-26-0008-4518
M&H NO: 0228800-161097.01
DATE: 03.29.2018
DESIGNED BY: ALP
DRAWN BY: ALP
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DO NOT SCALE DRAWINGS

SHEET CONTENTS
CONSTRUCTION
SAFETY PHASING
PLAN - PHASES 2, AND
3

SHEET NO: 4 of 76

G-051



PHASE	CONSTRUCTION	AFFECTED ACTIVE OPERATION AREAS (AOA'S)	WORK HOURS	SAFETY AND SECURITY	CONTRACT TIME
2	RECONSTRUCTION OF TWY C FROM STATION 5+02.46 TO STATION 32+24 AND EXPANSION OF RUN UP PAD. INCLUDES ALL WORK TO BE COMPLETED EXCEPT FULL RATE PAVEMENT MARKINGS. CONSTRUCTION OF BLAST FENCE INCLUDED IN THIS PHASE.	TWY C CLOSED SOUTH OF TWY D	ANY	CONTRACTOR SHALL INSTALL LOW-LEVEL BARRICADES AT LOCATIONS SHOWN ON THE PLANS. CONTRACTOR SHALL DISCONNECT APPROPRIATE EDGE LIGHTING CIRCUITS AND INSTALL JUMPERS AS NECESSARY. THE CONTRACTOR SHALL START PHASE 2 IMMEDIATELY FOLLOWING THE COMPLETION OF PHASE 1. CONTRACTOR SHALL COORDINATE WITH ENGINEER AND AIRPORT 7 DAYS PRIOR TO STARTING CONSTRUCTION. AIRPORT MANAGER SHALL ISSUE ALL NOTAMS. MAXIMUM EQUIPMENT HEIGHT SHALL BE 25'.	50
2A	RECONSTRUCTION OF TWY C WITHIN RWY5L/23R RSA. INCLUDES ALL WORK TO BE COMPLETED EXCEPT FULL RATE PAVEMENT MARKINGS.	RUNWAY 5L/23R CLOSED. TWY C CLOSED FROM RWY 5L/23R END TO SOUTH DUNCAN ENTRANCE.	THURSDAY TO SUNDAY (ANY HOURS)	IN ADDITION TO PHASE 2 LOW-LEVEL BARRICADES, CONTRACTOR SHALL INSTALL LOW-LEVEL BARRICADES AT LOCATIONS SHOWN ON THE PLAN LABELED AS 2A. LIGHTED RUNWAY CLOSURE CROSSES SHALL BE INSTALLED AS LABELED AS 2A CONTRACTOR SHALL DISCONNECT APPROPRIATE EDGE LIGHTING CIRCUITS AND INSTALL JUMPERS AS NECESSARY. CONTRACTOR SHALL COORDINATE WITH ENGINEER AND AIRPORT 7 DAYS PRIOR TO STARTING PHASE 2A. AIRPORT MANAGER SHALL ISSUE ALL NOTAMS. MAXIMUM EQUIPMENT HEIGHT SHALL BE 25'.	4 DAYS WITHIN PHASE 2
3	FULL RATE PAVEMENT MARKING	VARIOUS SHORT TERM CLOSURES	8 AM - 10 PM	CONTRACTOR SHALL COMPLETE PHASE 3 30 DAYS AFTER COMPLETION OF PHASE 2A, BUT NOT MORE THAN 60 DAYS AFTER COMPLETION OF PHASE 2A.	1 DAY

TOTAL PHASE 2 : 50 DAYS
TOTAL PHASE 3 : 1 DAYS
TOTAL : 65 DAYS

SAFETY PHASING NOTE:

- ALL COST ASSOCIATED WITH WORK SHOWN ON G-050-G-056 SHALL BE INCLUDED IN ITEM NO. 100001 MOBILIZATION AND GENERAL CONDITIONS OR NO. 100002 SAFETY AND SECURITY.
- ACCESS SHALL BE THROUGH THE ACCESS GATE SHOWN ABOVE ONLY, UNLESS GIVEN WRITTEN PERMISSION FROM THE AIRPORT OR ENGINEER. ANY WORK NECESSARY TO IMPROVE THE STAGING AREA SITE FOR THE CONTRACTOR OPERATIONS SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
- ALL VEHICLES ENTERING OR EXITING THE SITE SHALL BE DRIVEN BY EMPLOYEES THAT HAVE OBTAINED AIRFIELD DRIVING PRIVILEGES THOUGH THE AIRPORTS BADGING PROCESS OR SHALL BE ESCORTED TO THE CONSTRUCTION SITE BY A VEHICLE DRIVEN BY AN EMPLOYEE THAT HAS OBTAINED AIRFIELD DRIVING PRIVILEGES THROUGH THE AIRPORTS BADGING PROCESS. AT NO POINT SHALL THE CONTRACTOR RELY ON THE AIRPORT OR ENGINEER TO PROVIDE ESCORTING SERVICES. ALL PERSONNEL WORKING ON THE PROJECT SHALL EITHER OBTAIN A VALID BADGE FROM THE AIRPORT OR BE UNDER DIRECT SUPERVISION OF A BADGED EMPLOYEE AT ALL TIMES. DIRECT SUPERVISION REQUIRES LINE OF SIGHT AT A DISTANCE NO GREATER THAN CAN BE REASONABLY EXPECTED TO HEAR VOCAL COMMANDS FROM THE BADGED EMPLOYEE.
- AIRFIELD LIGHTS AND SIGNS IN CLOSED AREAS OR LEADING TO CLOSED AREAS SHALL BE DISABLED. THE CONTRACTOR SHALL INSTALL TEMPORARY JUMPERS AS NECESSARY TO PROVIDE POWER TO CIRCUITS AND PORTIONS OF CIRCUITS THAT ARE NOT UNDER CONSTRUCTION. SEE SHEET G-156 FOR TEMPORARY JUMPER PLAN.

CONSTRUCTION AREA COORDINATES
(MAX EQUIPMENT HEIGHT 25')

CORNER	LATITUDE	LONGITUDE	GROUND ELEVATION
A	N42° 17' 47.58"	W85° 16' 11.34"	935'
B	N42° 18' 18.75"	W85° 15' 33.05"	923'
C	N42° 18' 06.40"	W85° 15' 14.82"	927'
D	N42° 17' 35.24"	W85° 15' 53.11"	942'

CRANE AREA COORDINATES (MAX
EQUIPMENT HEIGHT 60')

CORNER	LATITUDE	LONGITUDE	GROUND ELEVATION
1	N42° 17' 42.16"	W85° 15' 47.84"	939'
2	N42° 17' 43.07"	W85° 15' 46.71"	939'
3	N42° 17' 42.73"	W85° 15' 46.22"	939'
4	N42° 17' 41.82"	W85° 15' 47.34"	939'

RUNWAY END COORDINATES

RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION
5L	N42° 17' 44.6330"	W85° 15' 57.5229"	951.8'
23R	N42° 18' 57.3726"	W85° 14' 27.4058"	928.4'
13	N42° 18' 51.0068"	W85° 15' 16.5345"	919.8'
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5R	N42° 17' 57.9332"	W85° 15' 00.7071"	925.0'
23L	N42° 18' 27.7449"	W85° 14' 23.7712"	930.0'

SAFETY AREA

ACTIVE OPERATION AREA	SAFETY AREA WIDTH	OBJECT FREE AREA WIDTH
RUNWAY 5L-23R	500'	800'
RUNWAY 13-31	150'	500'
RUNWAY 5R-23L	150'	500'

CONTRACTOR STAGING AREA
COORDINATES

CORNER	LATITUDE	LONGITUDE
W	N42° 17' 48.59"	W85° 15' 42.24"
X	N42° 17' 50.41"	W85° 15' 39.99"
Y	N42° 17' 49.41"	W85° 15' 38.52"
Z	N42° 17' 47.59"	W85° 15' 40.77"

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THE FOLLOWING NOTES ARE IN ACCORDANCE WITH FAA AC 5370-2G, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.

SAFETY PLAN COMPLIANCE DOCUMENT:
THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DESCRIBING HOW THE REQUIREMENTS OF THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) WILL BE MET. THE SPCD SHALL INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT: 1) INDICATES FULL UNDERSTANDING OF THE OPERATIONAL SAFETY REQUIREMENTS OF THE CONSTRUCTION SAFETY PHASING PLAN AND 2) ASSERTS NO DEVIATIONS SHALL BE MADE FROM THE APPROVED CONSTRUCTION SAFETY PHASING PLAN AND SPCD UNLESS WRITTEN APPROVAL IS GRANTED BY THE AIRPORT OPERATOR. THE SPCD SHALL FOLLOW THE FORMAT OF FAA AC 150/5370-2F, SECTION 204(b) AND SHALL ADDRESS ITEMS (1) - (18) PER THE REQUIREMENTS OF THE AC.

THE CONTRACTOR SHALL SUBMIT THE SPCD TO THE PROJECT ENGINEER FOR REVIEW AND APPROVAL BY THE AIRPORT SPONSOR FOURTEEN (14) DAYS PRIOR TO THE ANTICIPATED ISSUE OF THE NOTICE TO PROCEED. COPIES OF THE APPROVED CONSTRUCTION SAFETY PHASING PLAN AND SAFETY PLAN COMPLIANCE DOCUMENT SHALL BE MAINTAINED AT THE PROJECT SITE.

GENERAL CONSIDERATIONS:
AIRPORT OPERATORS, OR TENANTS CONDUCTING CONSTRUCTION ON LEASED PROPERTIES, SHOULD USE THE PRE-DESIGN, PRE-BID, AND PRE-CONSTRUCTION MEETINGS TO INTRODUCE AIRPORT OPERATIONAL SAFETY DURING CONSTRUCTION. COORDINATE THE FOLLOWING AS REQUIRED:

A. OPERATIONAL SAFETY SHOULD BE A STANDING AGENDA ITEM DURING EVERY PROGRESS MEETING.

B. CHANGES IN THE SCOPE OR DURATION OF THE PROJECT MAY REQUIRE REVISIONS TO THE SAFETY PHASING PLAN (AND REVIEW AND APPROVAL BY THE AIRPORT OPERATOR AND FAA). **REVISIONS WILL BE THE RESPONSIBILITY OF THE AIRPORT OR ENGINEER.**

C. EARLY COORDINATION WITH FAA ATO IS REQUIRED TO SCHEDULE AIRWAY FACILITY SHUTDOWNS AND RESTARTS. RELOCATION OR ADJUSTMENTS TO NAVAIDS OR THEIR CRITICAL AREAS MAY REQUIRE AN FAA FLIGHT INSPECTION PRIOR TO RESTARTING THE FACILITY. FLIGHT INSPECTIONS MUST BE COORDINATED WELL IN ADVANCE AND MAY REQUIRE A REIMBURSABLE AGREEMENT BETWEEN THE AIRPORT AND FAA ATO. REIMBURSABLE AGREEMENTS SHOULD BE COORDINATED A MINIMUM OF 12 MONTHS PRIOR TO THE START OF CONSTRUCTION.

AREAS AND OPERATIONS AFFECTED BY CONSTRUCTION ACTIVITY:
SEE THE CSPP FOR A DEPICTION OF ALL AREAS AFFECTED BY CONSTRUCTION, CLOSED OR PARTIALLY CLOSED RUNWAYS, TAXIWAYS, AND APRONS.

A. IF AFFECTED, ACCESS ROUTES AND DETOURS WILL BE SHOWN FOR AIRCRAFT RESCUE AND FIREFIGHTING (ARFF) OR AIRPORT AND AIRLINE SUPPORT VEHICLES.

B. IF AFFECTED, INTERRUPTIONS OF UNDERGROUND UTILITIES, INCLUDING WATER SUPPLY FOR FIREFIGHTING WILL BE SHOWN.

C. RUNWAY APPROACH/DEPARTURE AREAS AND CLEARWAYS WILL BE PROTECTED PER CSPP.

D. CONSTRUCTION AREAS, STORAGE AREAS AND ACCESS ROUTES WILL BE SHOWN.

E. THE CSPP ADDRESSES TEMPORARY CHANGES TO RUNWAY AND/OR TAXIWAY OPERATIONS, MAINTENANCE OF ESSENTIAL UTILITIES, AND TEMPORARY CHANGES TO AIR TRAFFIC CONTROL PROCEDURES TO BE COORDINATED WITH THE ATO.

NAVAIDS:
A. BEFORE COMMENCING CONSTRUCTION ACTIVITY, PARKING VEHICLES, OR STORING CONSTRUCTION EQUIPMENT AND MATERIALS NEAR A NAVAID, COORDINATE WITH THE APPROPRIATE FAA ATO/TECH OPS OFFICE TO EVALUATE THE EFFECT OF CONSTRUCTION ACTIVITY AND THE REQUIRED DISTANCE AND DIRECTION FROM THE NAVAID. SEE THE CSPP FOR APPLICABLE NAVAID CRITICAL AREAS. **ALL COORDINATION WITH THE FAA ATO/TECH OPS WILL BE THE RESPONSIBILITY OF THE AIRPORT OR ENGINEER.**

B. IF THIS PROJECT AFFECTS RUNWAY NAVAIDS, COORDINATION WITH LOCAL TECHNICAL OPERATIONS WILL BEGIN AT THE PRE-CONSTRUCTION MEETING. A LIST OF RESPONSIBLE PARTY REPRESENTATIVES, INCLUDING PROCEDURES FOR CONTACT AFTER-HOURS, SHOULD BE OBTAINED PRIOR TO CONSTRUCTION. A 45-DAY MINIMUM NOTICE SHOULD BE PROVIDED TO FAA ATO/TECH OPS REGARDING SHUT-DOWN OF A NAVAID MORE THAN 24 HOURS OR MORE THAN 4 HOURS ON CONSECUTIVE DAYS. **ALL COORDINATION WITH THE FAA ATO/TECH OPS WILL BE THE RESPONSIBILITY OF THE AIRPORT OR ENGINEER.**

C. INTERFERENCE FROM CONSTRUCTION EQUIPMENT AND ACTIVITIES MAY REQUIRE NAVAID SHUTDOWN OR ADJUSTMENT OF INSTRUMENT APPROACH MINIMUMS FOR LOW VISIBILITY OPERATIONS. THIS REQUIRES THAT A NOTAM BE FILED.

CONTRACTOR ACCESS:

A. SEE THE CSPP FOR AREAS ACCESSIBLE TO THE CONTRACTOR AND ACCESS ROUTES.

B. STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE SAFETY AREA OR OBJECT FREE AREA OF AN OPERATIONAL RUNWAY OR TAXIWAY (OFA STOCKPILES REQUIRE FAA APPROVAL). IF THE CONTRACTOR WISHES TO STOCKPILE MATERIALS OR EQUIPMENT ADJACENT TO AN OFA, HE MUST FIRST COORDINATE WITH THE ENGINEER AND AIRPORT OPERATOR TO ENSURE THAT 1) APPROPRIATE LIGHTING AND BARRICADES ARE IN PLACE, AND 2) THE STOCKPILED MATERIALS DO NOT CREATE A WILDLIFE ATTRACTANT OR FOREIGN OBJECT DEBRIS (FOD) HAZARD. EXCAVATION AND RESULTANT STOCKPILES ADJACENT TO OTHER PAVED SURFACES MUST BE APPROPRIATELY MARKED WITH BARRICADES, AS DIRECTED BY THE ENGINEER.

C. THE CONTRACTOR'S EQUIPMENT IS STRICTLY LIMITED TO THE CONSTRUCTION AREAS DURING CONSTRUCTION AND TO THE CONTRACTOR STORAGE AREA DURING NON-CONSTRUCTION PERIODS, EXCEPT AS PROVIDED ON THE PLANS, IN THE CONTRACT SPECIAL PROVISIONS, OR AS AUTHORIZED BY THE ENGINEER IN WRITING. CONTRACTOR PARKING SHALL BE LIMITED TO THE STAGING AREA, EXCEPT AS ALLOWED BY THE AIRPORT.

D. HAUL ROUTES AND ACCESS TO THE CONSTRUCTION SITE(S) WILL BE AS SHOWN ON THE CONSTRUCTION SAFETY PHASING PLAN AND DISCUSSED AT THE PRE-BID MEETING. THE CONTRACTOR IS RESPONSIBLE FOR RESTORING ALL HAUL ROADS, PLANT SITES, STAGING AND STORAGE AREAS TO ORIGINAL CONDITION OR TO THE SATISFACTION OF THE ENGINEER/AIRPORT. THIS WILL INCLUDE BUT NOT BE LIMITED TO GRADING (FILLING IF NECESSARY) AND SEEDING AND MULCHING ALL TURF AREAS USED BY THE CONTRACTOR. ANY PAVEMENT AREAS USED BY THE CONTRACTOR AS A HAUL ROUTE WHICH ARE DAMAGED WILL BE REPAVED AS APPROVED BY THE PROJECT ENGINEER AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL INSPECT ALL HAUL ROUTES PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR ANY DOCUMENTATION OF EXISTING HAUL ROUTES, EITHER WITH PICTURES OR VIDEO. ANTICIPATED COSTS ASSOCIATED WITH DOCUMENTING EXISTING HAUL ROUTE CONDITIONS, OR RECONSTRUCTING OR RESTORING HAUL ROUTES AND STORAGE AREAS, WILL BE INCLUDED IN THE PROJECT ITEMS.

E. ALL CONSTRUCTION EQUIPMENT MUST BE MARKED WITH A 3' X 3' ORANGE AND WHITE CHECKERED FLAG AND/OR AMBER BEACON. FOR NIGHT CONSTRUCTION, ALL EQUIPMENT MUST BE EQUIPPED WITH AN AMBER BEACON. ALL CONSTRUCTION VEHICLES MUST BE CLEARLY MARKED WITH THE COMPANY NAME/LOGO AT ALL TIMES. MARKING AND LIGHTING SHALL OTHERWISE BE IN ACCORDANCE WITH AC 150/5210-5.

F. AIRPORT RADIOS, FLAG PERSON AND DRIVER TRAINING: THE CONTRACTOR SHALL MAINTAIN AND MONITOR AN AIRPORT RADIO ON-SITE. THE RADIO FREQUENCY FOR THE AIRPORT IS 128.325 MHz, GROUND CONTROL FOR THE AIRPORT 121.7 MHz, AND UNICOM FOR AIRPORT 122.95 MHz. RADIOS ARE NOT AVAILABLE FROM THE AIRPORT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AN ADEQUATE NUMBER OF RADIOS FOR THEIR OPERATIONS. TRAINING OF CONTRACTORS ON PROPER COMMUNICATION PROCEDURES IS ESSENTIAL FOR MAINTAINING AIRPORT OPERATIONAL SAFETY. IF THE AIRPORT PROVIDES RADIO COMMUNICATION TRAINING, CONTRACTOR ATTENDANCE WILL BE REQUIRED PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR SHALL BE REQUIRED TO HAVE A TRAINED FLAG PERSON EQUIPPED WITH AN AIRPORT RADIO OPERATING ON 128.325 MHz, GROUND CONTROL FOR THE AIRPORT 121.7 MHz, AND UNICOM FOR AIRPORT 122.95 MHz TO ESCORT ANY CONTRACTOR VEHICLES WHEN CROSSING ACTIVE AOA'S. COST OF MAINTAINING THE FLAG PERSON SHALL BE INCLUDED IN THE PROJECT ITEMS. KEY CONTRACTOR PERSONNEL MAY BE EXPECTED TO TRAVEL ON OR NEAR ACTIVE AOA'S. THESE INDIVIDUALS MUST COMPLY WITH THE AIRPORT OPERATOR'S RULES AND REGULATIONS BY PARTICIPATING IN ANY APPLICABLE AIRFIELD DRIVER'S TRAINING PROCEDURES PROVIDED BY THE AIRPORT.

G. AIRPORTS SUBJECT TO 49 CFR PART 1542, AIRPORT SECURITY, SHALL BE REQUIRED TO MEET ALL APPLICABLE STANDARDS. THE CONTRACTOR SHALL PROVIDE A GATE GUARD AT ALL POINTS OF ENTRY USED BY THE CONTRACTOR. GATES MUST BE LOCKED WHEN NOT IN USE OR NOT GUARDED BY THE CONTRACTOR. AIRPORT OPERATORS AND CONTRACTORS MUST TAKE CARE TO MAINTAIN A HIGH LEVEL OF SAFETY AND SECURITY DURING CONSTRUCTION WHEN ACCESS POINTS ARE CREATED IN THE SECURITY FENCING TO PERMIT THE PASSAGE OF CONSTRUCTION VEHICLES OR PERSONNEL. TEMPORARY GATES SHALL BE EQUIPPED SO THEY CAN BE SECURELY CLOSED AND LOCKED TO PREVENT ACCESS BY ANIMALS AND PEOPLE. PROCEDURES SHALL BE IN PLACE TO ENSURE THAT ONLY AUTHORIZED PERSONS AND VEHICLES HAVE ACCESS TO THE AOA AND TO PROHIBIT PIGGYBACKING BEHIND ANOTHER VEHICLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER OPERATION AND PRESERVATION OF ALL AIRFIELD PERIMETER GATES, EITHER MANUAL OR AUTOMATED. FOR THE PURPOSES OF THIS CONTRACT, ALL FENCING/GATES SHALL BE CONSIDERED PART OF CONTRACTOR HAUL ROUTES, AS SUCH, ANY DAMAGE TO FENCING/GATES AS A RESULT OF CONSTRUCTION TRAFFIC SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

WILDLIFE MANAGEMENT:

THE CSPP AND SPCD MUST BE IN COMPLIANCE WITH THE AIRPORT'S WILDLIFE MANAGEMENT PLAN. IF APPLICABLE. CONTRACTORS MUST CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT CAN CREATE WILDLIFE HAZARDS OR ATTRACTANTS. THESE INCLUDE TRASH, STANDING WATER, TALL GRASS, SCATTERED SEEDS, POORLY MAINTAINED FENCING, AND DISRUPTION OF EXISTING HABITAT.

FOREIGN OBJECT DEBRIS MANAGEMENT:

A. DEBRIS SHALL NOT BE DEPOSITED ON ANY PORTION OF AN OPERATIONAL RUNWAY, TAXIWAY OR APRON. SHOULD ANY DEBRIS BE

ACCIDENTALLY DEPOSITED ON ACTIVE OPERATIONAL AREAS, IT SHALL BE REMOVED IMMEDIATELY. THE CONTRACTOR SHALL HAVE SWEEPING AND/OR VACUUMING CAPABILITIES ON-SITE IN ORDER TO CONTINUOUSLY REMOVE DEBRIS FROM ACTIVE OPERATIONAL AREAS DURING THE CONSTRUCTION PROJECT. PRIOR TO OPENING AIRCRAFT MOVEMENT AREAS CLOSED FOR THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS CLEAR OF ANY FOREIGN OBJECT DEBRIS (FOD) AND IS ACCEPTABLE TO THE AIRPORT. THE CONTRACTOR SHALL CONDUCT DAILY INSPECTIONS OF THE WORK AND ADJACENT AREAS FOR SAFETY AND CLEANLINESS. THE AIRPORT MAY ALSO PERFORM DAILY INSPECTIONS. UPON COMPLETION OF THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS RETURNED TO ORIGINAL CONDITION.

B. CONTRACTOR SHALL USE ALL MEANS NECESSARY TO MINIMIZE DUST DURING CONSTRUCTION OPERATIONS. IF THE AIRPORT OR ENGINEER REQUESTS DUST CONTROL IT SHALL BE APPLIED IMMEDIATELY.

HAZARDOUS MATERIALS MANAGEMENT:

ANY TYPE OF FUELING SUPPORT FACILITY OR DEVICE USED TO REFUEL CONSTRUCTION EQUIPMENT IS SUBJECT TO LOCAL FIRE INSPECTION. LOCAL FIRE CODES AND SAFETY STANDARDS SHALL BE MET PRIOR TO COMMENCEMENT OF WORK. CONTRACTORS OPERATING CONSTRUCTION VEHICLES AND EQUIPMENT ON THE AIRPORT MUST BE PREPARED TO EXPEDITIOUSLY CONTAIN AND CLEAN UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS. TRANSPORT AND HANDLING OF OTHER HAZARDOUS MATERIALS ALSO REQUIRE SPECIAL PROCEDURES (SEE AC 150/5320-15, MANAGEMENT OF AIRPORT INDUSTRIAL WASTE).

NOTIFICATION OF CONSTRUCTION ACTIVITIES:

A. THE CONTRACTOR SHALL INCLUDE IN THE SPCD A PROCEDURE FOR IMMEDIATE NOTIFICATION OF THE AIRPORT AND LOCAL FAA OF ANY ADVERSE CONDITIONS AFFECTING SAFETY ON THE AIRPORT.

B. THE CONTRACTOR SHALL OBTAIN CONTACT INFORMATION FOR ALL INVOLVED PARTIES AND PROCEDURES FOR CONTACTING THEM 24 HOURS A DAY, 7 DAYS A WEEK. THIS INCLUDES, IF APPLICABLE, A LIST OF LOCAL FAA ATO PERSONNEL, ATCT MANAGERS ON DUTY, AUTHORIZED REPRESENTATIVES TO THE FAA OPERATIONS CONTROL CENTER (OCC).

C. THE AIRPORT SHALL ISSUE ALL NOTAMS RELEVANT TO THE CONSTRUCTION PROGRESS. THE AIRPORT OPERATOR MAY NEED TO PROVIDE INFORMATION ON CLOSED OR HAZARDOUS CONDITIONS TO THE OCC.

D. THE CONTRACTOR SHALL PREPARE AND SUBMIT CONTACT INFORMATION FOR LOCAL MEDICAL, FIREFIGHTING, AND POLICE RESPONSE IN CASE OF EMERGENCIES DURING CONSTRUCTION.

E. THE CONTRACTOR MAY BE REQUIRED TO COORDINATE WITH AIRPORT ARFF PERSONNEL EVEN FOR NON-EMERGENCIES, SUCH AS DEACTIVATION OF WATERLINES, REROUTING OF EMERGENCY ACCESS ROUTES, OR USE OF HAZARDOUS MATERIALS.

F. PER 14 CFR PART 77, THIS CSPP HAS BEEN SUBMITTED TO THE FAA BY THE TIME OF CONSTRUCTION. 14 CFR PART 157 IS NOT APPLICABLE TO THIS PROJECT.

G. TO REPORT EMERGENCY IMPACTS TO NAVAIDS, CALL (866) 432-2622.

INSPECTION REQUIREMENTS:

A. AIRPORT SAFETY SELF-INSPECTIONS SHOULD BE CONDUCTED BY THE CONTRACTOR AT LEAST DAILY, BUT MORE FREQUENTLY IF NECESSARY TO CONFORM WITH THE CSPP. A SAMPLE DAILY INSPECTION CHECKLIST IS AVAILABLE IN APPENDIX D OF AC 150/5370-2G.

B. A FINAL INSPECTION MEETING WILL ALSO BE HELD FOR THIS PROJECT THAT MUST BE ATTENDED BY THE CONTRACTOR.

UNDERGROUND UTILITIES:

A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF EXISTING AIRPORT UTILITIES, AND ELECTRICAL CIRCUITS, WHETHER OWNED BY THE AIRPORT OR OTHER AGENCIES OPERATING AT THE AIRPORT (SEE GENERAL PROVISION 70-15). ANY LOCATIONS OF EXISTING CABLE SHOWN IN THE PLANS ARE BASED ON AVAILABLE AS-BUILT DATA AND ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL COORDINATE THE LOCATION OF ANY CROSSING OF UNDERGROUND ELECTRICAL CIRCUITS WITH THE AIRPORT AND FAA FACILITIES PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL PROVIDE TONING EQUIPMENT CAPABLE OF DETECTING A 60 HZ SIGNAL FOR LOCATING CABLES IN THE CONSTRUCTION AREA. WORKING IN CONJUNCTION WITH THE ENGINEER, THE CONTRACTOR SHALL LOCATE EXISTING CABLES PRIOR TO START OF CONSTRUCTION. COST OF LOCATING THE CABLES AND PROVIDING THE TONING EQUIPMENT SHALL BE INCLUDED IN THE PROJECT ITEMS. ANY ELECTRICAL CIRCUITS TO BE CROSSED SHALL THEN BE TESTED FOR RESISTANCE TO GROUND BY THE CONTRACTOR UNDER SUPERVISION OF THE ENGINEER AND THE READINGS RECORDED. FOLLOWING CONSTRUCTION, THE CONTRACTOR, UNDER THE SUPERVISION OF THE ENGINEER, SHALL CHECK ALL CIRCUITS CROSSED DURING THE PROJECT. ANY CIRCUITS SHOWING A DECREASE IN RESISTANCE TO GROUND SHALL BE CORRECTED BY THE CONTRACTOR SO AS TO PROVIDE AT LEAST 50 MEGOHMS RESISTANCE IN THAT CIRCUIT. NO ADDITIONAL COMPENSATION WILL BE MADE FOR ANY CORRECTIVE ELECTRICAL WORK DUE TO CONTRACTOR'S OPERATIONS.

B. A 7-DAY ADVANCE NOTICE SHALL BE GIVEN TO THE SSC MANAGER BEFORE LOCATION OF FAA CABLES IS REQUIRED. FAA SHALL LOCATE CABLES ONLY ONCE DURING CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR RECORDING FAA CABLE LOCATIONS IN THE EVENT THAT MARKS ARE LOST DURING CONSTRUCTION. THE CONTRACTOR SHALL HYDRO-EXCAVATE ONLY WHEN DIGGING WITHIN 5 FEET OF ANY MARKED FAA BURIED CABLE LOCATION. ANY FAA CABLE(S) DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE COMPLETELY REPLACED (NOT SPLICED OR REPAIRED), AT THE CONTRACTOR'S EXPENSE.

PENALTIES:

THIS PROJECT INVOLVES WORK NEAR ACTIVE AIRCRAFT OPERATIONAL AREAS, ANY RUNWAY OR TAXIWAY INCURSIONS OCCURRING DURING THE PROJECT, AS DETERMINED BY THE JUDGEMENT OF THE ENGINEER AND/OR AIRPORT, MAY BE SUBJECT TO FAA FINES IN EXCESS OF \$10,000. FAA DEFINES A RUNWAY INCURSION AS "ANY OCCURRENCE AT AN AERODROME INVOLVING THE INCORRECT PRESENCE OF AN AIRCRAFT, VEHICLE OR PERSON ON THE PROTECTED AREA OF A SURFACE DESIGNATED FOR THE LANDING AND TAKEOFF OF AIRCRAFT."

RUNWAY & TAXIWAY VISUAL AIDS—MARKING, LIGHTING, SIGNS, AND VISUAL NAVAIDS:

A. AIRPORT MARKINGS, LIGHTING, SIGNS, AND VISUAL NAVAIDS MUST BE CLEARLY VISIBLE TO PILOTS, NOT MISLEADING, CONFUSING OR DECEPTIVE. ALL MUST BE SECURED IN PLACE TO PREVENT MOVEMENT BY PROP WASH, JET BLAST, WIND VORTICES OR OTHER WIND CURRENTS, BUT FRANGIBLE AND CONSTRUCTED OF MATERIALS THAT WOULD MINIMIZE DAMAGE TO AN AIRCRAFT IN THE EVENT OF INADVERTENT CONTACT.

B. THE FOLLOWING ARE APPLICABLE STANDARDS FOR ANY TEMPORARY OR PERMANENT INSTALLATIONS OF THESE RESPECTIVE ITEMS:
AIRPORT MARKINGS: AC 150/5340-1
LIGHTING: AC 150/5340-30, 150/5345-50, 150/5345-53
SIGNAGE: AC 150/5345-44, 150/5340-18, 150/5345-53, ENGINEERING BRIEF 93

C. SHOULD A RUNWAY BE TEMPORARILY CLOSED; THE CONTRACTOR SHALL MARK THE AFFECTED RUNWAY WITH A CLOSURE CROSS IN ACCORDANCE WITH THE RUNWAY CLOSURE CROSS SPECIFICATIONS FOUND IN THE PLANS. CLOSURE CROSSES SHALL REMAIN IN PLACE UNTIL NORMAL RUNWAY OPERATIONS RESUME.

D. SHOULD A TAXIWAY BE TEMPORARILY CLOSED WITH AN OPEN RUNWAY; THE CONTRACTOR SHALL MARK THE AFFECTED TAXIWAY WITH A CLOSURE CROSS IN ACCORDANCE WITH THE TAXIWAY CLOSURE CROSS SPECIFICATIONS FOUND IN THE PLANS. CLOSURE CROSSES SHALL REMAIN IN PLACE UNTIL NORMAL TAXIWAY OPERATIONS RESUME.

E. WHEN NECESSARY TO CLOSE OR CHANGE THE STANDARD OPERATIONS OF A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL, THROUGH THE ENGINEER, NOTIFY THE AIRPORT 72 BUSINESS HOURS IN ADVANCE OF THE PROPOSED CHANGE IN OPERATIONS.

F. IF REQUIRED, CONSTRUCTION SIGNS SHALL COMPRISE A MESSAGE IN BLACK ON AN ORANGE BACKGROUND AT LOCATIONS SHOWN ON THE CSPP.

MARKING AND SIGNS FOR ACCESS ROUTES:

PAVEMENT MARKINGS AND SIGNS INTENDED FOR CONSTRUCTION PERSONNEL SHOULD CONFORM TO AC 150/5340-18, AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND/OR STATE HIGHWAY SPECIFICATIONS AS PRACTICAL.

HAZARD MARKING AND LIGHTING:

A. IF ANY AIRCRAFT MOVEMENT AREAS MUST BE CLOSED, THE CONTRACTOR SHALL FURNISH AND PLACE PORTABLE BARRICADES ACROSS RUNWAYS OR TAXIWAYS TO KEEP VEHICLES FROM ENTERING ACTIVE OPERATION AREAS AND TO KEEP AIRCRAFT FROM TAXING INTO CONSTRUCTION AREAS. BARRICADES SHALL BE HIGHLY-REFLECTIVE, MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AND SUPPLEMENTED WITH EITHER FLASHING OR STEADY-BURNING LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. LIGHTS SHALL BE BARRICADE TYPE TYPICAL FOR CONSTRUCTION ZONES AND RED IN COLOR, MEETING ALL STATE HIGHWAY REQUIREMENTS. BARRICADES LOCATED WITHIN AIRCRAFT MOVEMENT AREAS (RUNWAYS, TAXIWAYS, APRONS) SHALL BE LOW LEVEL AVIATION BARRICADES (NO HIGHER THAN 18") SPECIFICALLY MANUFACTURED AND DESIGNED FOR SUCH PURPOSE. THEY SHALL BE ALTERNATING ORANGE AND WHITE IN COLOR 10" HIGH AND 96" LONG, MADE OF UV-RESISTANT POLYETHYLENE AS MANUFACTURED BY MULTI-BARRIER (MODEL AR 10x96 HDPE), OR APPROVED EQUAL. ENOUGH BARRICADES MUST BE PROVIDED TO SAFELY DELINEATE ALL CONSTRUCTION BOUNDARIES. BARRICADES SHALL BE PLACED CLOSELY ENOUGH TO PREVENT TRAFFIC FROM ENTERING THE CONSTRUCTION AREA, GENERALLY NO MORE THAN 4 FEET APART. BARRICADES SHALL BE FILLED WITH WATER TO PREVENT MOVEMENT BY JET BLAST. BARRICADES LOCATED OUTSIDE OF AIRCRAFT MOVEMENT AREAS MAY BE MADE FROM VARIOUS MATERIALS, INCLUDING RAILROAD TIES, SAWHORSES, JERSEY BARRICADES, BARRELS, OR OTHER STANDARD HIGHWAY BARRICADES. IF BARRICADES ARE SUBJECT TO PROP WASH, JET BLAST, WIND VORTEX OR ANY OTHER SURFACE WIND CURRENTS, THE BARRICADES WILL BE WEIGHTED OR FASTENED TO THE GROUND TO PREVENT DISPLACEMENT.

B. HAZARDS SUCH AS OPEN MANHOLES, AREAS UNDER REPAIR, STOCKPILE AND WASTE AREAS SHOULD ALSO BE MARKED.

C. THE CONTRACTOR MUST ALSO PROVIDE AN EMERGENCY CONTACT AVAILABLE 24 HOURS FOR MAINTENANCE OF CONSTRUCTION LIGHTING AND BARRICADING.
D. USE HIGHLY REFLECTIVE BARRICADES WITH LIGHTS TO CLOSE TAXIWAYS LEADING TO CLOSED RUNWAYS. CLOSE RUNWAY/TAXIWAY INTERSECTIONS WITH BARRICADES EVEN FOR SHORT (TEMPORARY) CLOSURES.

E. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MOVEMENT OF BARRICADES BETWEEN PHASES, OR AS OTHERWISE CRITICAL TO AIRPORT OPERATIONS. ALL BARRICADE LIGHTS MUST BE CHECKED NIGHTLY AND REPLACED BY THE CONTRACTOR IF NOT FUNCTIONING. CONTRACTOR SHALL BE COMPENSATED FOR COST OF ALL BARRICADES, THEIR MAINTENANCE AND MOVEMENT UNDER THE PROJECT ITEMS.

WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION:

LIGHTING EQUIPMENT MUST ADEQUATELY ILLUMINATE THE WORK AREA IF THE CONSTRUCTION IS TO BE PERFORMED DURING NIGHTTIME HOURS. LIGHT TOWERS SHOULD BE POSITIONED AND ADJUSTED TO AIM AWAY FROM ATCT CAB AND ACTIVE RUNWAYS TO PREVENT BLINDING EFFECTS. TOWERS MUST BE REMOVED PRIOR TO REOPENING TO AIRCRAFT OPERATIONS.

PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS:

A. NO CONSTRUCTION MAY OCCUR WITHIN THE EXISTING RUNWAY SAFETY AREA (RSA) WHILE THE RUNWAY IS OPEN TO AIRCRAFT OPERATIONS. RSA DIMENSIONS MAY BE TEMPORARILY ADJUSTED, IF THE RUNWAY IS RESTRICTED TO AIRCRAFT OPERATIONS REQUIRING AN RSA EQUAL TO THE WIDTH AND LENGTH THAT ARE AVAILABLE. THIS ADJUSTMENT MUST BE COORDINATED WITH THE FAA REGIONAL OR DISTRICT OFFICE AND LOCAL AIR TRAFFIC MANAGER BY THE AIRPORT OPERATOR, AND APPROPRIATE NOTAMS ISSUED. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE RSA WHILE OPEN. OPEN TRENCHES AND EXCAVATIONS WITHIN THE CONSTRUCTION AREA(S) MUST BE PROMINENTLY MARKED WITH RED OR ORANGE FLAGS, AND LIGHTED DURING HOURS OF LOW VISIBILITY/DARKNESS. IF THE RUNWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, THE EXCAVATIONS MUST BE COVERED IN SUCH A WAY AS TO ALLOW THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY TO CROSS THE COVERING WITHOUT DAMAGE. SOIL EROSION MUST BE CONTROLLED TO MAINTAIN RSA STANDARDS (NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS, AND CAPABLE UNDER DRY CONDITIONS OF SUPPORTING SNOW REMOVAL EQUIPMENT (SRE), ARFF, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT STRUCTURAL DAMAGE.

B. CONSTRUCTION, INCLUDING EXCAVATIONS, MAY BE PERMITTED IN THE RUNWAY OBJECT FREE AREA (ROFA). EQUIPMENT MUST BE REMOVED FROM THE ROFA WHEN NOT IN USE, AND MATERIAL SHOULD NOT BE STOCKPILED IN IT IF NOT NECESSARY. STOCKPILING WITHIN THE ROFA REQUIRES SUBMITTAL OF A 7460-1 FORM AND FAA APPROVAL.

C. NO CONSTRUCTION MAY OCCUR WITHIN THE EXISTING TAXIWAY SAFETY AREA (TSA) WHILE THE TAXIWAY IS OPEN TO AIRCRAFT OPERATIONS. TSA DIMENSIONS MAY BE TEMPORARILY ADJUSTED, IF THE TAXIWAY IS RESTRICTED TO AIRCRAFT OPERATIONS REQUIRING A TSA EQUAL TO THE WIDTH THAT IS AVAILABLE. THIS ADJUSTMENT MUST BE COORDINATED WITH THE FAA REGIONAL OR DISTRICT OFFICE AND LOCAL AIR TRAFFIC MANAGER BY THE AIRPORT OPERATOR, AND APPROPRIATE NOTAMS ISSUED. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE TSA WHILE OPEN. OPEN TRENCHES AND EXCAVATIONS WITHIN THE CONSTRUCTION AREA(S) MUST BE PROMINENTLY MARKED WITH RED OR ORANGE FLAGS, AND LIGHTED DURING HOURS OF LOW VISIBILITY/DARKNESS. IF THE TAXIWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, THE EXCAVATIONS MUST BE COVERED IN SUCH A WAY AS TO ALLOW THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY TO CROSS THE COVERING WITHOUT DAMAGE. SOIL EROSION MUST BE CONTROLLED TO MAINTAIN TSA STANDARDS (NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS, AND CAPABLE UNDER DRY CONDITIONS OF SUPPORTING SRE, ARFF, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT STRUCTURAL DAMAGE.

IN RARE CIRCUMSTANCES WHERE THE SECTION OF TAXIWAY IS INDISPENSABLE FOR AIRCRAFT MOVEMENT, OPEN TRENCHES OR EXCAVATIONS MAY BE PERMITTED WHILE OPEN IF SUBJECT TO THE FOLLOWING: TAXING IS LIMITED TO 10 MPH; APPROPRIATE NOTAMS ARE ISSUED; APPROPRIATE LIGHTING AND MARKING ARE IMPLEMENTED; LOW MASS, LOW PROFILE LIGHTED BARRICADES ARE INSTALLED; APPROPRIATE TEMPORARY ORANGE CONSTRUCTION SIGNS ARE INSTALLED.

D. UNLIKE THE ROFA, NO CONSTRUCTION MAY OCCUR WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) WHILE OPEN TO AIRCRAFT OPERATIONS, UNLESS 1) THE TOFA DIMENSION IS TEMPORARILY ADJUSTED FOR USE BY SMALLER AIRCRAFT ONLY; 2) TEMPORARY OFFSET TAXIWAY MARKINGS ARE USED.

CONSTRUCTION, INCLUDING OPEN EXCAVATIONS, MAY BE ACCOMPLISHED WITHOUT ADJUSTING THE TOFA SUBJECT TO THE FOLLOWING RESTRICTIONS: TAXING IS LIMITED TO 10 MPH; APPROPRIATE NOTICES TO AIRMEN (NOTAM'S) HAVE BEEN ISSUED BY THE AIRPORT; MARKING AND LIGHTING PROVISIONS HAVE BEEN IMPLEMENTED BY THE CONTRACTOR. APPROPRIATE ORANGE CONSTRUCTION SIGNS ARE INSTALLED (IF DESIRED); FIVE-FOOT CLEARANCE BETWEEN EQUIPMENT AND ANY PART OF AN AIRCRAFT IS MAINTAINED (MOVING PERSONNEL AND EQUIPMENT FOR PASSAGE OF AN AIRCRAFT MAY BE NECESSARY); AND CONTRACTOR'S FLAGGERS ARE UTILIZED TO DIRECT AND CONTROL EQUIPMENT AND PERSONNEL TO A PRE-ESTABLISHED SETBACK DISTANCE. (AIRLINES SHOULD PROVIDE FLAGGERS TO DIRECT THE ACTUAL TAXING AIRCRAFT.)

E. PERSONNEL, MATERIAL, AND/OR EQUIPMENT MAY NOT PENETRATE THE RUNWAY OBSTACLE FREE ZONE (OFZ) WHILE THE RUNWAY IS OPEN. IF WORK IN THE OFZ IS NECESSARY, IT MAY BE POSSIBLE TO CONTINUE AIRCRAFT OPERATIONS THROUGH OPERATIONAL RESTRICTIONS (COORDINATE WITH THE FAA).

F. ALL PERSONNEL, MATERIAL, AND/OR EQUIPMENT MUST REMAIN CLEAR OF APPLICABLE THRESHOLD SITING SURFACES. OBJECTS THAT DO NOT PENETRATE MAY STILL BE OBSTRUCTIONS TO AIR NAVIGATION AND MAY AFFECT STANDARD INSTRUMENT APPROACH PROCEDURES (COORDINATE WITH THE FAA).

G. ADEQUATE DISTANCE FROM BLASTING OPERATIONS: N/A

OTHER LIMITATIONS ON CONSTRUCTION:

THE FOLLOWING ARE PROHIBITED FROM USE ON THE AIRFIELD: TALL EQUIPMENT BEYOND THE MAXIMUM EQUIPMENT HEIGHT SHOWN ON THE SAFETY/PHASING PLAN, OPEN-FLAME WELDING/TORCH CUTTING (UNLESS PROPER PRECAUTIONS ARE TAKEN, AND AIRPORT APPROVAL IS OBTAINED), ELECTRICAL BLASTING CAPS (WITHIN 1000 FEET OF AIRPORT PROPERTY).

OTHER CONSTRUCTION NOTES:

A. CRITICAL AIRCRAFT DURING CONSTRUCTION OPERATIONS CONSIST OF DESIGN GROUP III AIRCRAFT.

B. CONSTRUCTION SHALL BEGIN IN EARLY JULY, 2018. THERE SHALL BE 65 CALENDAR DAYS FOR THE BASE BID.

C. LIQUIDATED DAMAGES IN THE AMOUNT OF \$1,000 SHALL BE ASSESSED FOR EACH CALENDAR DAY CONSTRUCTION EXCEEDS 65 DAYS FOR THE TOTAL PROJECT. \$1,000 FOR EACH CALENDAR DAY WORK ON PHASES 1 AND 2 THAT EXCEEDS THE CONTRACT TIME ALLOTTED AND \$2,500 FOR EACH CALENDAR DAY WORK ON EACH SUB-PHASE, 1A AND 2A, THAT EXCEEDS THE CONTRACT TIME ALLOTTED.

D. ALL SAFETY AND SECURITY ITEMS REQUIRED ON THE CONSTRUCTION SAFETY PHASING PLAN OR IN SAFETY AND CONSTRUCTION NOTES WILL BE INCLUDED IN THE COST FOR THE PROJECT ITEMS.

CONSTRUCTION STAKE AND LATH REQUIREMENTS:

THE PROJECT ENGINEER SHALL FURNISH ENOUGH WOOD STAKES AND LATH TO ESTABLISH ALL LINES, GRADES AND MEASUREMENTS CONSIDERED BY THE PROJECT ENGINEER AS NECESSARY TO THE PROPER PROSECUTION AND CONTROL OF THE WORK CONTRACTED FOR UNDER THESE SPECIFICATIONS. THE CONTRACTOR SHALL TAKE ALL REASONABLE PRECAUTIONS TO PROTECT THE LINES AND GRADES ESTABLISHED BY THE ENGINEER. THE CONTRACTOR WILL BE RESPONSIBLE FOR ESTABLISHING ALL LINES, GRADES AND MEASUREMENTS OUTLINE IN ITEM 800 COLD MILLING AND PAVING GRADE CONTROL.

WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

ISSUED
1 03.29.18 ISSUED FOR
BID

AIP NO: B-26-0008-4518
M&H NO.: 0228800-161097.01
DATE: 03.29.2018
DESIGNED BY: ALP
DRAWN BY: ALP
CHECKED BY: JET
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SHEET CONTENTS
CONSTRUCTION &
SAFETY NOTES

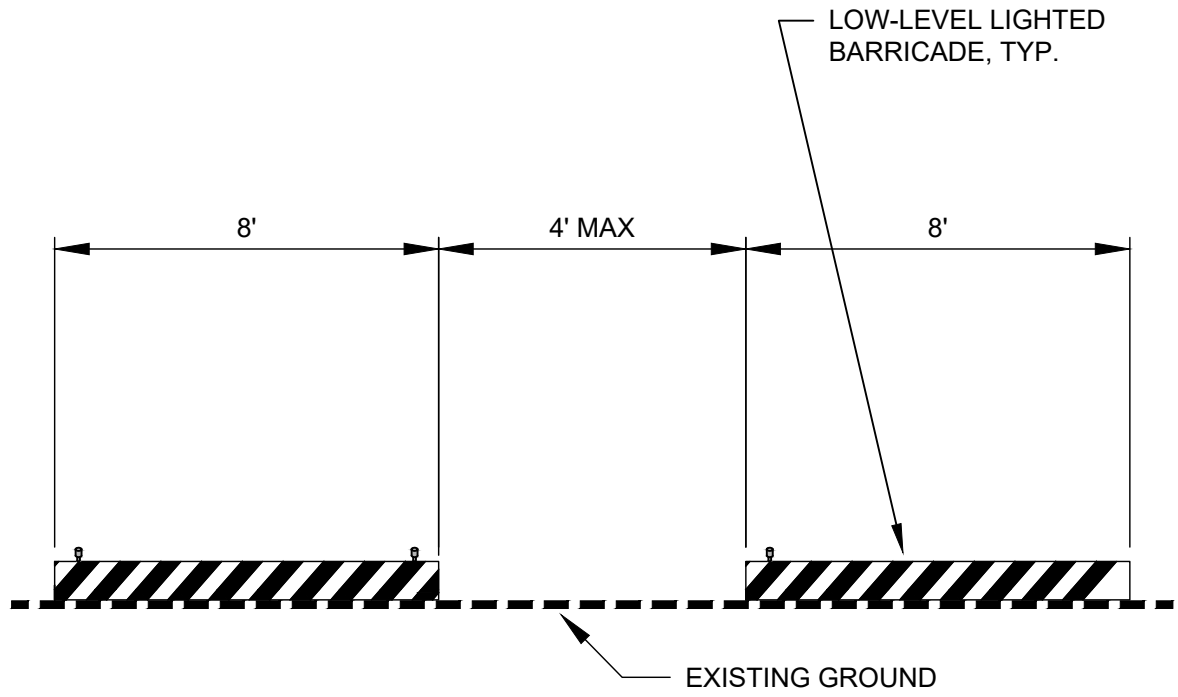
SHEET NO. 5 of 76

G-052

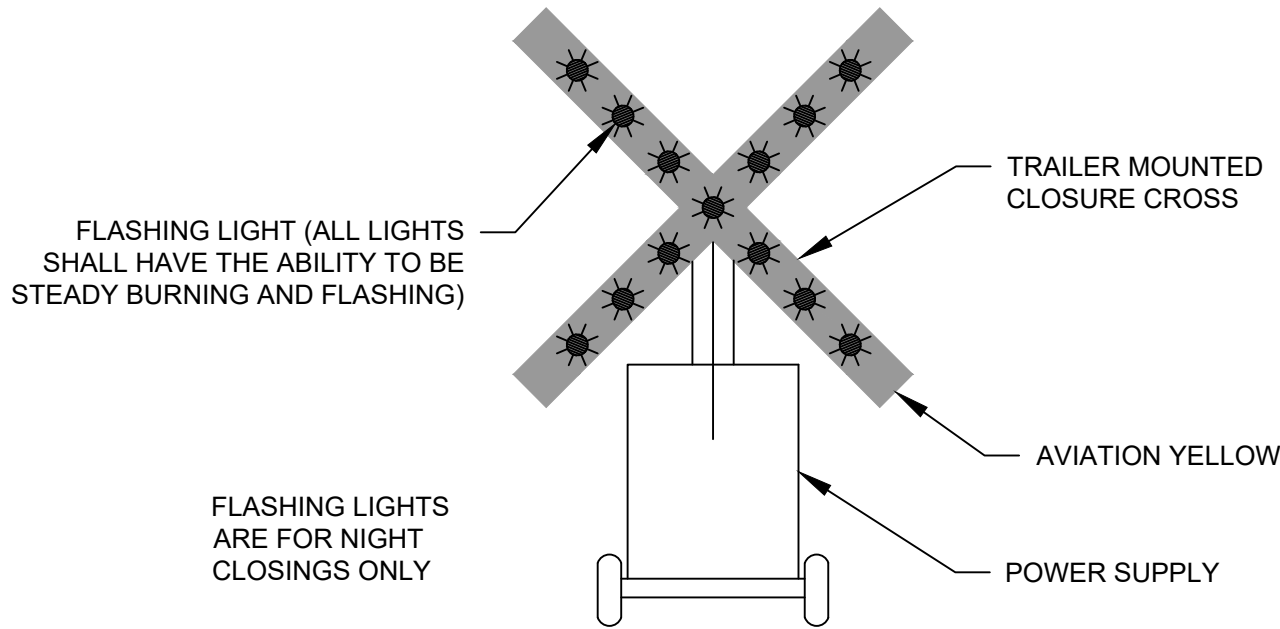
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Mead
& Hunt
Mead and Hunt, Inc.
2605 Port Lansing Road
Lansing, MI 48906
phone: 517-321-8334
meadhunt.com

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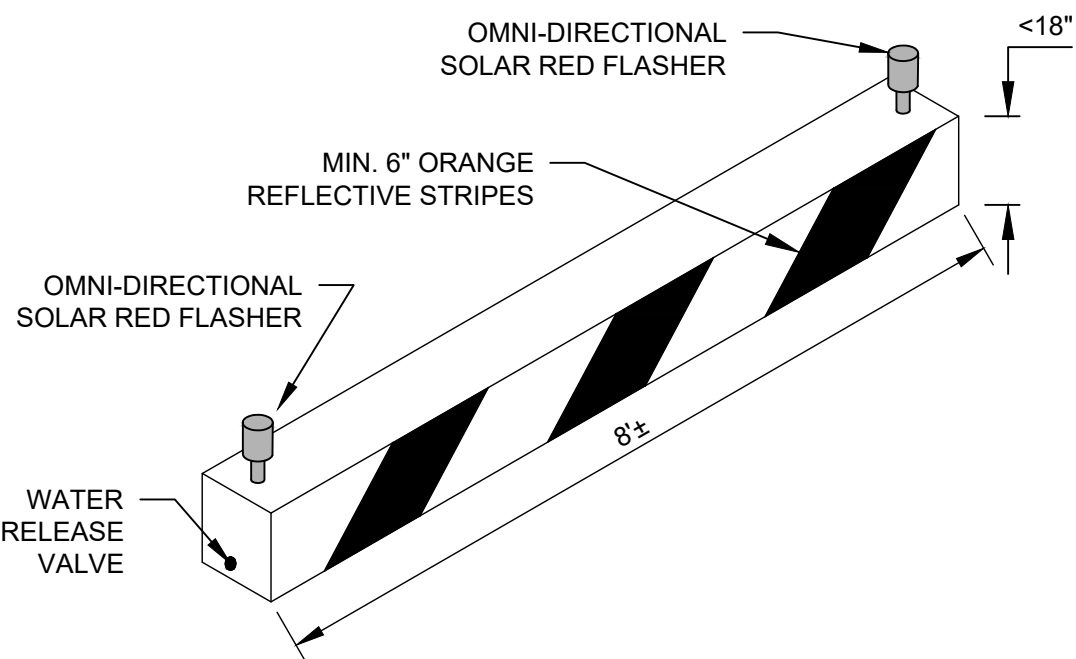
3 **LOW-LEVEL LIGHTED BARRICADE DETAIL LAYOUT**
NOT TO SCALE



NOTE:
GENERATOR POWERED, LIGHTED RUNWAY CLOSURE MARKERS SHALL BE PLACED, MOVED, FUELED, OILED AND MAINTAINED BY THE CONTRACTOR THROUGHOUT THE PROJECT. LIGHTED CLOSURE CROSSES WILL NOT BE PROVIDED BY THE AIRPORT, THE CONTRACTOR SHALL OBTAIN THEIR OWN MARKERS INCIDENTAL TO ITEM NO. 1000410 - SAFETY AND SECURITY.

MARKERS SHALL BE PLACED ON RUNWAY NUMERALS, OR WHERE DESIGNATED ON THE PLANS.

1 **LIGHTED RUNWAY CLOSURE MARKER**
NOT TO SCALE



- NOTES:
1. MAINTENANCE OF LOW-LEVEL LIGHTED BARRICADES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR THE DURATION OF THE PROJECT.
 2. GAPS IN BARRICADES SHALL NOT EXCEED 4-FEET.
 3. ALL BARRICADES SHALL BE FILLED WITH WATER OR OTHER APPROVED LIQUID SOLUTIONS SO THAT THEY RESIST MOVEMENT DUE TO HIGH WINDS OR JET BLASTS. THE CONTRACTOR SHALL CHECK THE LIQUID LEVEL DAILY AND SHALL FILL AS NEEDED THROUGHOUT THE PROJECT.

2 **LOW-LEVEL LIGHTED BARRICADE DETAIL**
NOT TO SCALE

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**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI**

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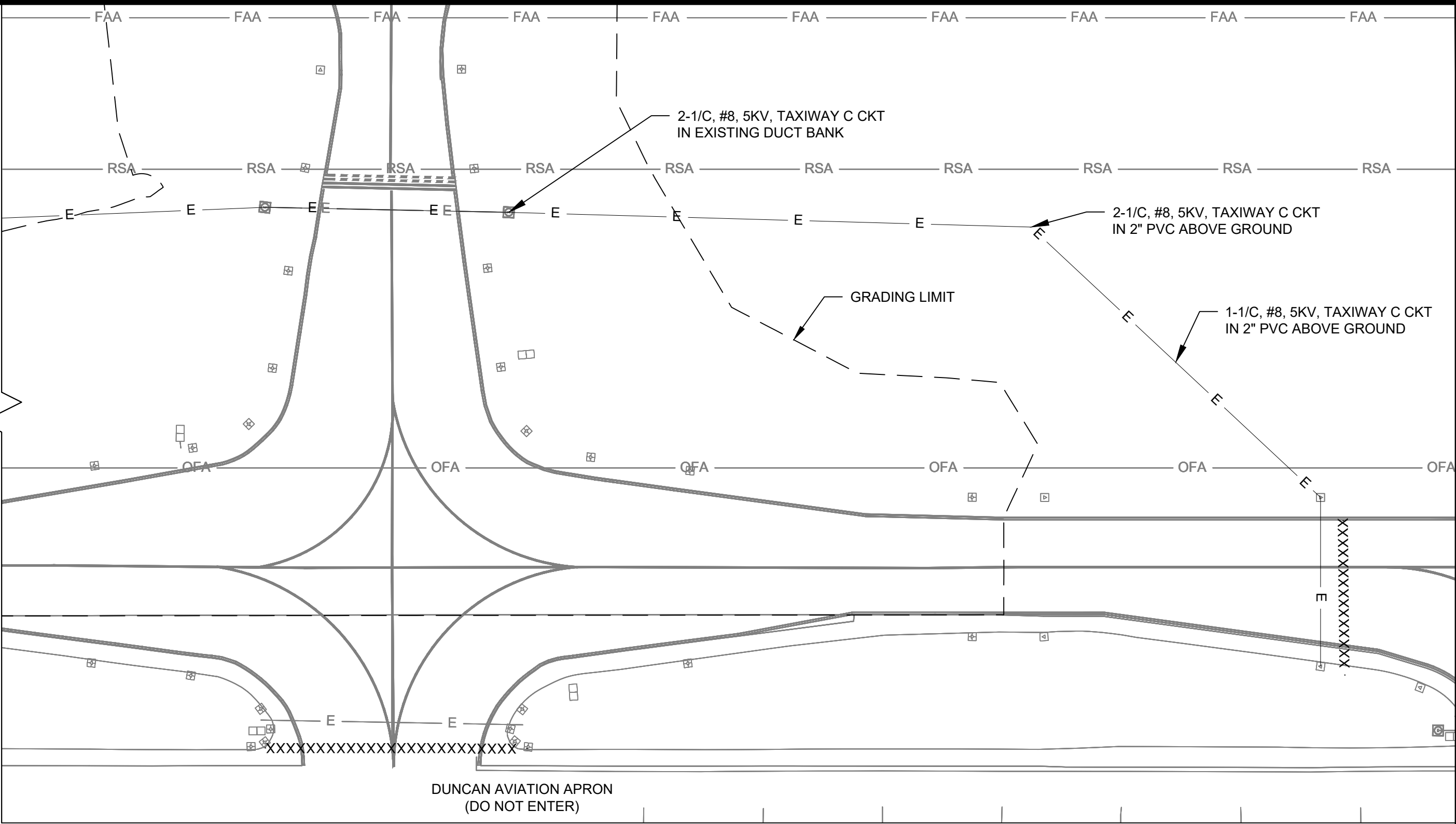
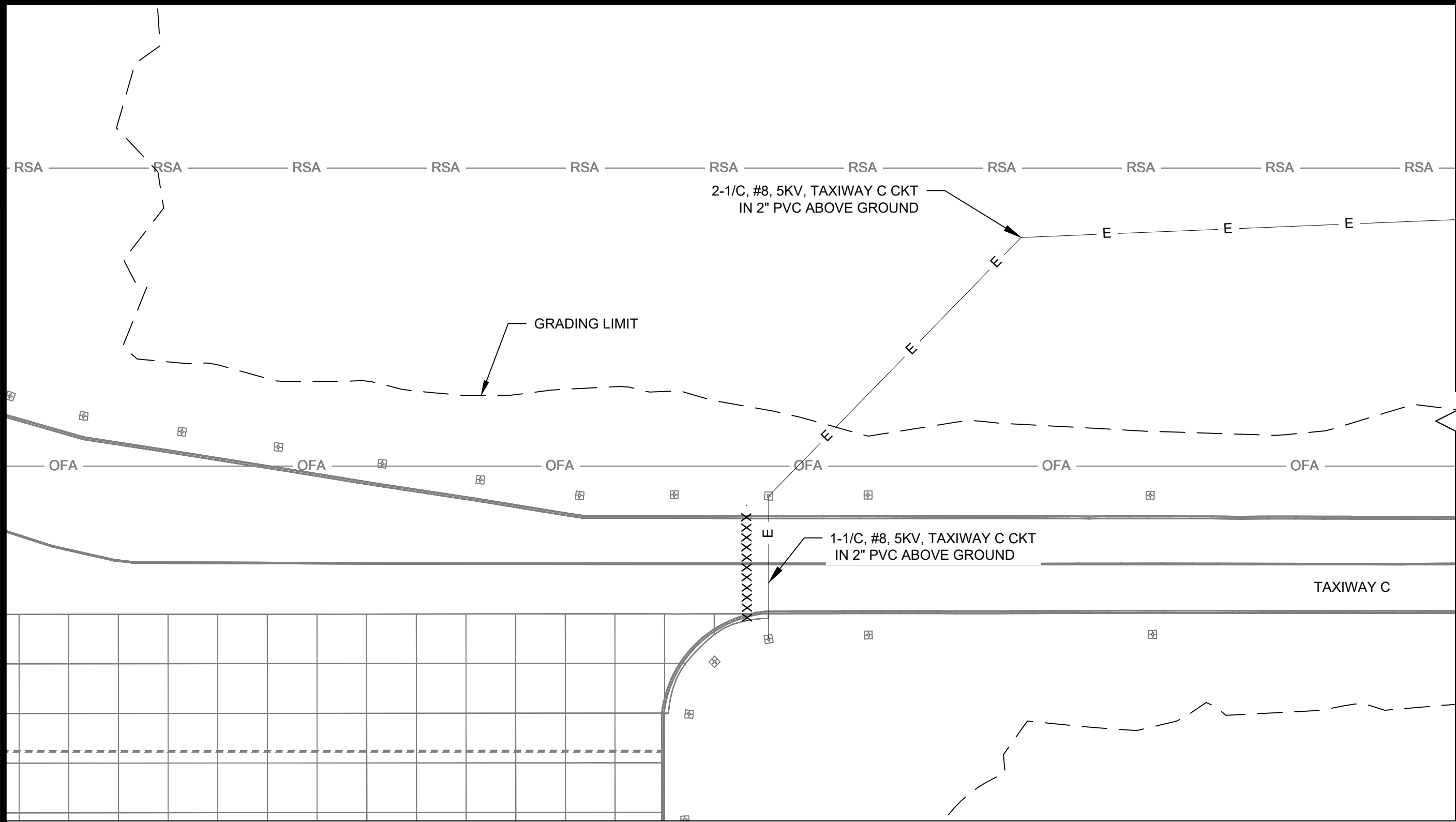
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SHEET CONTENTS
CONSTRUCTION &
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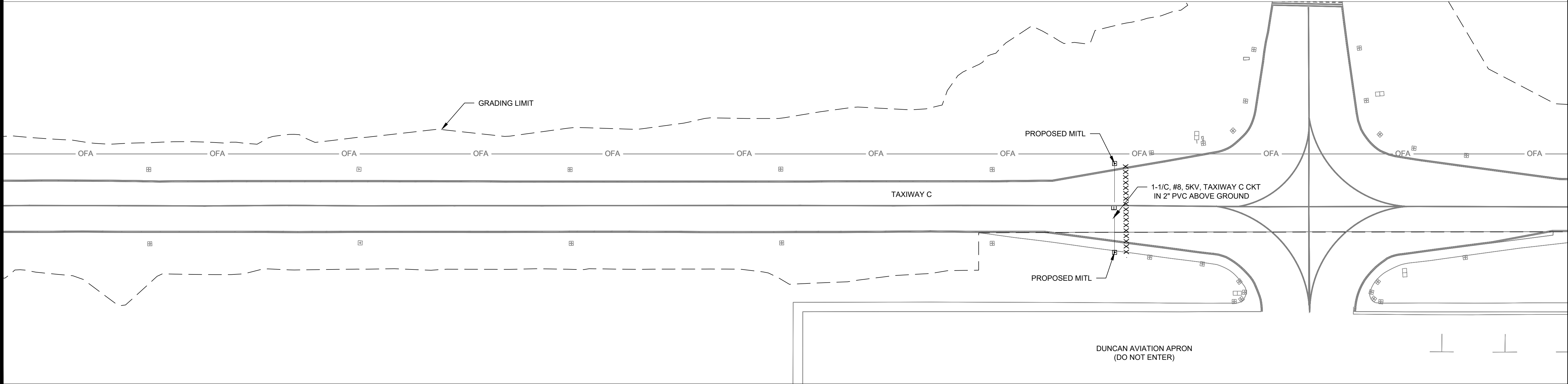
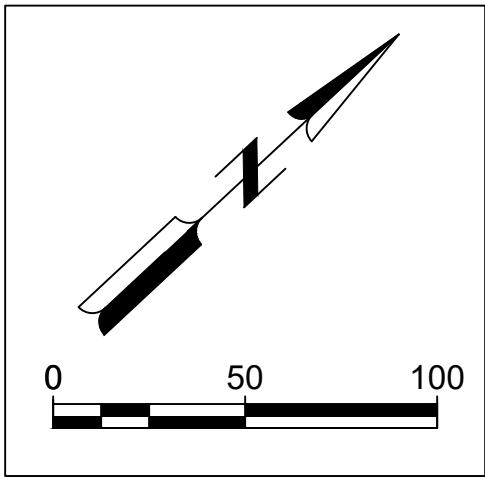
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3/21/2018 8:39:48 AM



NOTES:

1. ALONG WITH DISABLING TAXIWAY C CIRCUIT WITHIN EACH CLOSURE AREA, THE CONTRACTOR MUST COVER ANY GUIDANCE SIGNS RELATED TO THE CLOSURE AREA.
2. PAYMENT FOR ANY EQUIPMENT AND MATERIALS USED FOR TEMPORARY CLOSURES SHALL BE INCLUDED IN ITEM NO. 100002 SAFETY AND SECURITY.
3. ABOVE GROUND CABLES SHALL NOT BE INSTALLED IN ACTIVE TAXIWAY/RUNWAY SAFETY AREAS. CABLES SHALL BE FLAGGED EVERY 50' NEXT TO THE CABLE TO EASILY DELINEATE ABOVE GROUND CABLE LOCATION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT THE JUMPER CABLE DURING CONSTRUCTION.
4. ALL CABLE AND CONDUIT USED SHALL BE APPROVED FAA MATERIALS. THE CABLE SHALL BE L-824 5 KV CABLE AND CONDUIT SHALL BE 2" PVC SCH. 80.
5. ANY MODIFICATIONS TO THIS TEMPORARY JUMPER PLANS SHALL BE APPROVED BY THE ENGINEER.

TEMPORARY JUMPER PLAN
FOR PHASE 1



TEMPORARY JUMPER PLAN
FOR PHASE 2

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**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

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SHEET CONTENTS
TEMPORARY
ELECTRICAL JUMPER
PLAN

SHEET NO. 7 of 76

G-054

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BATTLE CREEK, MI

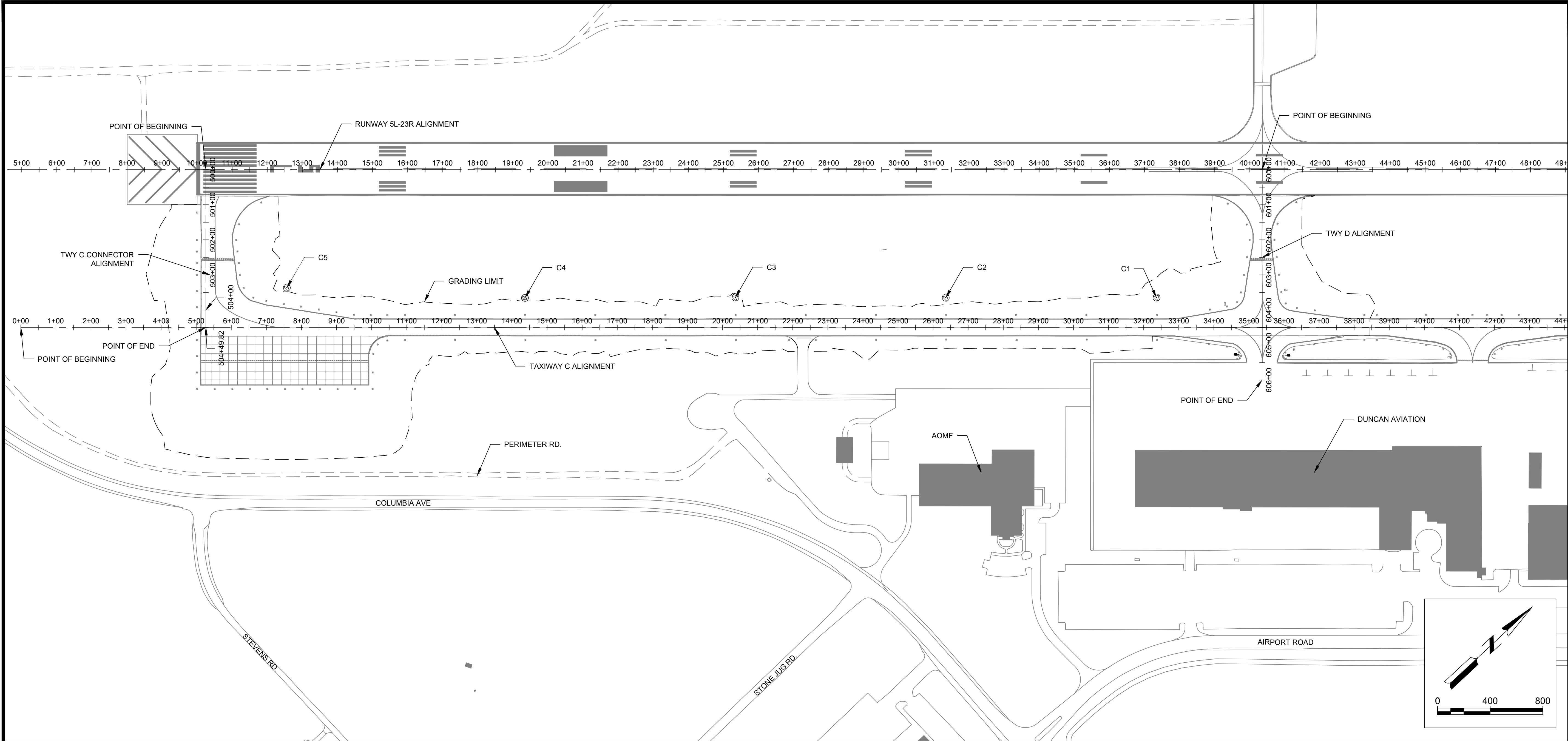
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SHEET CONTENTS
SURVEY CONTROL
PLAN

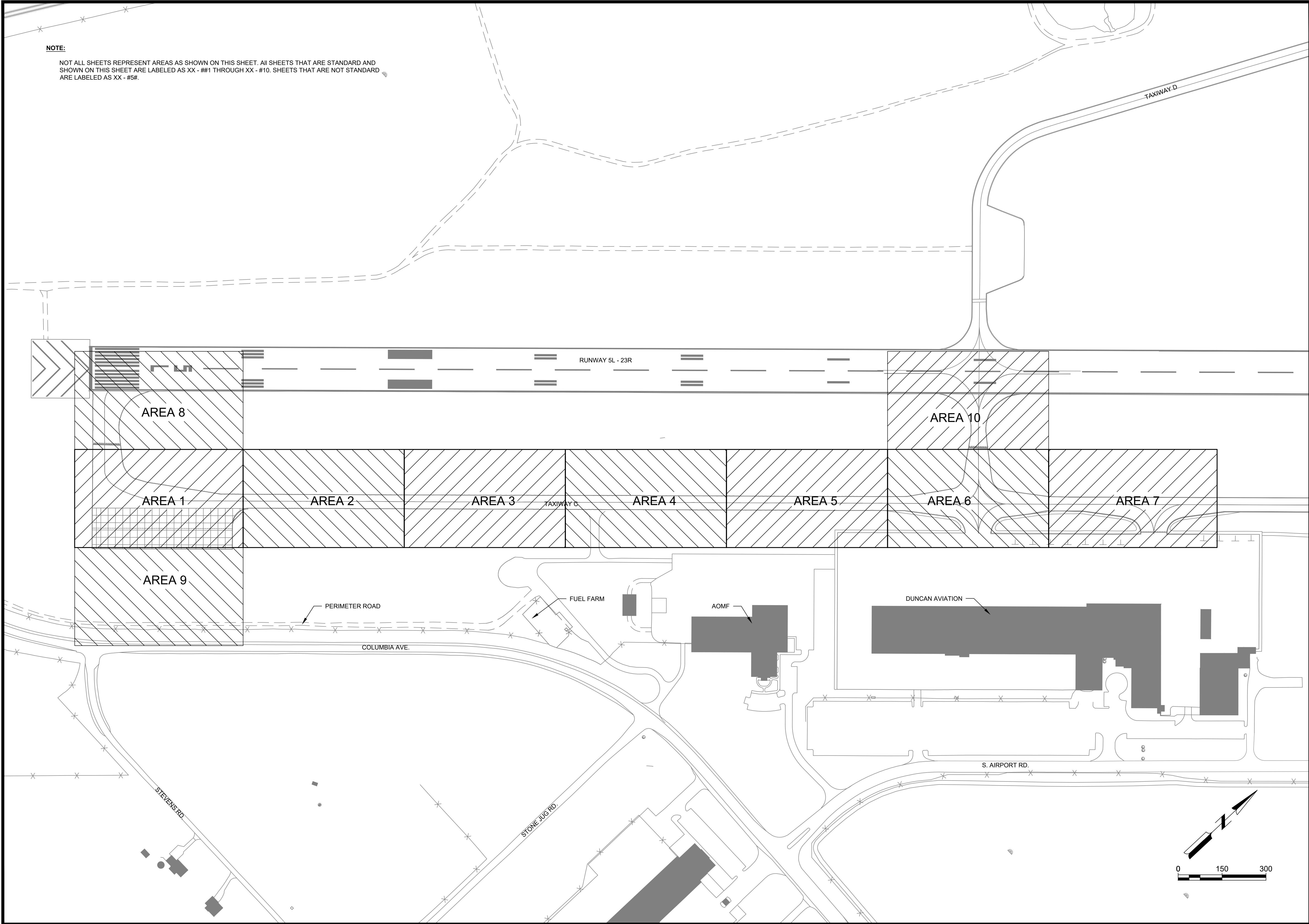
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G-060



CONTROL POINTS				
POINT	TYPE	NORTHING	EASTING	ELEVATION
C1	REBAR	293030.866	12882196.161	936.817'
C2	REBAR	292592.913	12881785.181	941.278'
C3	REBAR	292156.100	12881374.228	943.911'
C4	REBAR	291718.416	12880964.483	946.432'
C5	REBAR	291243.750	12880478.729	948.259'

ALIGNMENT DATA						
ALIGNMENT NAME	STA	POINT OF BEGINNING		POINT OF ENDING		
		NORTHING	EASTING	NORTHING	EASTING	STA
RUNWAY 5L-23R	0+00	290558.844	12879373.745	299305.662	12887589.169	120+00
TAXIWAY C	0+00	290613.546	12880042.241	296445.450	12885519.788	80+00.91
TWY C CONNECTOR	500+00	291315.080	12880084.037	291007.128	12880411.908	504+49.82
TWY D	600+00	293500.013	12882136.299	293089.243	12882573.642	606+00



NOTE:
NOT ALL SHEETS REPRESENT AREAS AS SHOWN ON THIS SHEET. ALL SHEETS THAT ARE STANDARD AND SHOWN ON THIS SHEET ARE LABELED AS XX - ##1 THROUGH XX - #10. SHEETS THAT ARE NOT STANDARD ARE LABELED AS XX - #5#.

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SHEET CONTENTS
KEY MAP

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G-061

WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

ISSUED
1 03.29.18 ISSUED FOR
BID

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SHEET CONTENTS
SOIL BORING LAYOUT
SHEET 1 OF 2

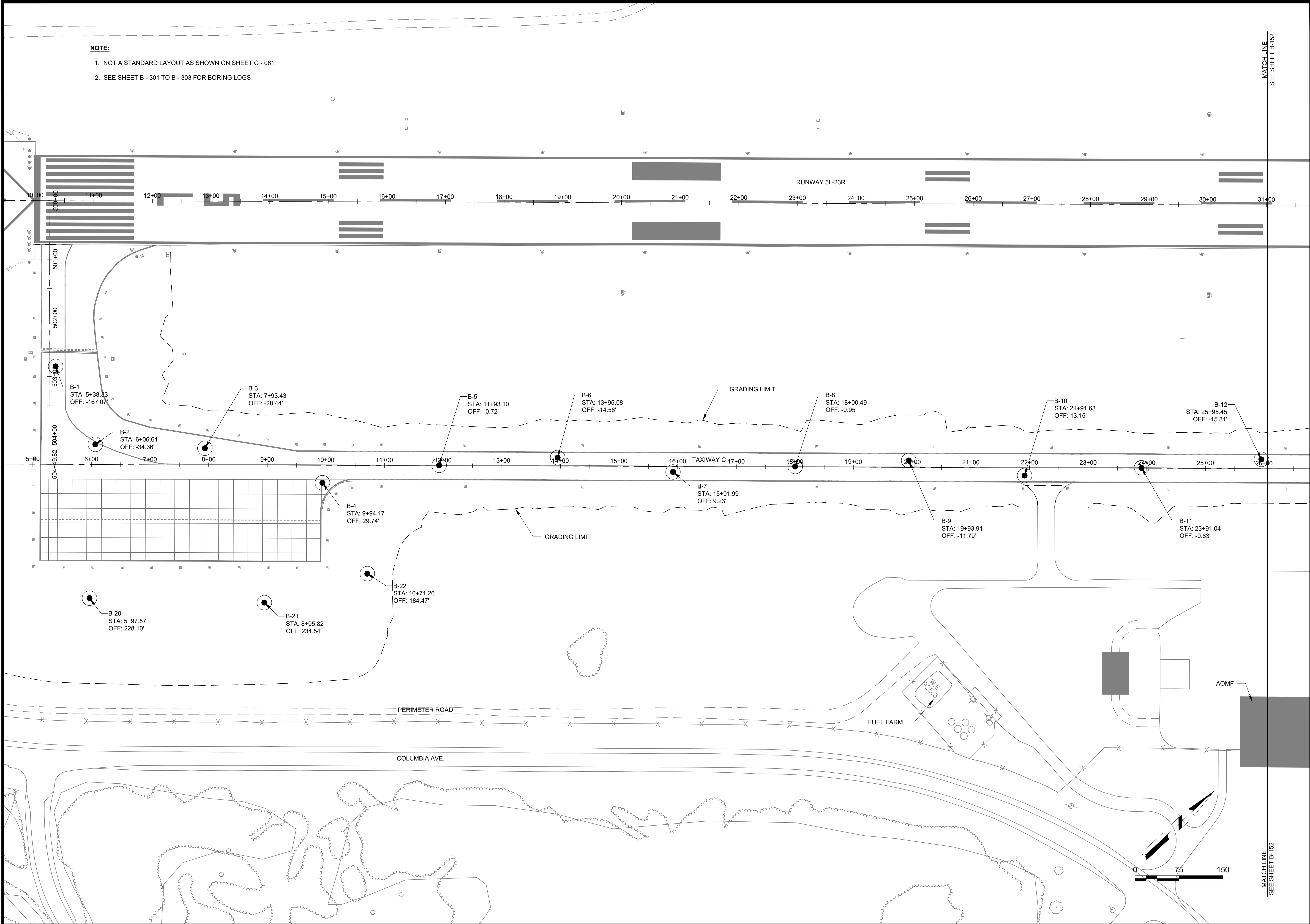
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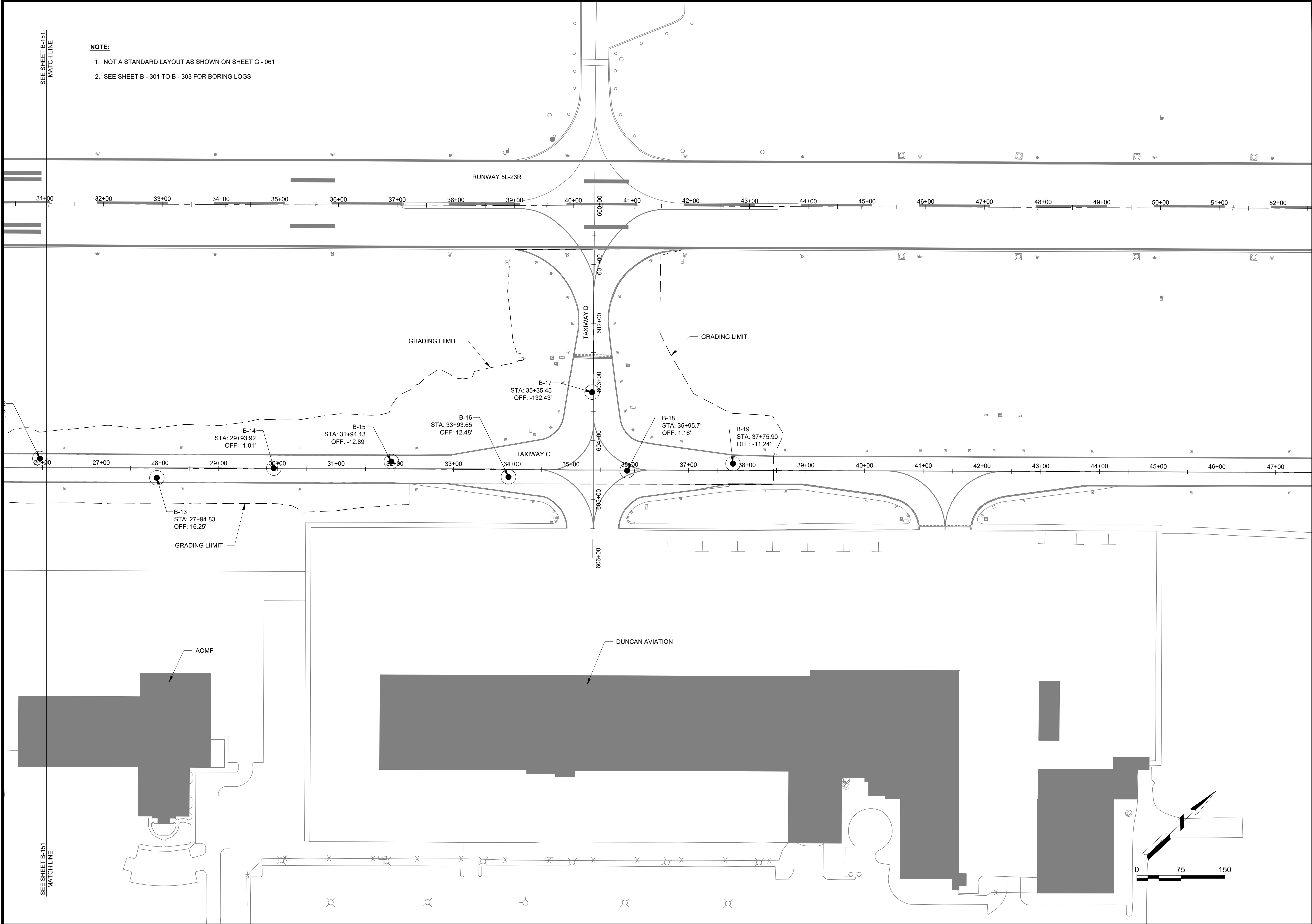
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NOTE:

1. NOT A STANDARD LAYOUT AS SHOWN ON SHEET G - 061
2. SEE SHEET B - 301 TO B - 303 FOR BORING LOGS



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NOTE:

1. NOT A STANDARD LAYOUT AS SHOWN ON SHEET G - 061
2. SEE SHEET B - 301 TO B - 303 FOR BORING LOGS

Mead & Hunt
Mead and Hunt, Inc.
2605 Port Lansing Road
Lansing, MI 48906
phone: 517-321-8334
meadhunt.com

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

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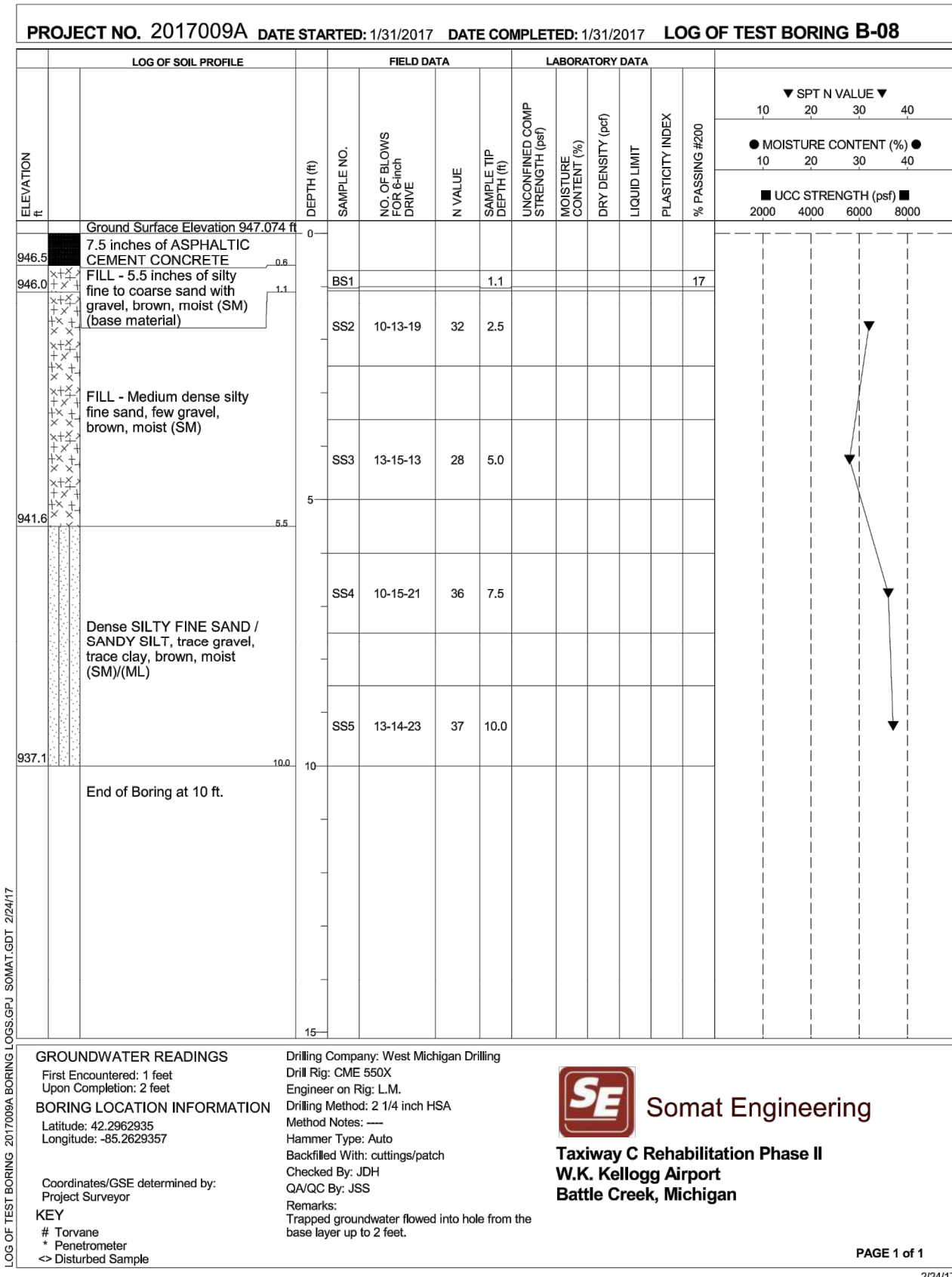
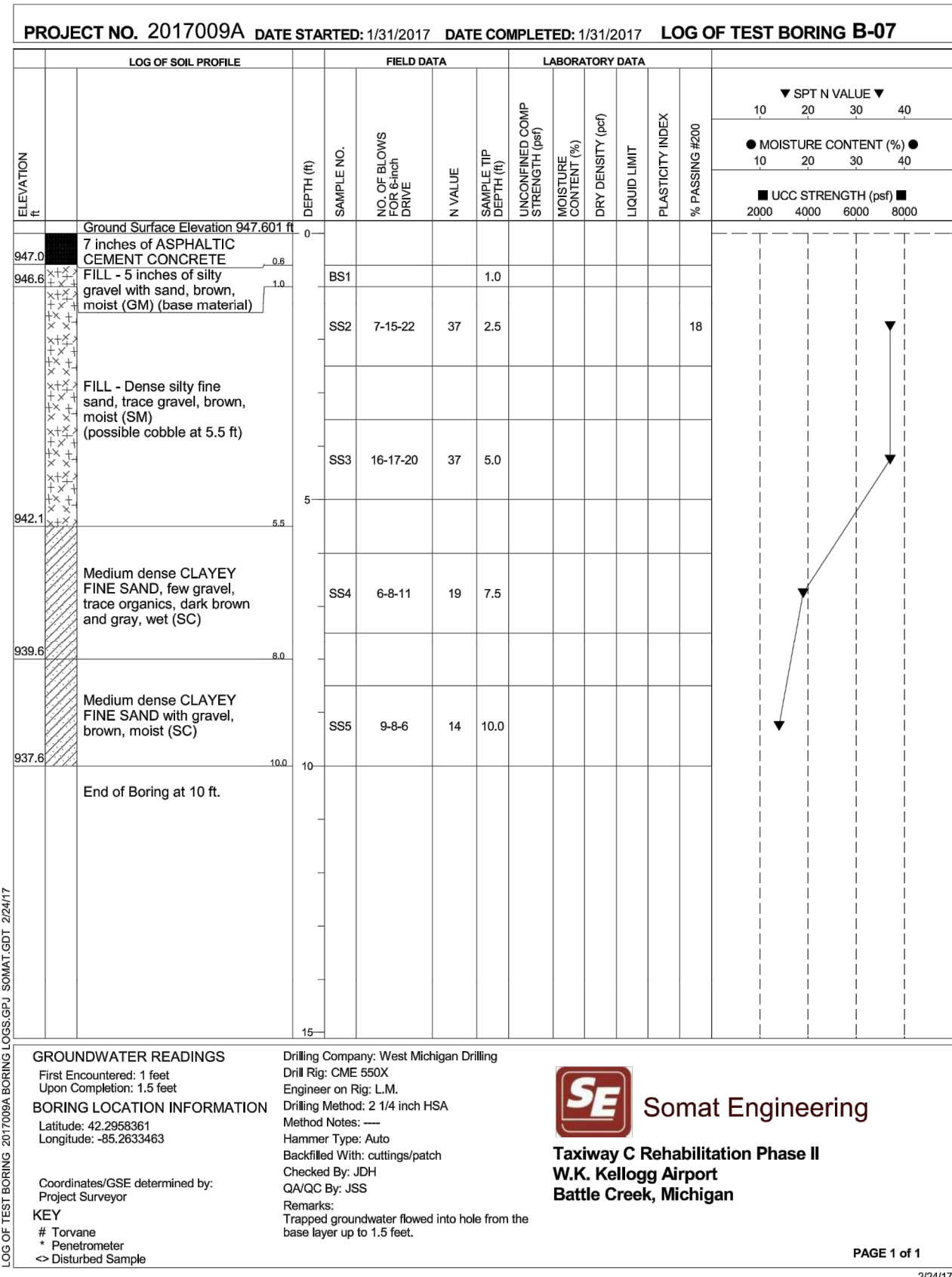
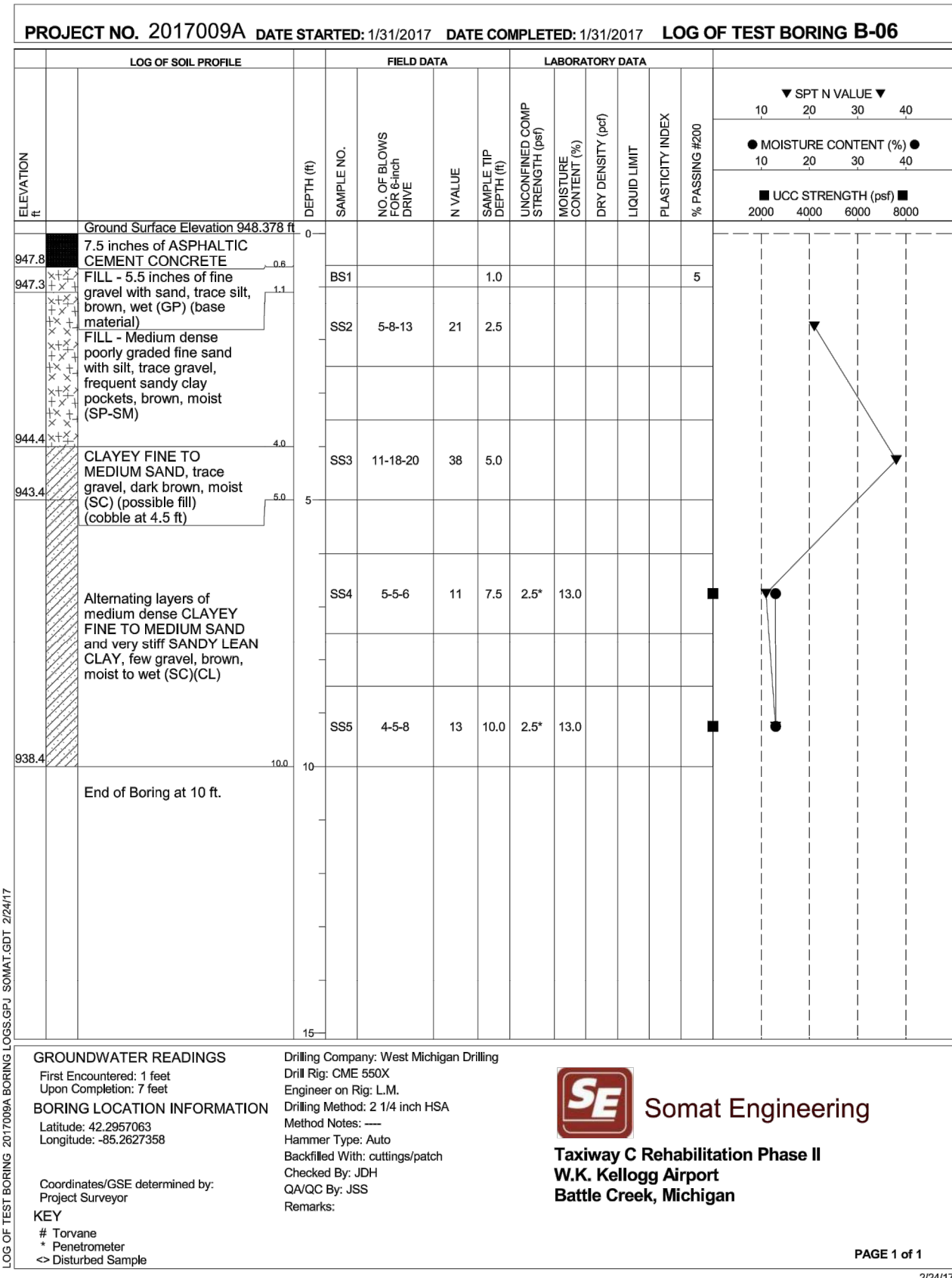
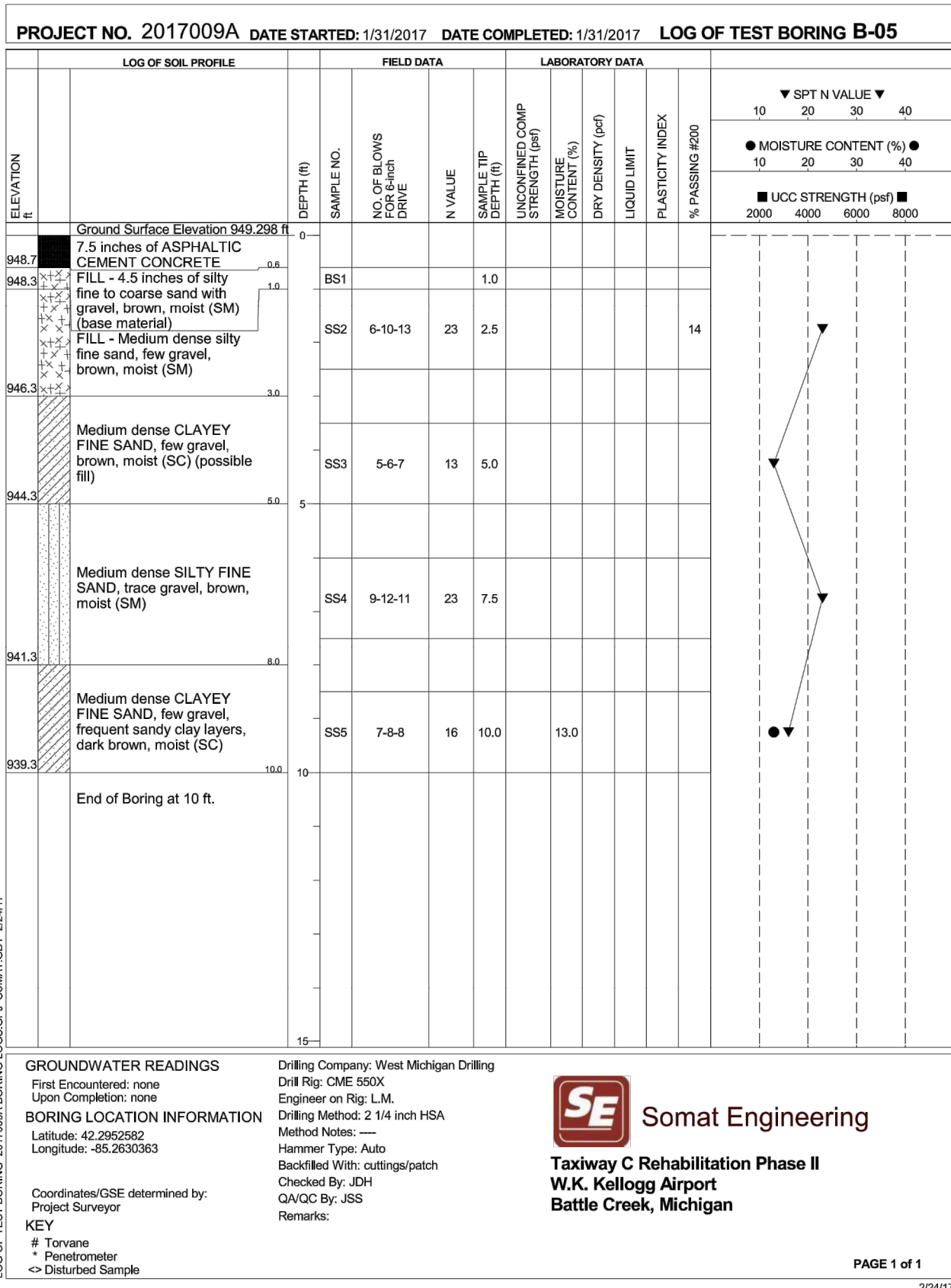
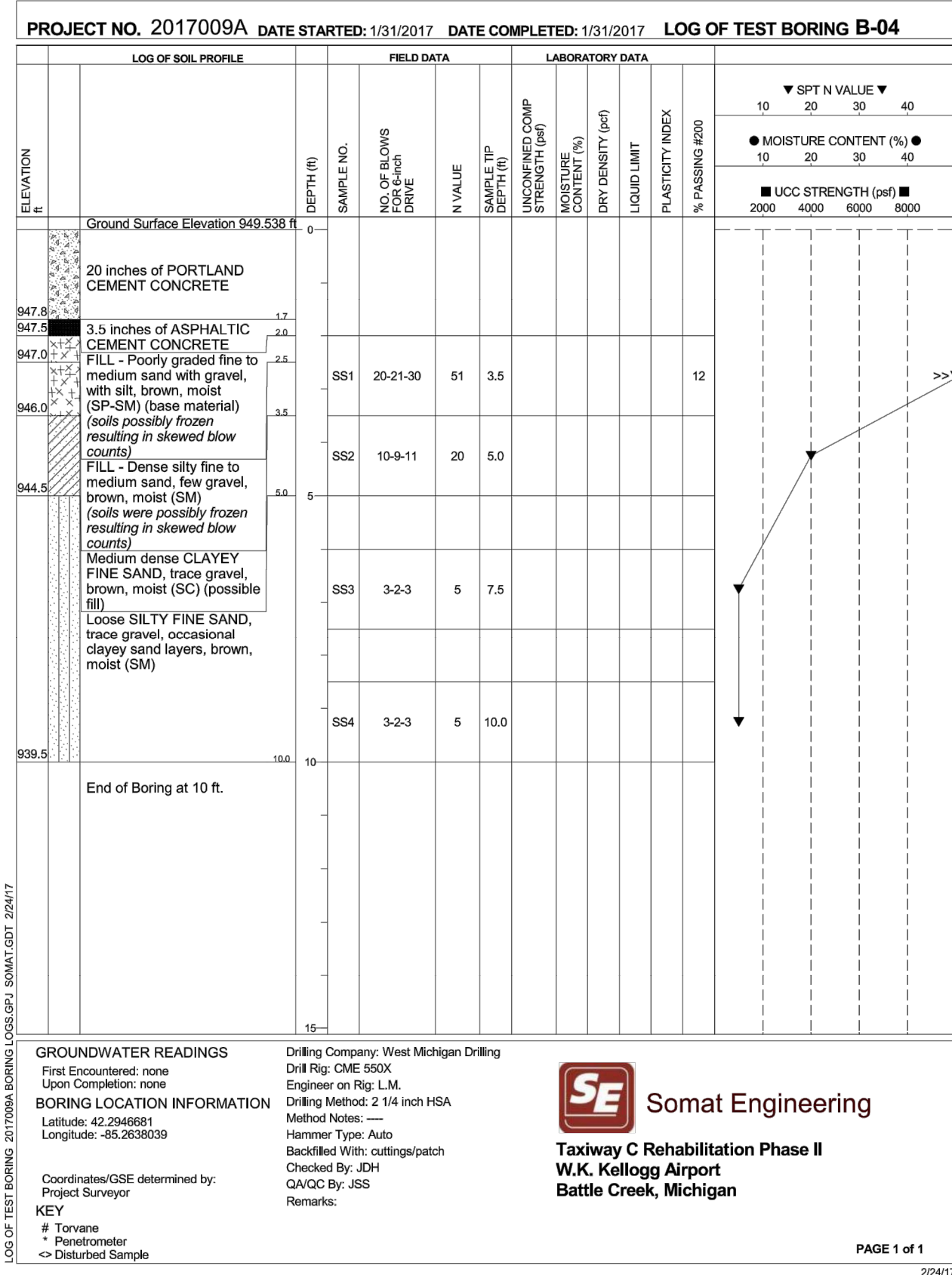
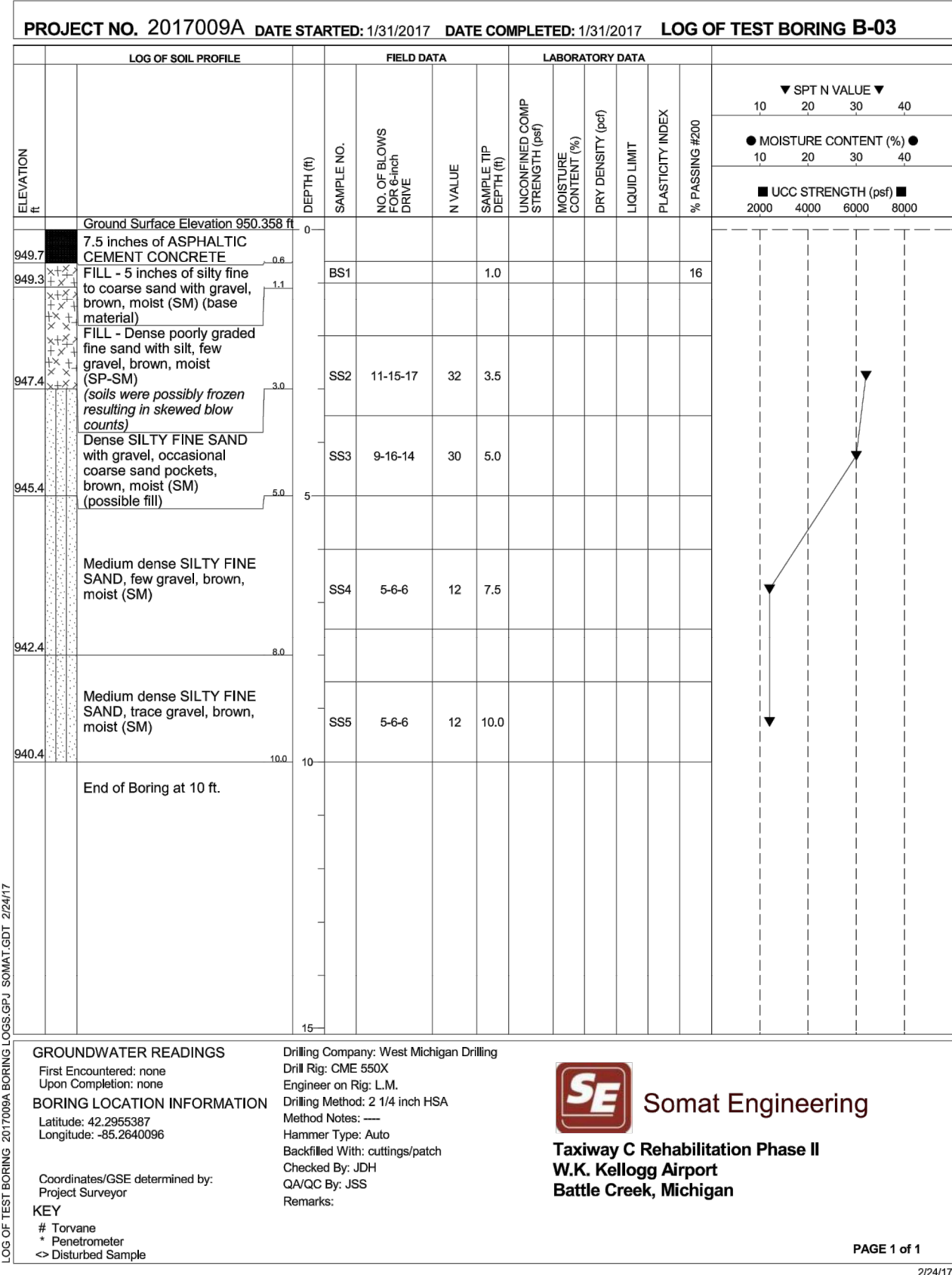
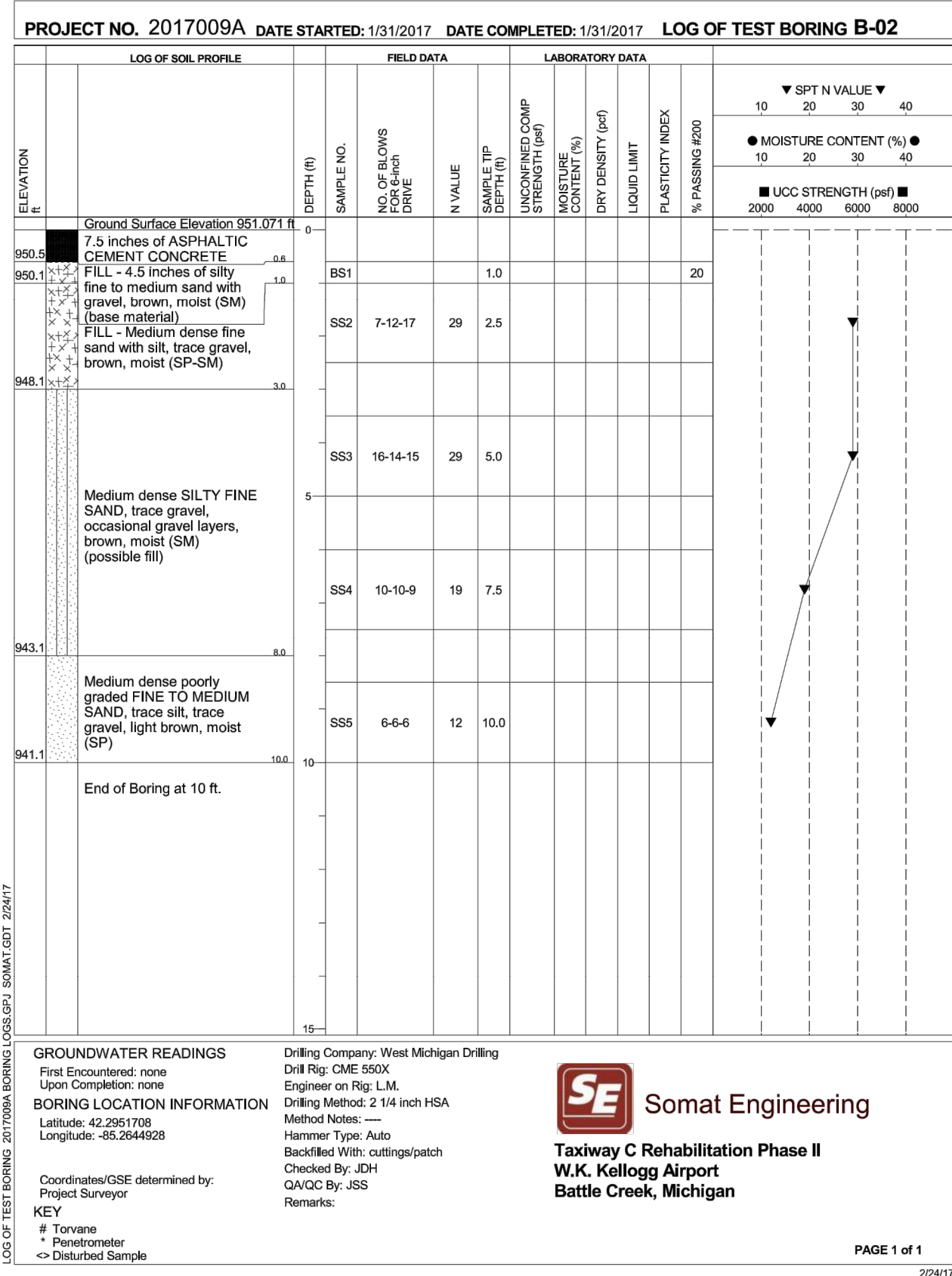
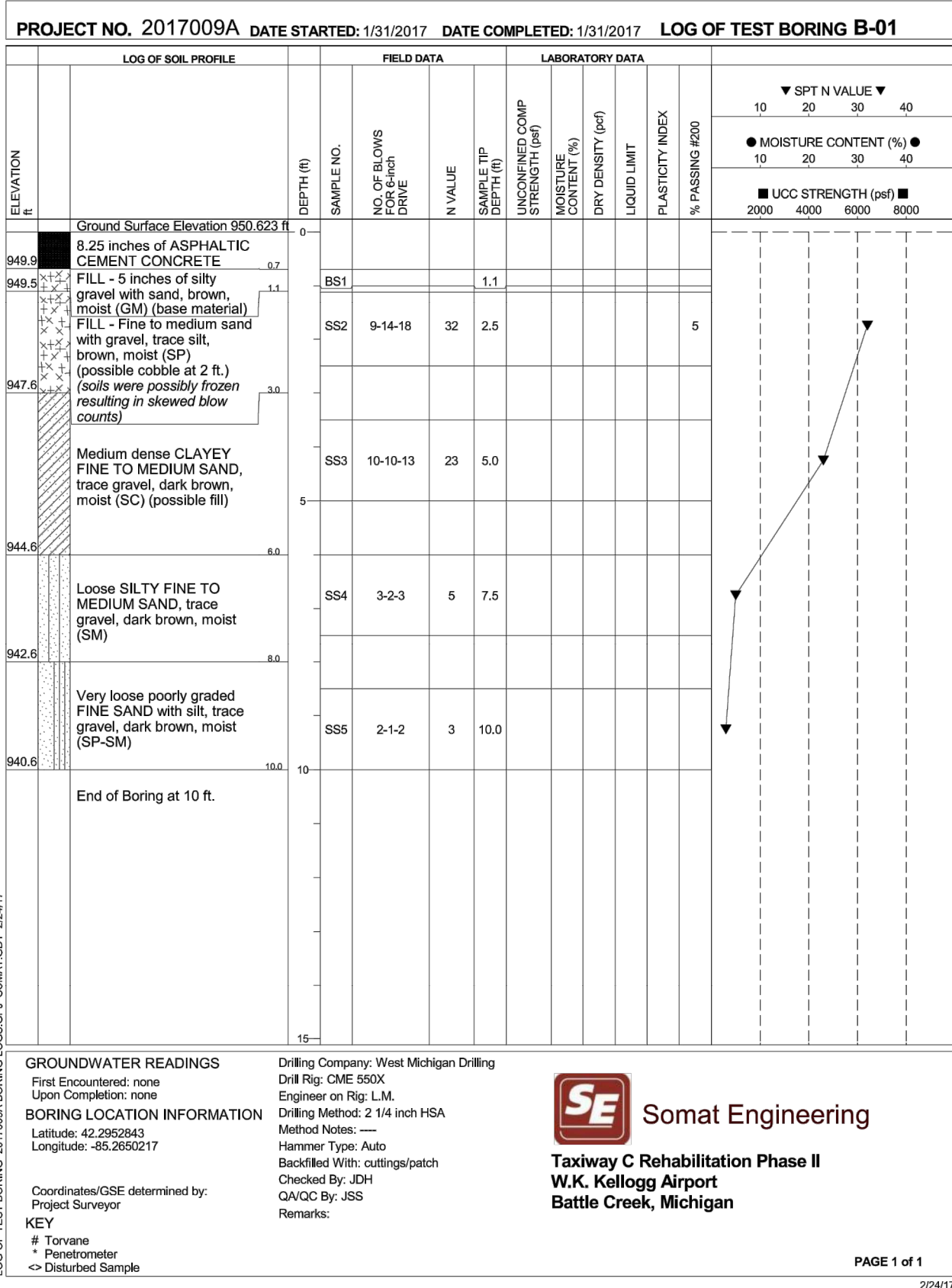
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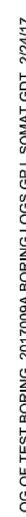
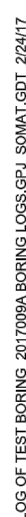
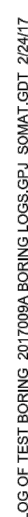
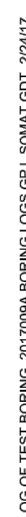
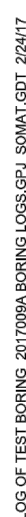
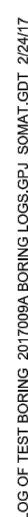
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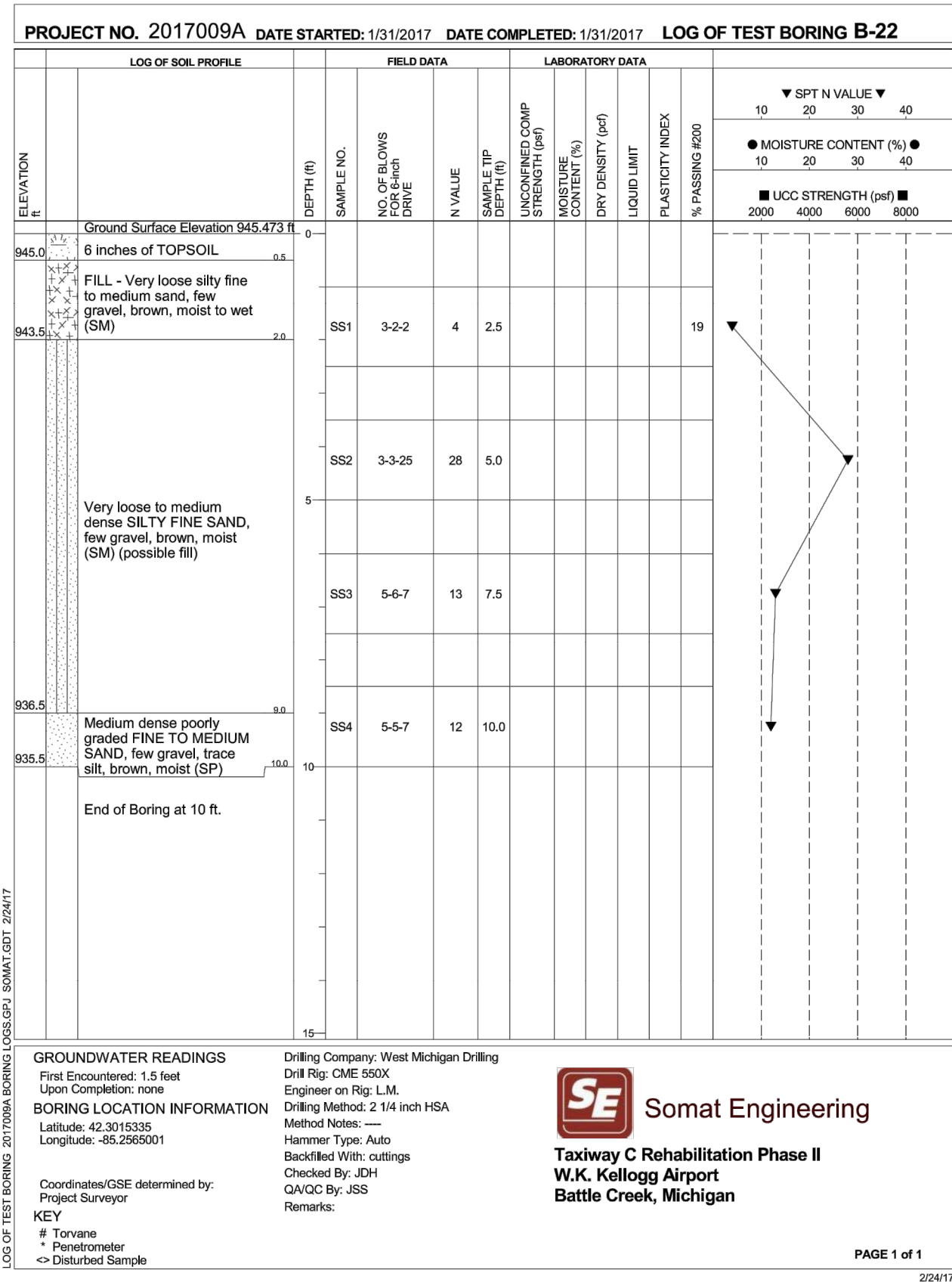
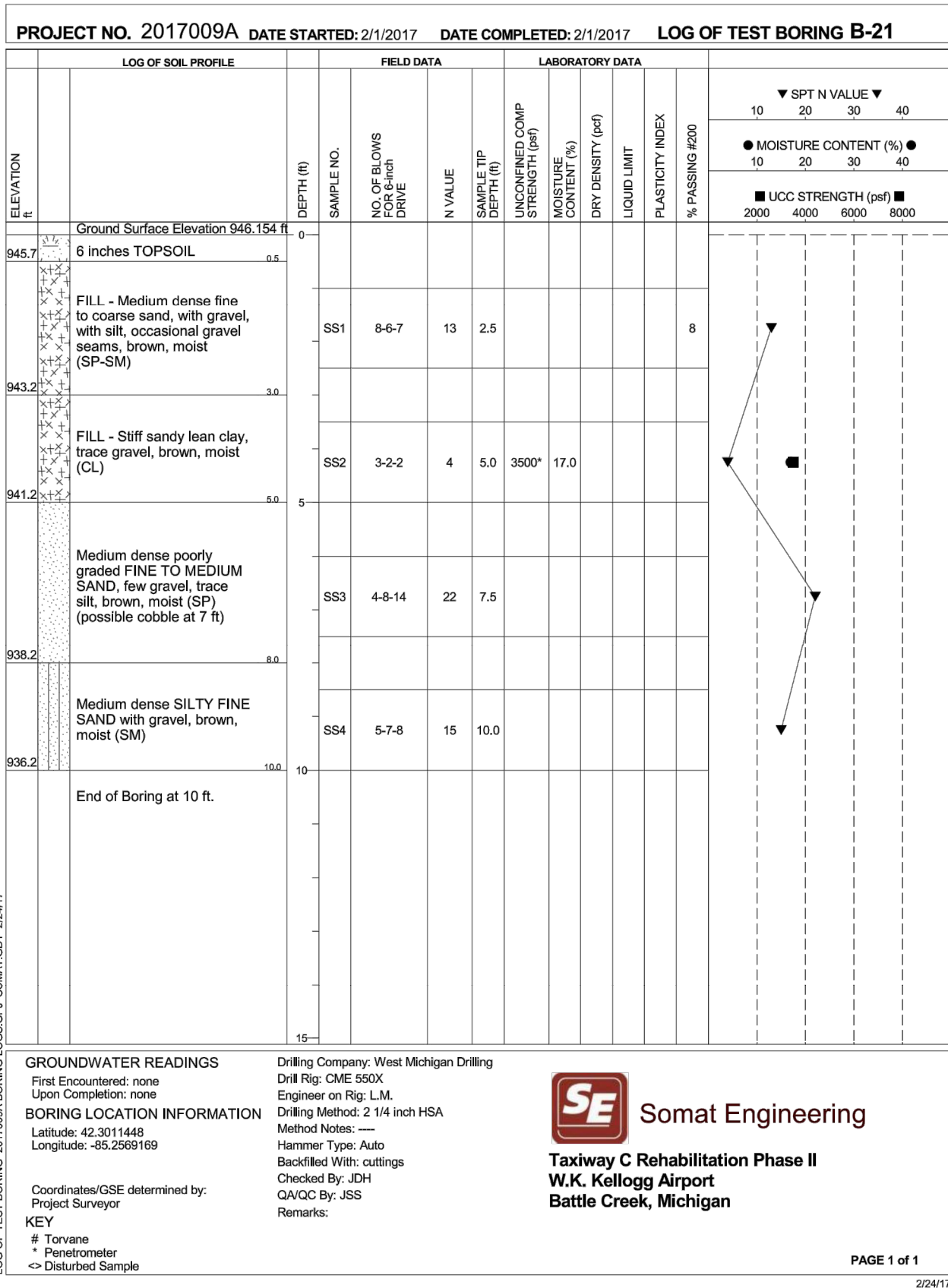
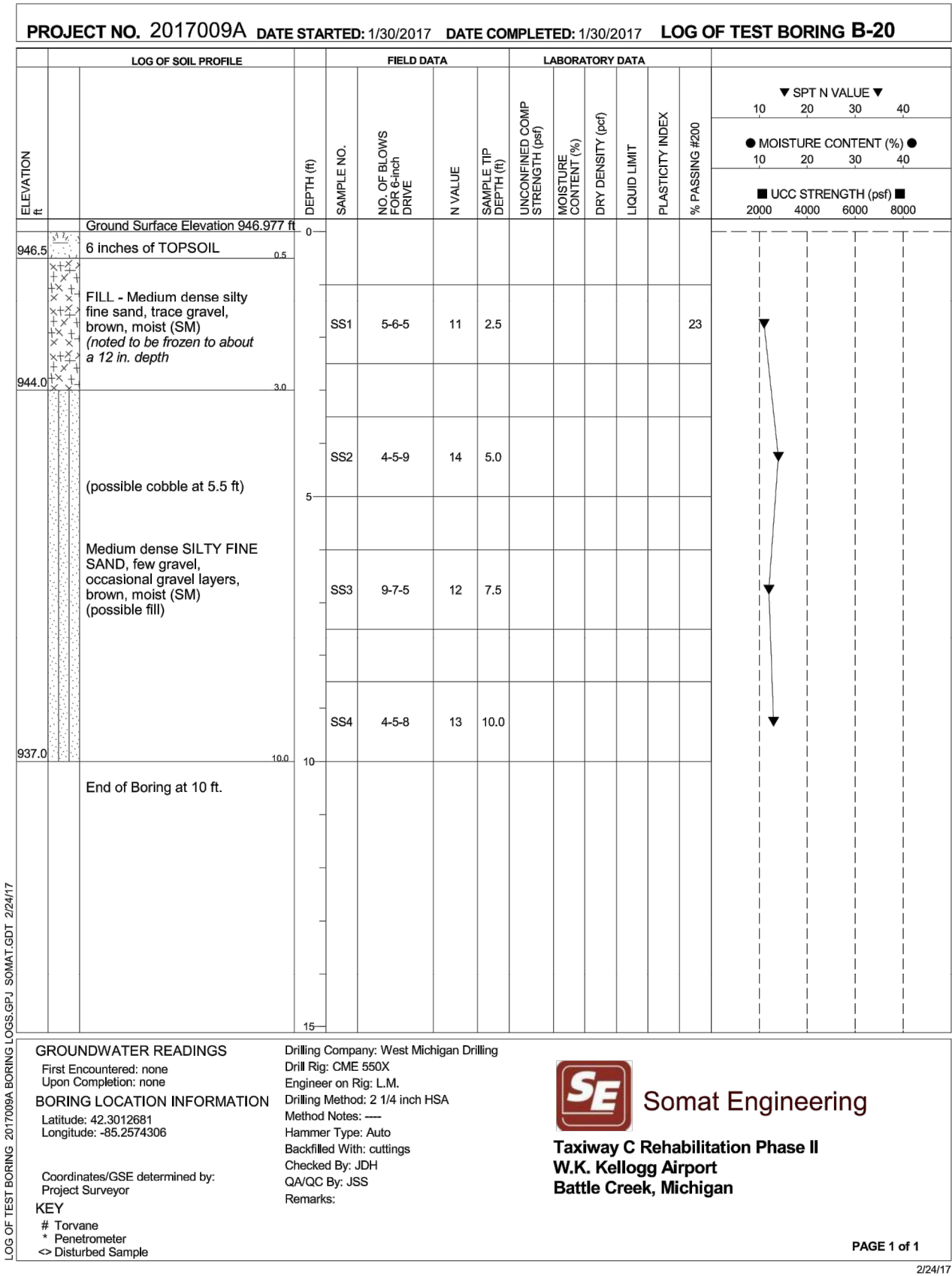
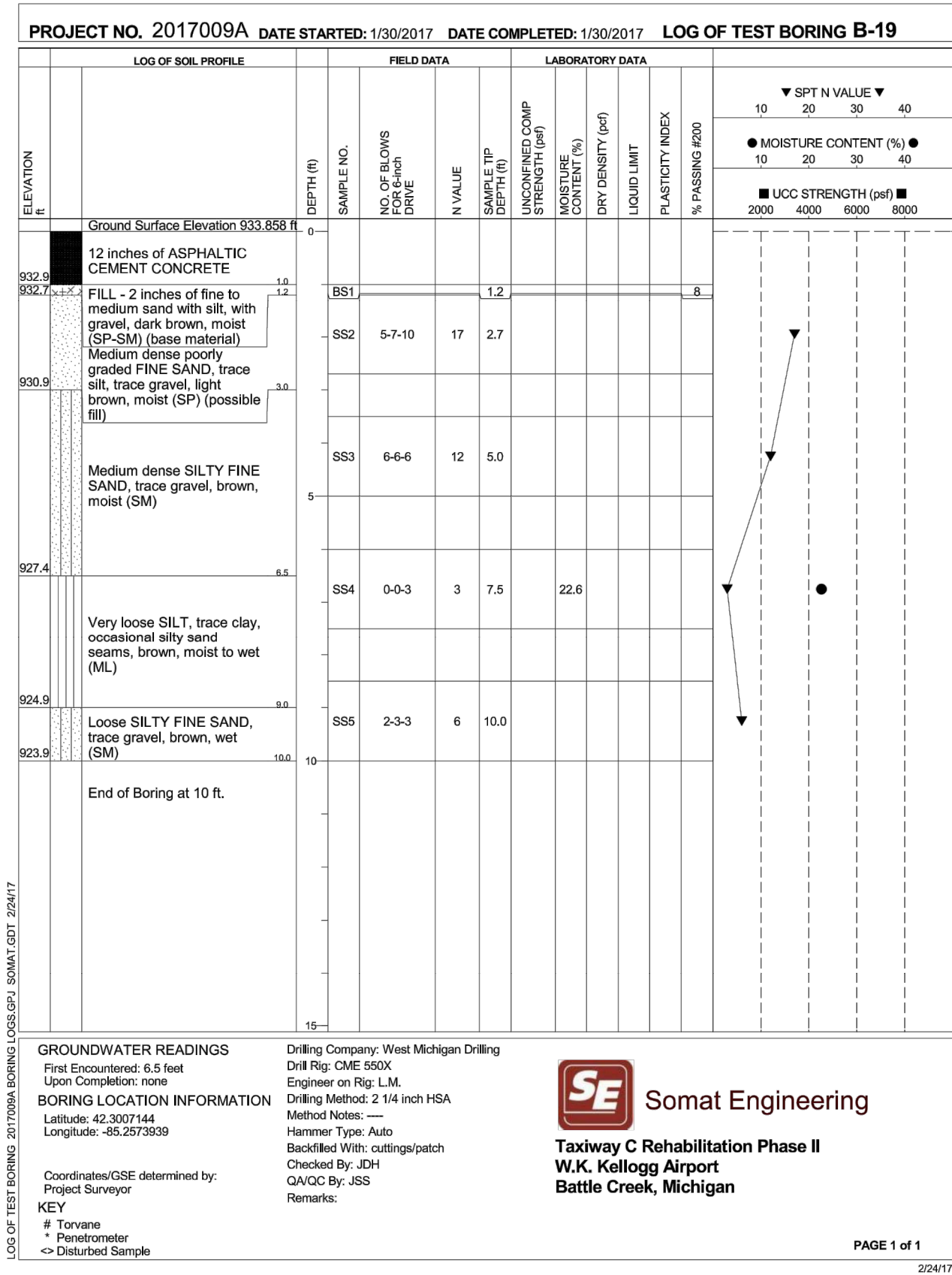
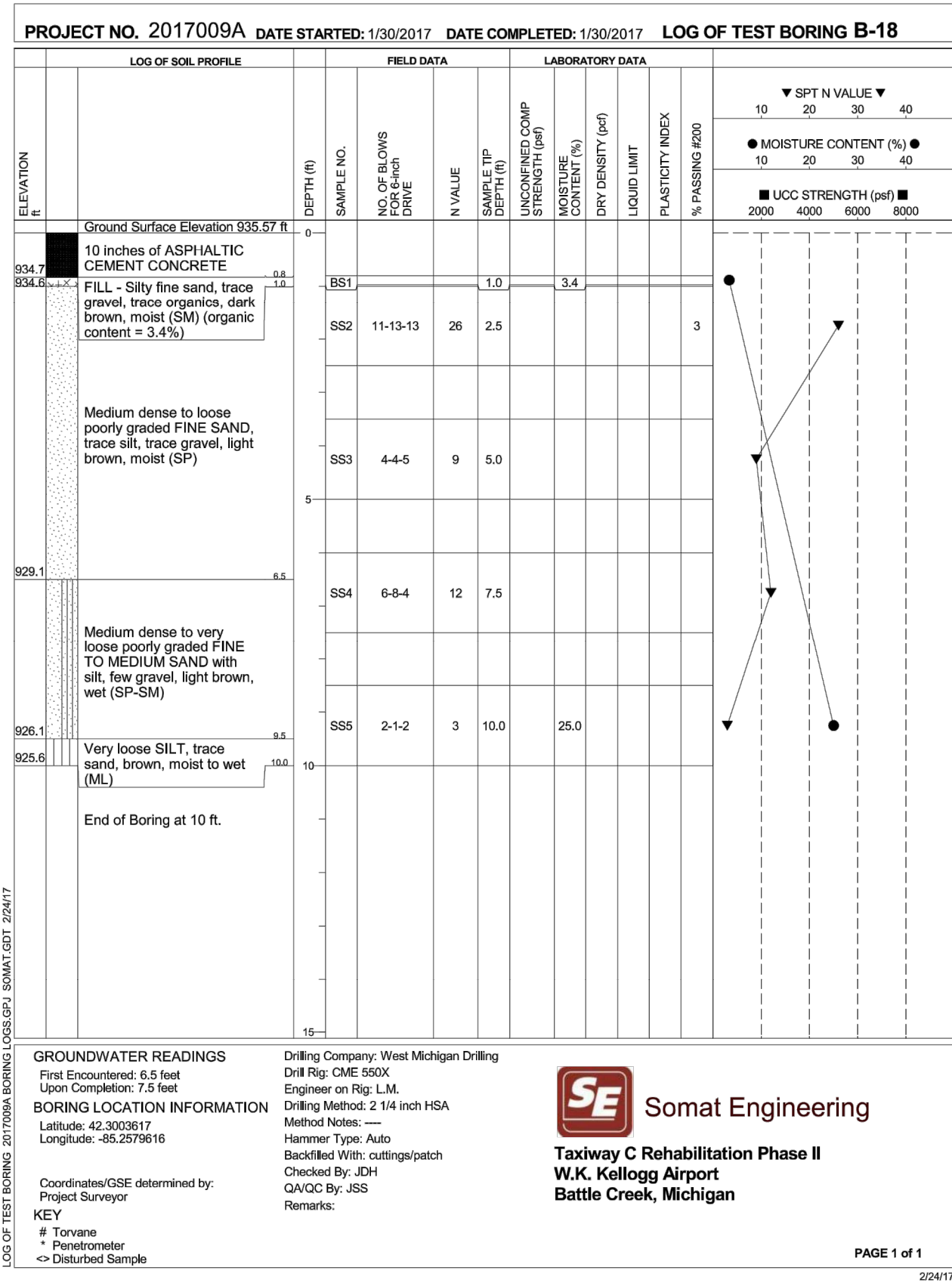
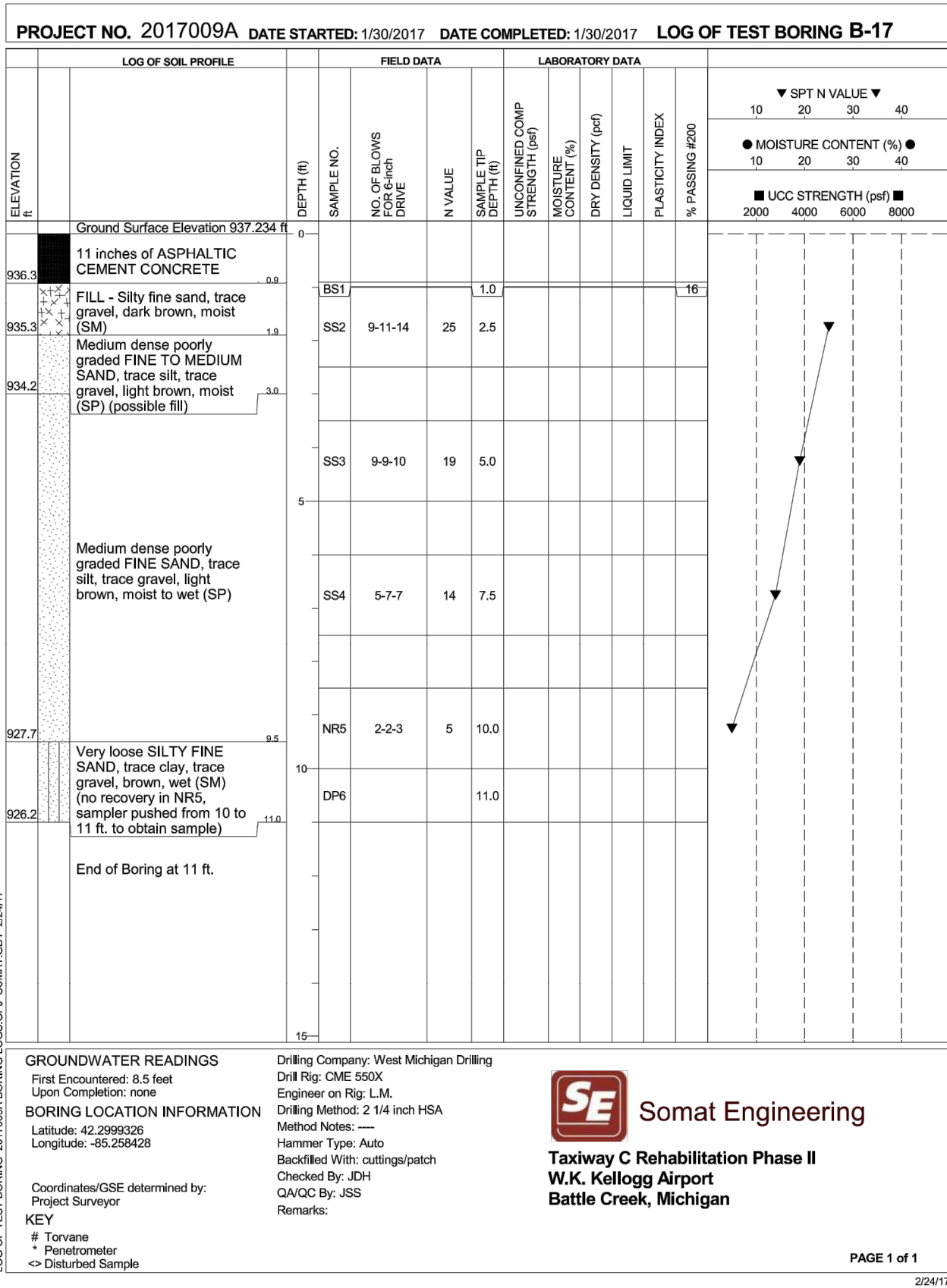
SOIL BORING LOGS
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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI**

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WK KELLOGG AIRPORT RECONSTRUCT TAXIWAY C PAVEMENT & LIGHTING PHASE 2

BATTLE CREEK, MI

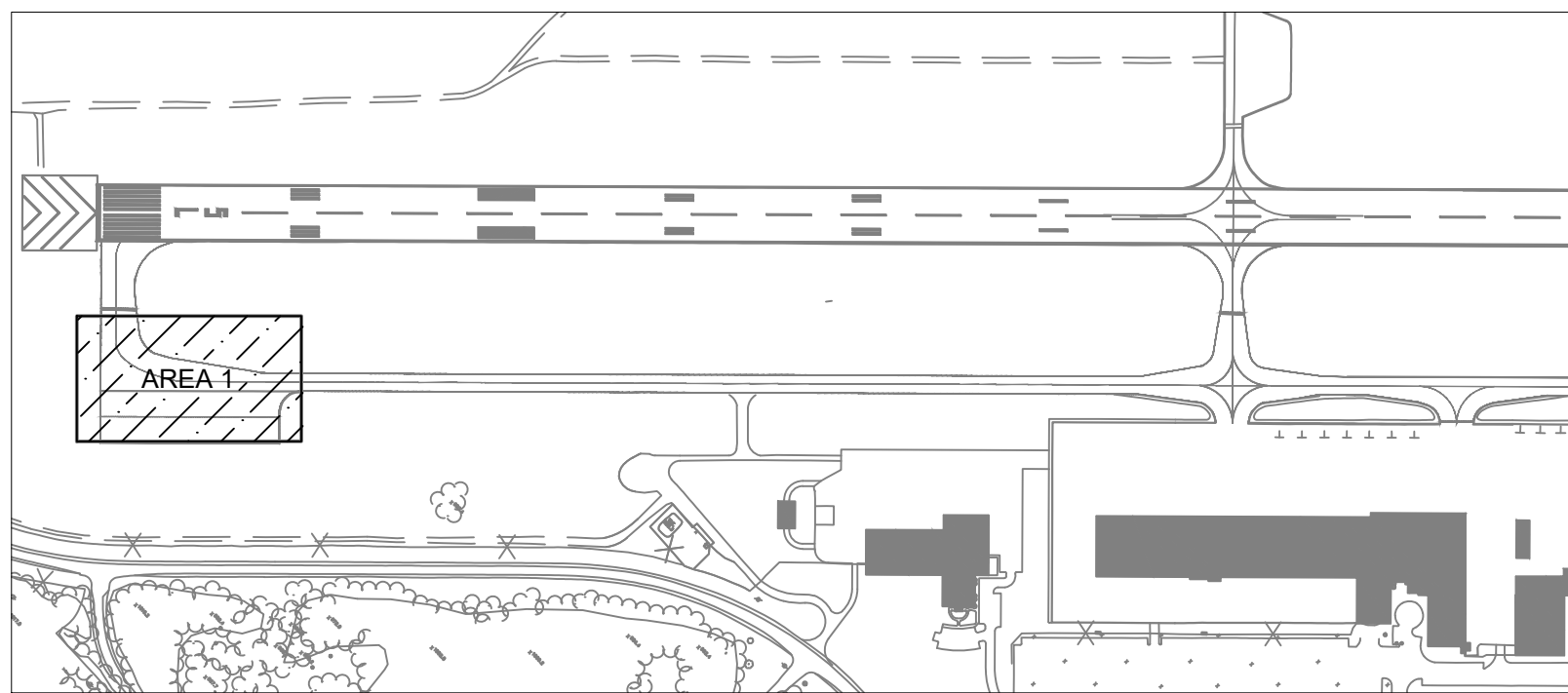
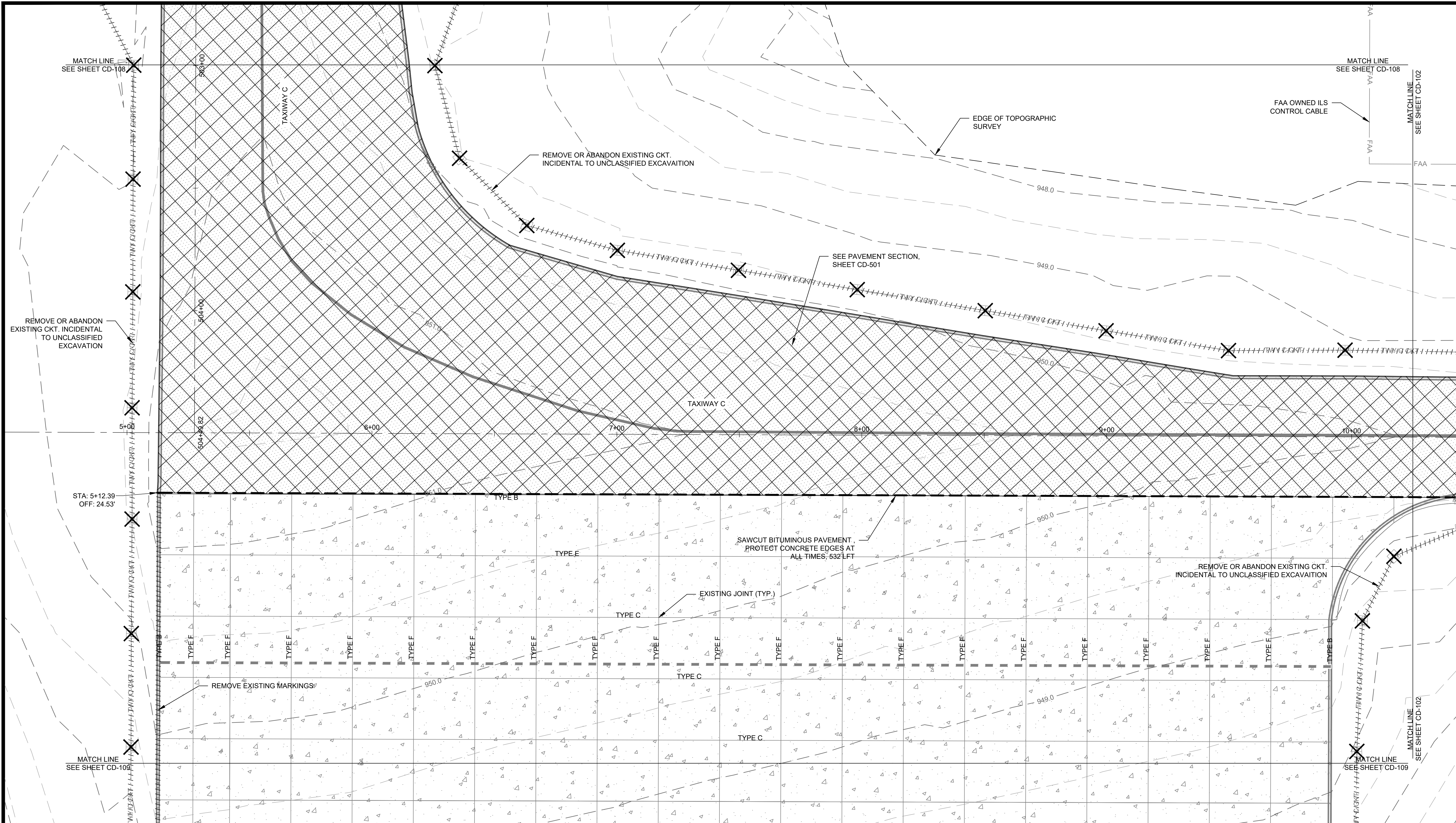
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SHEET CONTENTS
EXISTING
CONDITIONS AND
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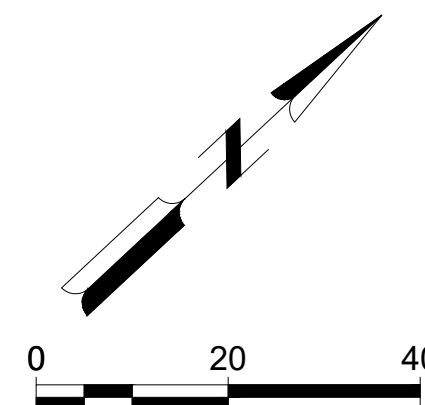


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

NOTES:

1. THE CONTRACTOR SHALL USE CAUTION WHEN REMOVING ANY ITEMS MARKED FOR REMOVAL AS TO NOT CAUSE UNNECESSARY DAMAGE TO THE ITEMS. THE AIRPORT SHALL HAVE THE OPTION TO SALVAGE ANY OF THE REMOVED ITEMS PRIOR TO THE CONTRACTOR'S DISPOSAL.
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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

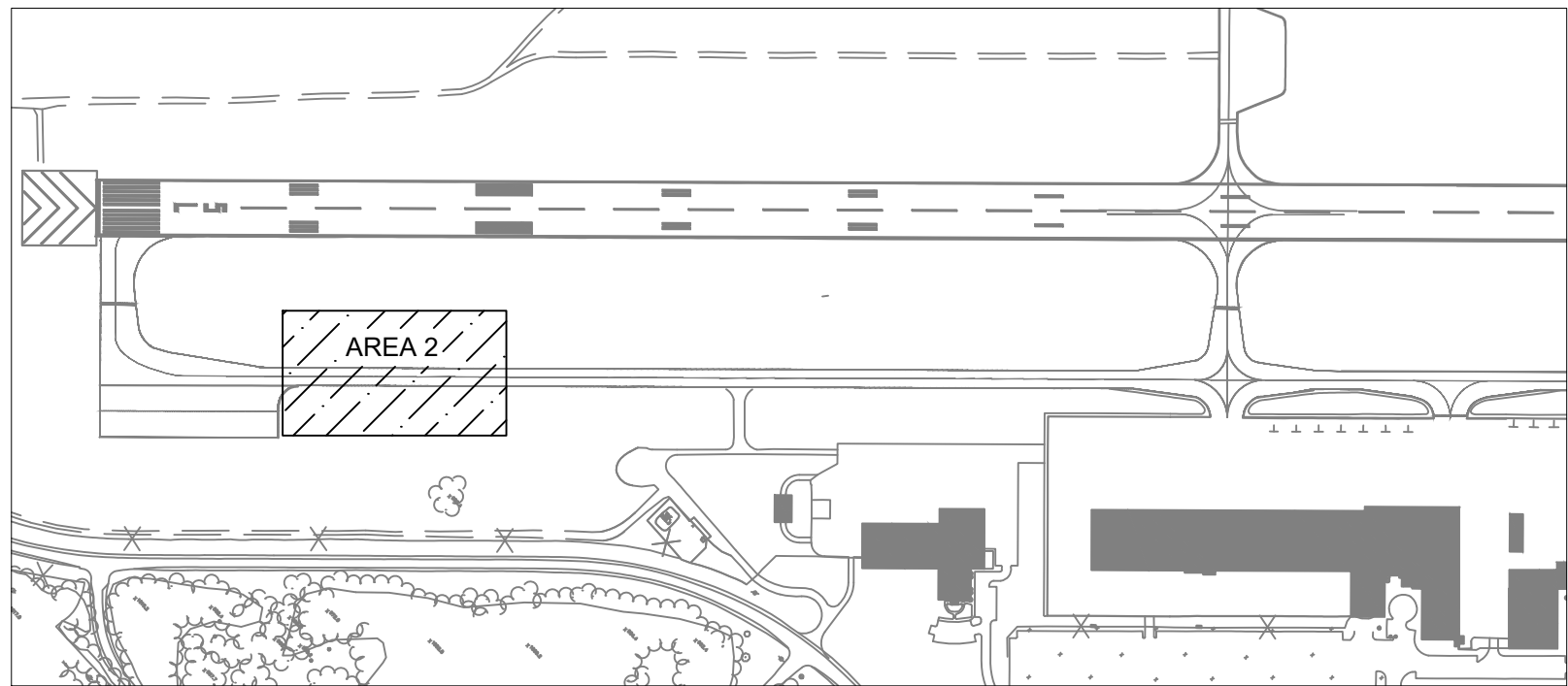
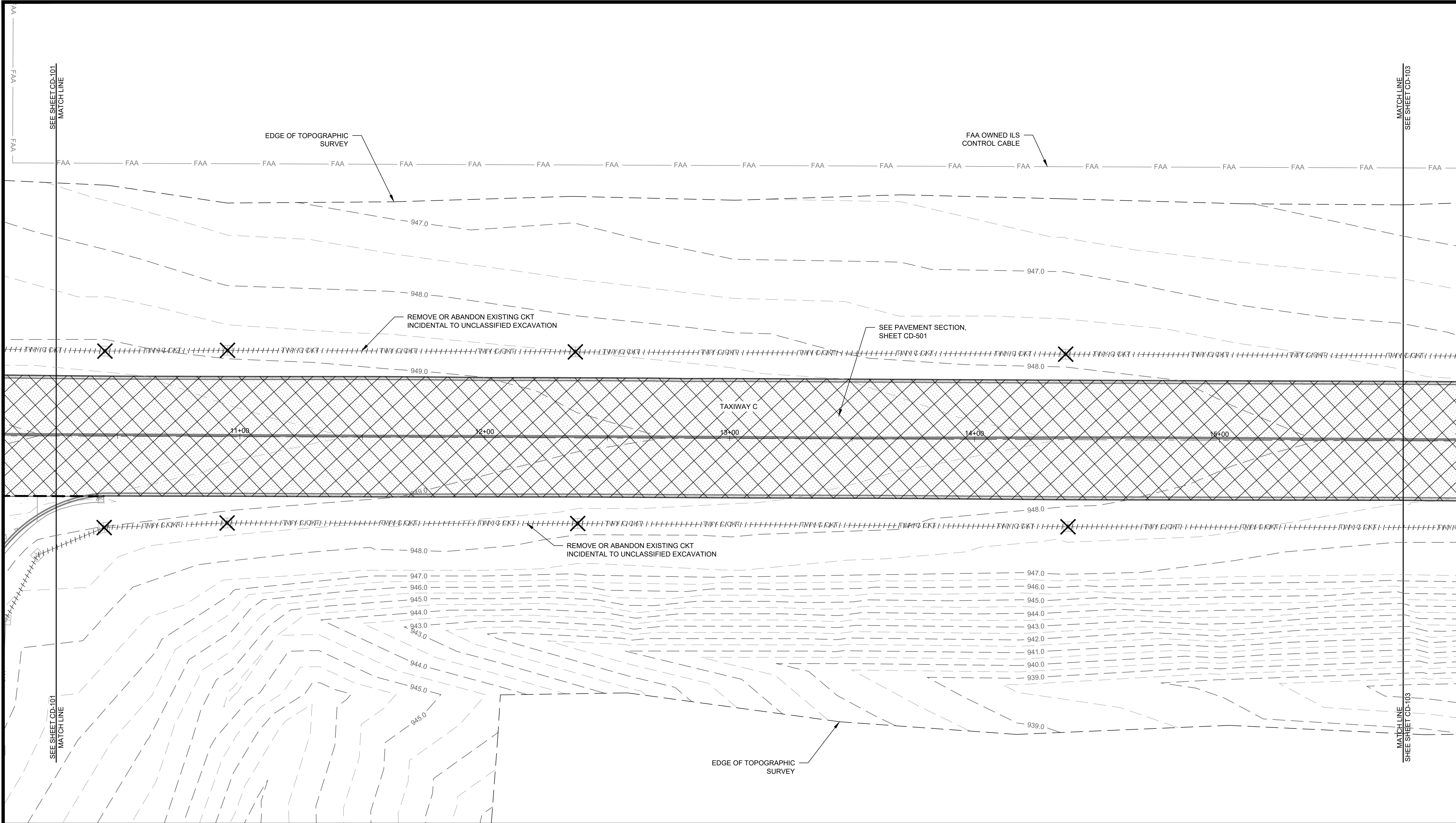
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SHEET CONTENTS
EXISTING
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DEMOLITION PLAN -
AREA 2

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CD-102

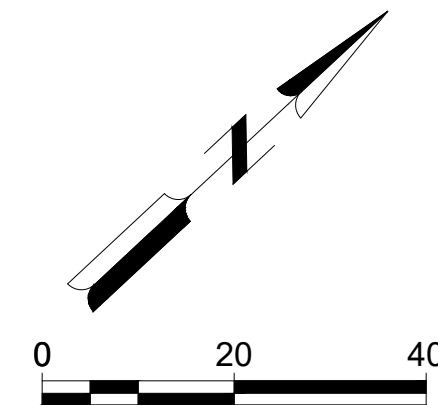


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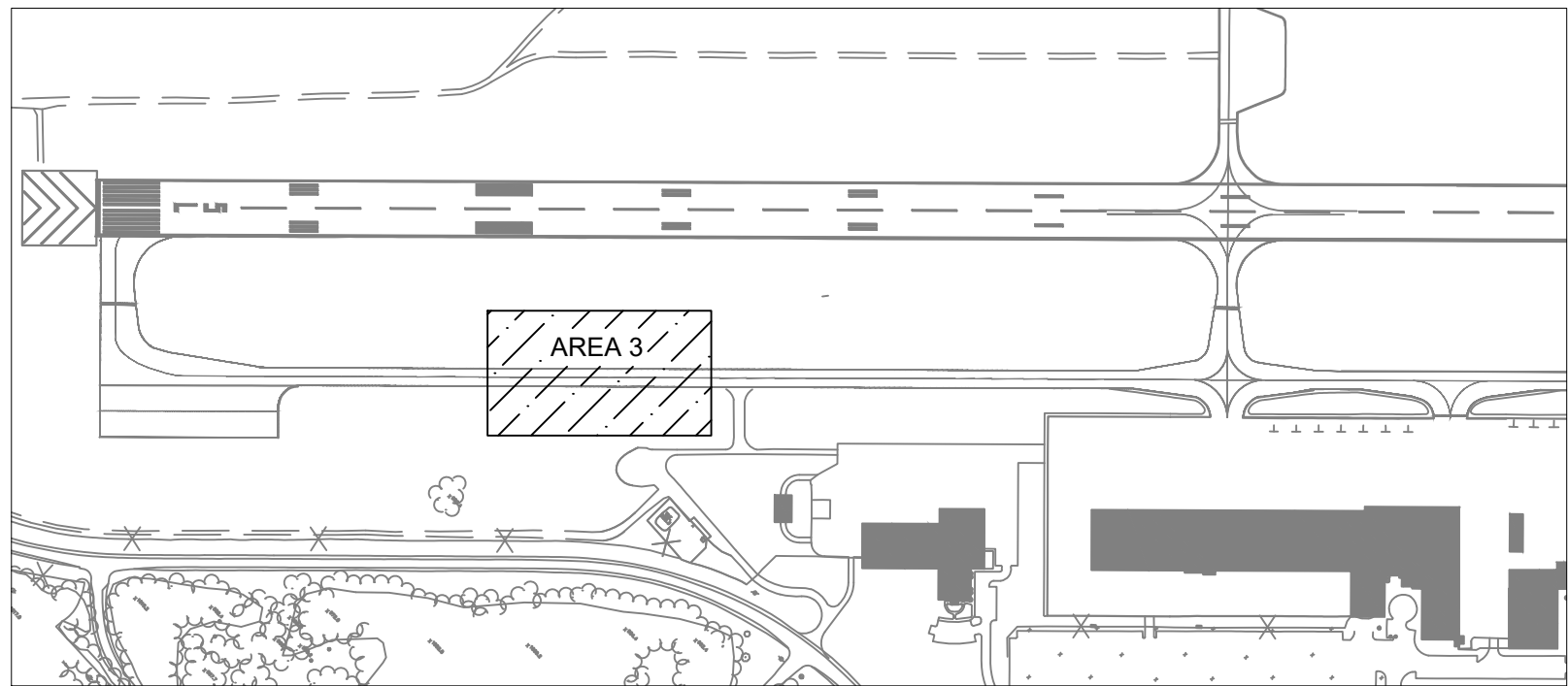
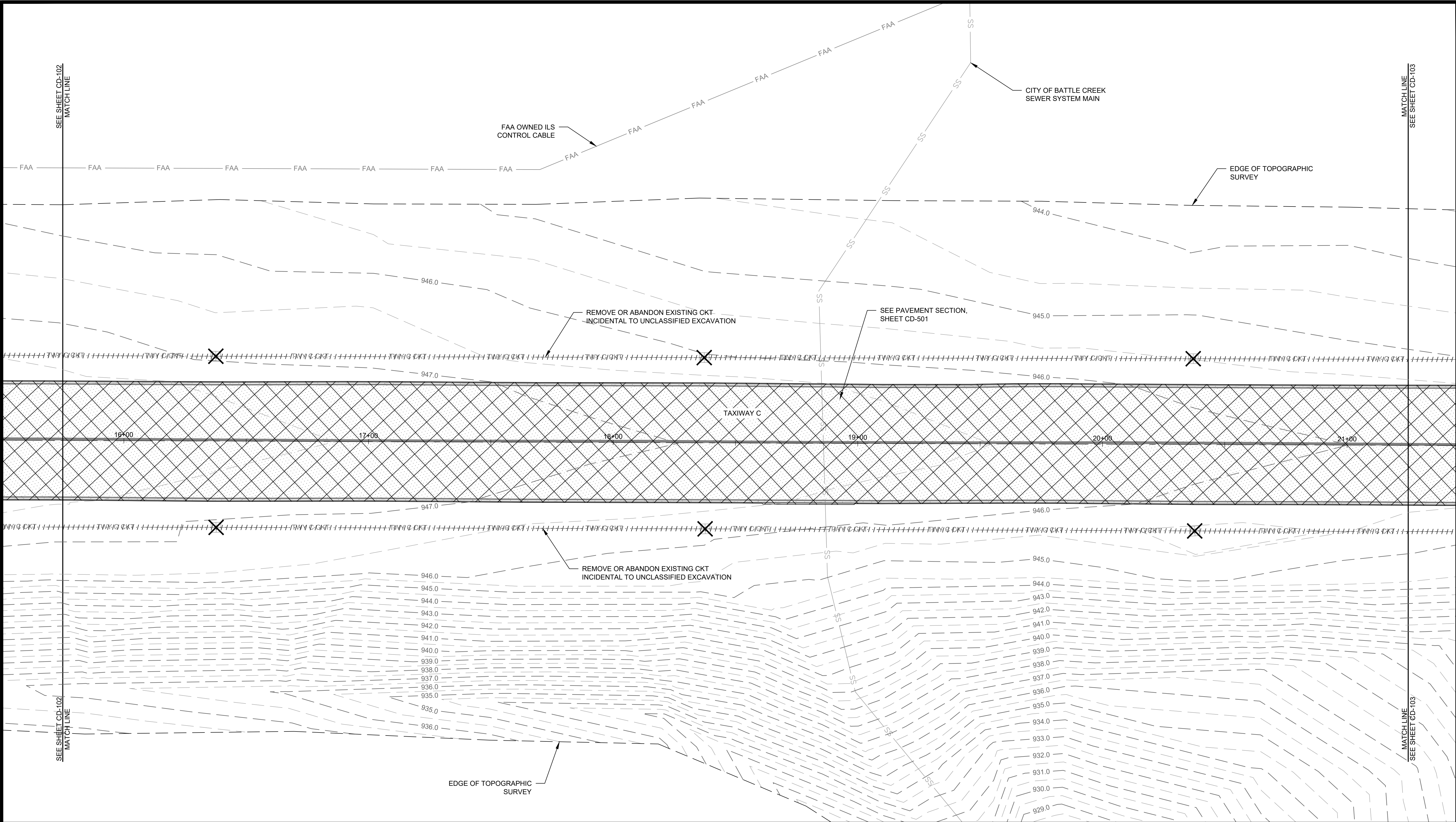
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	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

NOTES:

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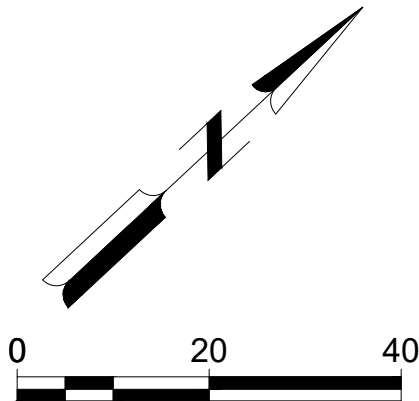


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LEGEND							
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	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

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SHEET CONTENTS
EXISTING
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AREA 3

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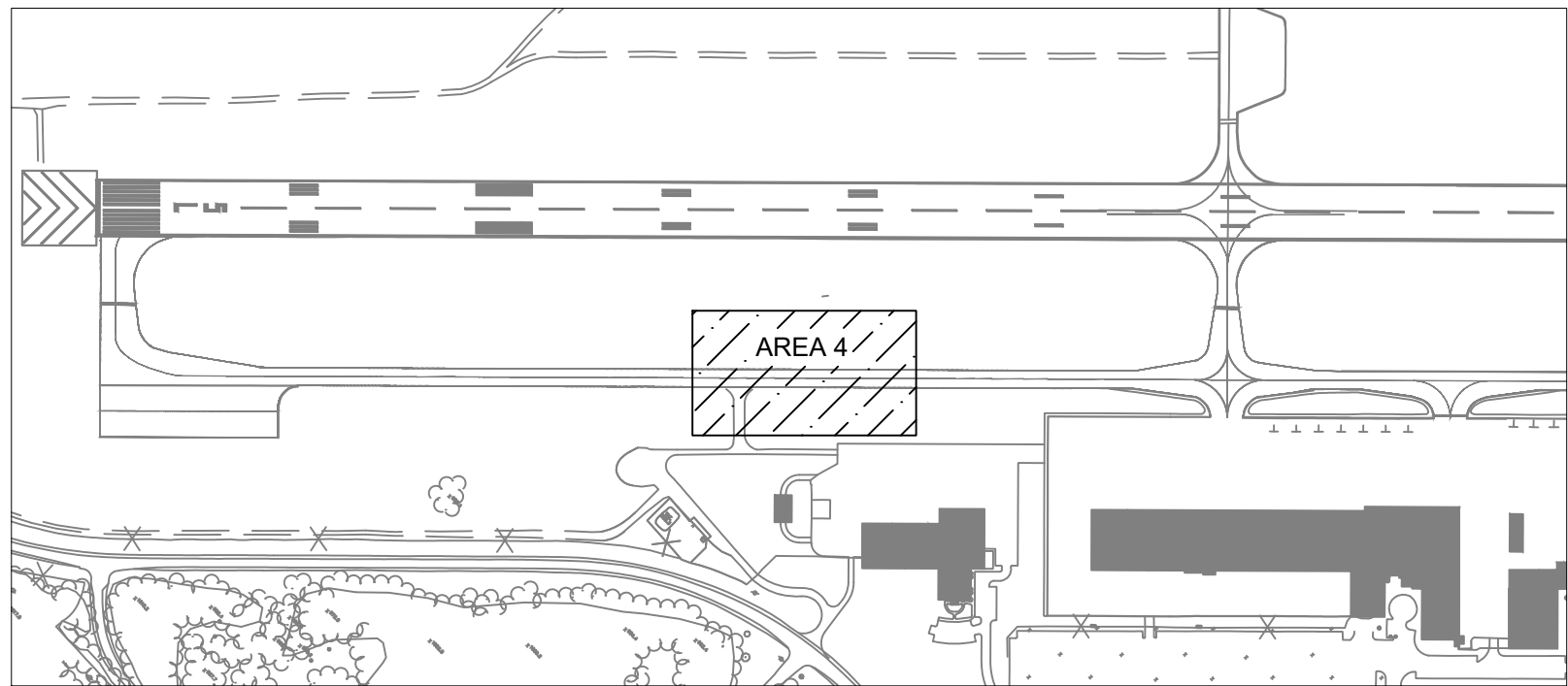
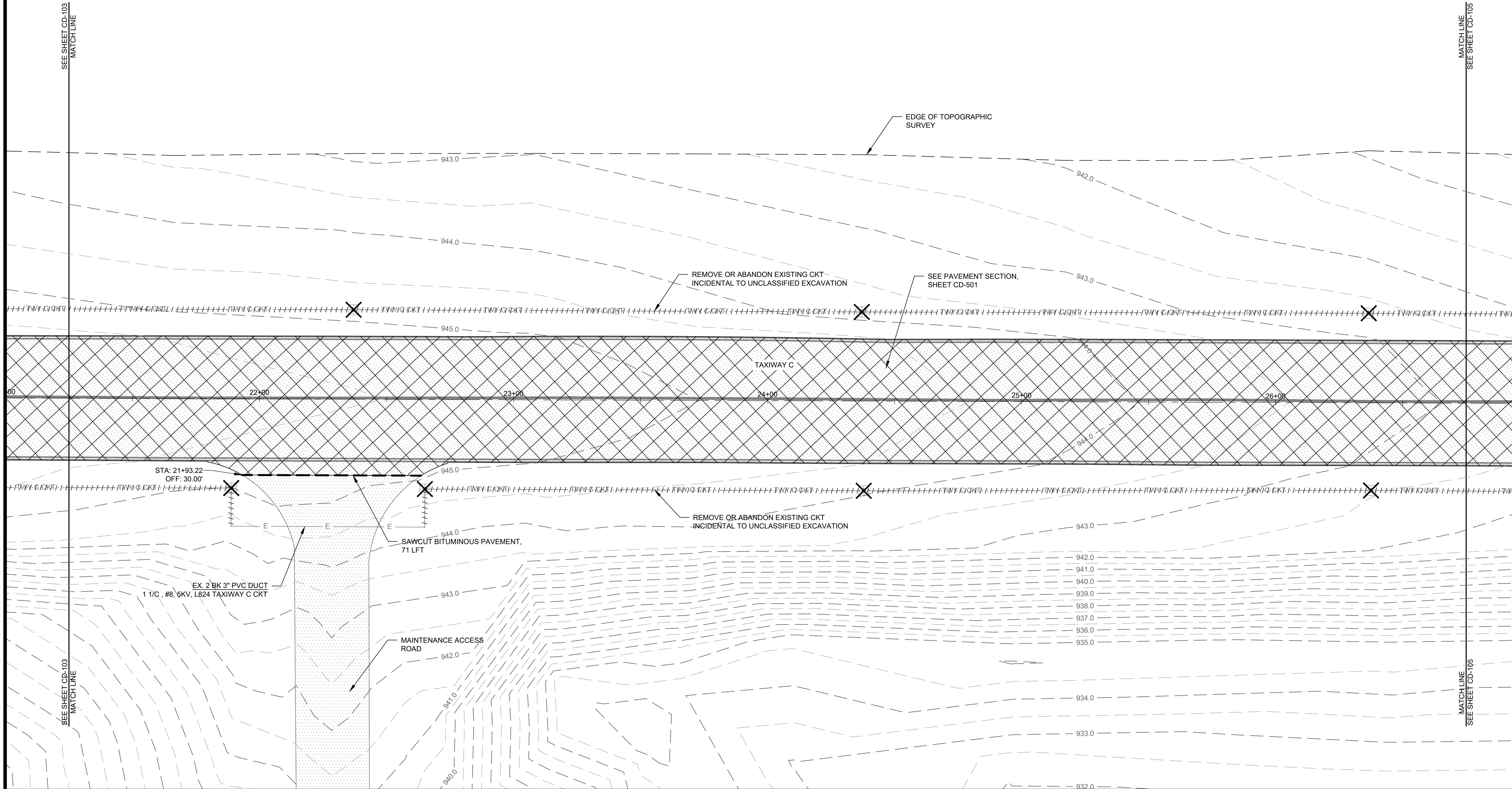
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SHEET CONTENTS
EXISTING
CONDITIONS AND
DEMOLITION PLAN -
AREA 4

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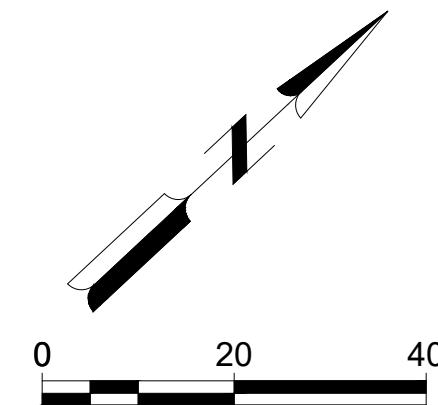


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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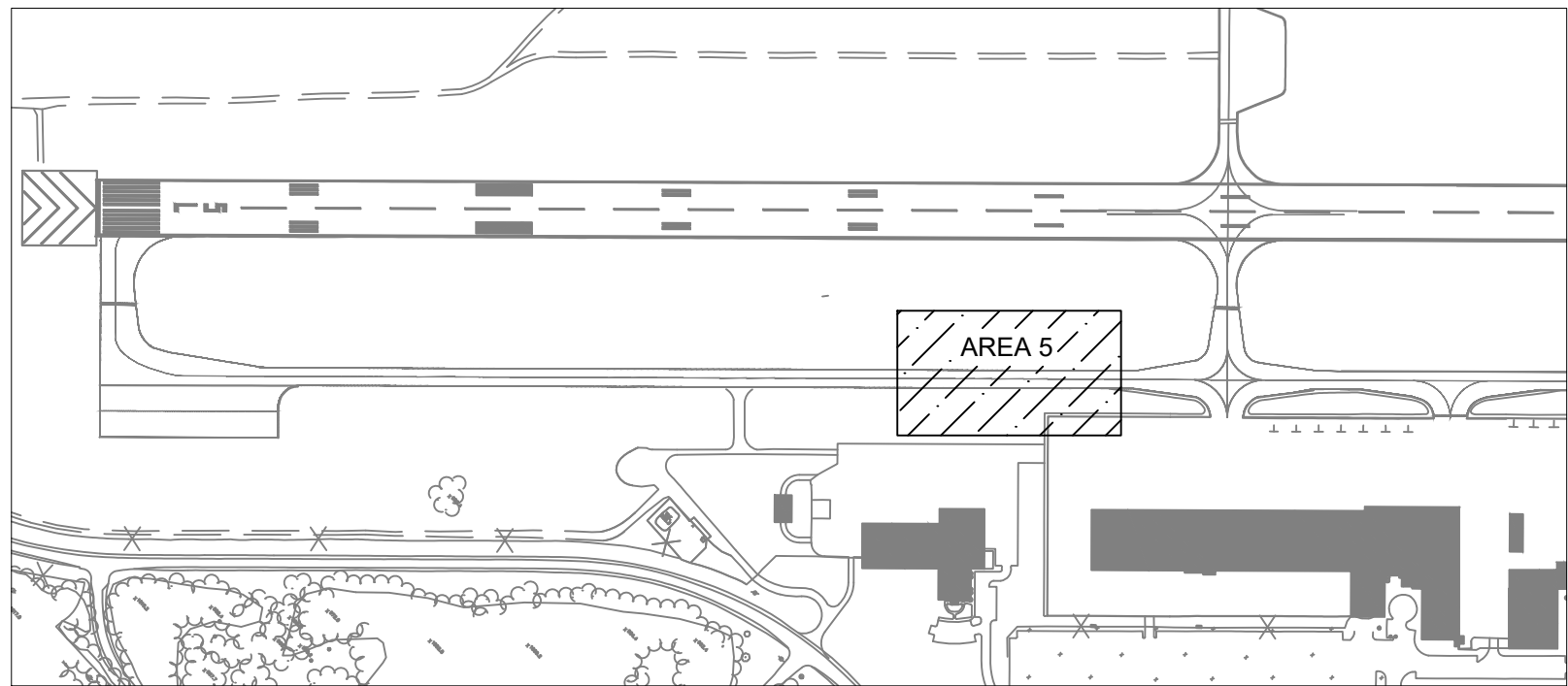
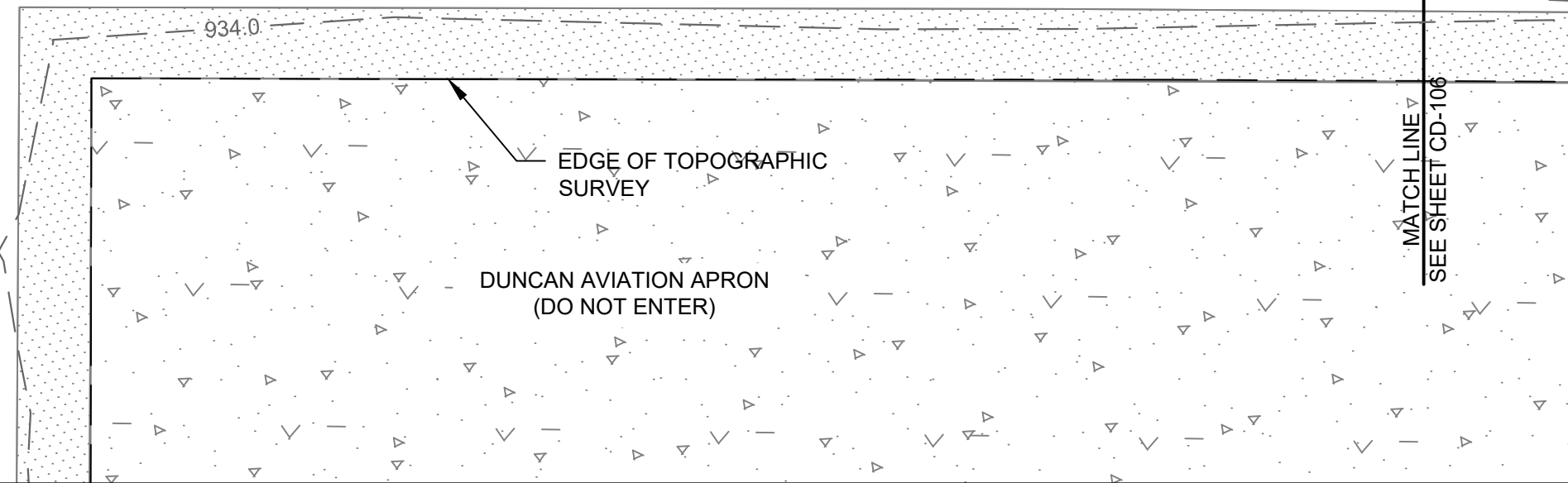
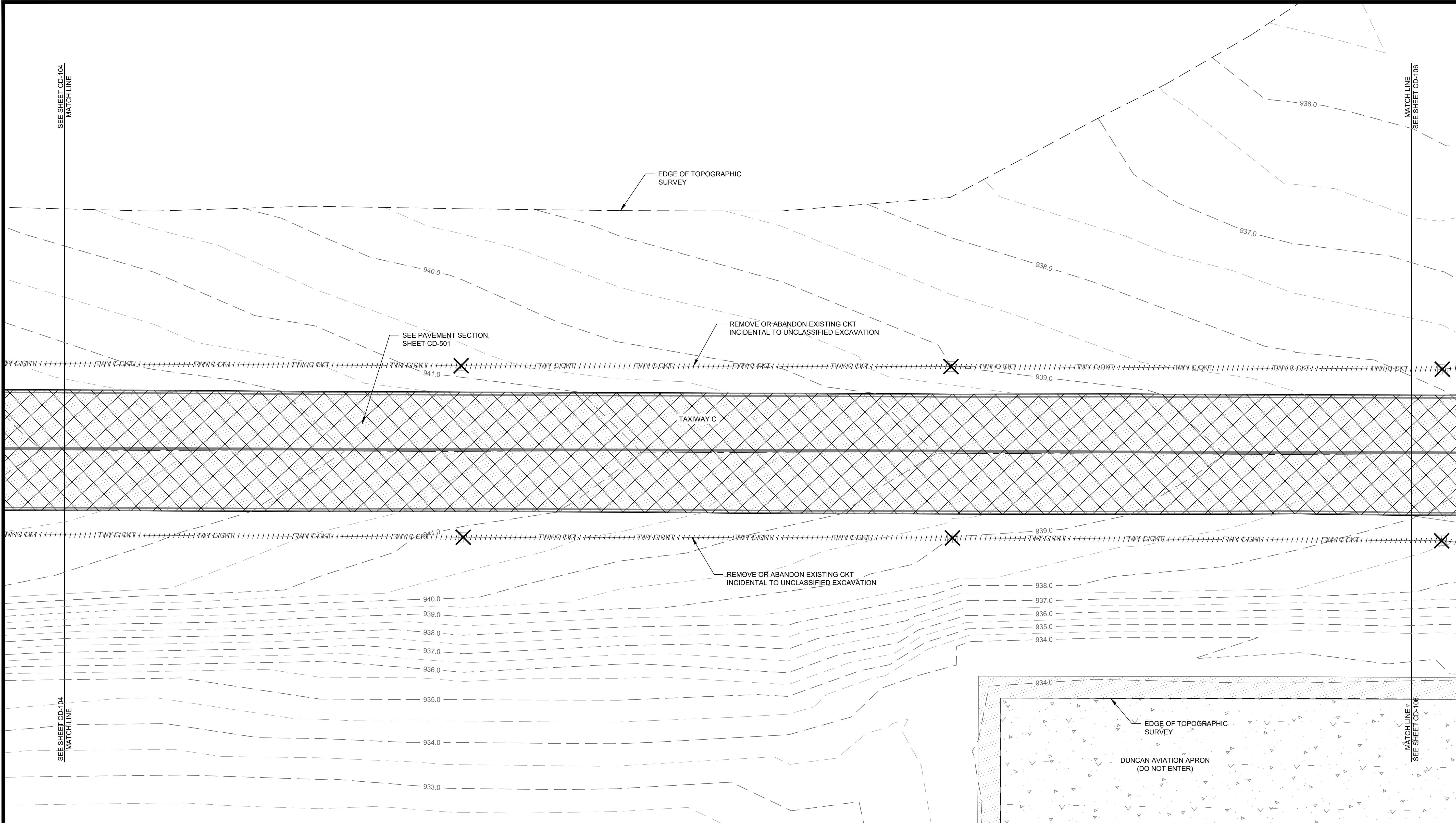
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SHEET CONTENTS
EXISTING
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AREA 5

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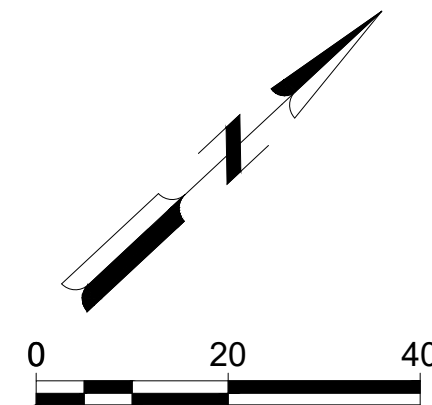


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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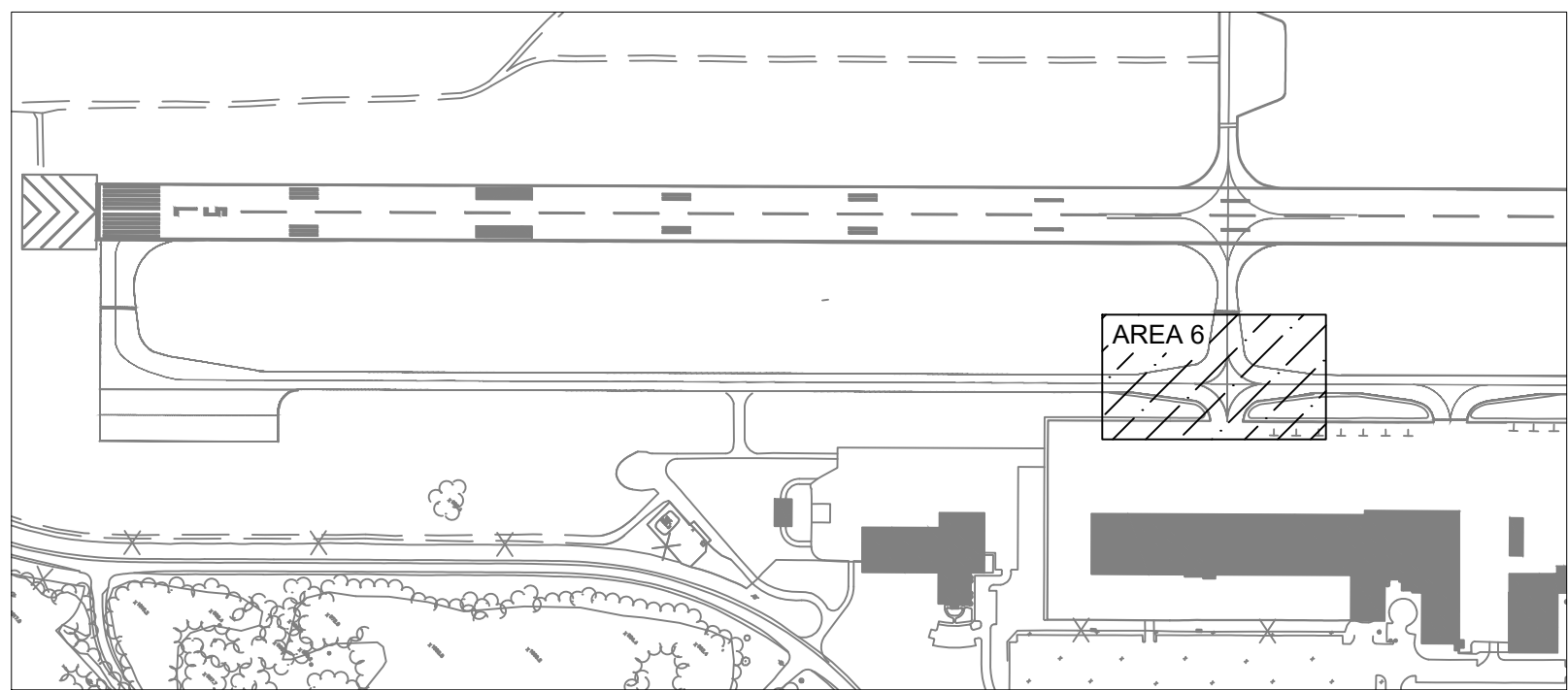
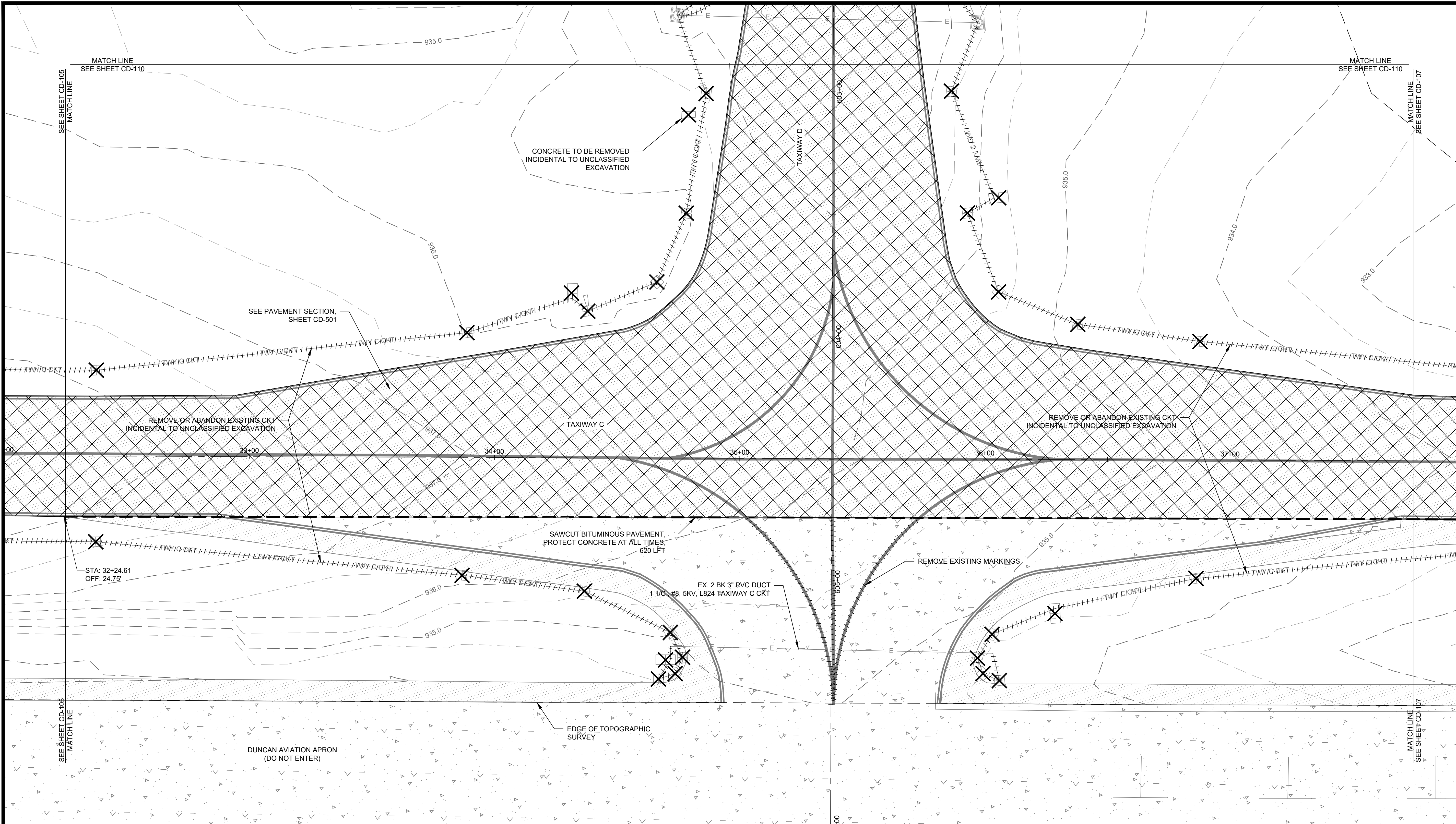
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SHEET CONTENTS
EXISTING
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DEMOLITION PLAN -
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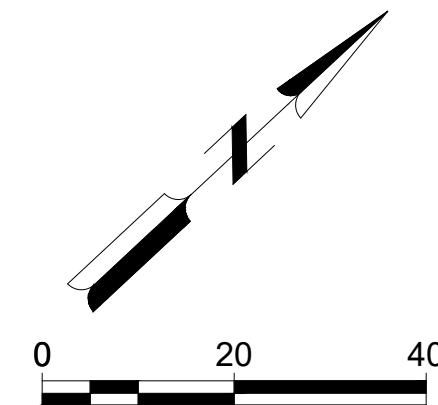


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
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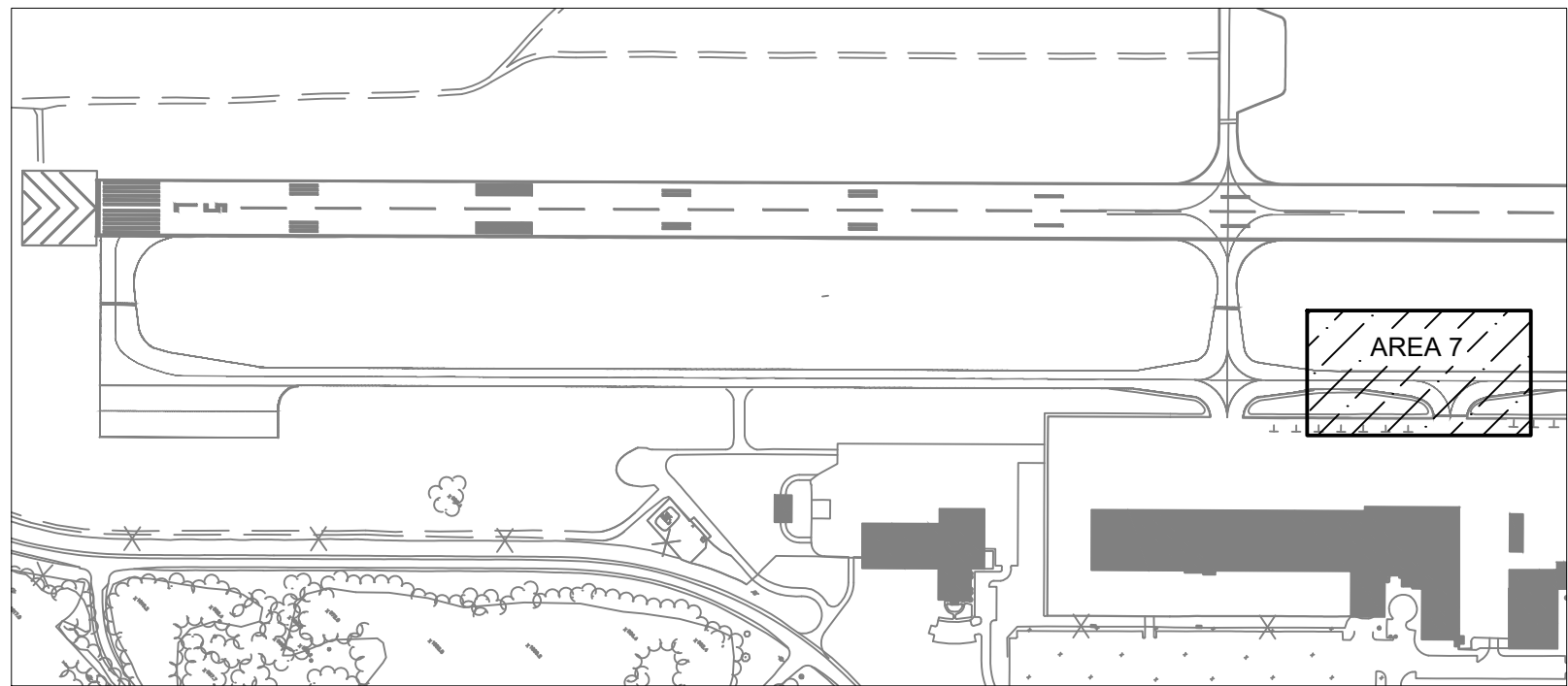
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 &H NO.: 0228800-161097.01
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SHEET CONTENTS

EXISTING
CONDITIONS AND
DEMOLITION PLAN -
AREA 7

SHEET NO. 21 of 76

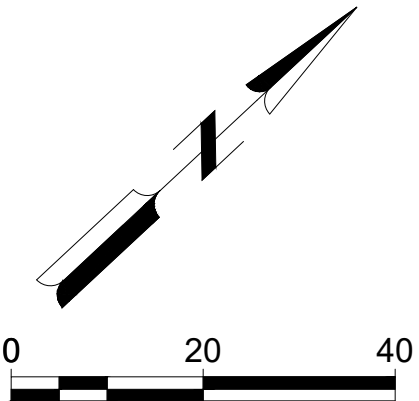
CD-107



LEGEND							
	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

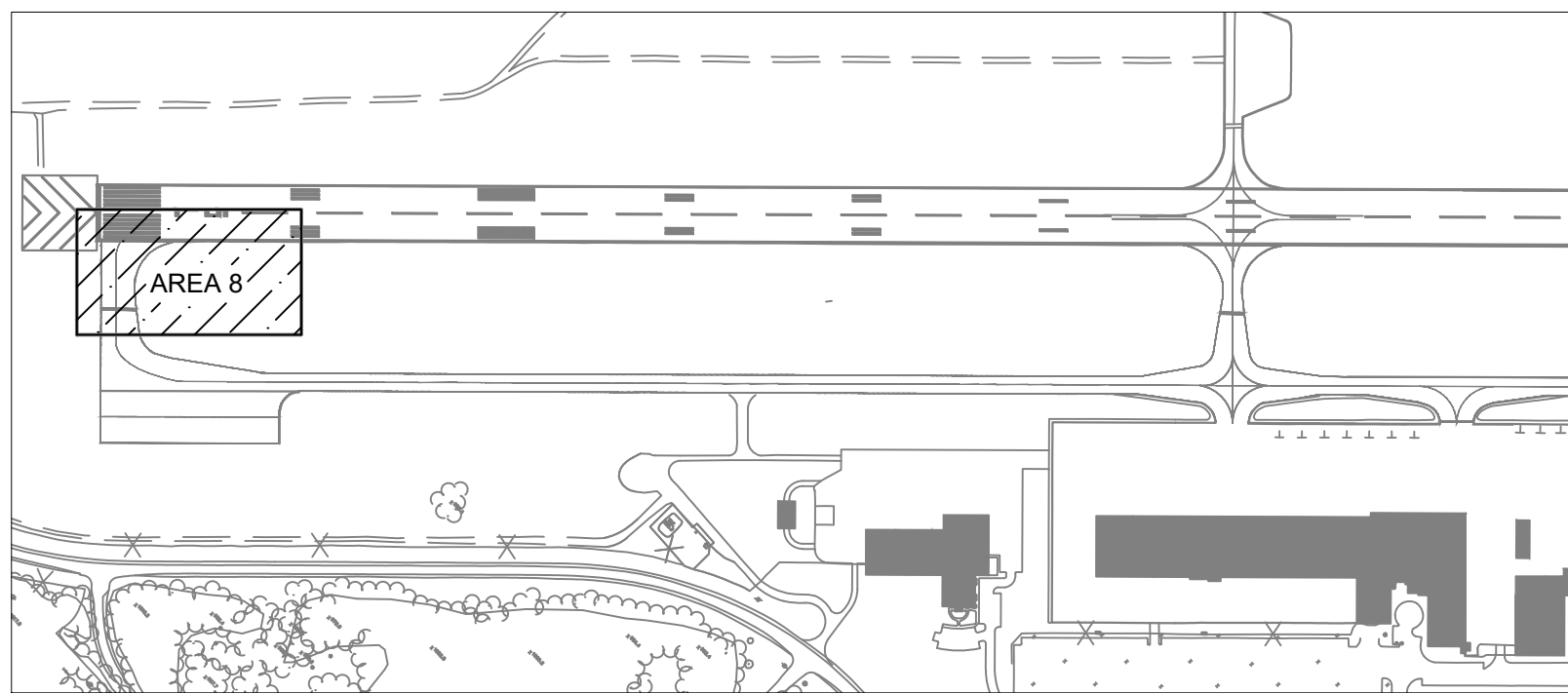
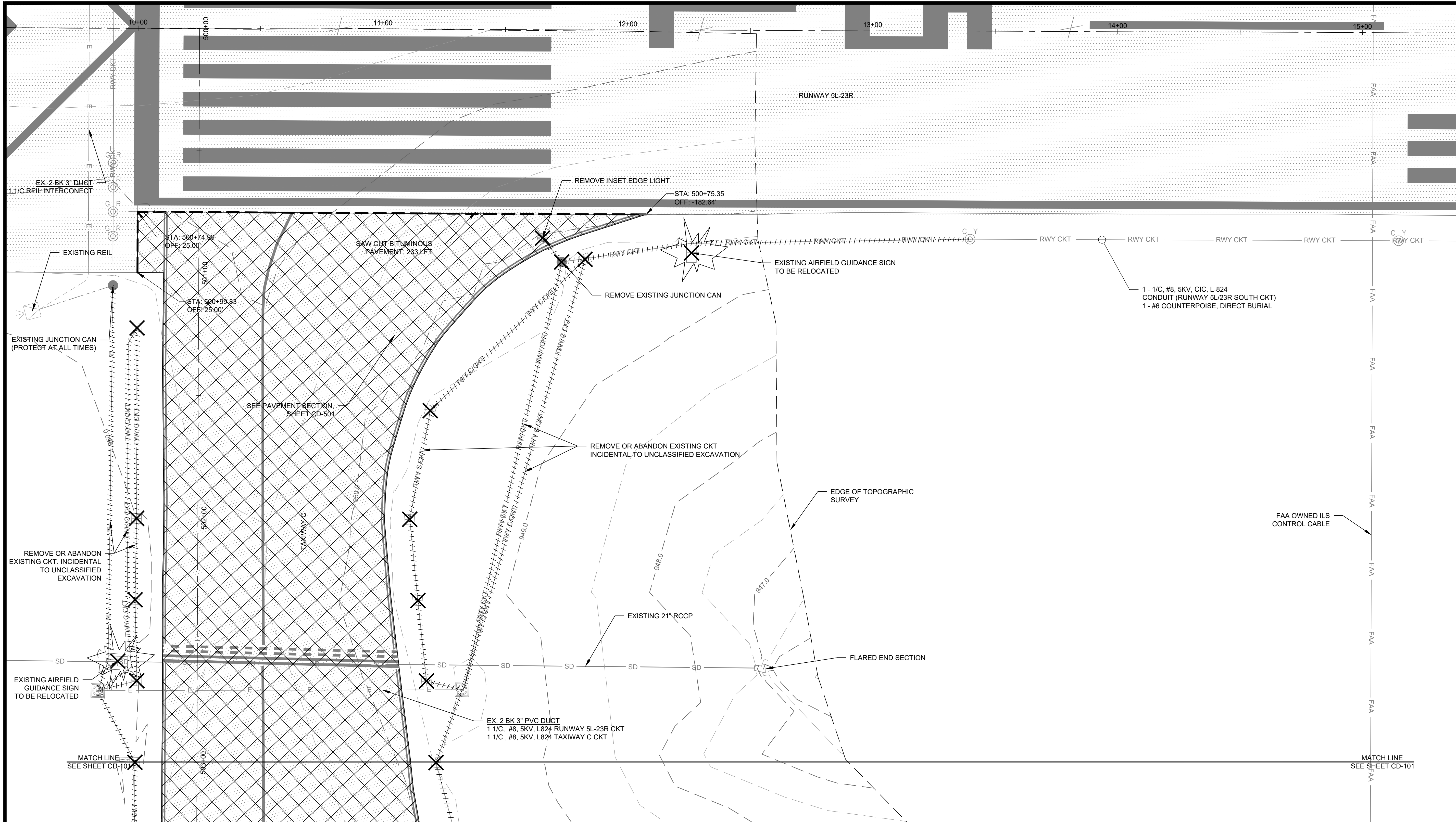
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SHEET CONTENTS
EXISTING
CONDITIONS AND
DEMOLITION PLAN -
AREA 8

SHEET NO 22 of 76

CD-108

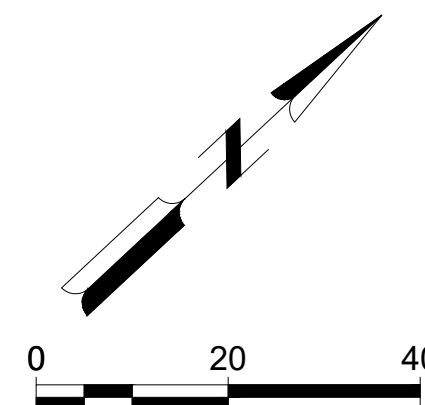


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		EXISTING STAKE MOUNTED MITL		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

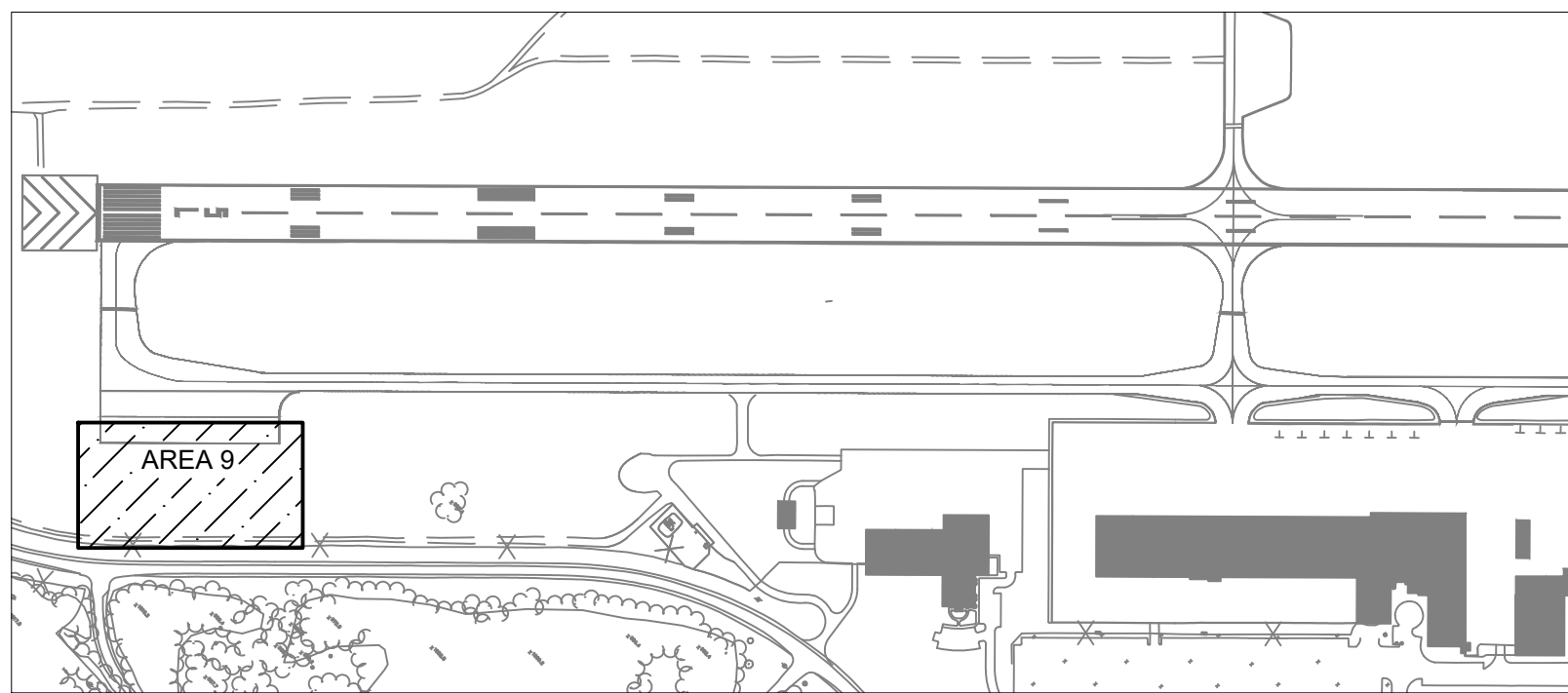
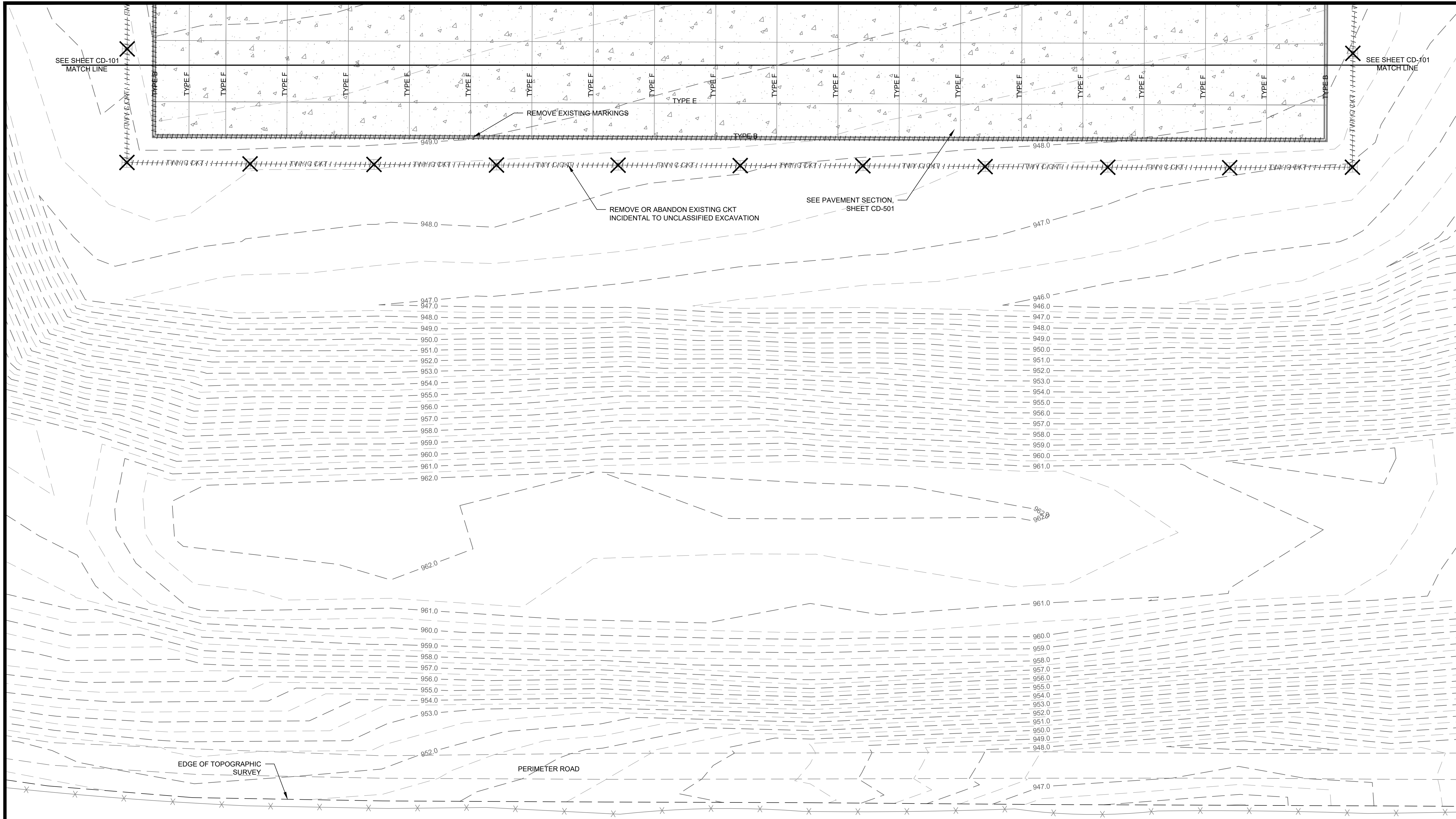
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SHEET CONTENTS
EXISTING
CONDITIONS AND
DEMOLITION PLAN -
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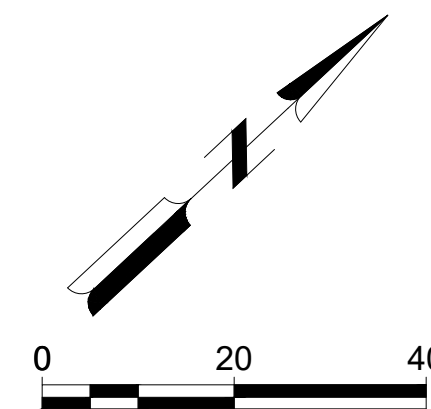


LEGEND

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	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		CAUTION - PROTECT ITEM

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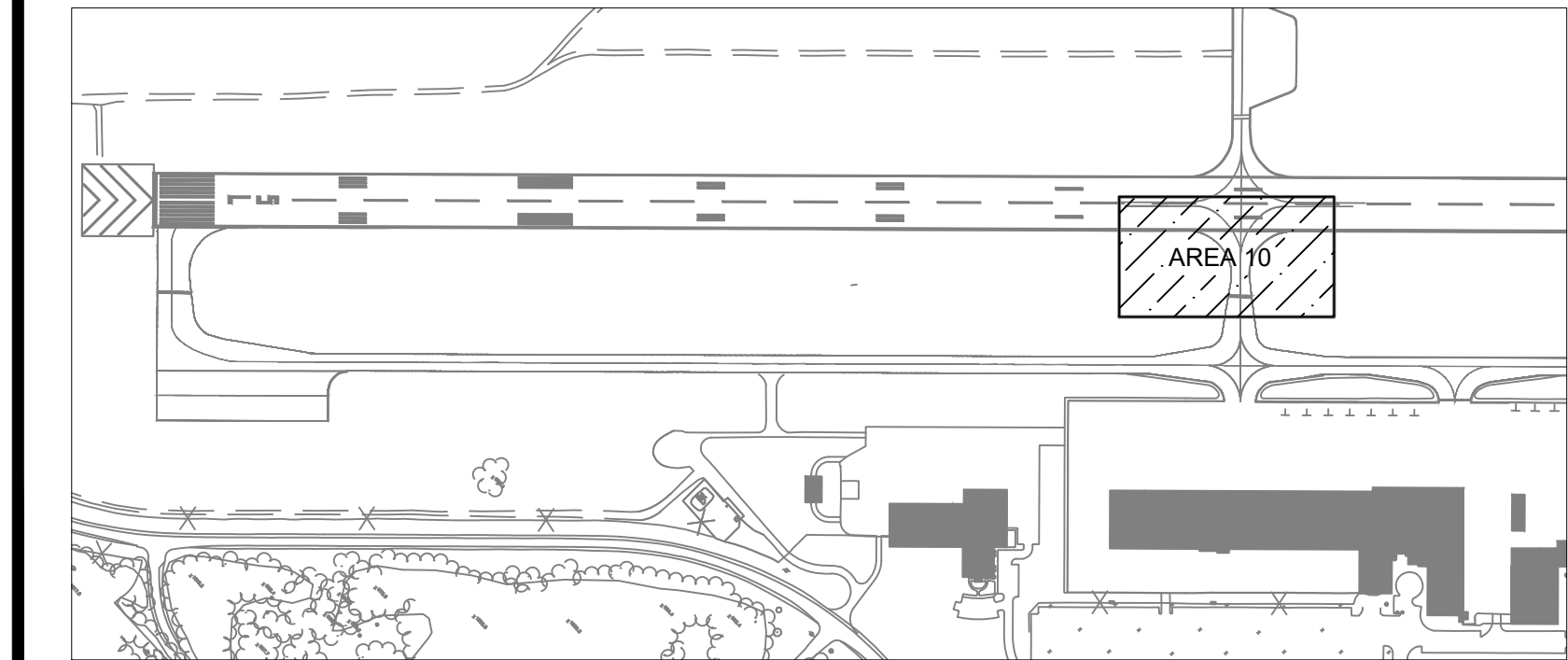
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EXISTING CONDITIONS AND DEMOLITION PLAN - AREA 10

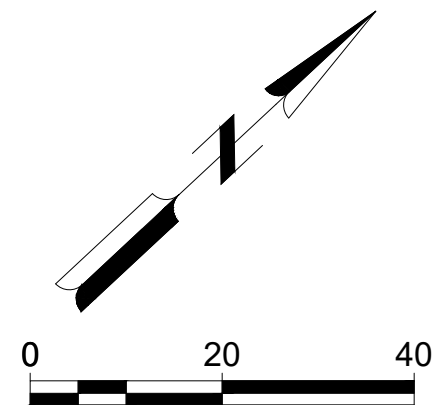
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CD-110

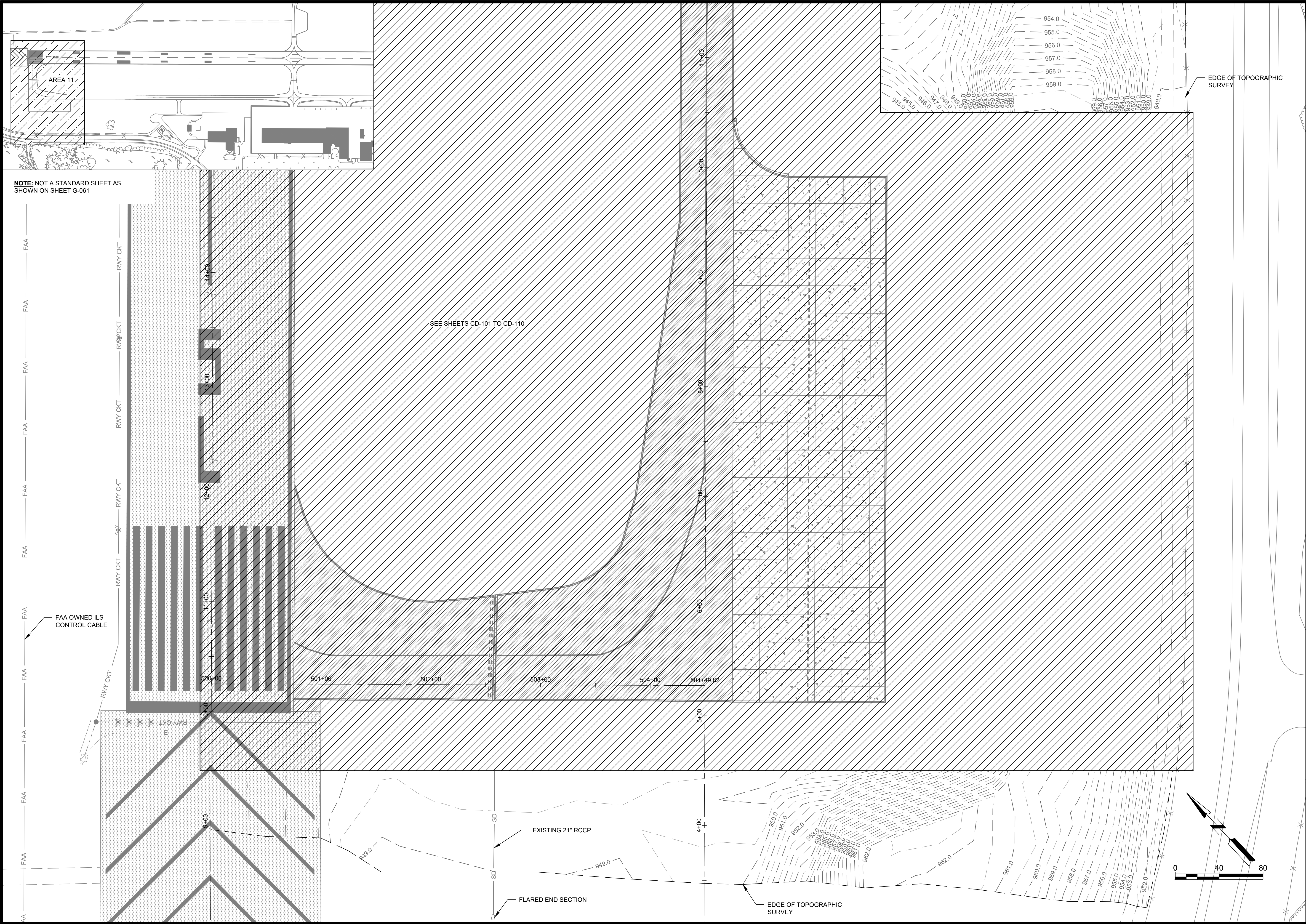


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	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		EXISTING BASE MOUNTED MITL		EXISTING DUCT BANK
	ASPHALT PAVEMENT TO BE REMOVED		EXISTING INSET HIRL		EXISTING GUIDANCE SIGN		REMOVE ITEM (LIGHT, CAN OR SIGN)
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WK KELLOGG AIRPORT RECONSTRUCT TAXIWAY C PAVEMENT & LIGHTING PHASE 2 BATTLE CREEK, MI

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SHEET CONTENTS
EXISTING
CONDITIONS AND
DEMOLITION PLAN -
AREA 11

SHEET NO 25 of 76

CD-151

**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI**

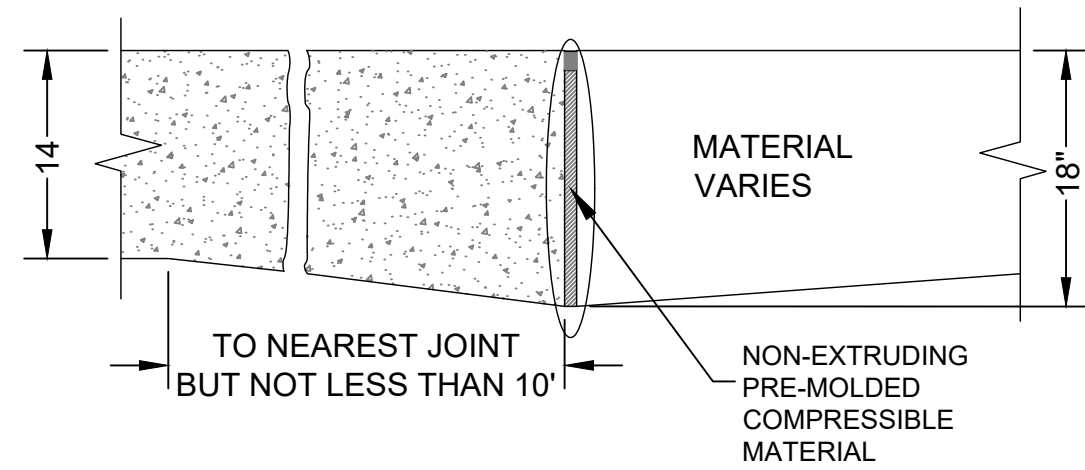
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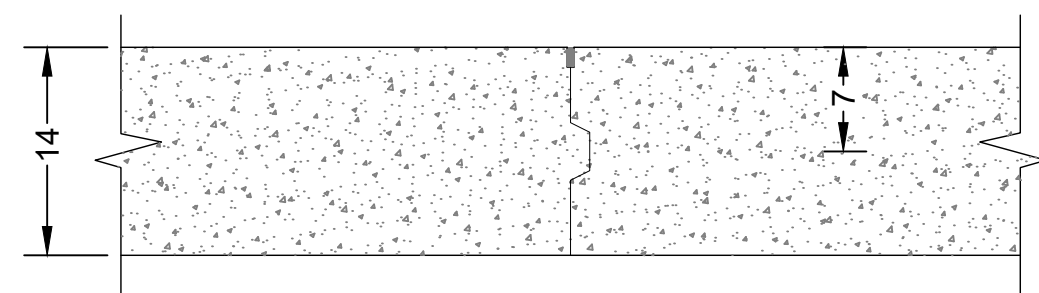
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TYPICAL EXISTING
PAVEMENT SECTION
DETAIL SHEET 1 OF 1

SHEET NO 26 of 76

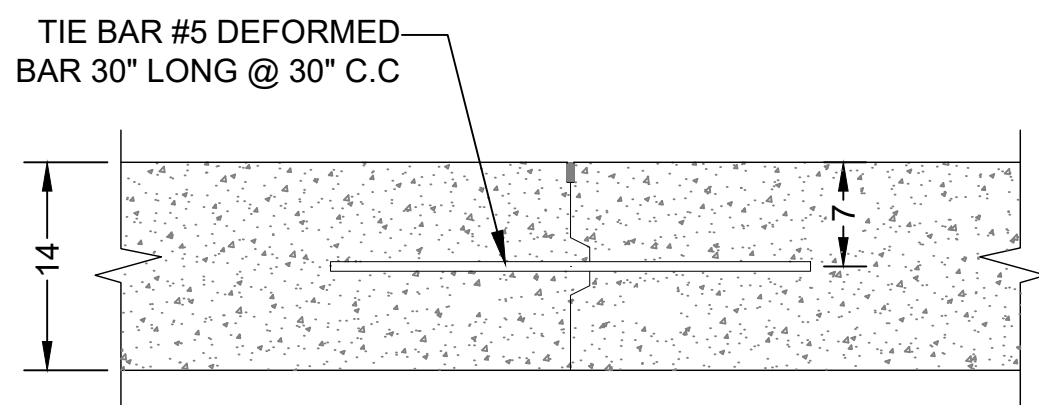
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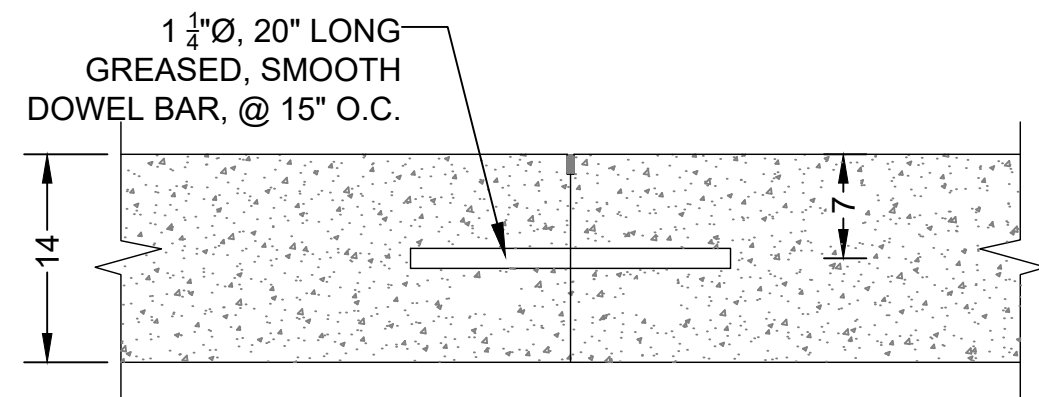
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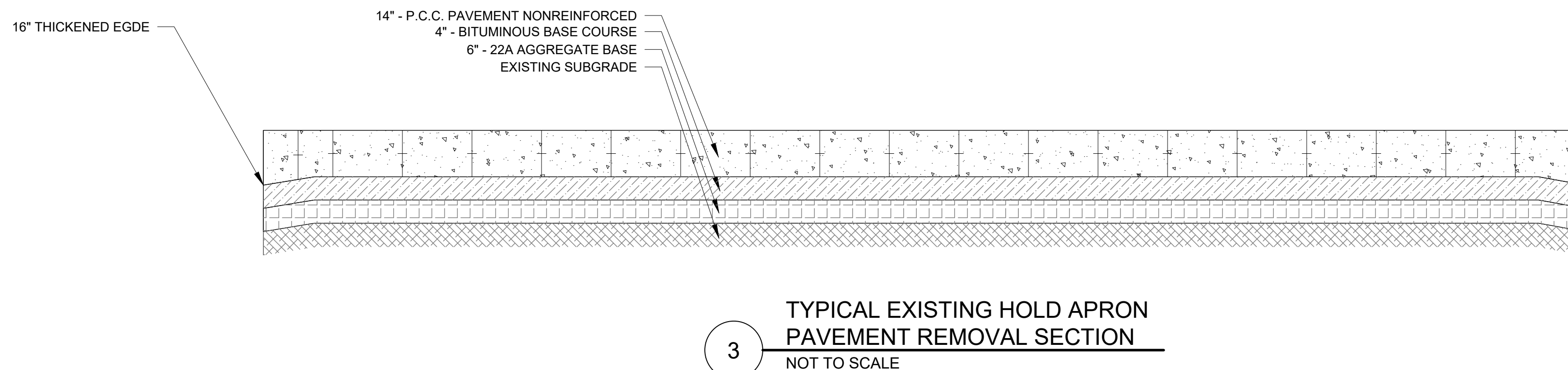
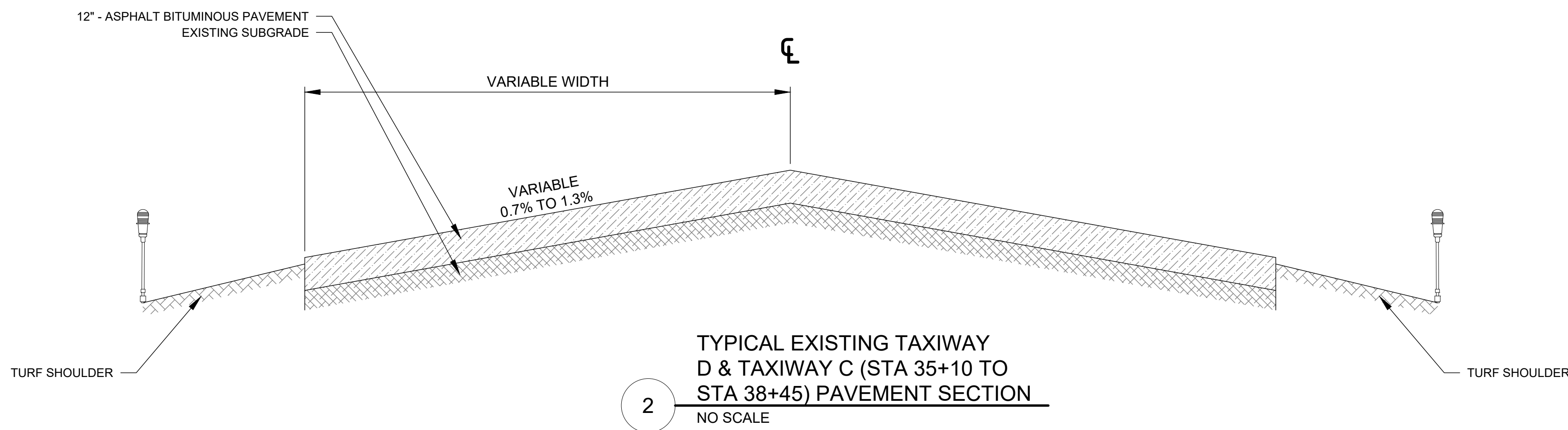
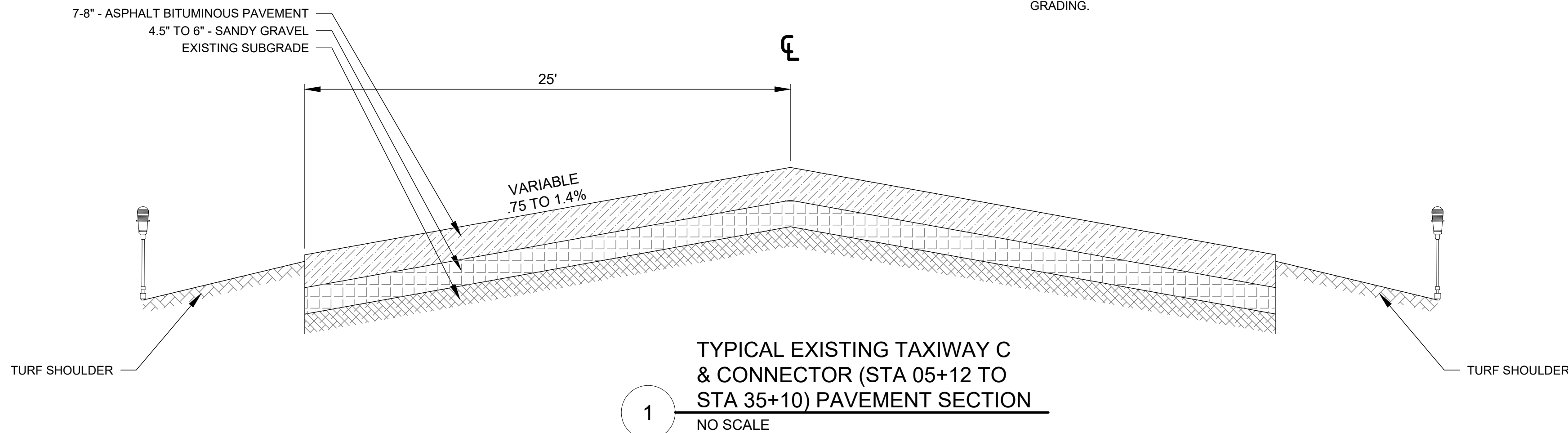
5 TYPE C - KEYED
NOT TO SCALE



6 TYPE E - HINGED
NOT TO SCALE



7 TYPE F - DOWELED
NOT TO SCALE



NOTES:

1. SECTIONS SHOWN ARE REPRESENTATIVE OF "TYPICAL" WIDTHS AND GRADES. SLOPES AND WIDTHS MAY NOT MATCH SECTIONS AS SHOWN FOR EVERY STATION. SEE CD AND CG SERIES SHEETS FOR EXACT GEOMETRY AND GRADING.

WK KELLOGG AIRPORT RECONSTRUCT TAXIWAY C PAVEMENT & LIGHTING PHASE 2 BATTLE CREEK, MI

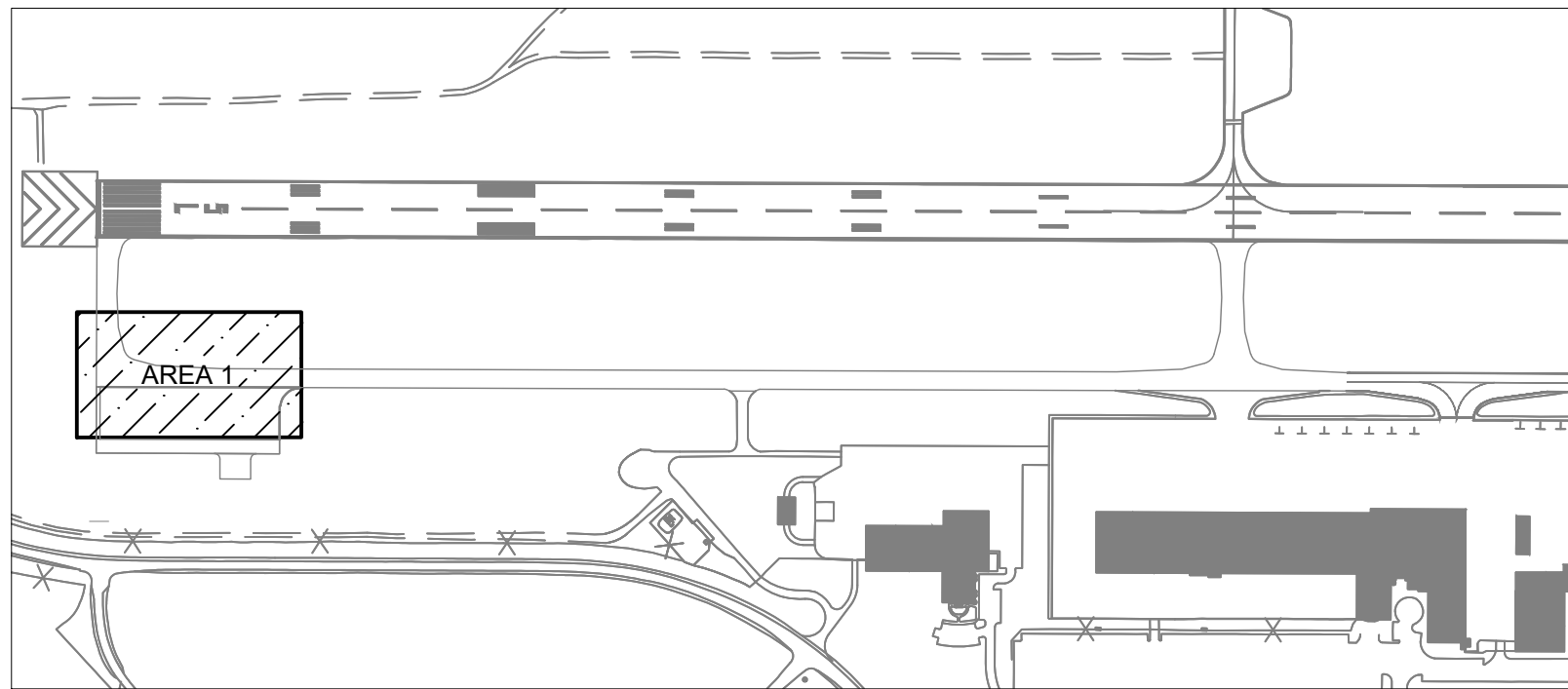
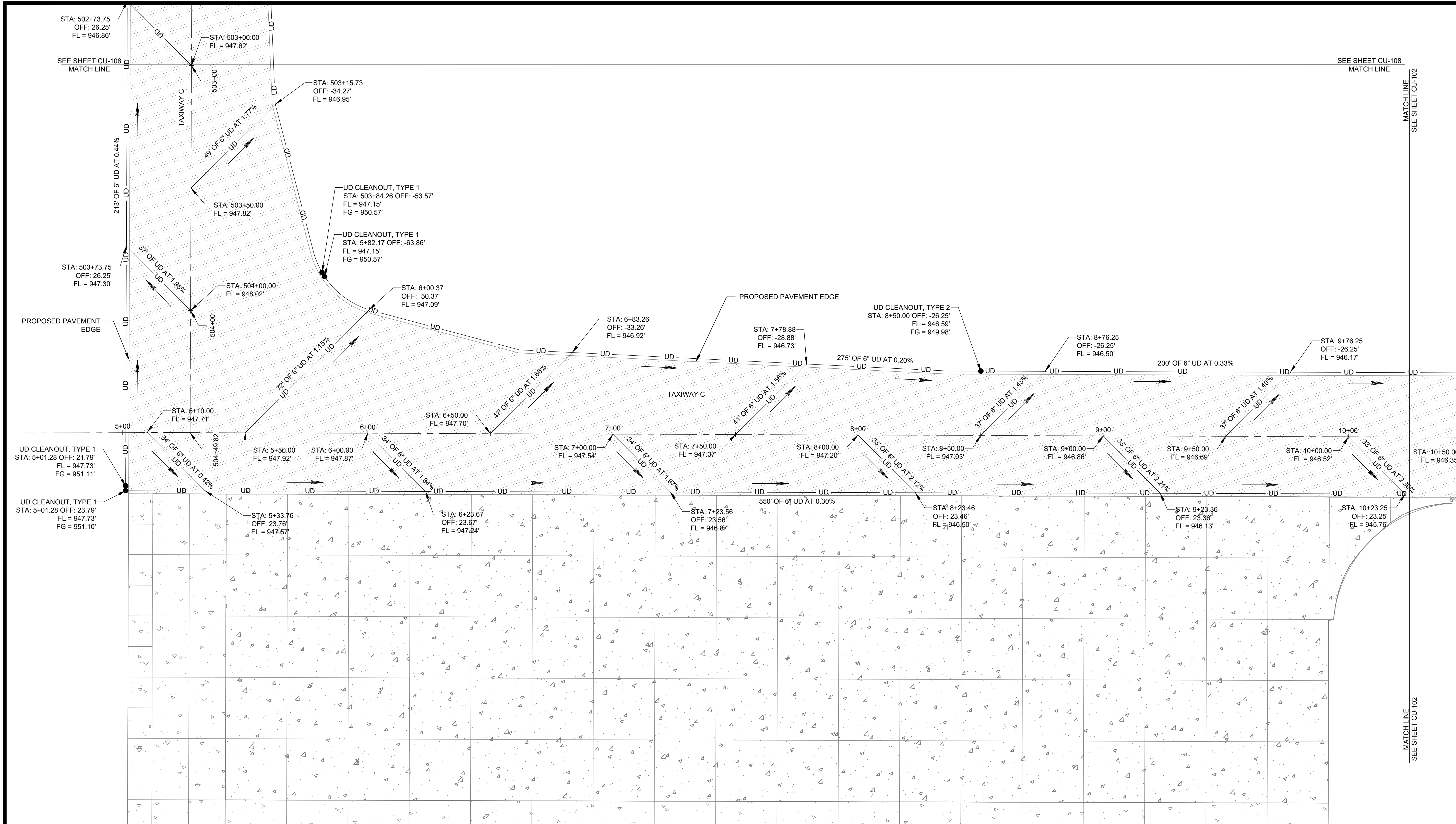
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 1

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CU-101

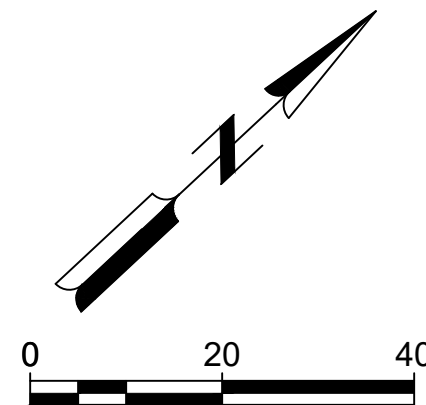


LEGEND

	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT		EXISTING RCCP
	PCC PAVEMENT		EXISTING BASE MOUNTED HRL		FLOW DIRECTION ARROW		EXISTING DUCT BANK
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		PROPOSED UNDER DRAIN

NOTES:

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

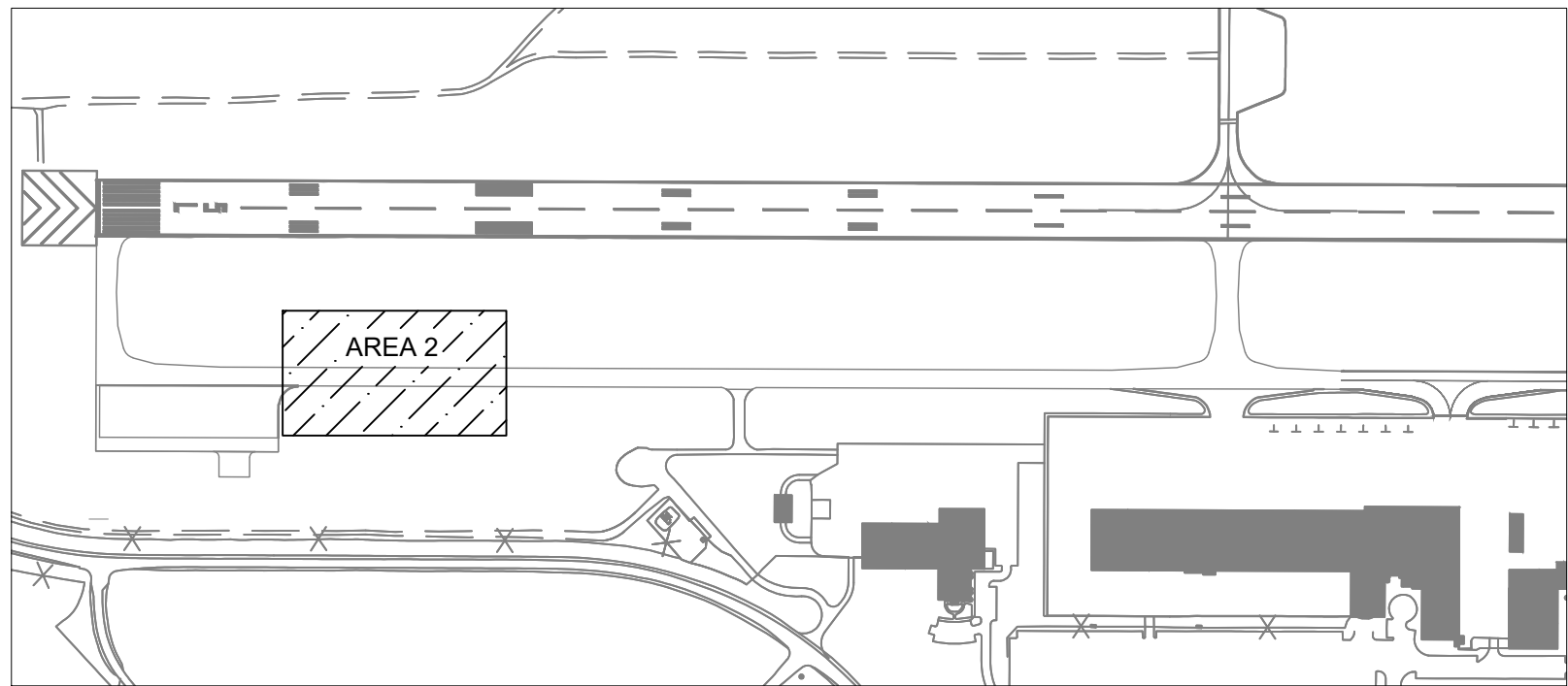
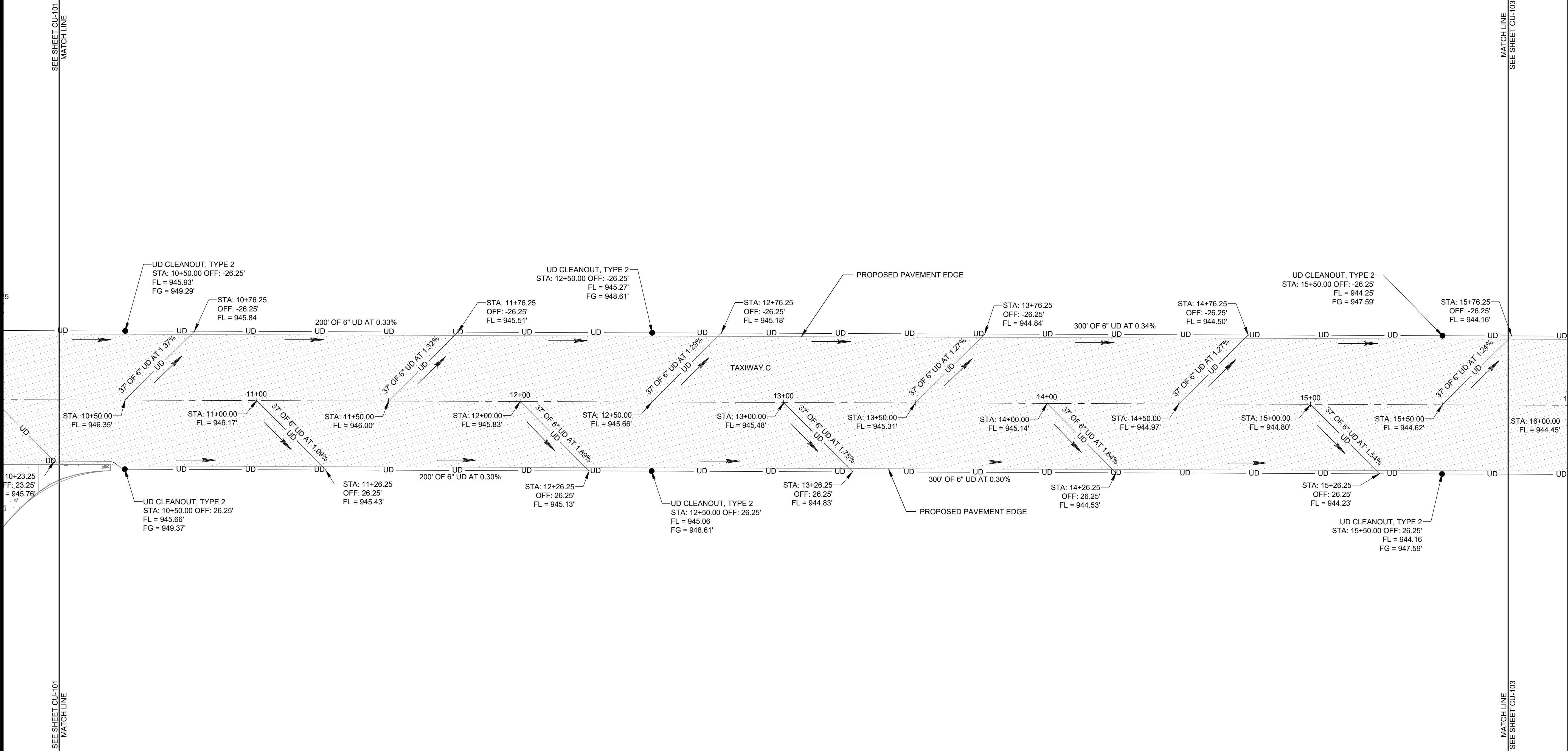
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 2

SHEET NO. 28 of 76

CU-102

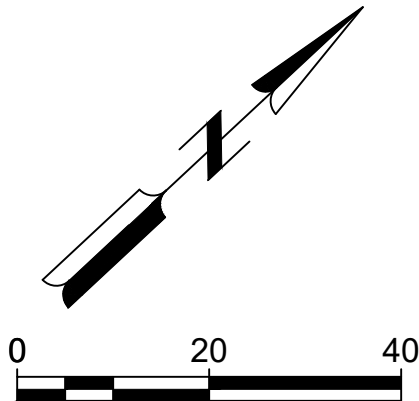


LEGEND

	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT		EXISTING RCCP
	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW		EXISTING DUCT BANK
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		PROPOSED UNDER DRAIN

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

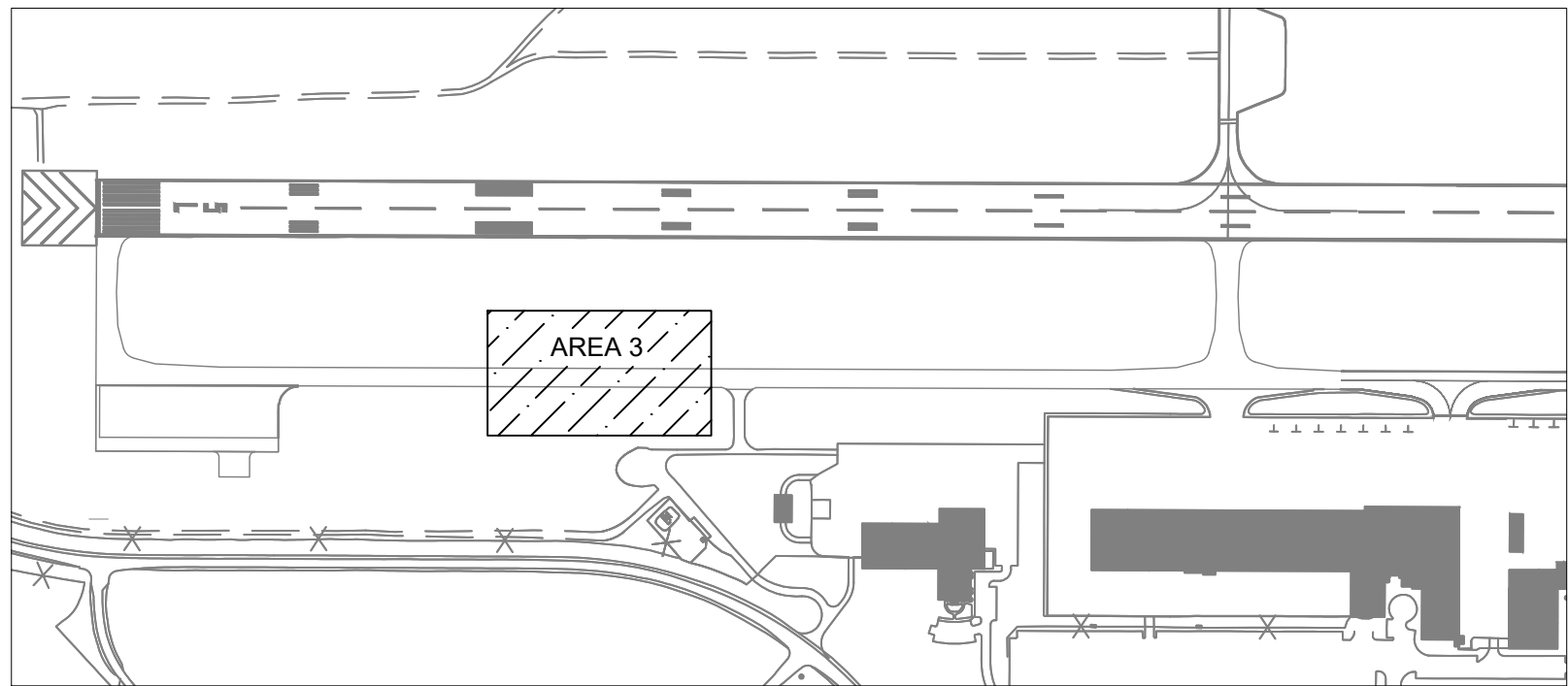
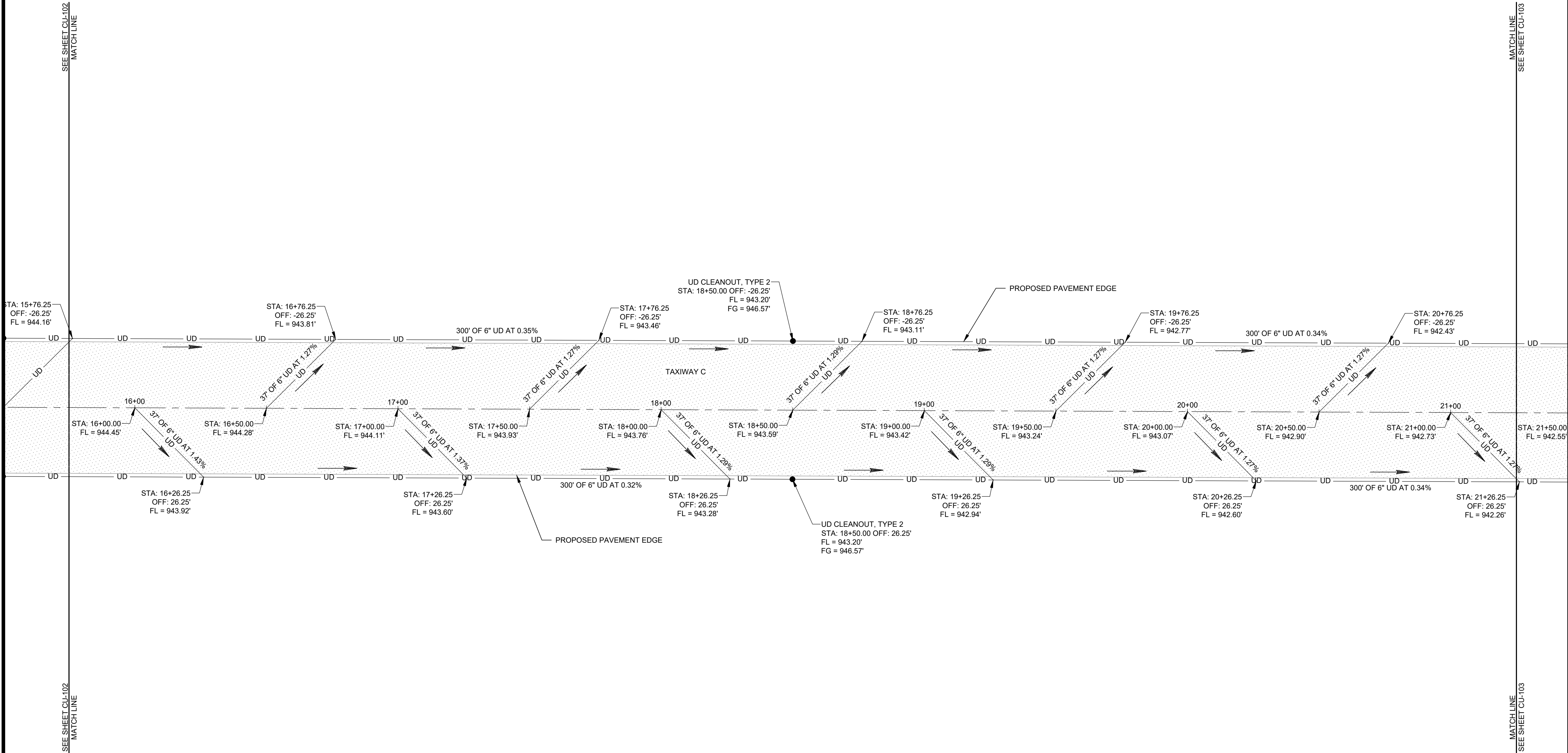
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








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SHEET CONTENTS
DRAINAGE PLAN -
AREA 3

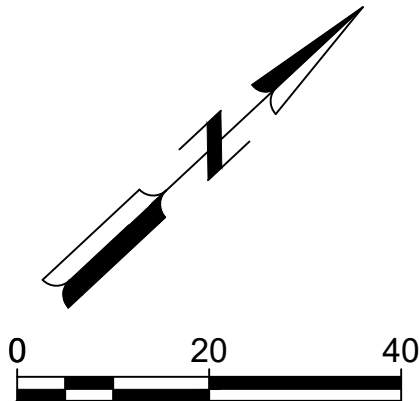
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CU-103

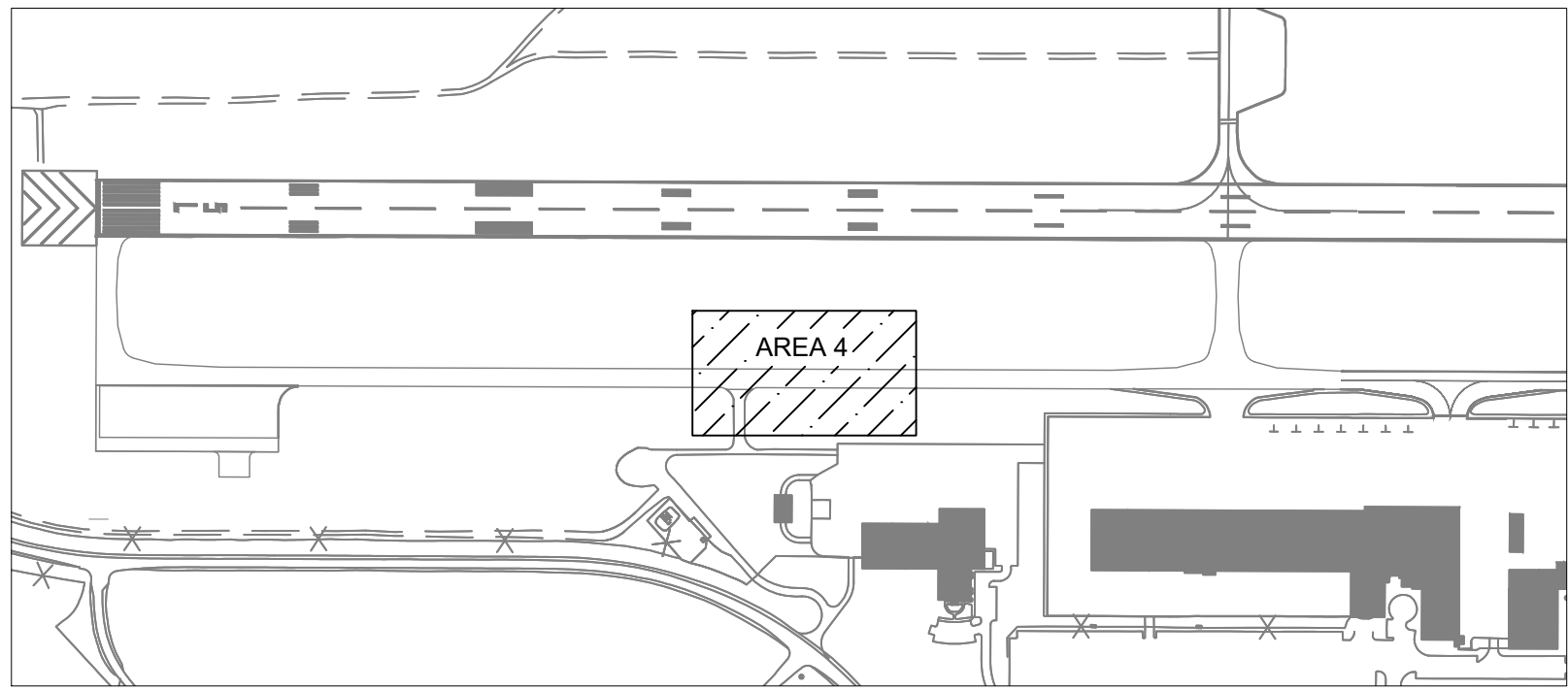
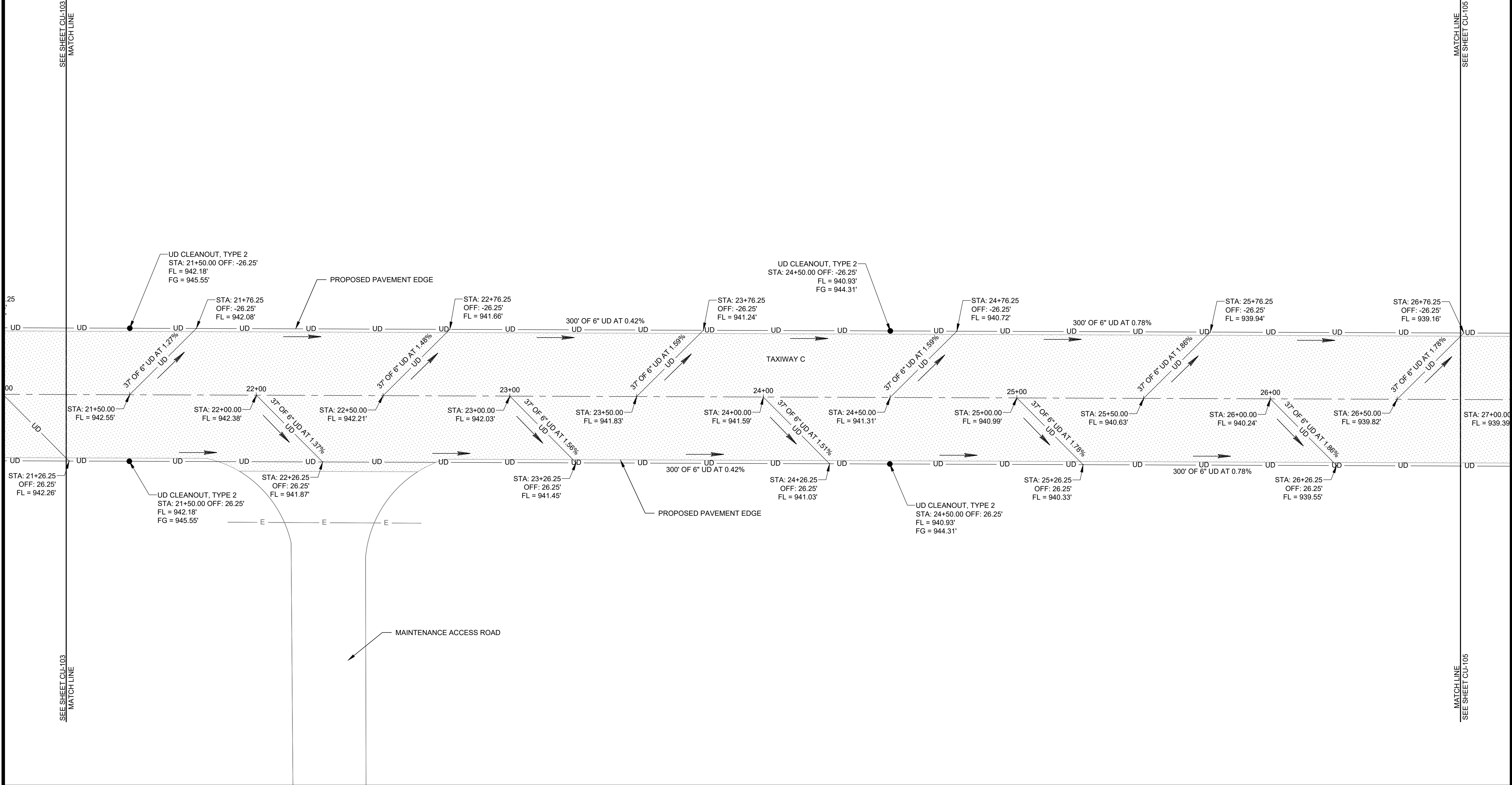











LEGEND					
	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT
	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			UD		PROPOSED UNDER DRAIN

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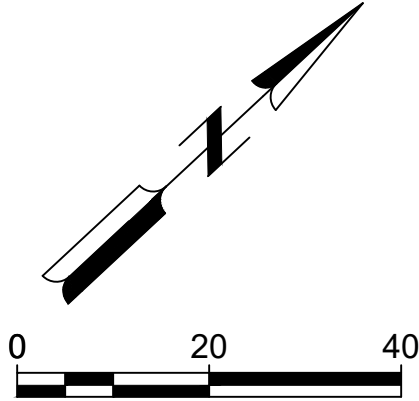


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LEGEND					
	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT
	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION
			EXISTING RCCP		EXISTING DUCT BANK
			PROPOSED UNDER DRAIN		

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

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SHEET CONTENTS
DRAINAGE PLAN -
AREA 4

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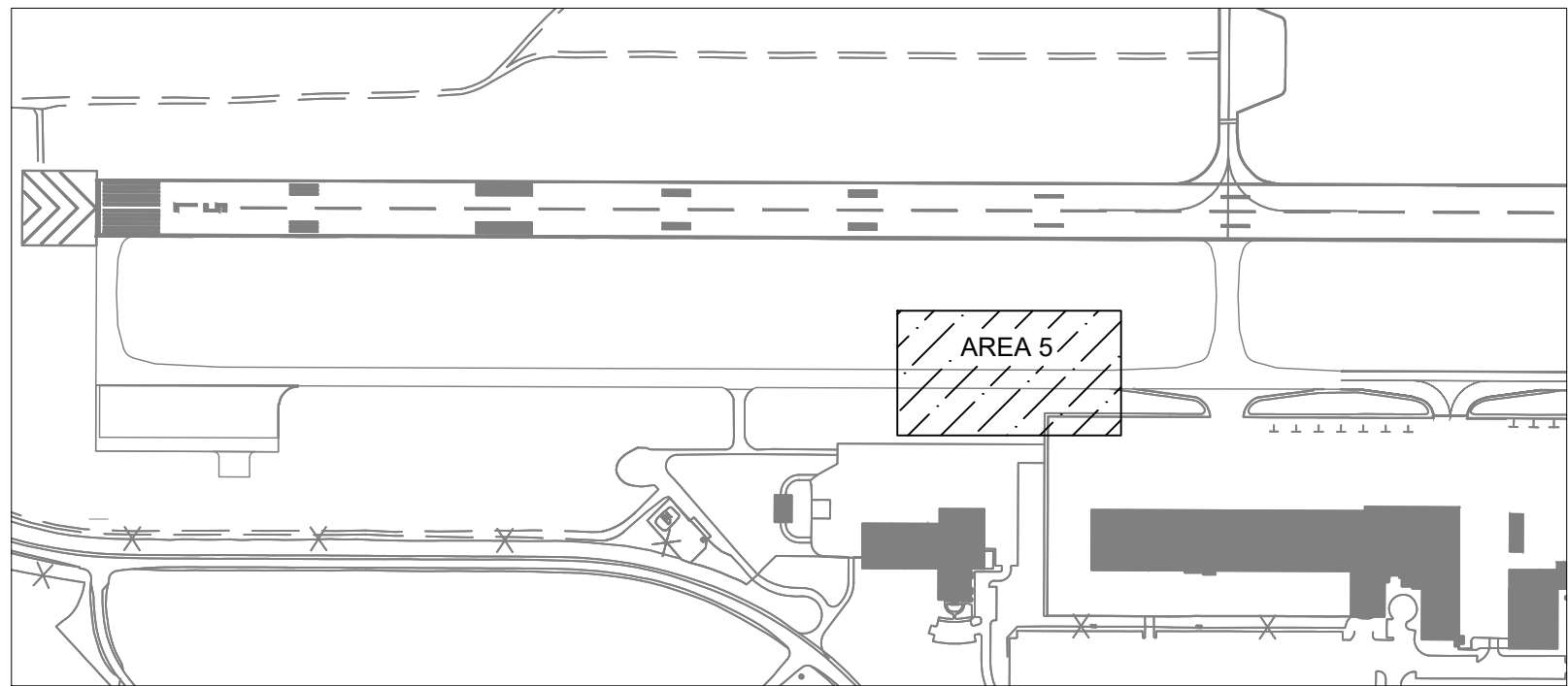
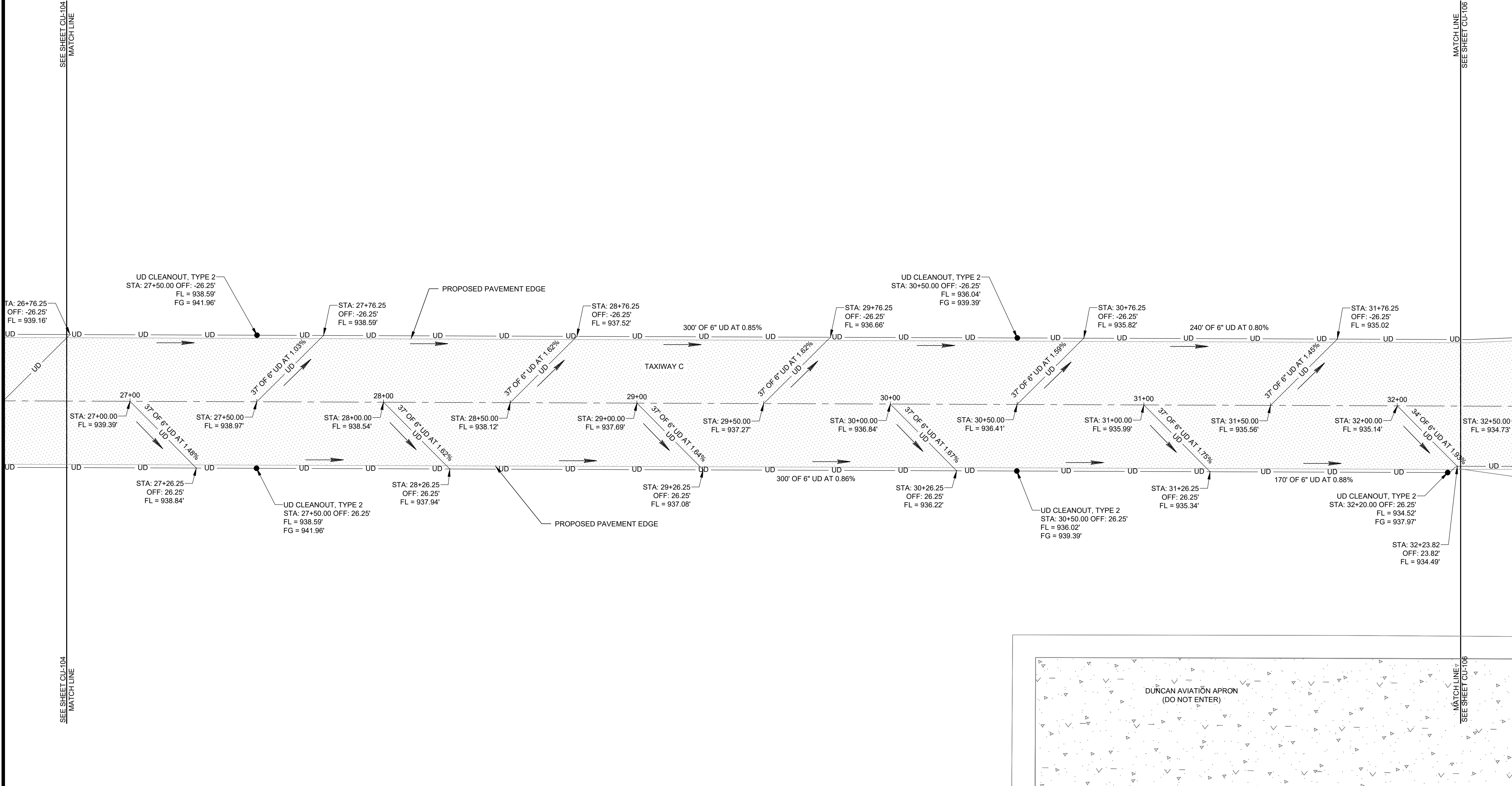
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 5

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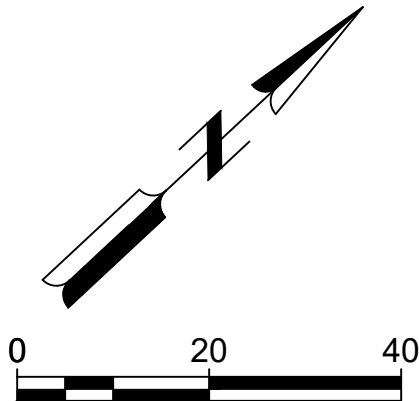


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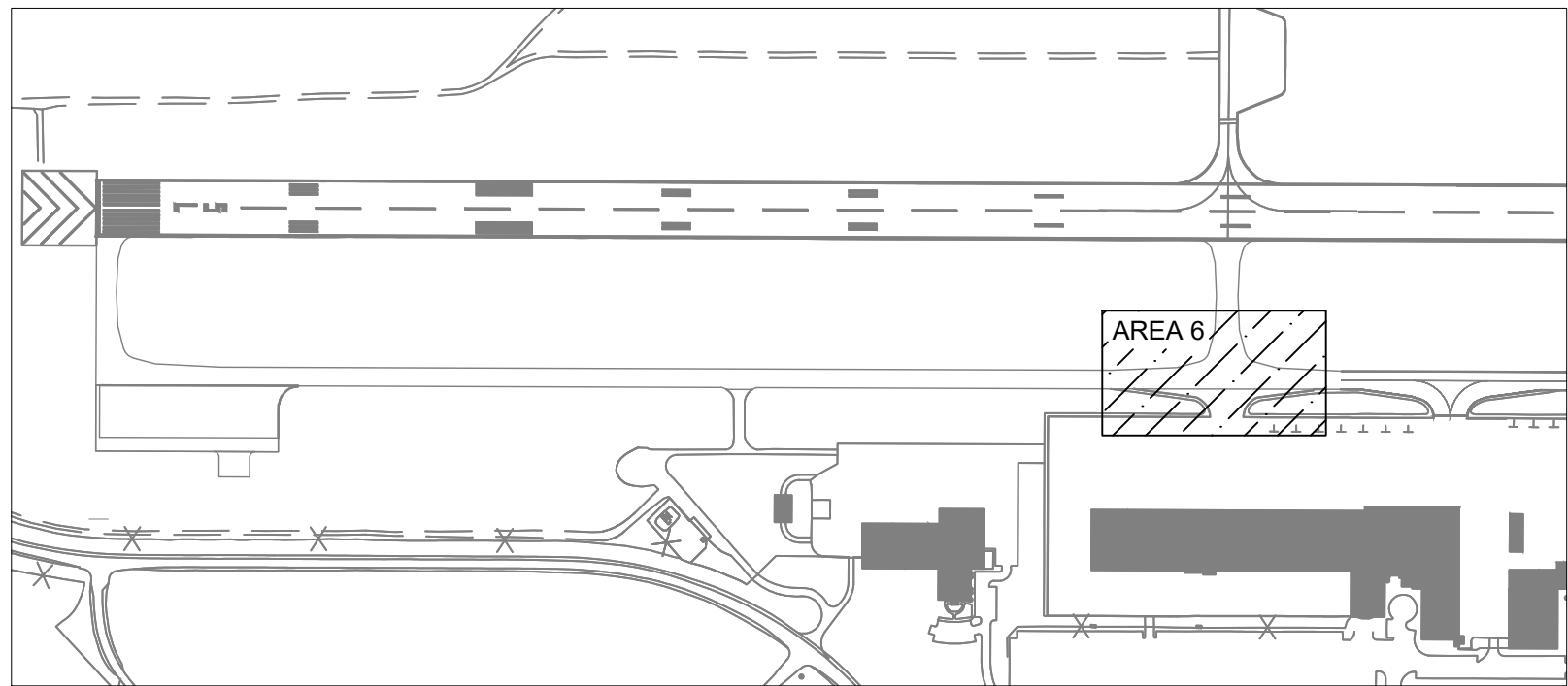
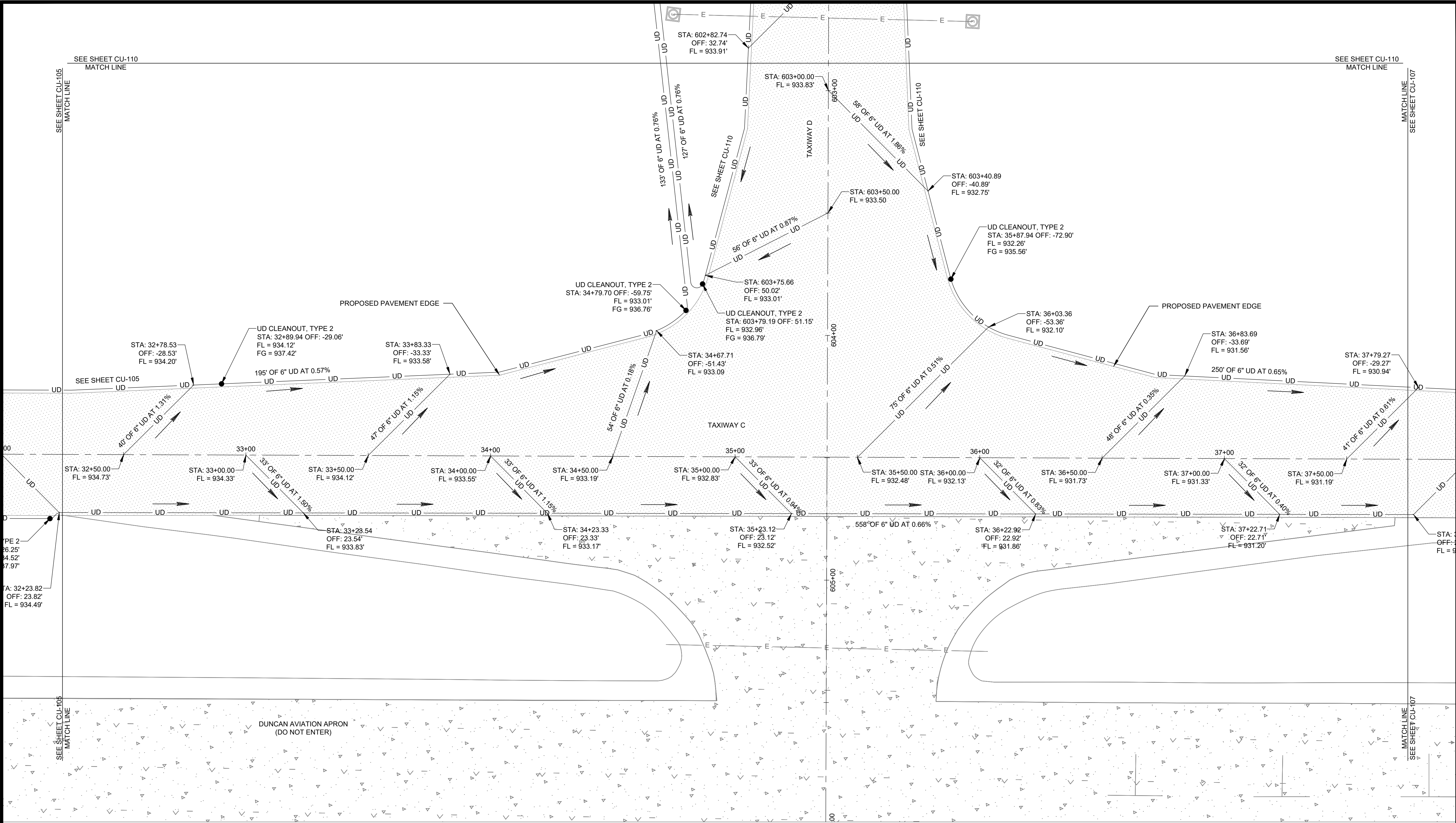
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	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW		EXISTING DUCT BANK
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		PROPOSED UNDER DRAIN

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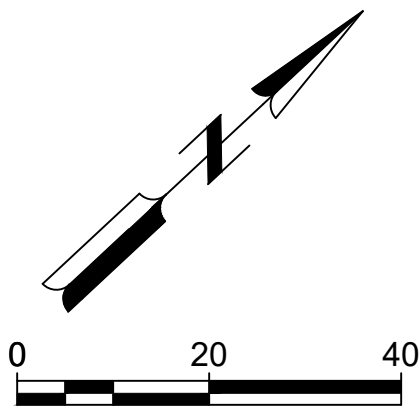


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3/21/2018 8:47:10 AM



LEGEND					
	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT
	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION
			EXISTING RCCP		EXISTING DUCT BANK
			PROPOSED UNDER DRAIN		

- NOTES:**
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**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI**

ISSUED
1 03.29.18 ISSUED FOR
BID

AIP NO: B-26-0008-4518
M&H NO: 0228800-161097.01
DATE: 03.29.2018
DESIGNED BY: ALP
DRAWN BY: ALP
CHECKED BY: JET
DO NOT SCALE DRAWINGS

SHEET CONTENTS
DRAINAGE PLAN -
AREA 6

SHEET NO 32 of 76

CU-106

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PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

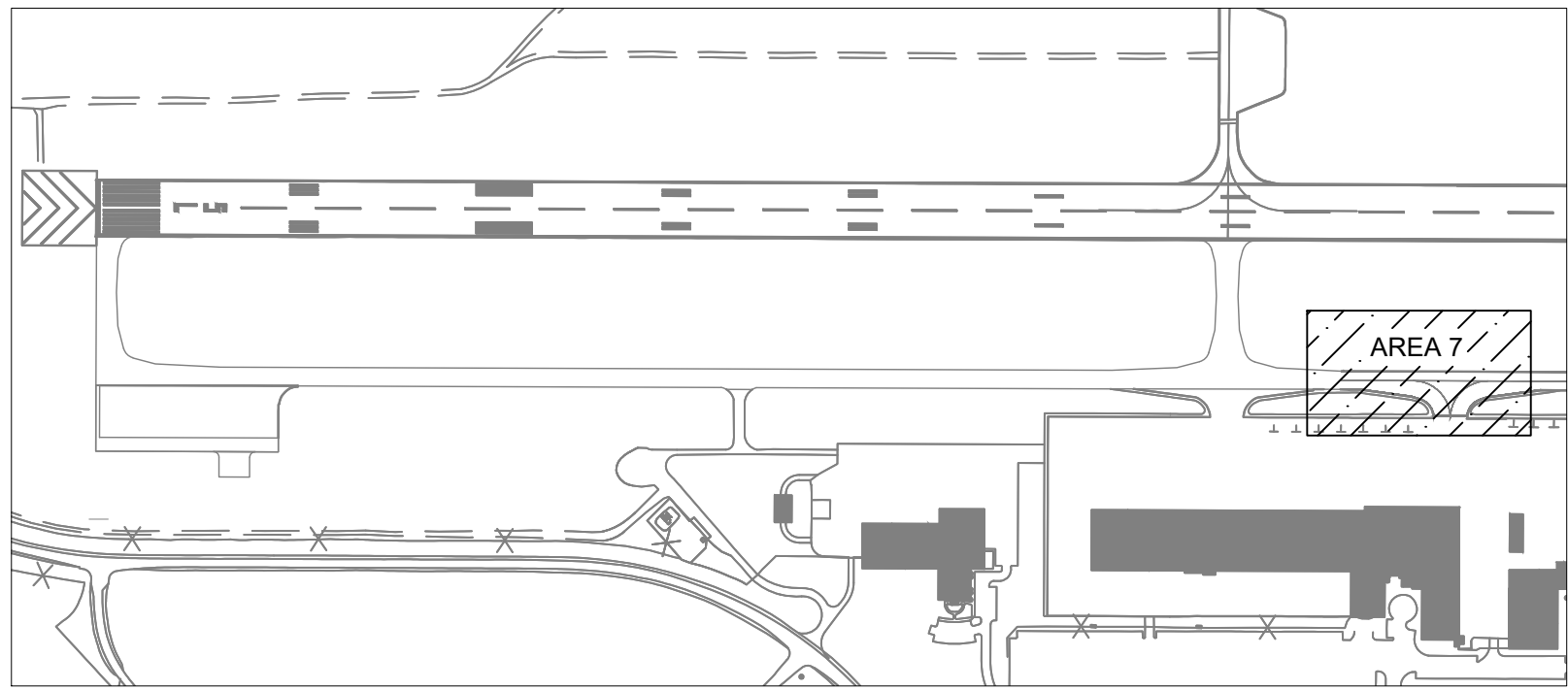
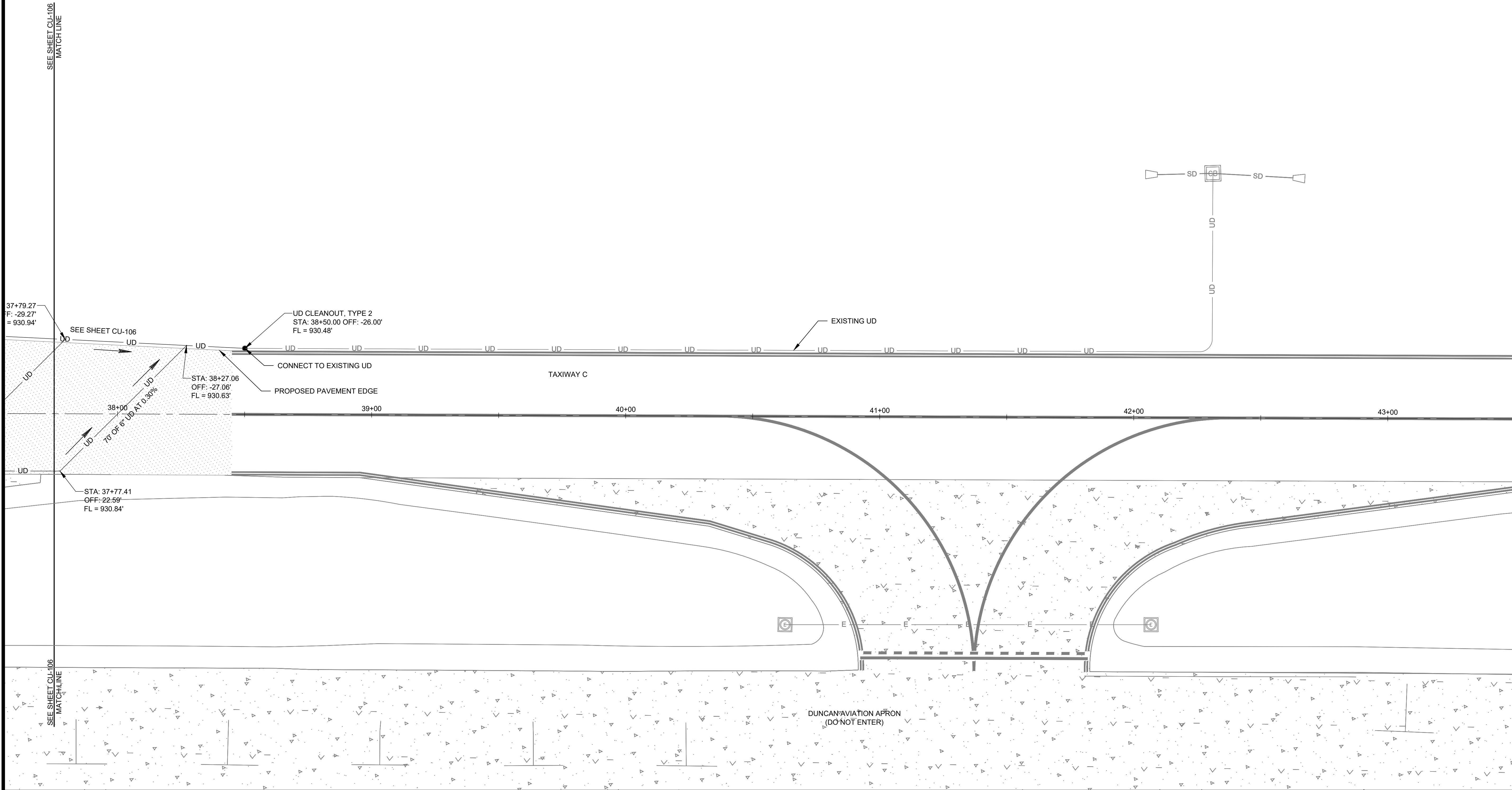
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 7

SHEET NO 33 of 76

CU-107



LEGEND					
	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT
	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			UD		PROPOSED UNDER DRAIN

- NOTES:**
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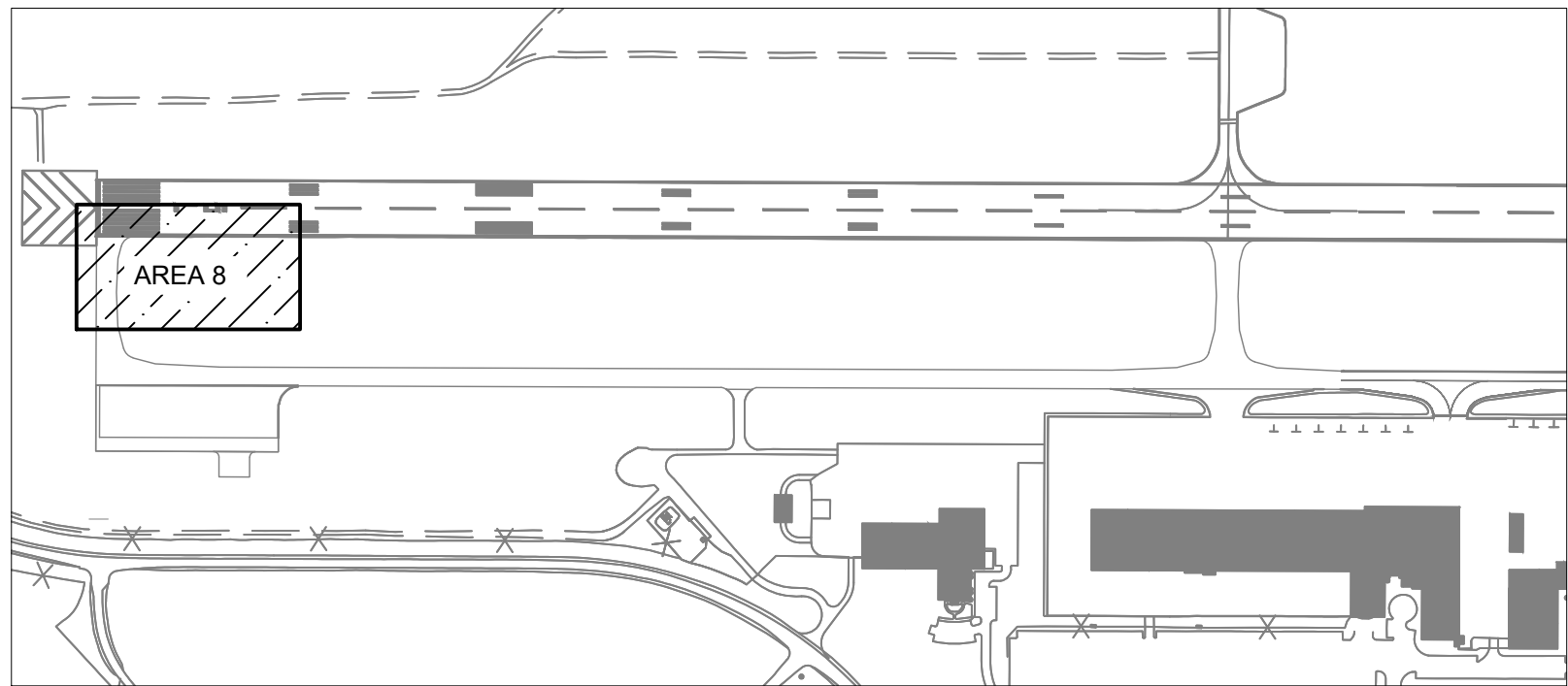
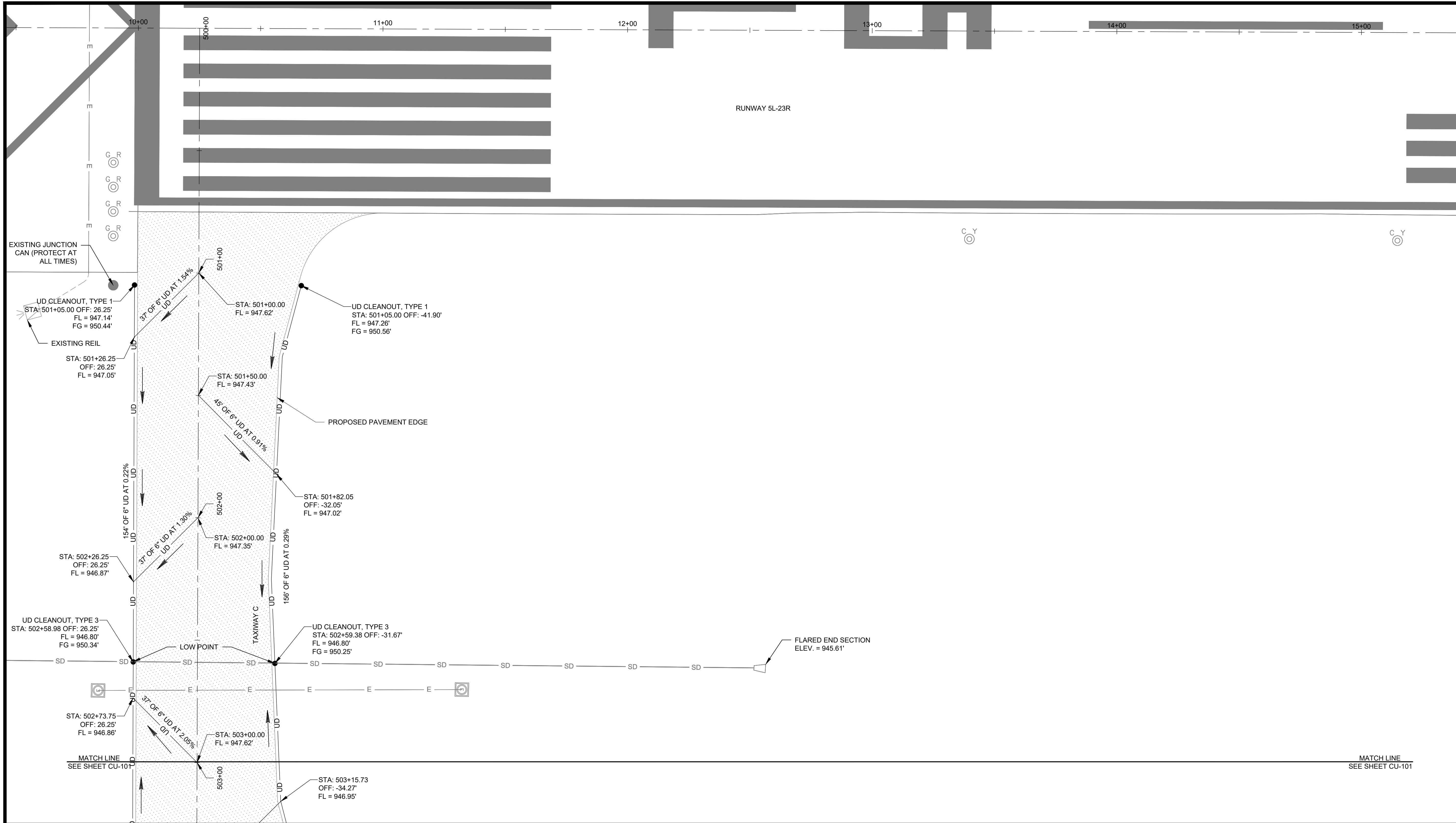
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 8

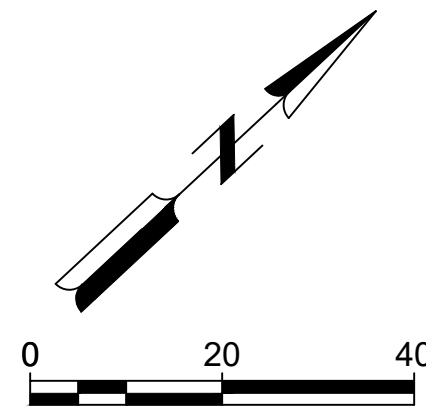
SHEET NO. 34 of 76

CU-108



LEGEND					
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	PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		FLOW DIRECTION ARROW
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION
	EXISTING RCCP		EXISTING DUCT BANK		PROPOSED UNDER DRAIN

- NOTES:**
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BATTLE CREEK, MI

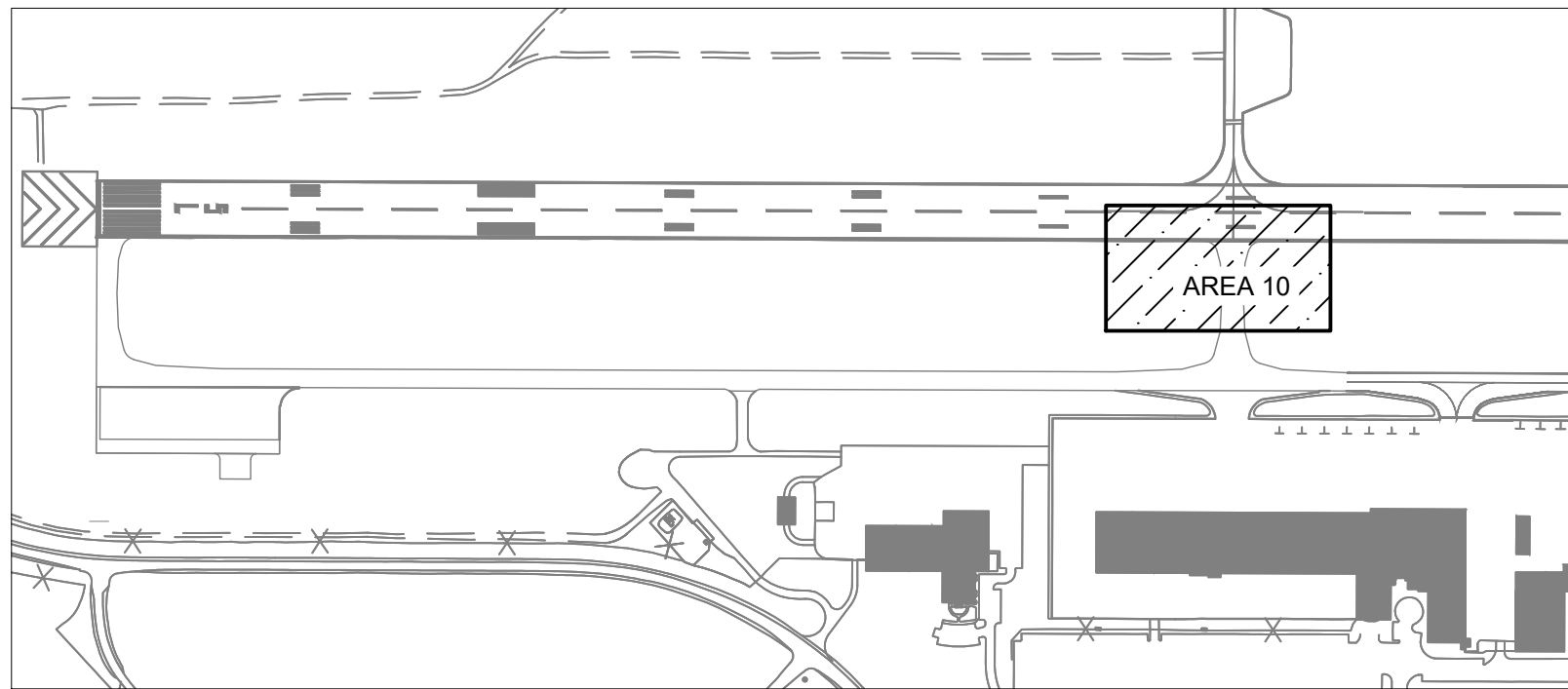
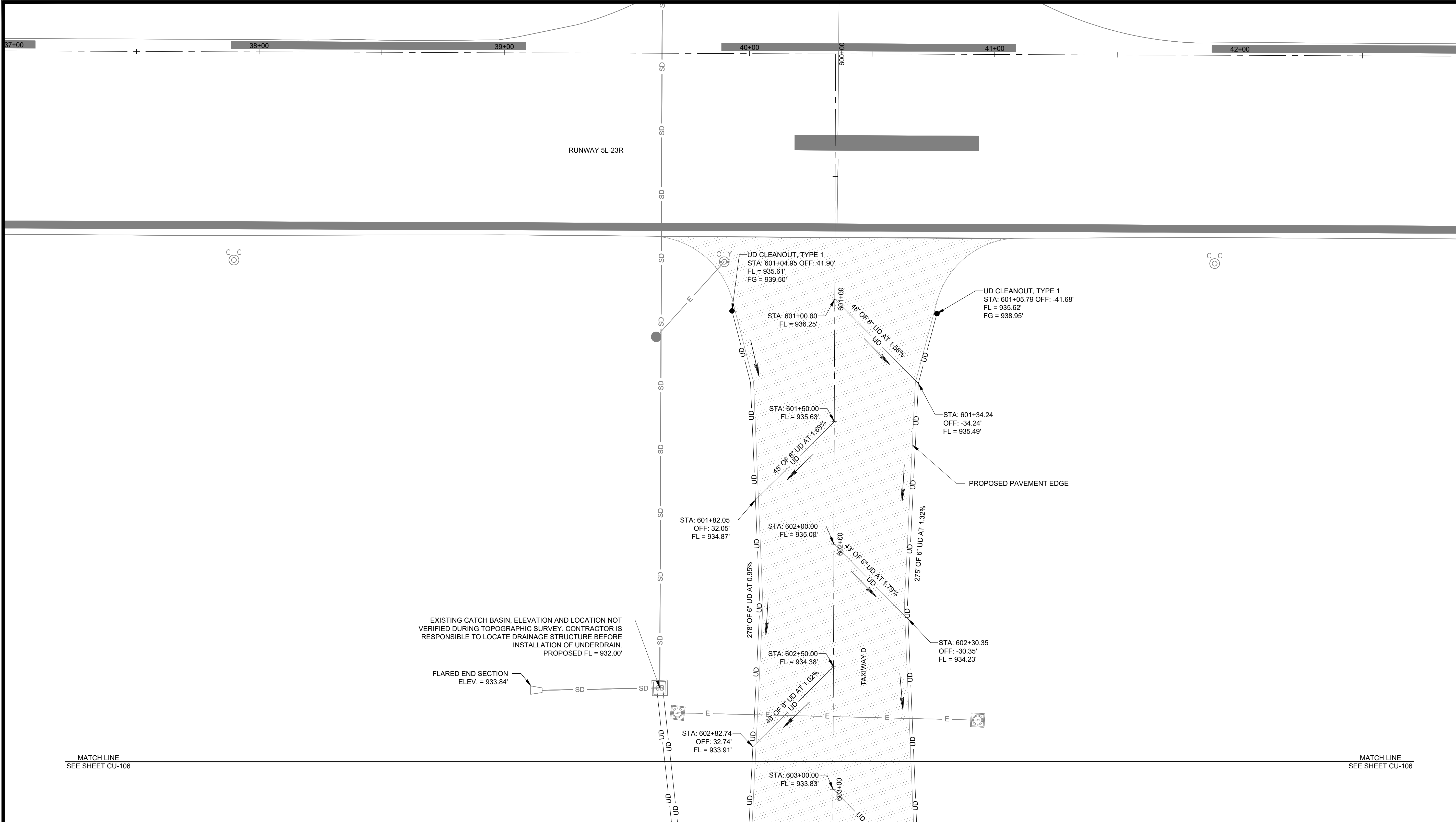
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SHEET CONTENTS
DRAINAGE PLAN -
AREA 10

SHEET NO. 35 of 76

CU-110

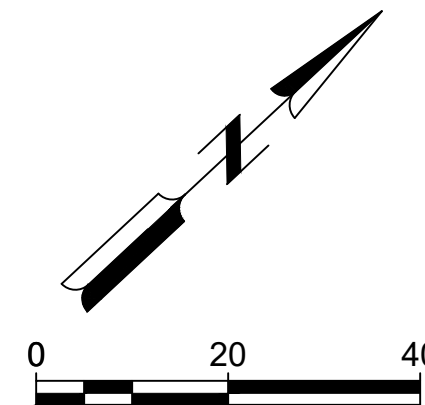


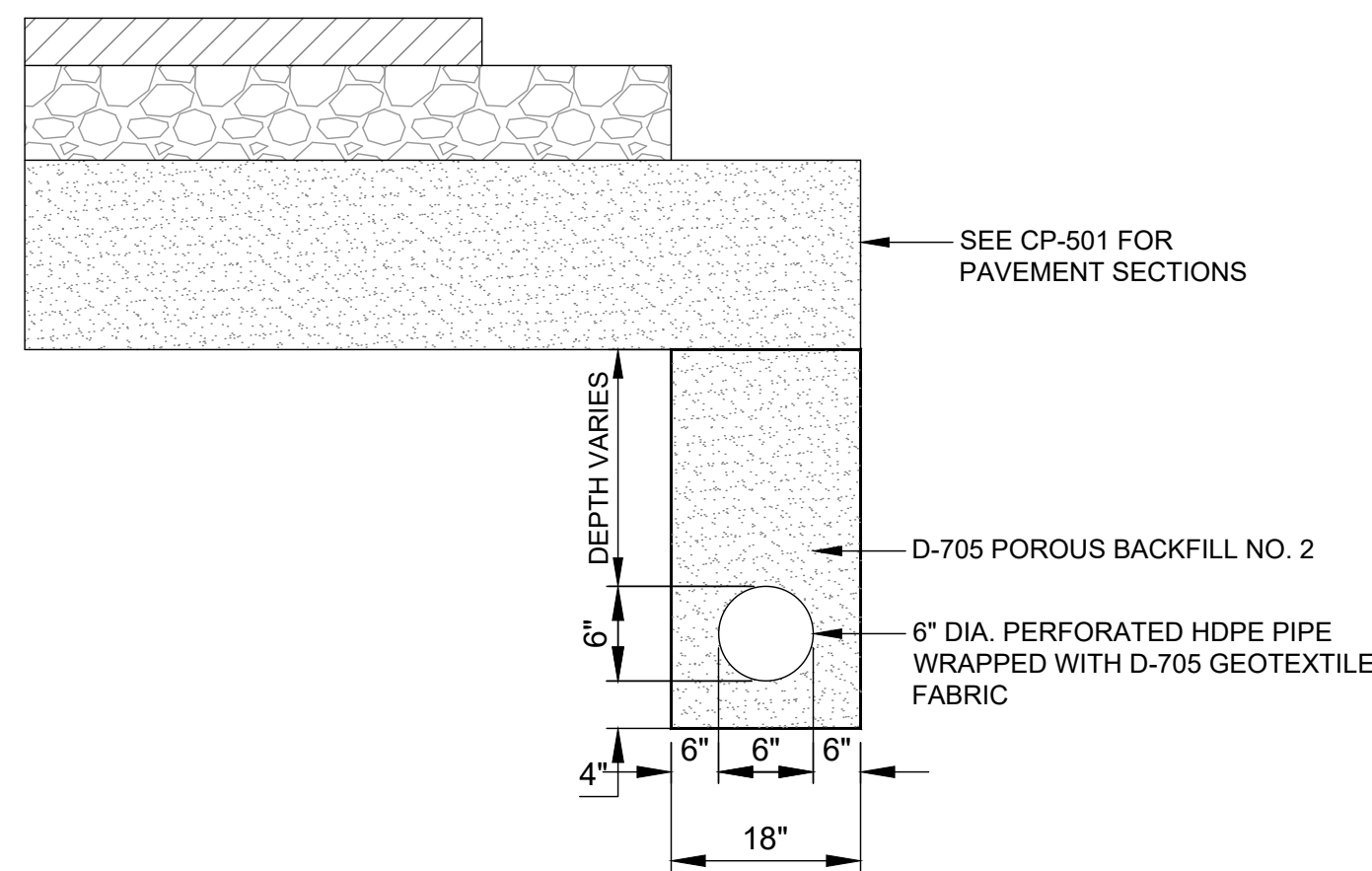
LEGEND

	PROPOSED ASPHALT PAVEMENT		EXISTING JUNCTION STRUCTURE		PROPOSED UNDER DRAIN CLEAN OUT		EXISTING RCCP
	PCC PAVEMENT		EXISTING BASE MOUNTED HRL		FLOW DIRECTION ARROW		EXISTING DUCT BANK
	EXISTING ELECTRIC MANHOLE		EXISTING DRAINAGE STRUCTURE		EXISTING FLARED END SECTION		PROPOSED UNDER DRAIN

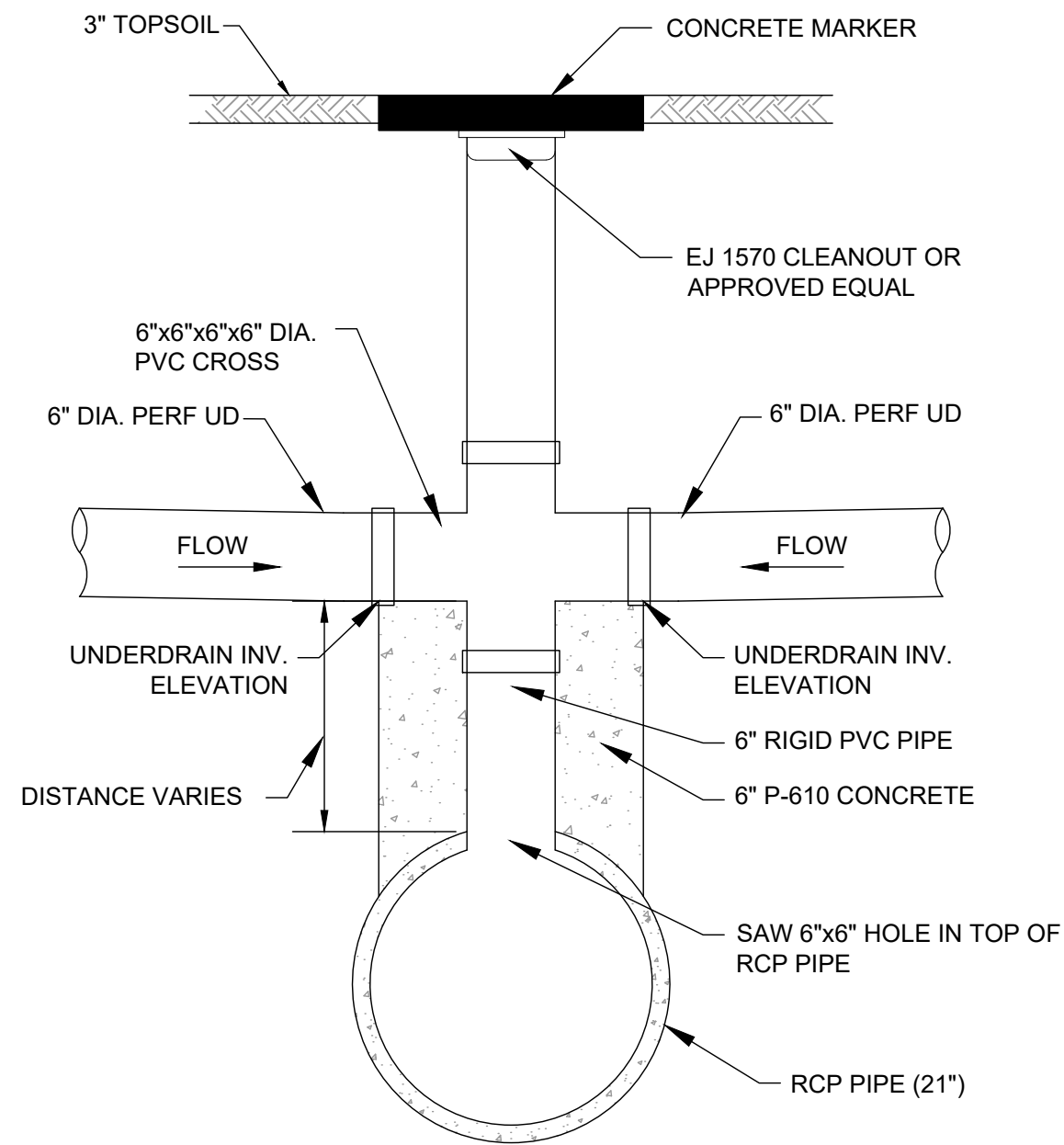
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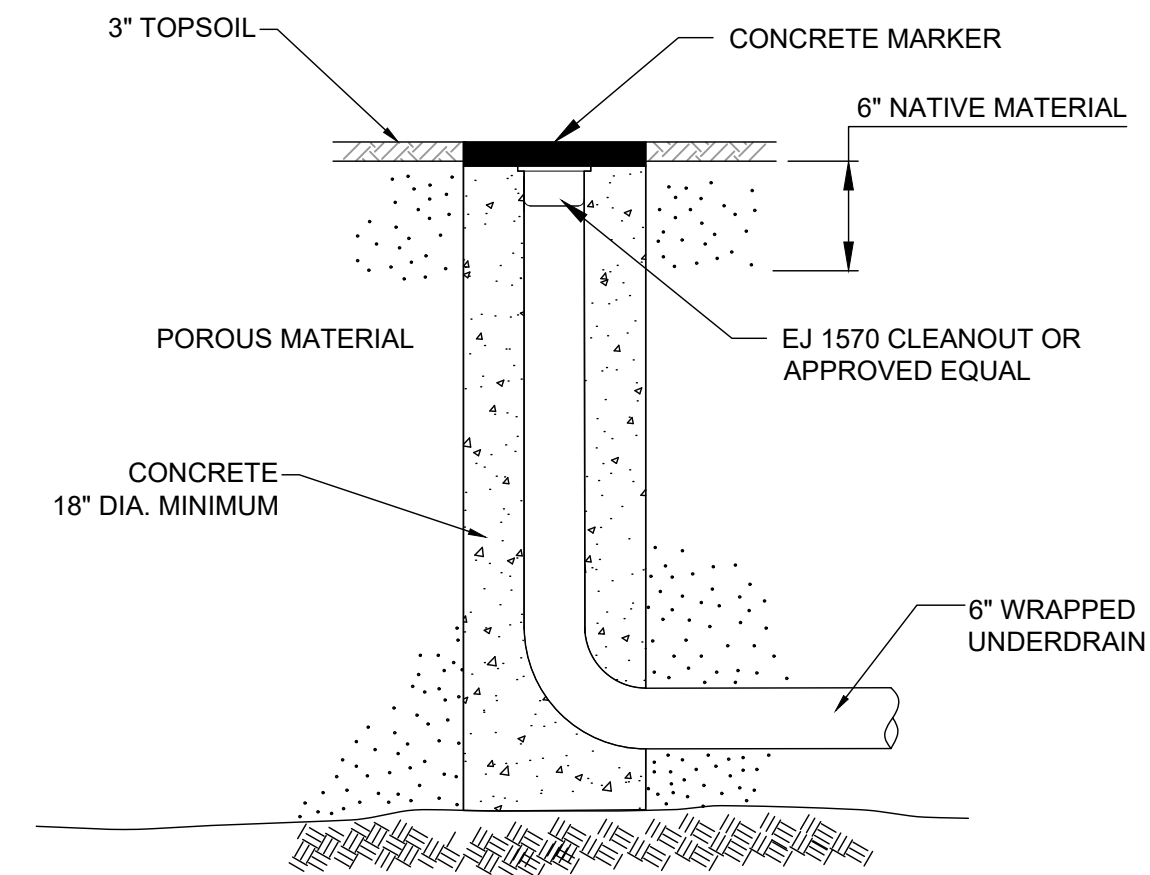




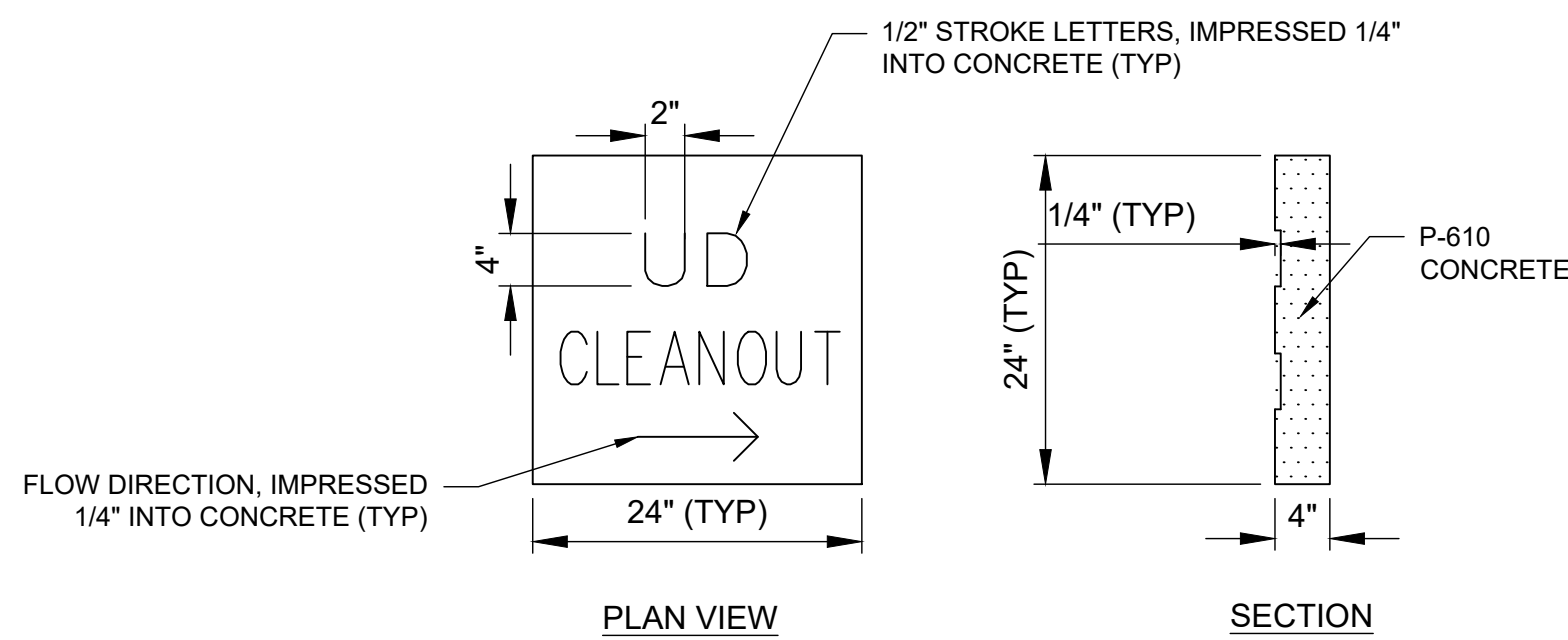
5 TYPICAL UNDERDRAIN TRENCH
UNDER PAVEMENT DETAIL
NOT TO SCALE



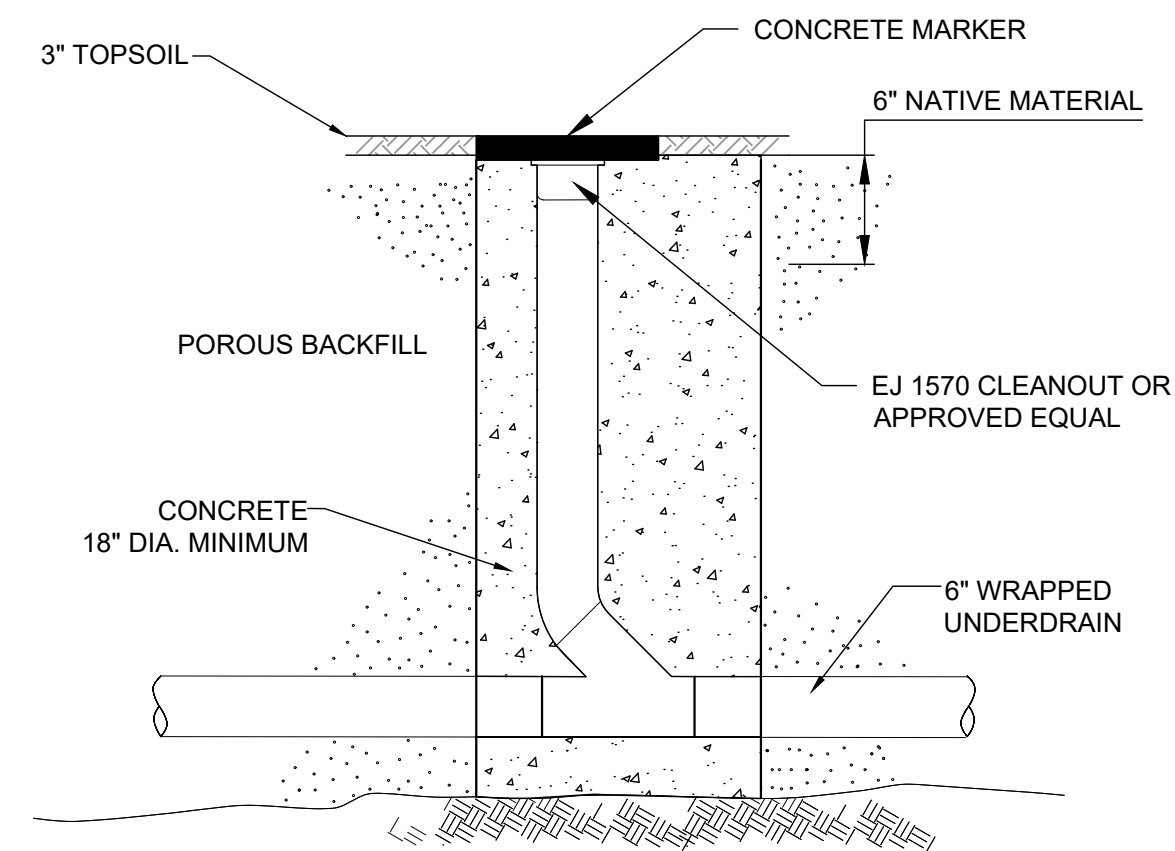
3 TYPE 3 UNDERDRAIN CLEANOUT
AND CONNECTION TO RCP DETAIL
NOT TO SCALE



1 UNDERDRAIN
CLEANOUT TYPE 1 DETAIL
NOT TO SCALE



4 UNDERDRAIN
CLEANOUT CONCRETE MARKER
NOT TO SCALE
(INCIDENTAL TO UNDERDRAIN CLEANOUT)



2 UNDERDRAIN
CLEANOUT TYPE 2 DETAIL
NOT TO SCALE

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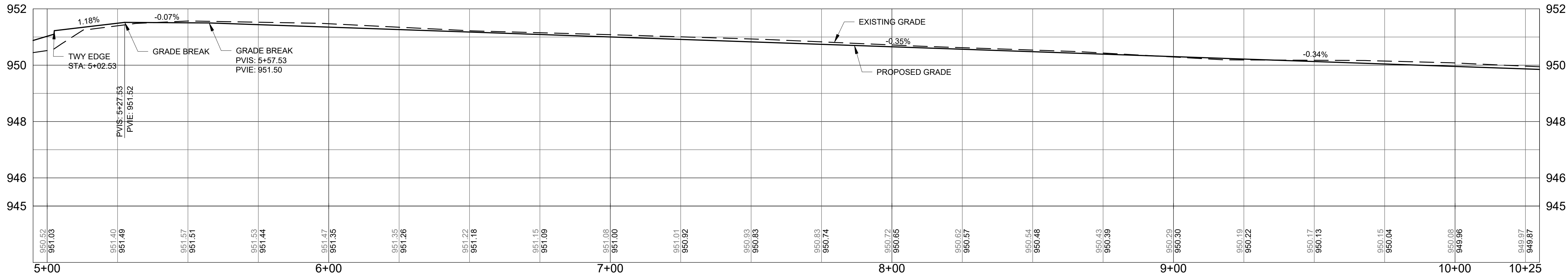
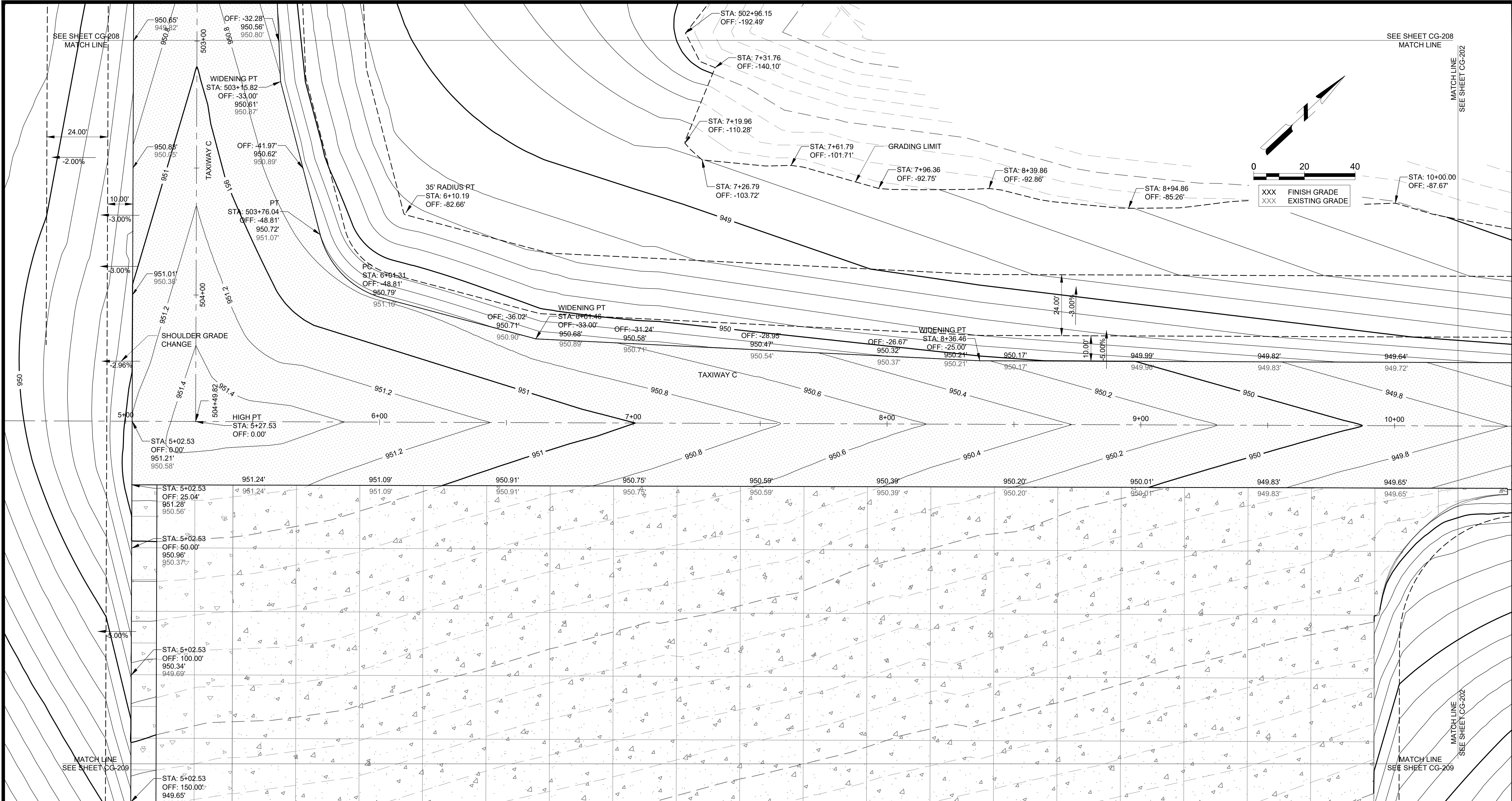
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 1

SHEET NO 37 of 76

CG-201



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PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

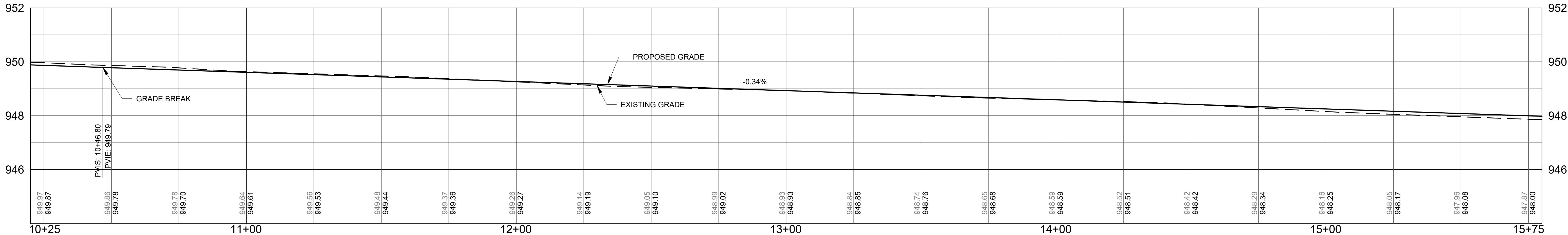
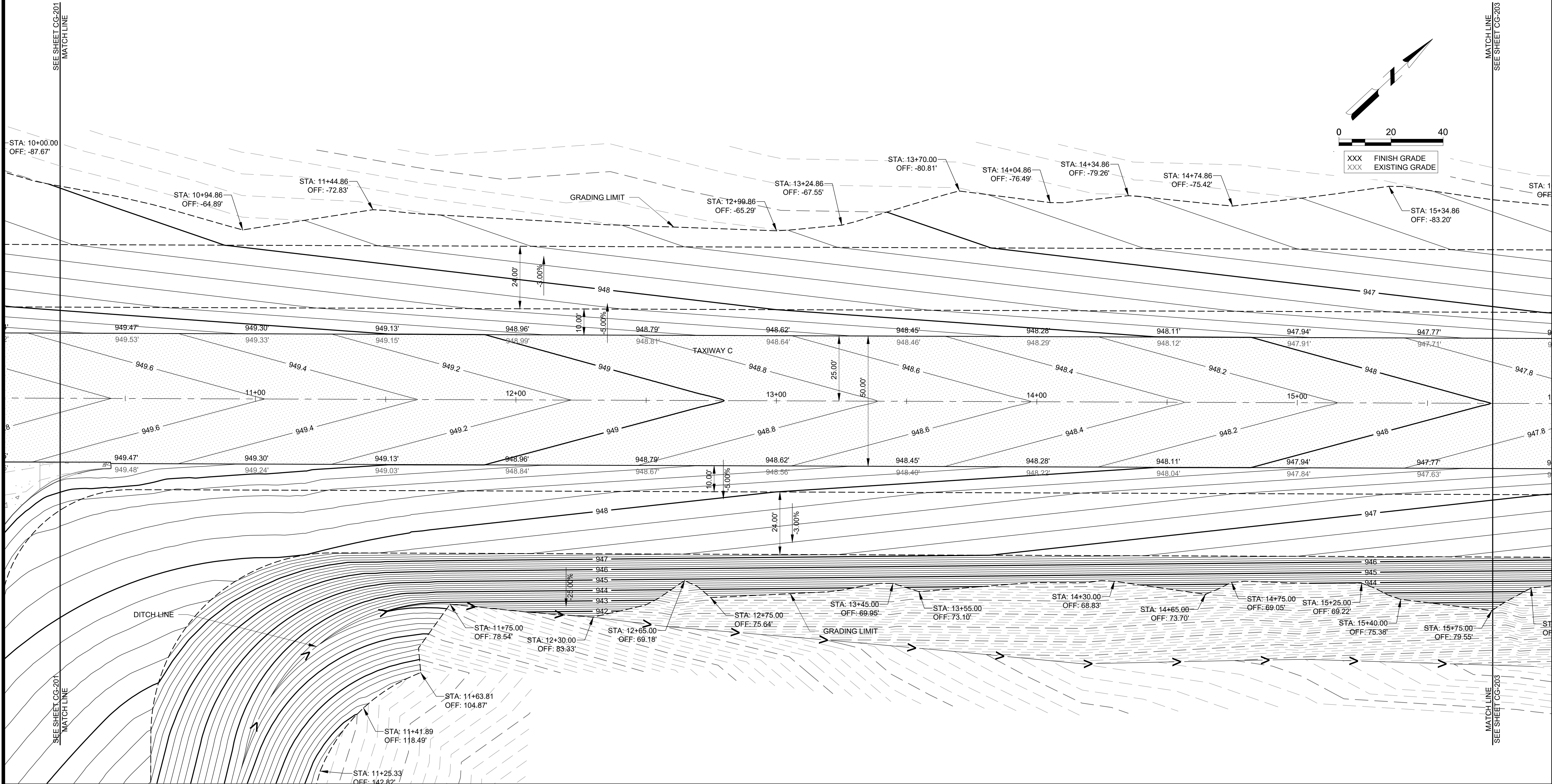
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 2

SHEET NO. 38 of 76

CG-202



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PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

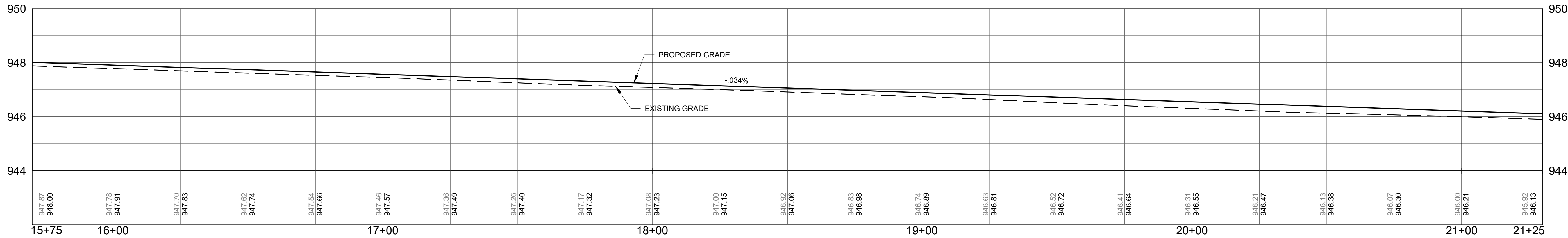
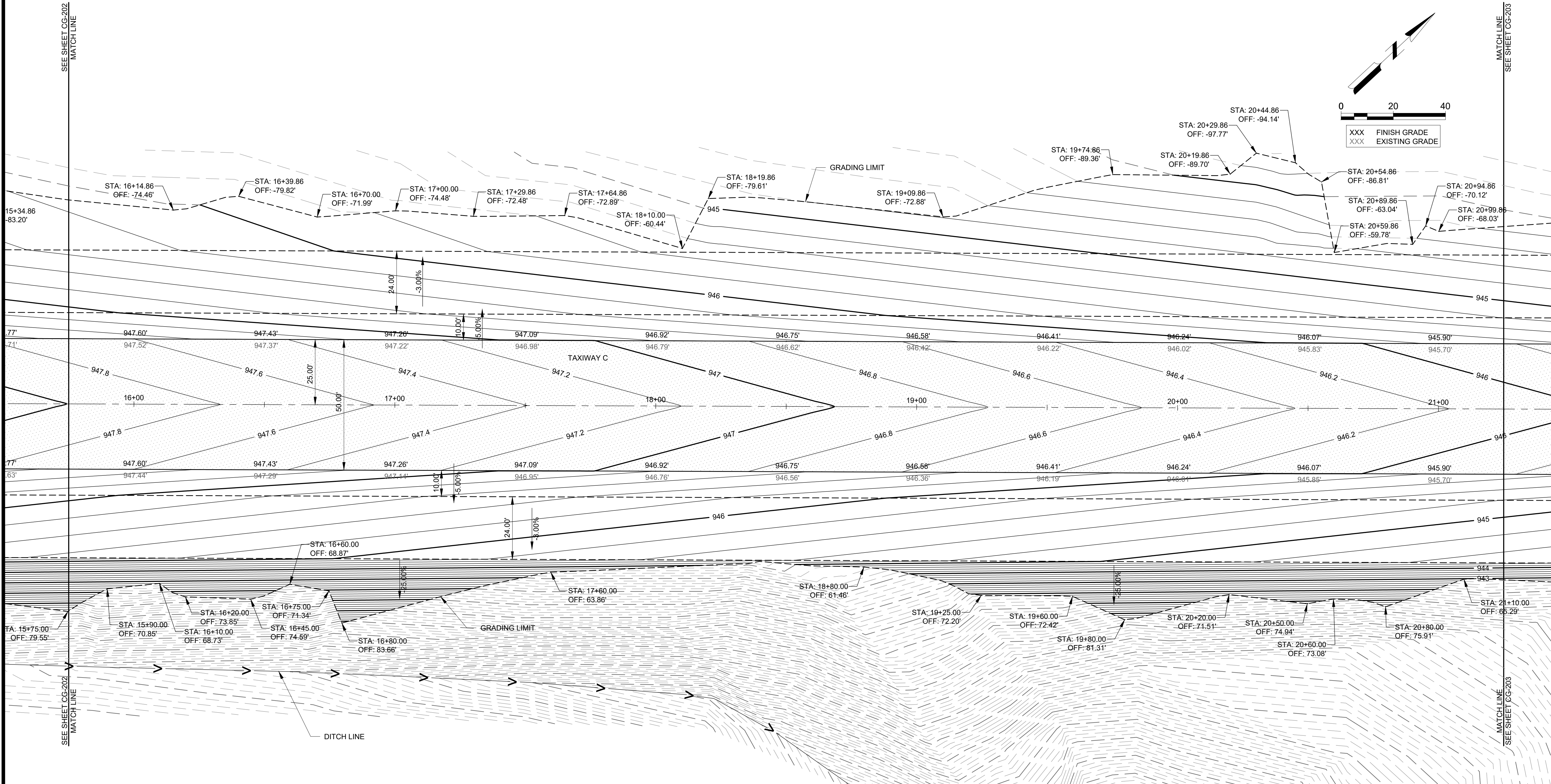
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 3

SHEET NO. 39 of 76

CG-203



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PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

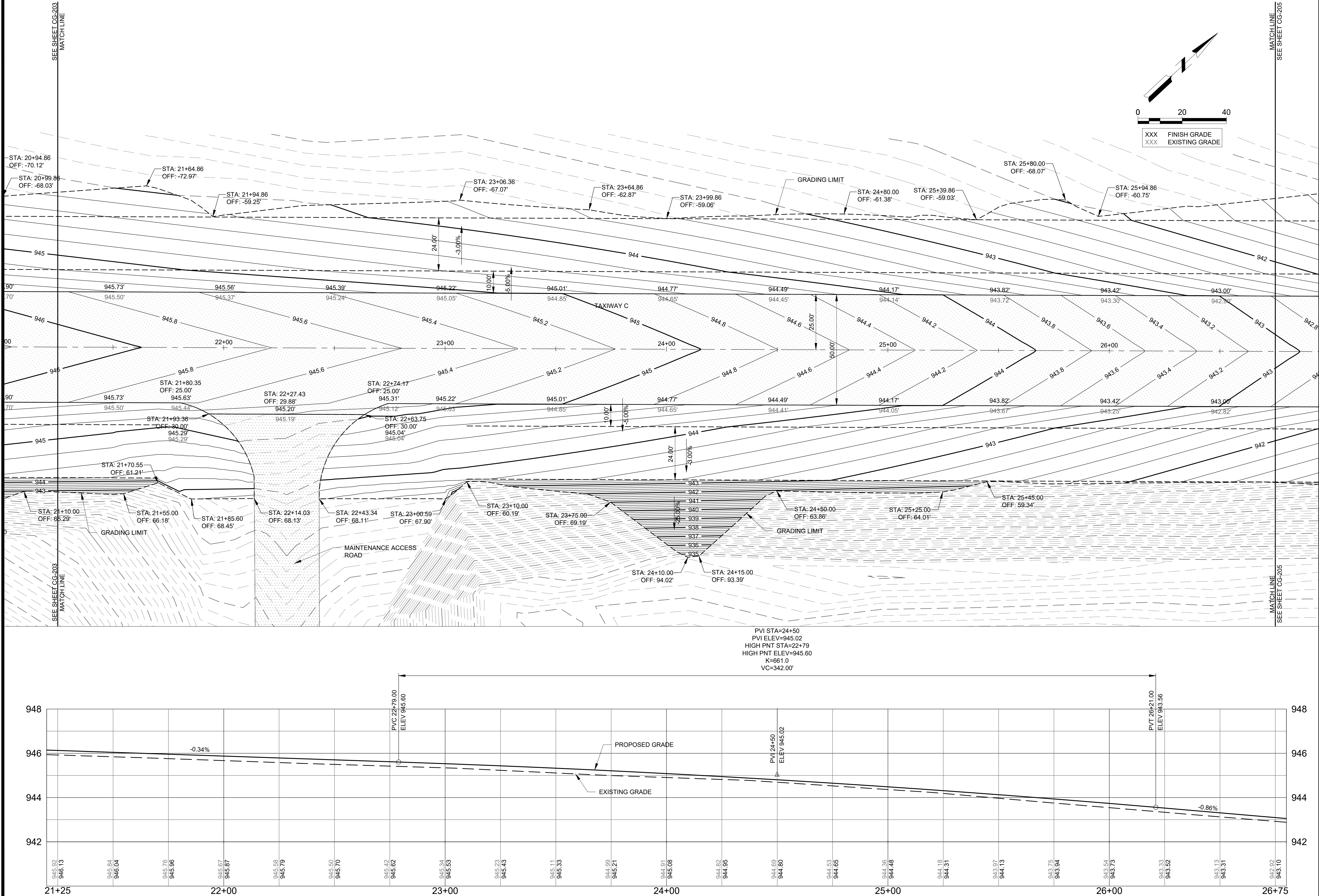
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 4

SHEET NO 40 of 76

CG-204



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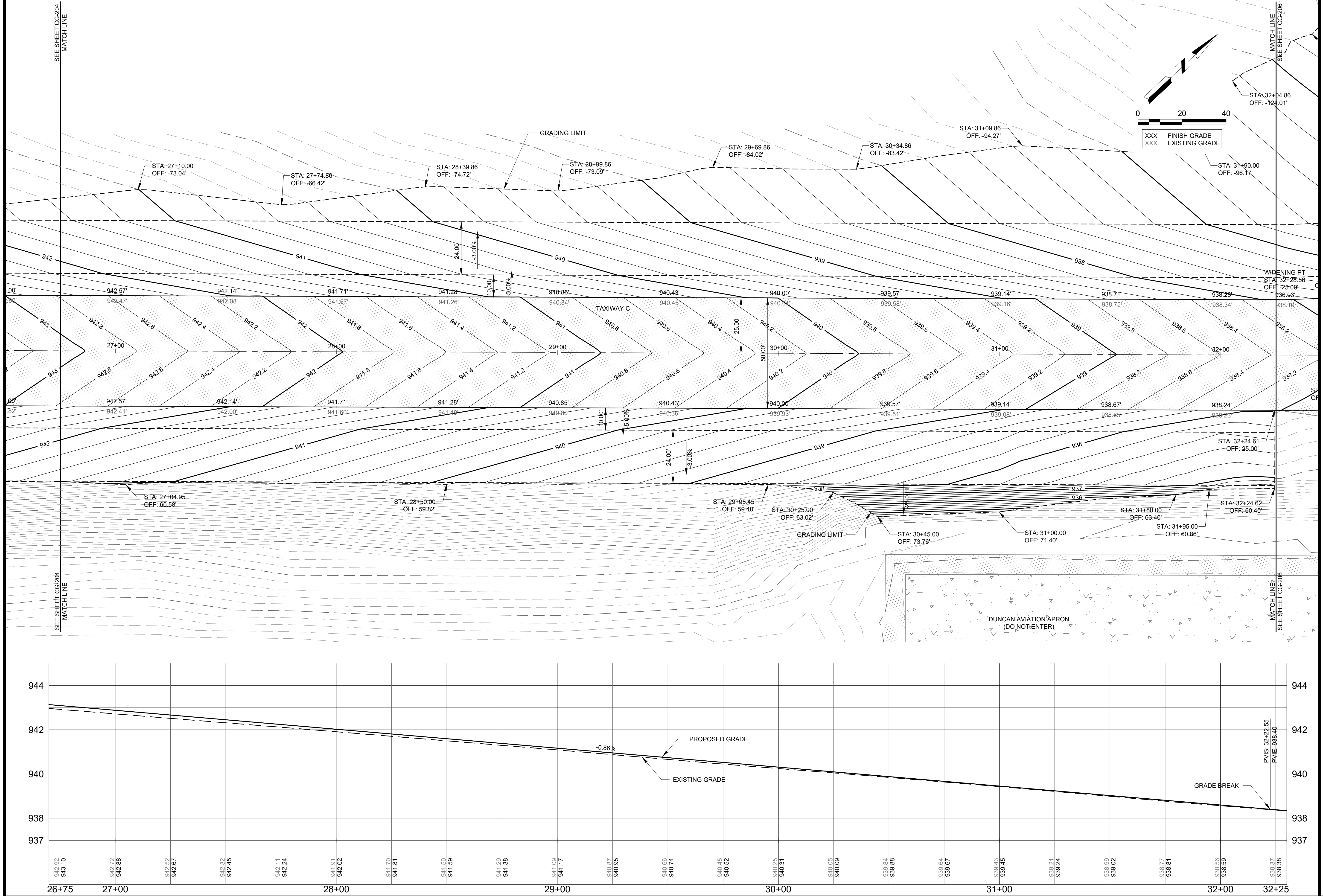
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**GRADING PLAN AND
PROFILE - AREA 5**

SHEET NO. 41 of 76

CG-205



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PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

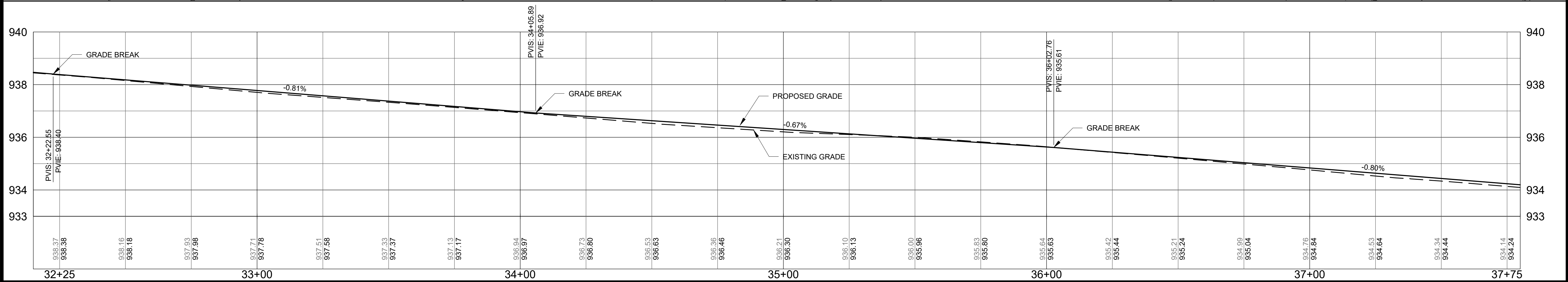
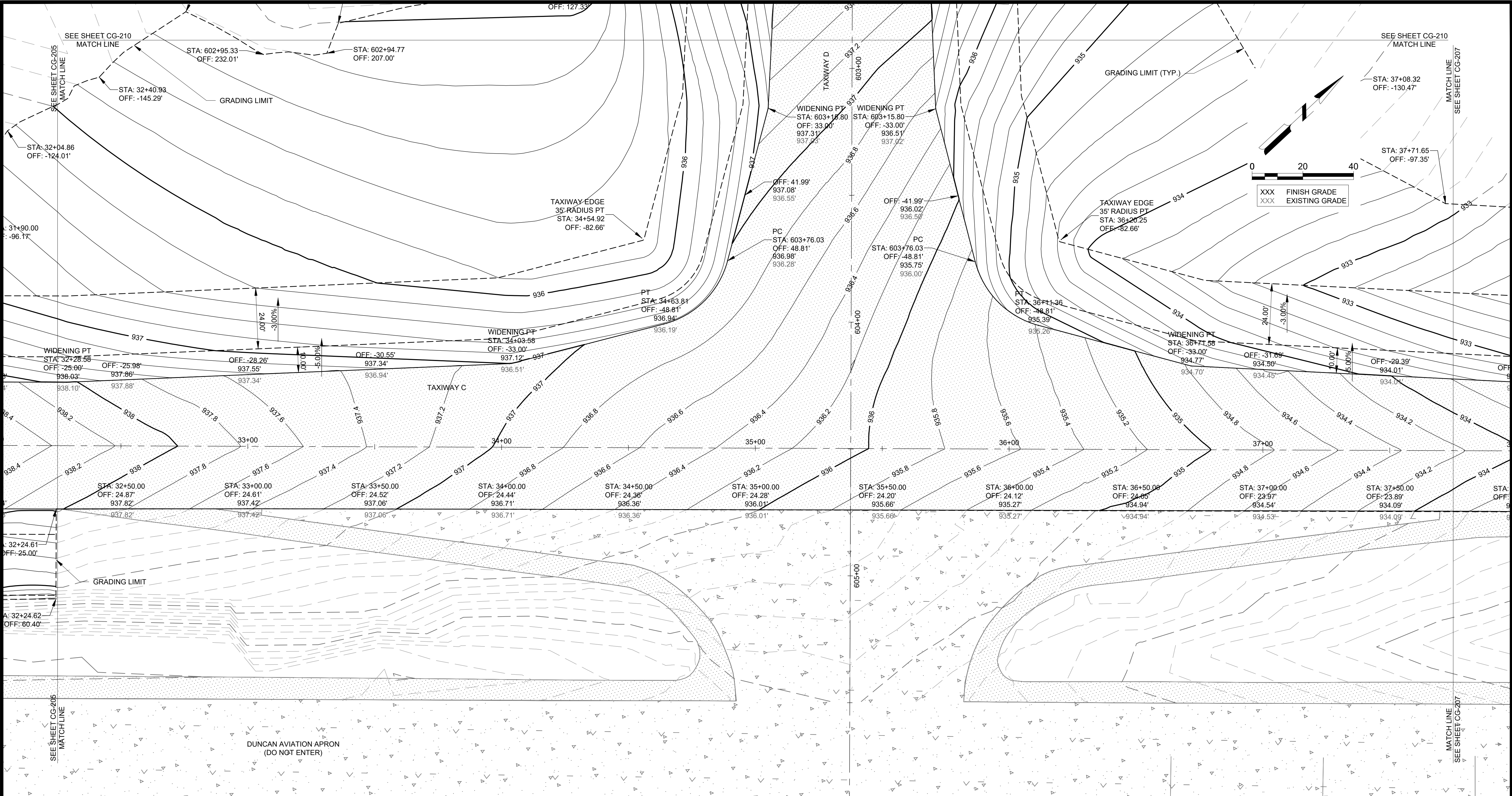
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 6

SHEET NO. 42 of 76

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

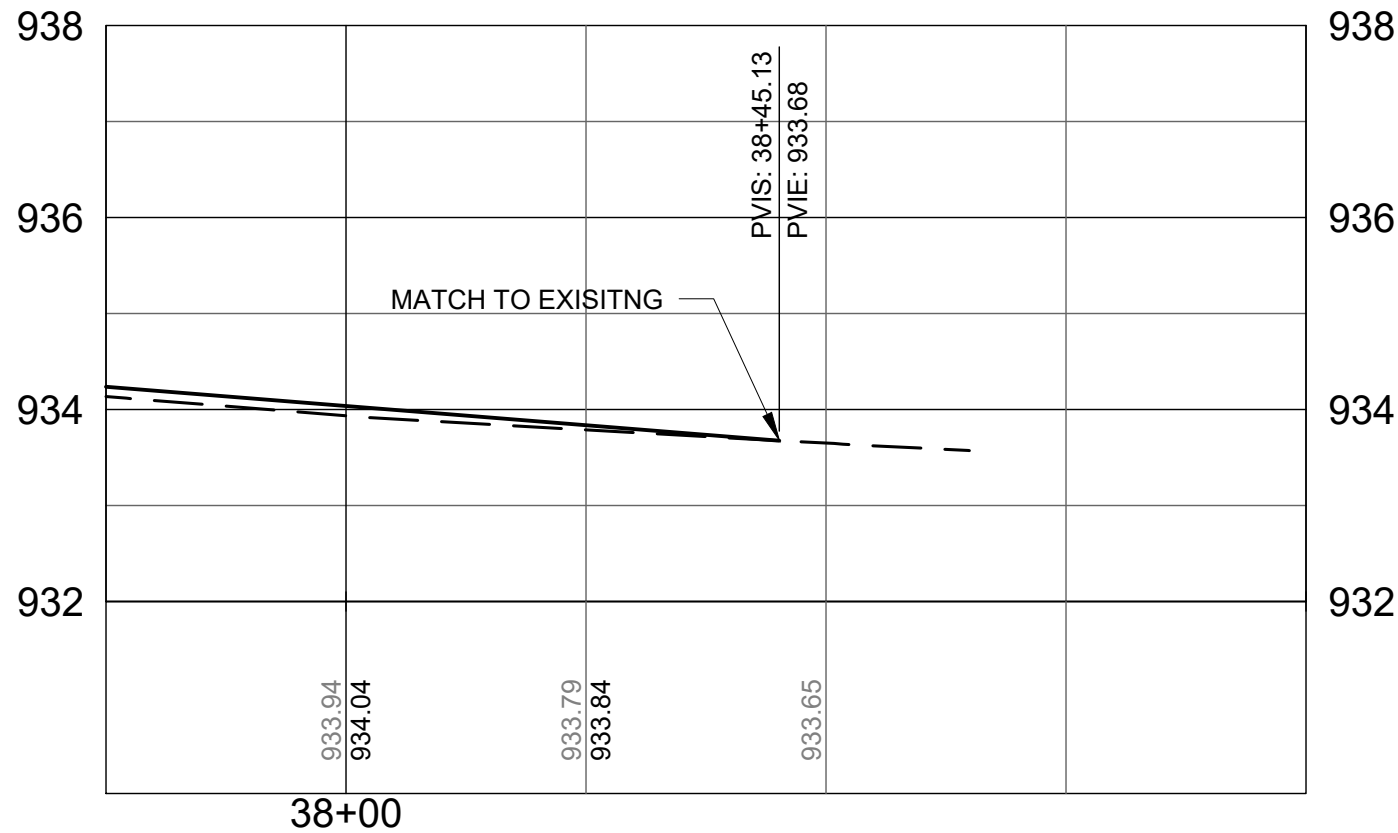
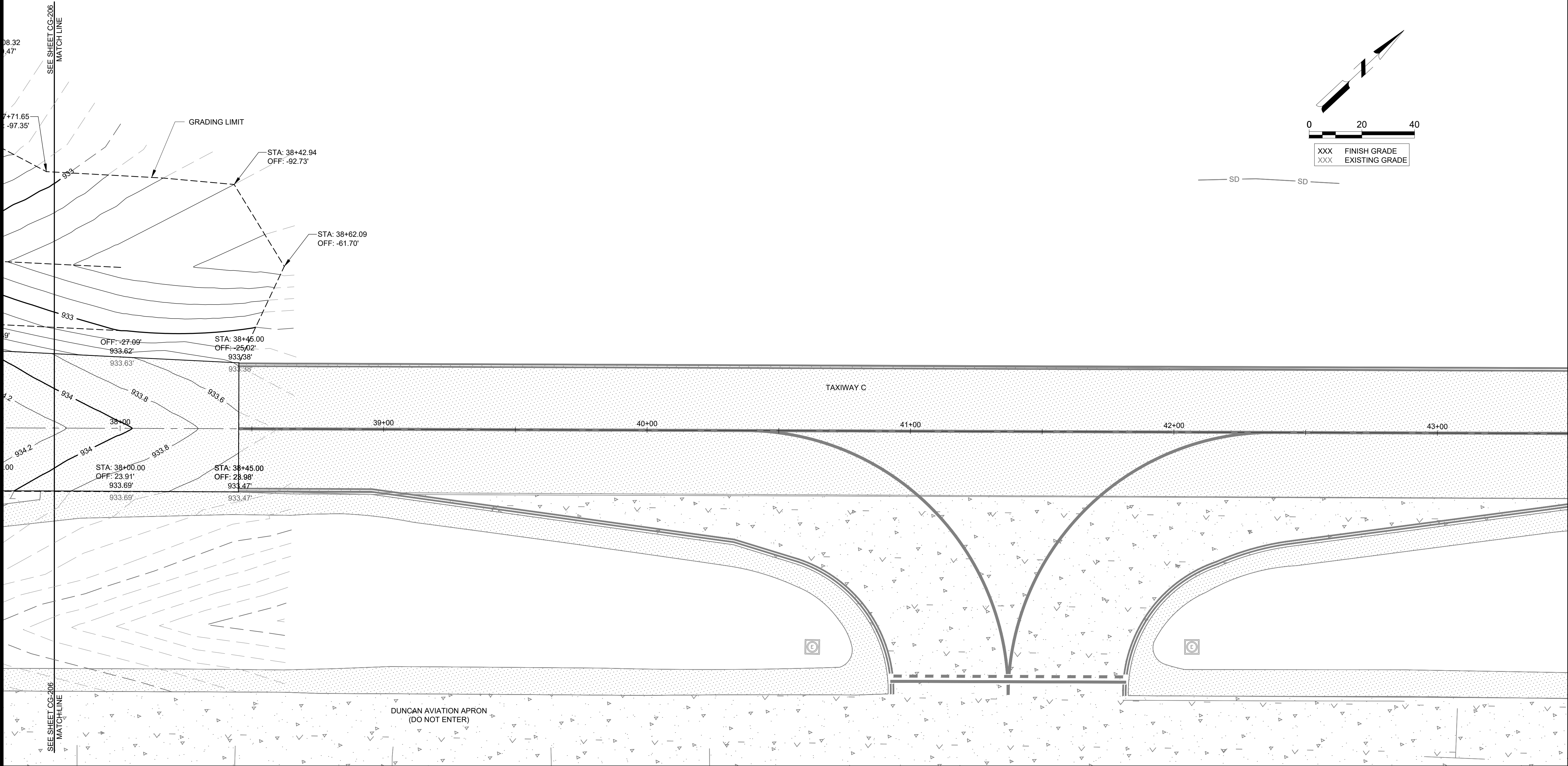
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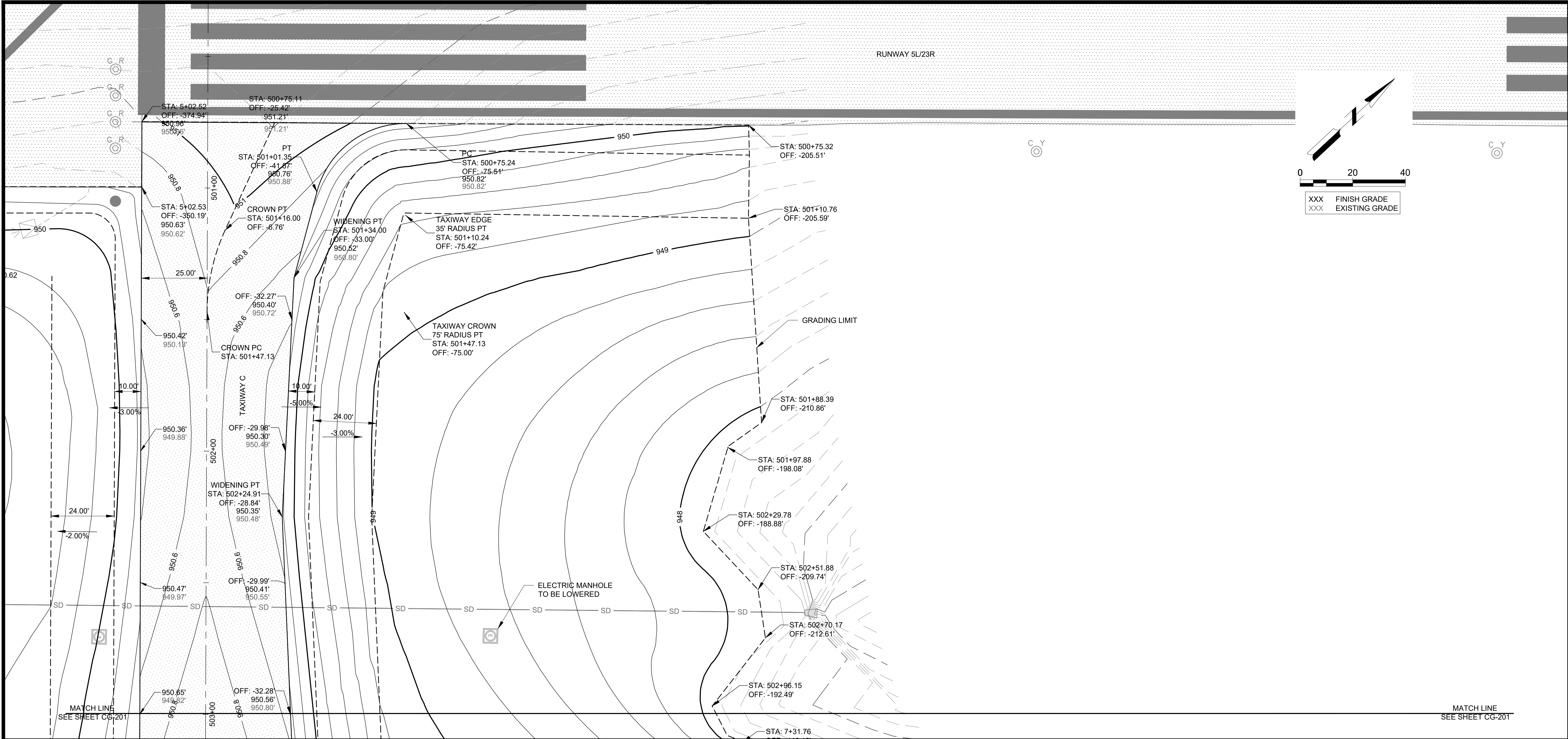
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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 7

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BATTLE CREEK, MI

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SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 8

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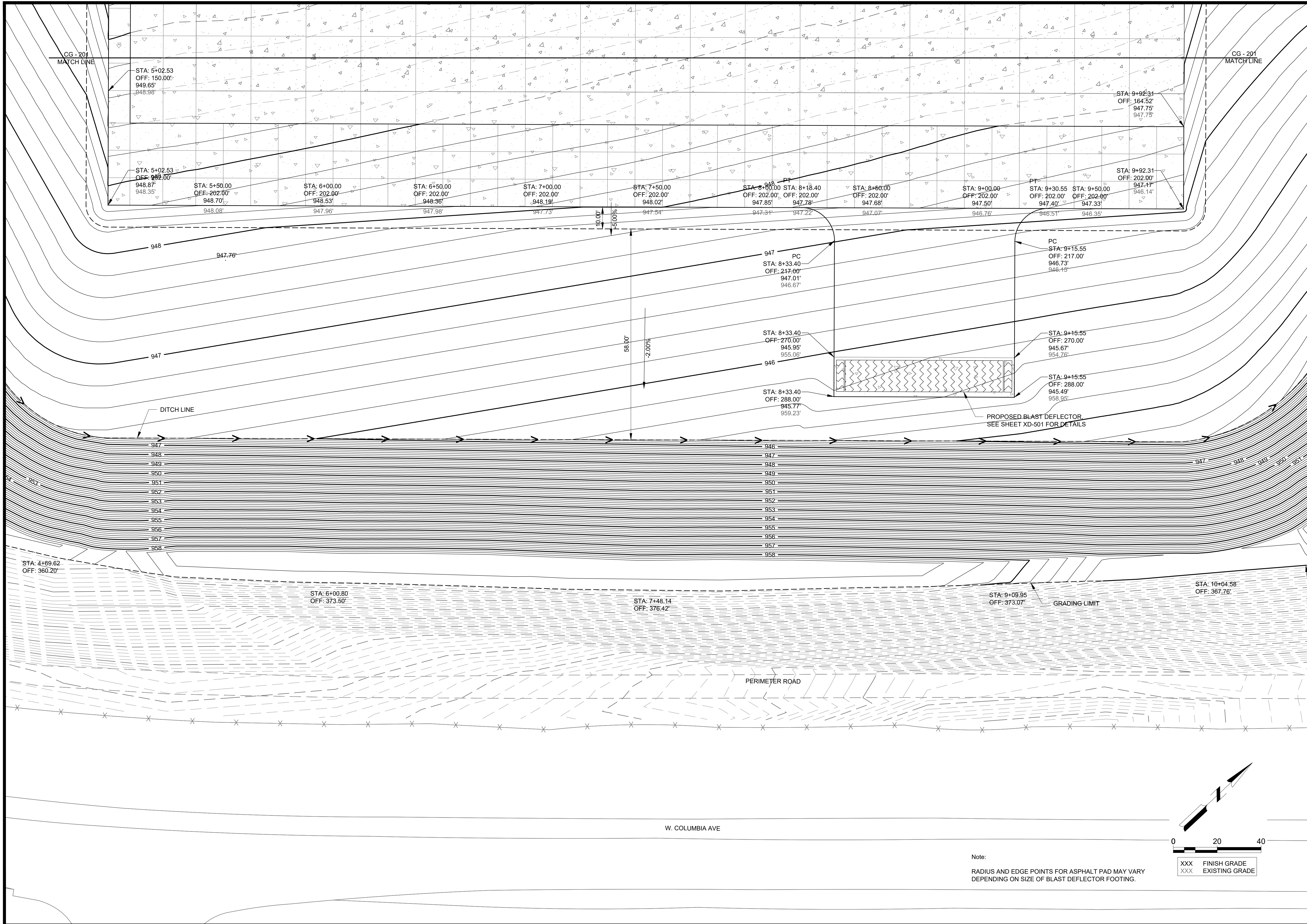
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SHEET CONTENTS
GRADING PLAN AND
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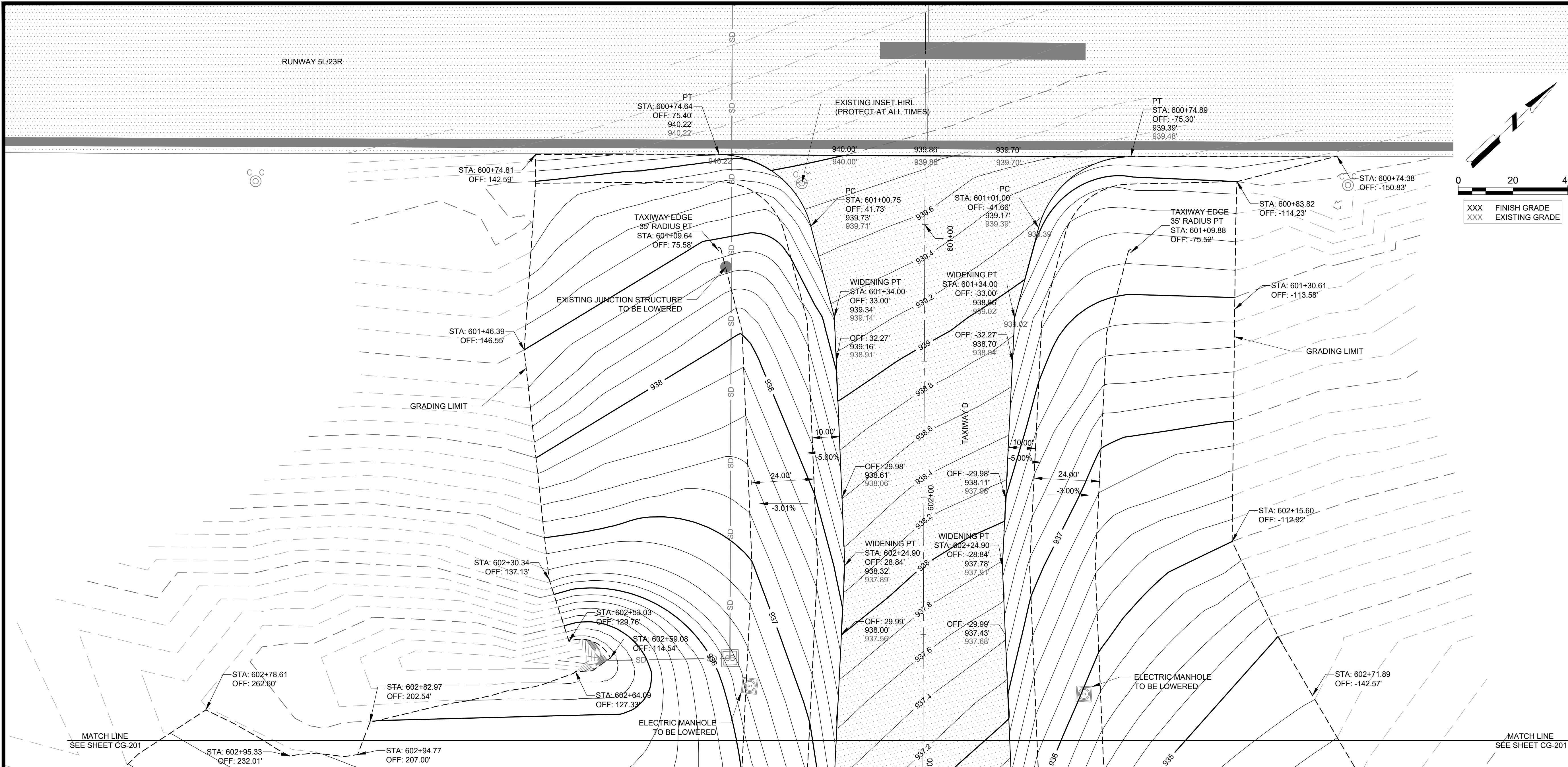
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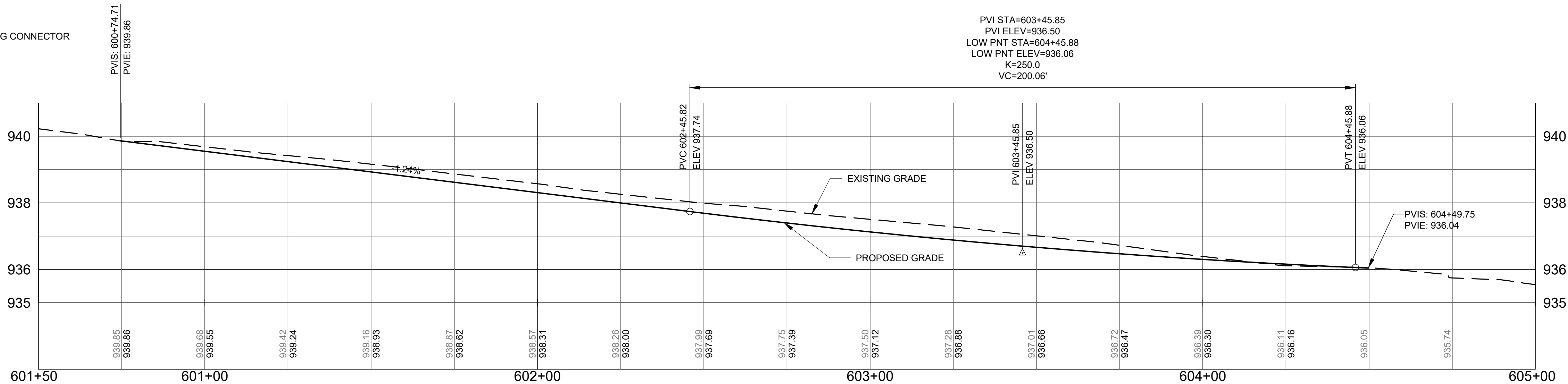
SHEET CONTENTS
GRADING PLAN AND
PROFILE - AREA 10

SHEET NO. 46 of 76

CG-210



TAXIWAY D PROFILE ALONG CONNECTOR





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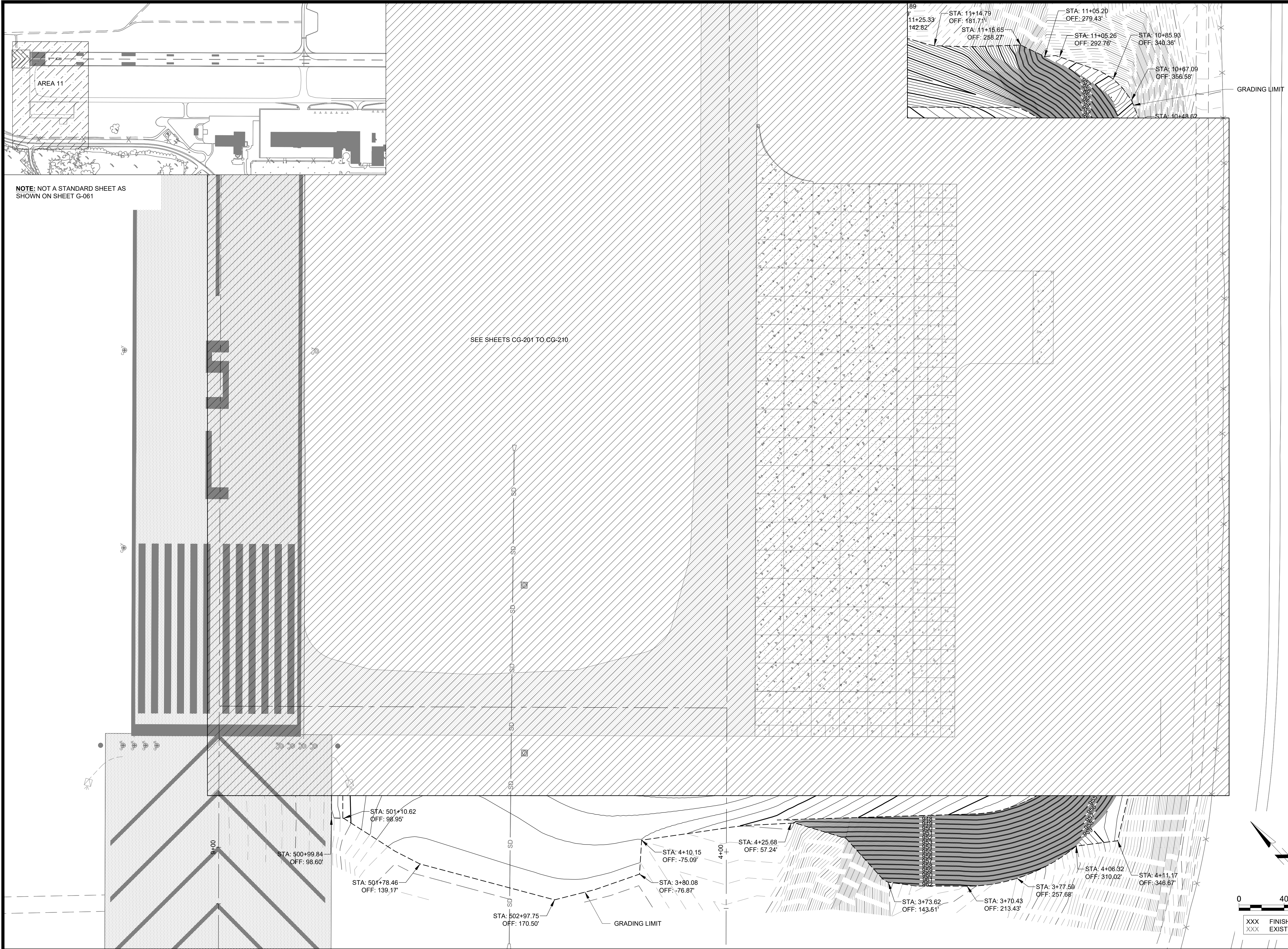
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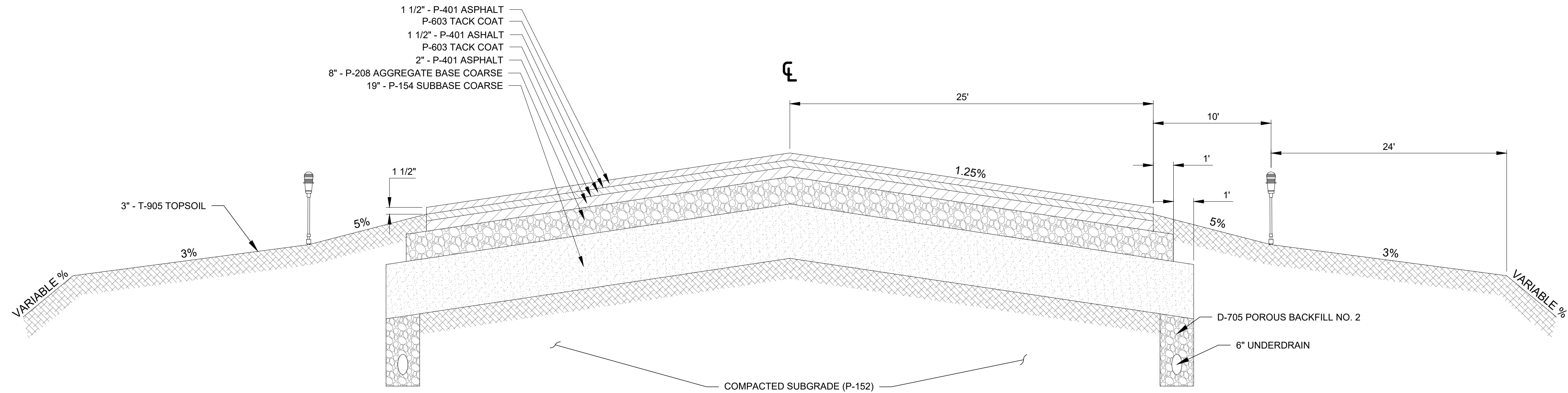
GRADING PLAN AND
PROFILE - AREA 11

SHEET NO. 47 of 76

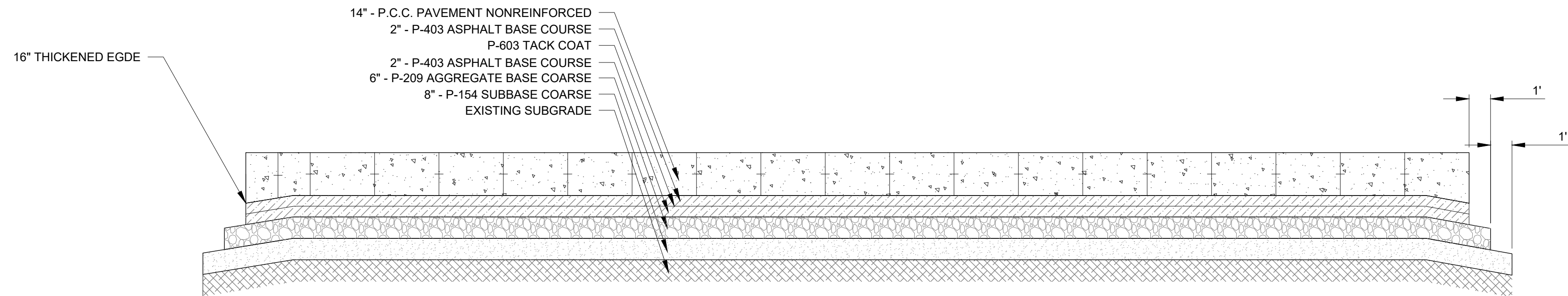
CG-251



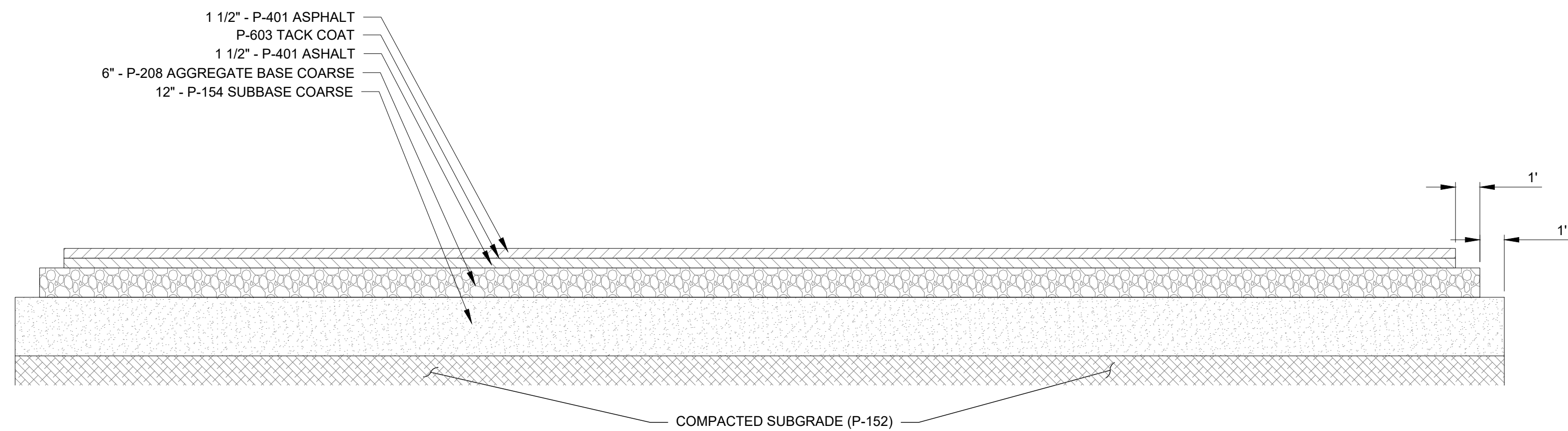
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3/21/2018 9:04:11 AM



1 TYPICAL PROPOSED TAXYWAY SECTION
NOT TO SCALE



2 TYPICAL PROPOSED HOLD APRON SECTION
NOT TO SCALE



3 TYPICAL PROPOSED BLAST PAD PAVEMENT SECTION
NOT TO SCALE

NOTES:
1. SECTIONS SHOWN ARE REPRESENTATIVE OF "TYPICAL" WIDTHS AND GRADES. SLOPES AND WIDTHS MAY NOT MATCH SECTIONS AS SHOWN FOR EVERY STATION. SEE CD AND CG SERIES SHEETS FOR EXACT GEOMETRY AND GRADING.

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SHEET CONTENTS
TYPICAL PROPOSED
PAVEMENT SECTION
DETAIL SHEET 1 OF 1

SHEET NO. 48 of 76

CP-501

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**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

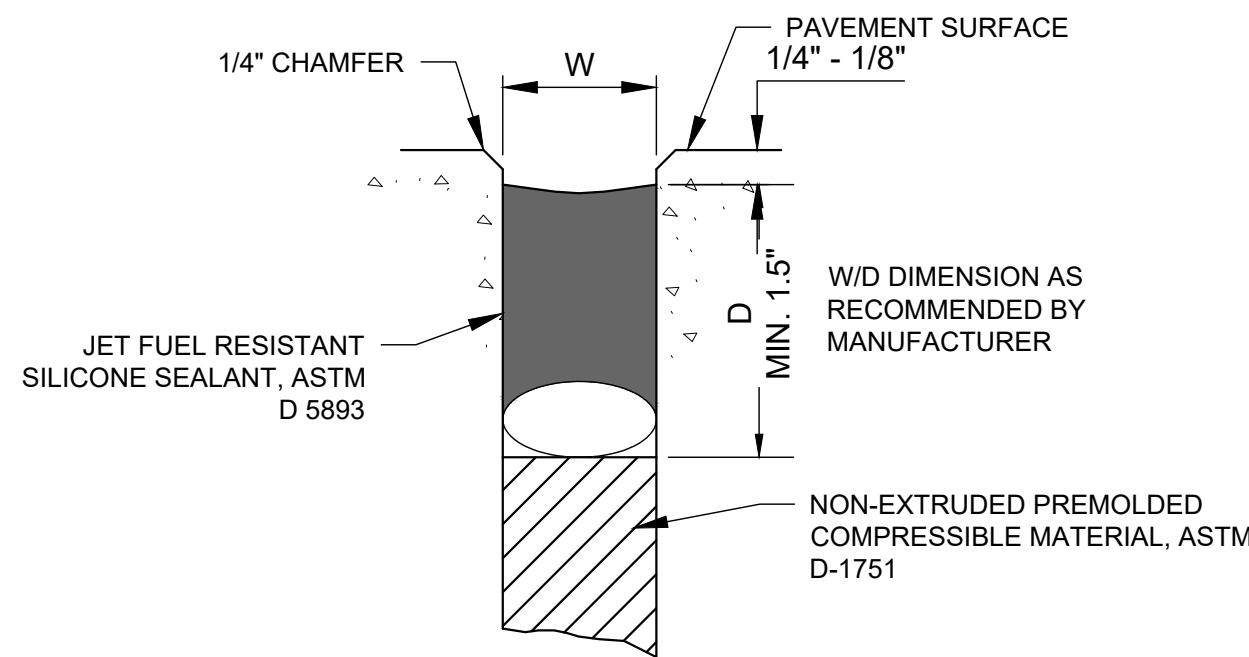
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1 03.29.18 ISSUED FOR
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AIP NO. B-26-0008-4518
M&H NO.: 0228800-161097.01
DATE: 03.29.2018
DESIGNED BY: ALP
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DO NOT SCALE DRAWINGS

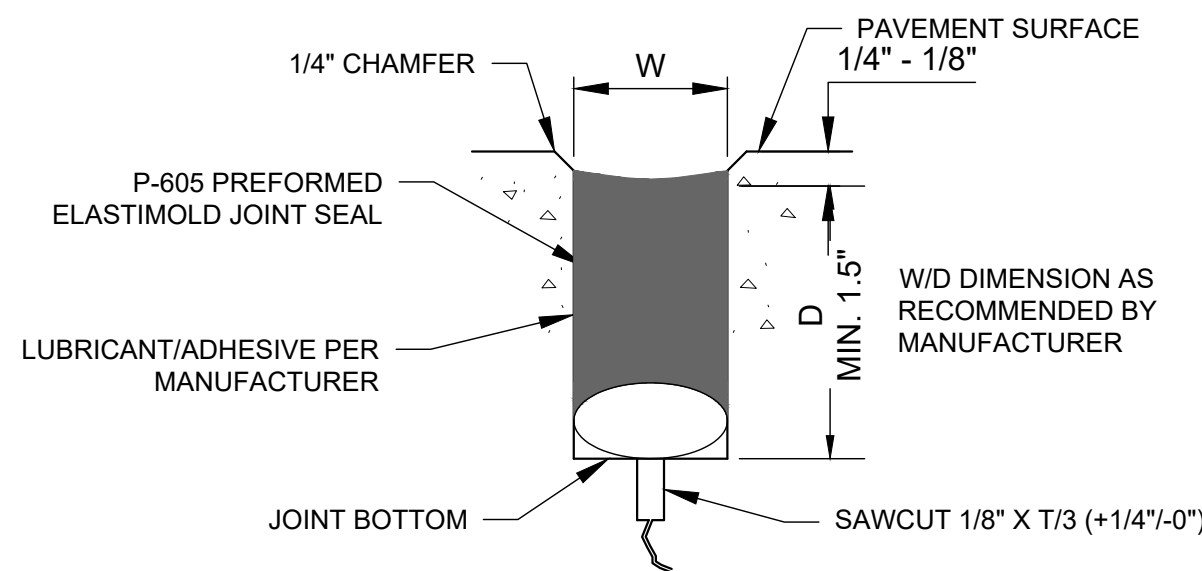
SHEET CONTENTS
TYPICAL PROPOSED
JOINT DETAILS SHEET
1 OF 1

SHEET NO. 49 of 76

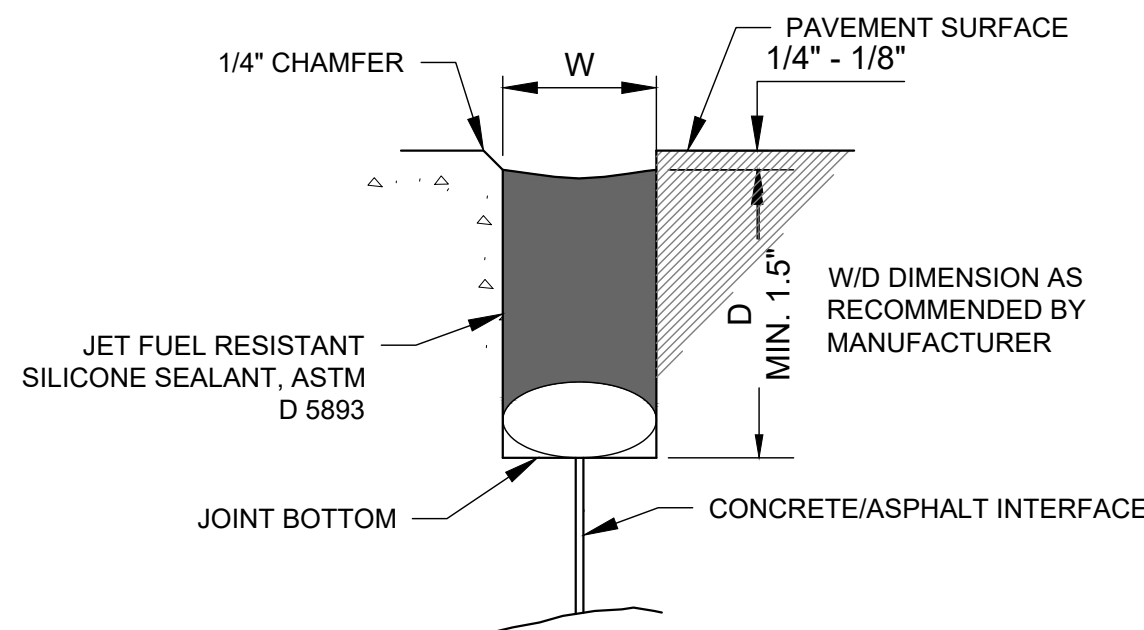
CP-502



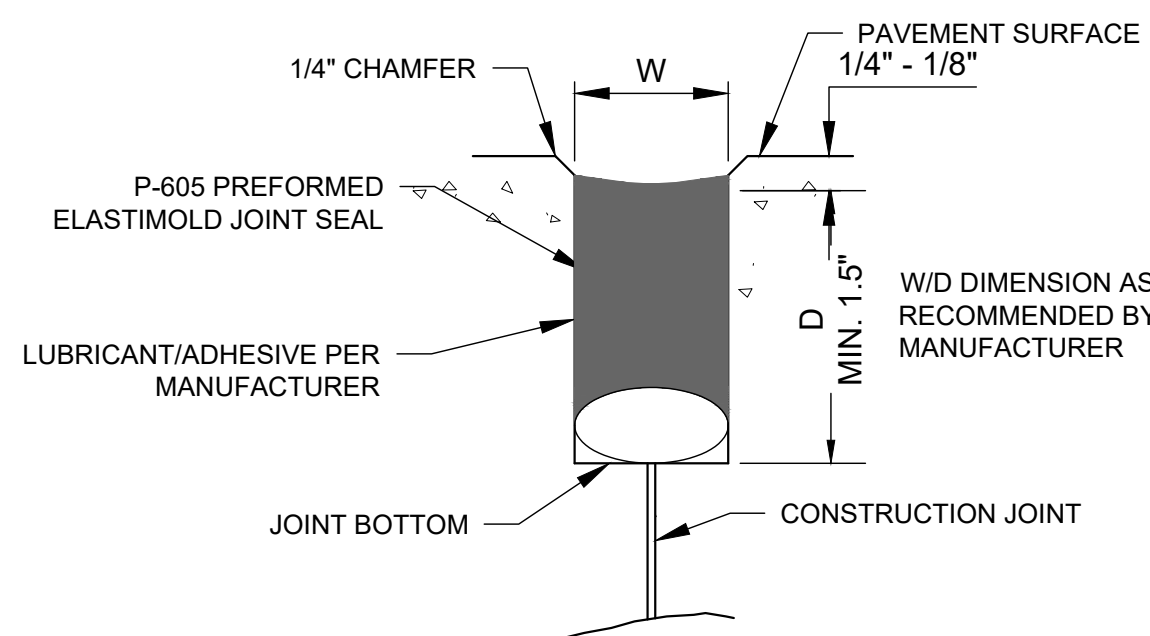
6 JOINT SEALING DETAIL 1
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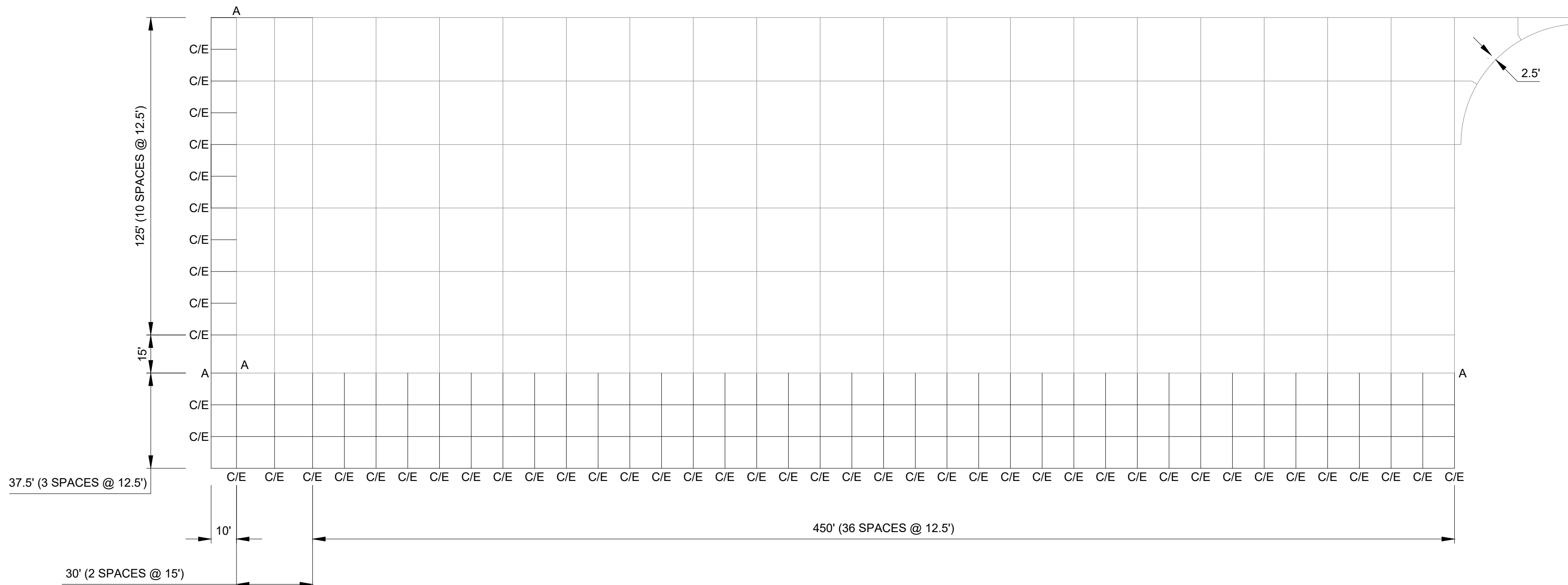
7 JOINT SEALING DETAIL 2
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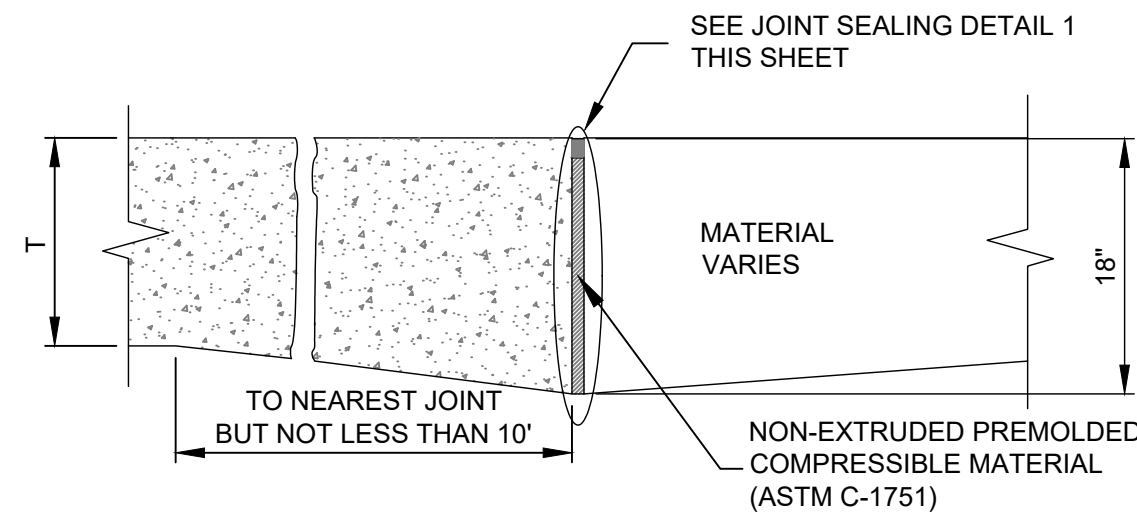
**8 CONCRETE/
ASPHALT INTERFACE SEALING**
NOT TO SCALE



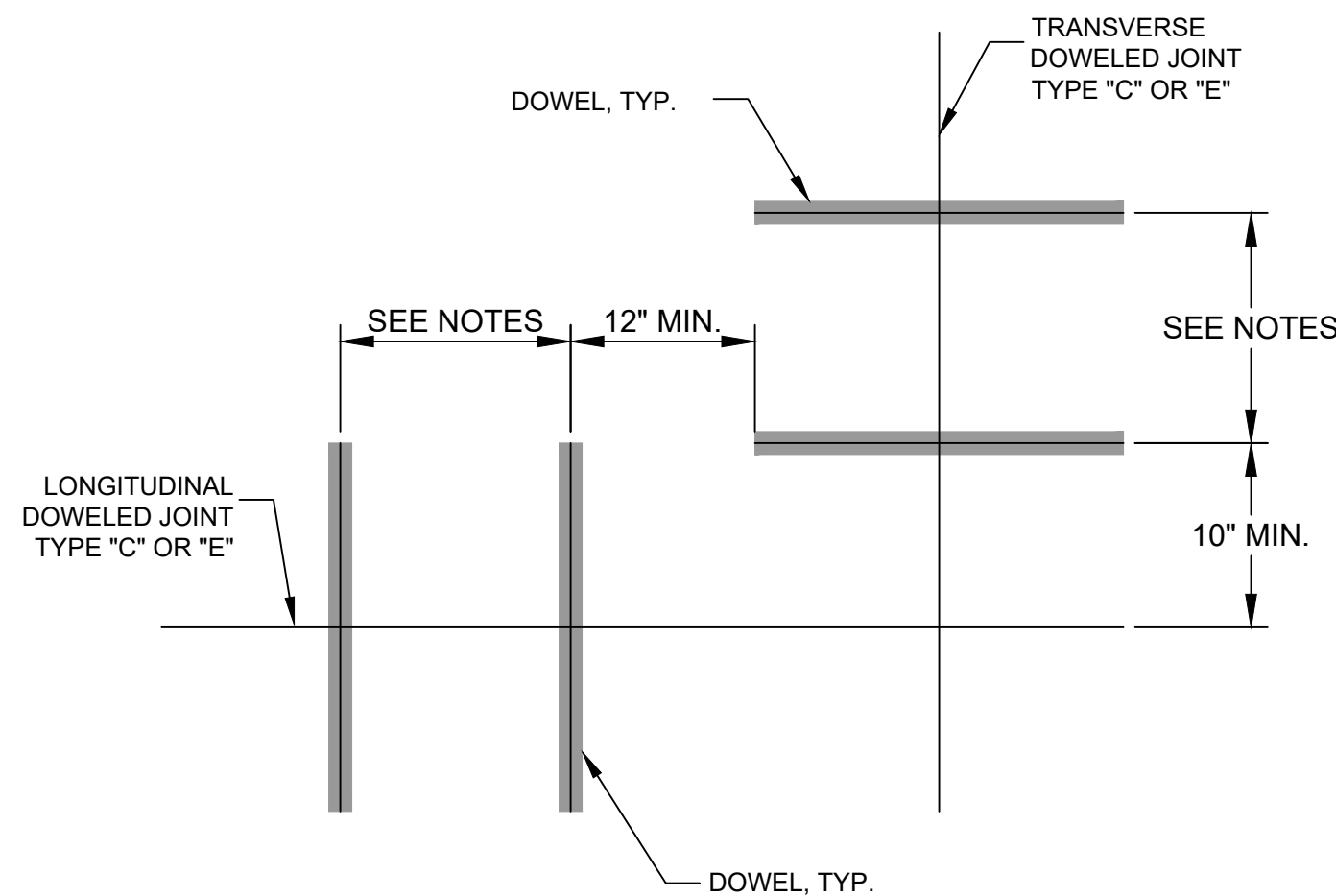
9 JOINT SEALING DETAIL 3
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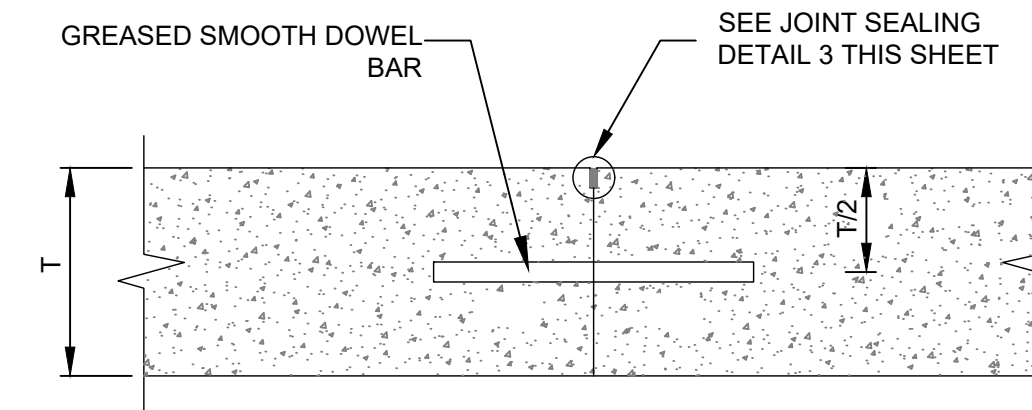
1 HOLD APRON JOINT LAYOUT
NOT TO SCALE



4 TYPE 'A' THICKENED EDGE
NOT TO SCALE

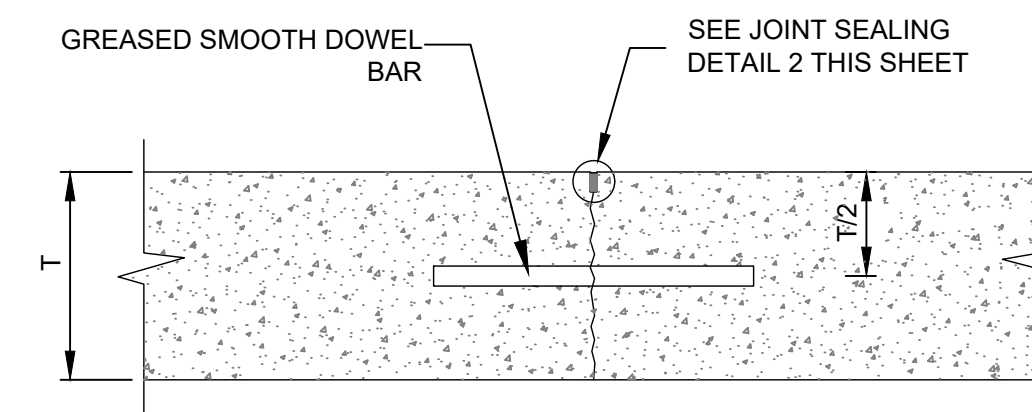


**5 DOWEL SPACING AT
JOINT INTERSECTION DETAIL**
NOT TO SCALE



NOTE:
HOLES FOR DOWELS MUST BE DRILLED AFTER CONCRETE HAS SET USING A PRECISION MECHANICAL GUIDE TO ENSURE EACH HOLE IS EXACTLY PERPENDICULAR IN ALL DIRECTIONS TO THE FACE OF THE SLAB.

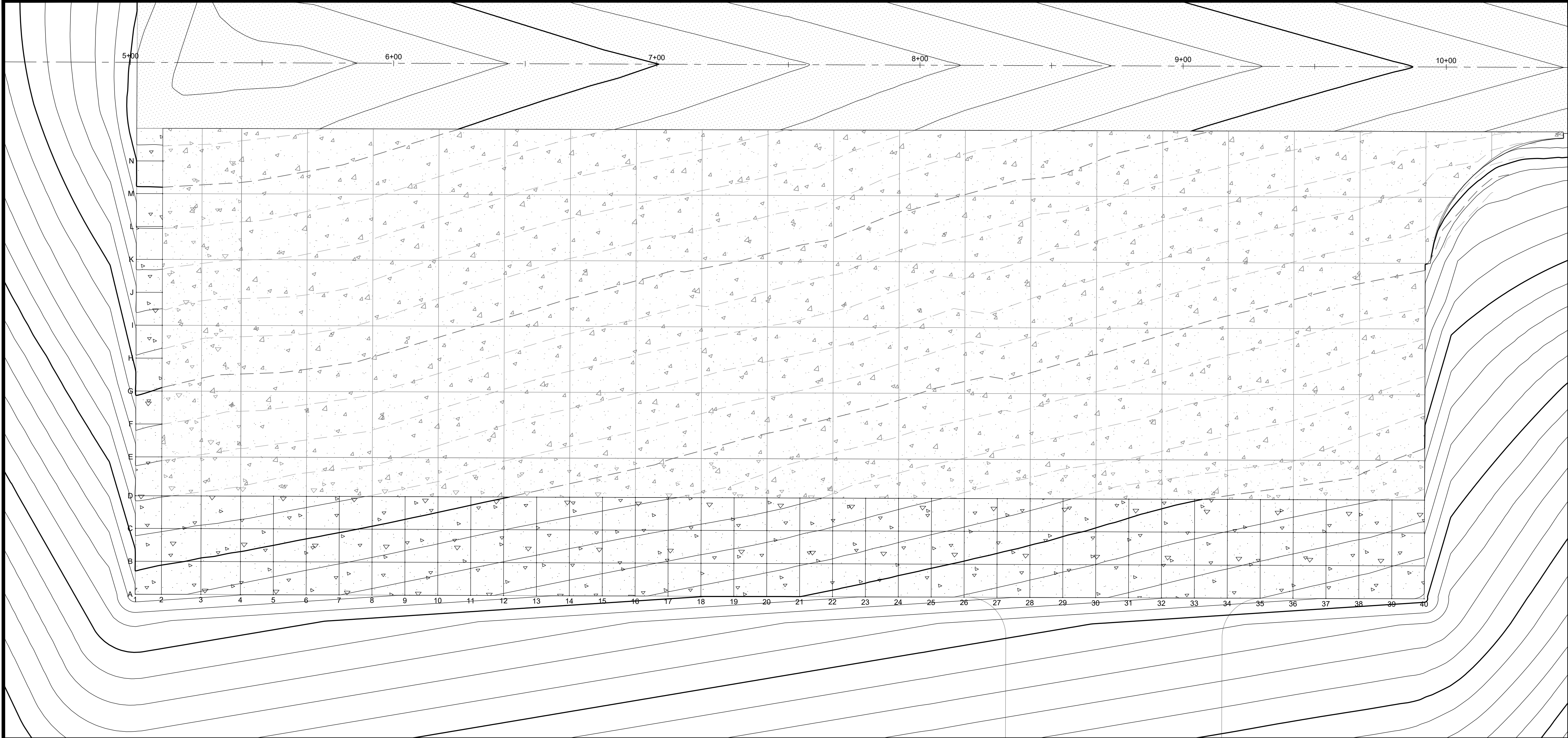
**2 TYPE 'E' DOWELED
CONSTRUCTION JOINT DETAIL**
NOT TO SCALE



**3 TYPE 'C' DOWELED
CONTRACTION JOINT DETAIL**
NOT TO SCALE

NOTES:

- DOWELS FOR ALL APPLICABLE JOINT TYPES IN 14" CONCRETE SHALL BE 1 1/4" DIA., 20" LONG, SPACED 15" ON CENTER.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT DOWEL PLACEMENT IS IN ACCORDANCE WITH SPECIFICATIONS. DOWEL PLACEMENT AND SECURING METHOD SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR APPROVAL A MINIMUM OF 2 WEEKS PRIOR TO THE FIRST DAY OF PAVING.
- ALL JOINT SEALANT, JOINT SAWING, JOINT PREPARATION, AND CLEANUP IS INCIDENTAL TO CONCRETE PLACEMENT, P-501



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**WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

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	STA.	OFF.	ELEV.
A1	5+02.53	202.00'	948.867
A2	5+12.46	202.00'	948.833
A3	5+27.46	202.00'	948.782
A4	5+42.47	202.00'	948.730
A5	5+54.97	202.00'	948.687
A6	5+67.47	202.00'	948.644
A7	5+79.97	202.00'	948.601
A8	5+92.47	202.00'	948.559
A9	6+04.97	202.00'	948.516
A10	6+17.47	202.00'	948.473
A11	6+29.97	202.00'	948.430
A12	6+42.47	202.00'	948.387
A13	6+54.97	202.00'	948.344
A14	6+67.47	202.00'	948.301
A15	6+79.97	202.00'	948.258
A16	6+92.47	202.00'	948.216
A17	7+04.97	202.00'	948.173
A18	7+17.47	202.00'	948.130
A19	7+29.97	202.00'	948.087
A20	7+42.47	202.00'	948.044
A21	7+54.97	202.00'	948.001
A22	7+67.47	202.00'	947.958
A23	7+79.97	202.00'	947.915
A24	7+92.47	202.00'	947.872
A25	8+04.97	202.00'	947.830
A26	8+17.47	202.00'	947.787
A27	8+29.97	202.00'	947.744
A28	8+42.47	202.00'	947.701
A29	8+54.97	202.00'	947.658
A30	8+67.47	202.00'	947.615

	STA.	OFF.	ELEV.
A31	8+79.97	202.00'	947.572
A32	8+92.47	202.00'	947.529
A33	9+04.97	202.00'	947.487
A34	9+17.47	202.00'	947.444
A35	9+29.97	202.00'	947.401
A36	9+42.47	202.00'	947.358
A37	9+54.97	202.00'	947.315
A38	9+67.47	202.00'	947.272
A39	9+79.97	202.00'	947.229
A40	9+92.31	202.00'	947.174
B1	5+02.53	189.50'	949.055
B2	5+12.45	189.50'	949.023
B3	5+27.45	189.50'	948.979
B4	5+42.47	189.50'	948.937
B5	5+54.97	189.50'	948.896
B6	5+67.47	189.50'	948.853
B7	5+79.97	189.50'	948.812
B8	5+92.47	189.50'	948.770
B9	6+04.97	189.50'	948.726
B10	6+17.47	189.50'	948.683
B11	6+29.97	189.50'	948.638
B12	6+42.47	189.50'	948.594
B13	6+54.97	189.50'	948.553
B14	6+67.47	189.50'	948.510
B15	6+79.97	189.50'	948.468
B16	6+92.47	189.50'	948.427
B17	7+04.97	189.50'	948.387
B18	7+17.47	189.50'	948.344
B19	7+29.97	189.50'	948.304
B20	7+42.47	189.50'	948.260

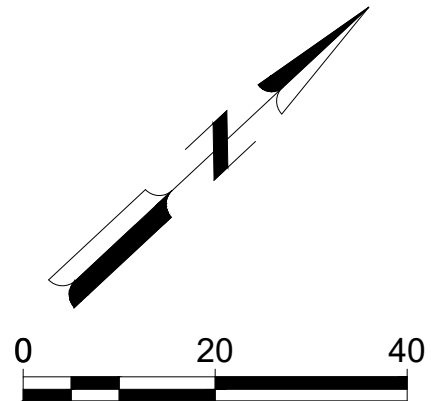
	STA.	OFF.	ELEV.
B21	7+54.97	189.50'	948.212
B22	7+67.47	189.50'	948.169
B23	7+79.97	189.50'	948.119
B24	7+92.47	189.50'	948.070
B25	8+04.97	189.50'	948.025
B26	8+17.47	189.50'	947.982
B27	8+29.97	189.50'	947.936
B28	8+42.47	189.50'	947.889
B29	8+54.97	189.50'	947.842
B30	8+67.47	189.50'	947.799
B31	8+79.97	189.50'	947.749
B32	8+92.47	189.50'	947.703
B33	9+04.97	189.50'	947.662
B34	9+17.47	189.50'	947.620
B35	9+29.97	189.50'	947.578
B36	9+42.47	189.50'	947.538
B37	9+54.97	189.50'	947.497
B38	9+67.47	189.50'	947.454
B39	9+79.97	189.50'	947.404
B40	9+92.31	189.50'	947.351
C1	5+02.53	177.00'	949.242
C2	5+12.45	177.00'	949.219
C3	5+27.45	177.00'	949.179
C4	5+42.46	177.00'	949.143
C5	5+54.96	177.00'	949.104
C6	5+67.46	177.00'	949.063
C7	5+79.96	177.00'	949.023
C8	5+92.46	177.00'	948.981
C9	6+04.96	177.00'	948.936
C10	6+17.46	177.00'	948.892

	STA.	OFF.	ELEV.
C11	6+29.96	177.00'	948.846
C12	6+42.46	177.00'	948.802
C13	6+54.96	177.00'	948.760
C14	6+67.46	177.00'	948.719
C15	6+79.96	177.00'	948.679
C16	6+92.46	177.00'	948.639
C17	7+04.96	177.00'	948.600
C18	7+17.46	177.00'	948.559
C19	7+29.96	177.00'	948.520
C20	7+42.46	177.00'	948.477
C21	7+54.96	177.00'	948.426
C22	7+67.46	177.00'	948.376
C23	7+79.96	177.00'	948.321
C24	7+92.46	177.00'	948.269
C25	8+04.96	177.00'	948.221
C26	8+17.46	177.00'	948.175
C27	8+29.96	177.00'	948.128
C28	8+42.46	177.00'	948.079
C29	8+54.96	177.00'	948.028
C30	8+67.46	177.00'	947.979
C31	8+79.96	177.00'	947.926
C32	8+92.46	177.00'	947.877
C33	9+04.96	177.00'	947.836
C34	9+17.46	177.00'	947.796
C35	9+29.96	177.00'	947.756
C36	9+42.46	177.00'	947.717
C37	9+54.96	177.00'	947.678
C38	9+67.46	177.00'	947.638
C39	9+79.96	177.00'	947.589
C40	9+92.31	177.00'	947.531

	STA.	OFF.	ELEV.
D1	5+02.53	164.62'	949.428
D2	5+12.45	164.62'	949.407
D3	5+27.45	164.61'	949.379
D4	5+42.45	164.61'	949.346
D5	5+54.95	164.61'	949.310
D6	5+67.45	164.61'	949.271
D7	5+79.95	164.61'	949.232
D8	5+92.45	164.60'	949.191
D9	6+04.95	164.60'	949.146
D10	6+17.45	164.60'	949.099
D11	6+29.95	164.60'	949.053
D12	6+42.45	164.59'	949.008
D13	6+54.95	164.59'	948.966
D14	6+67.45	164.59'	948.927
D15	6+79.95	164.58'	948.887
D16	6+92.45	164.58'	948.848
D17	7+04.95	164.58'	948.811
D18	7+17.45	164.58'	948.773
D19	7+29.95	164.57'	948.736
D20	7+42.45	164.57'	948.693
D21	7+54.95	164.57'	948.640
D22	7+67.45	164.57'	948.581
D23	7+79.95	164.56'	948.523
D24	7+92.45	164.56'	948.468
D25	8+04.95	164.56'	948.417
D26	8+17.45	164.56'	948.368
D27	8+29.95	164.55'	948.319
D28	8+42.45	164.55'	948.268
D29	8+54.95	164.55'	948.214
D30	8+67.45	164.55'	948.158

	STA.	OFF.	ELEV.
D31	8+79.95	164.54'	948.102
D32	8+92.45	164.54'	948.051
D33	9+04.95	164.54'	948.009
D34	9+17.45	164.54'	947.971
D35	9+29.95	164.54'	947.933
D36	9+42.45	164.53'	947.895
D37	9+54.95	164.53'	947.858
D38	9+67.45	164.53'	947.821
D39	9+79.95	164.52'	947.785
D40	9+92.31	164.52'	947.749
E1	5+02.53	149.84'	949.650
E2	5+12.44	149.83'	949.620
F1	5+02.53	137.34'	949.837
F2	5+12.42	137.33'	949.801
G1	5+02.53	124.84'	950.025
G2	5+12.43	124.83'	949.982
H1	5+02.53	112.34'	950.186
H2	5+12.41	112.33'	950.149
I1	5+02.53	99.84'	950.340
I2	5+12.42	99.83'	950.316
J1	5+02.53	87.34'	950.494
J2	5+12.40	87.33'	950.483
K1	5+02.53	74.84'	950.648
K2	5+12.41	74.83'	950.649
L1	5+02.53	62.34'	950.806
L2	5+12.40	62.33'	950.809
M1	5+02.53	49.84'	950.965
M2	5+12.40	49.83'	950.969
N1	5+02.53	37.43'	951.123
N2	5+12.40	37.43'	951.127

NOTE: NOT A STANDARD SHEET AS
SHOWN ON SHEET G-061



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SHEET CONTENTS
JOINT ELEVATION
DETAIL SHEET 1 OF 1

SHEET NO. 50 of 76

CP-551

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

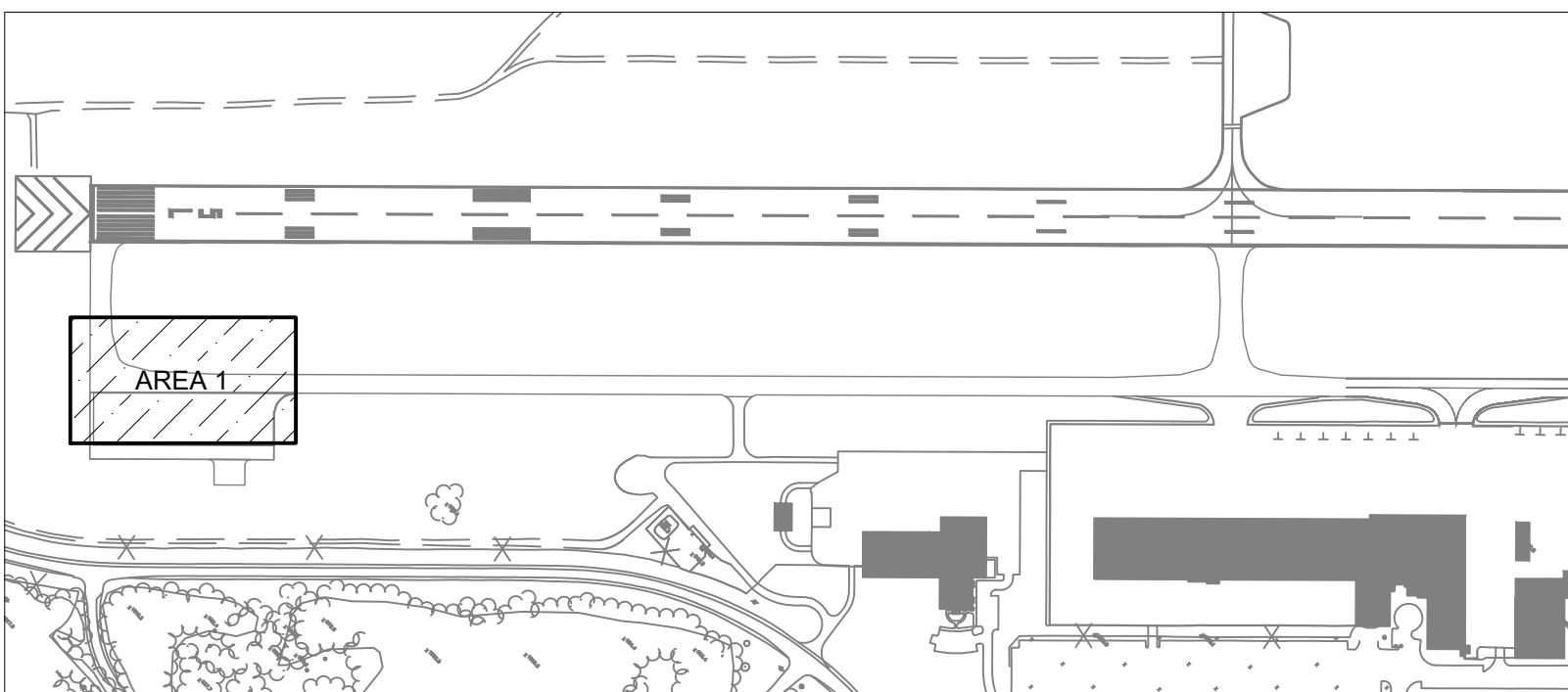
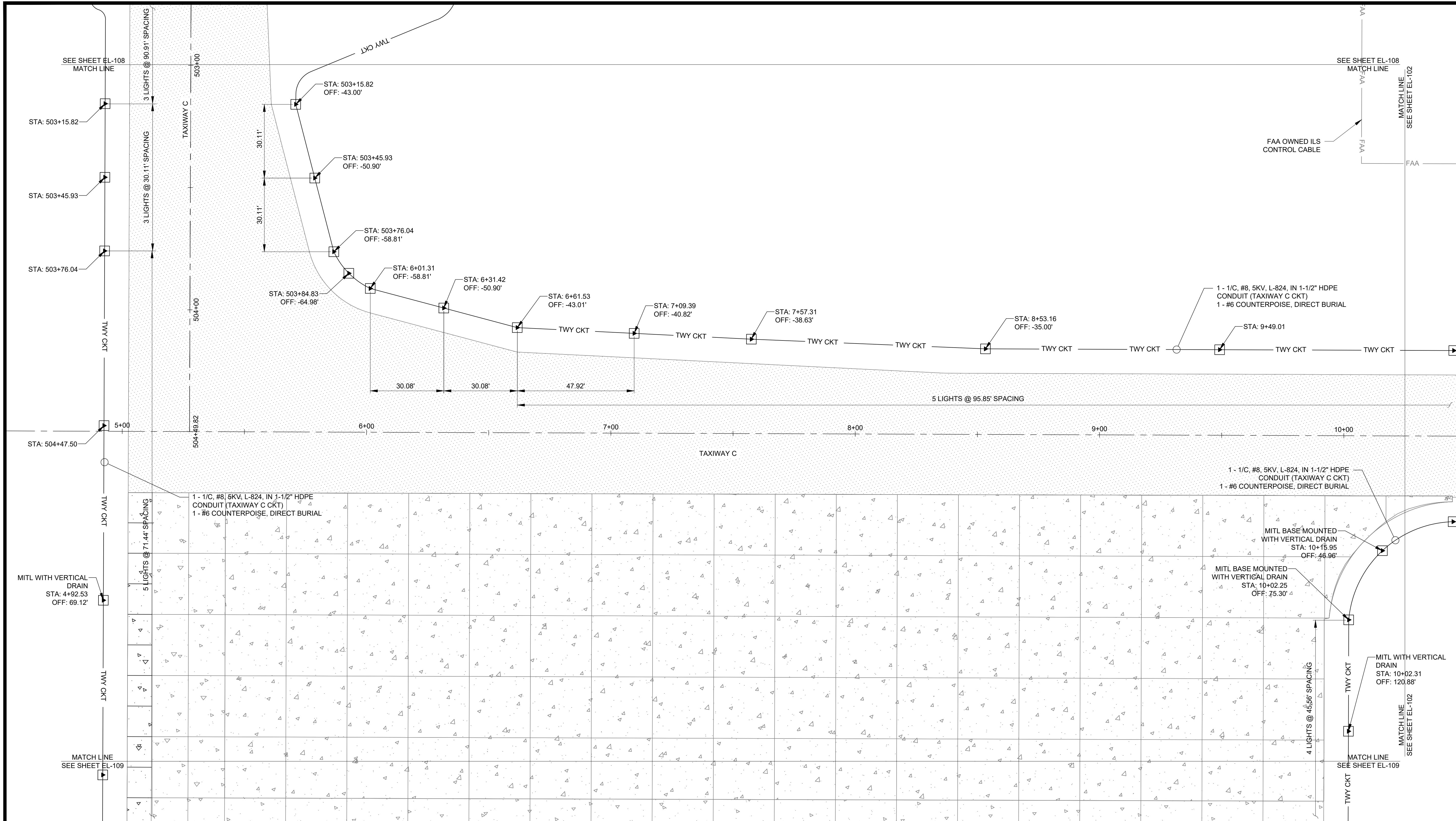
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 1

SHEET NO. 51 of 76

EL-101

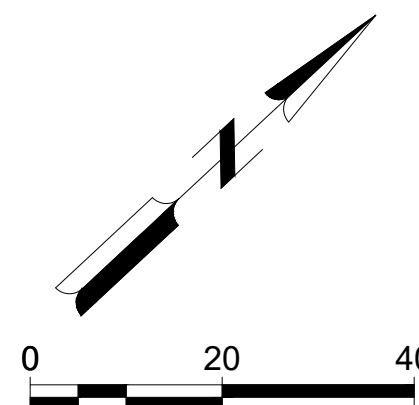


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN		SD		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN		E		EXISTING DUCT BANK
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL		TWY CKT		PROPOSED TAXIWAY C CKT
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL		RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

NOTES:

- ALL PROPOSED MITLS SHALL HAVE HORIZONTAL DRAINS UNLESS OTHERWISE NOTED.
- ALL PROPOSED MITLS SHALL BE OFFSET 35' FROM TAXIWAY CENTERLINE UNLESS OTHERWISE NOTED.



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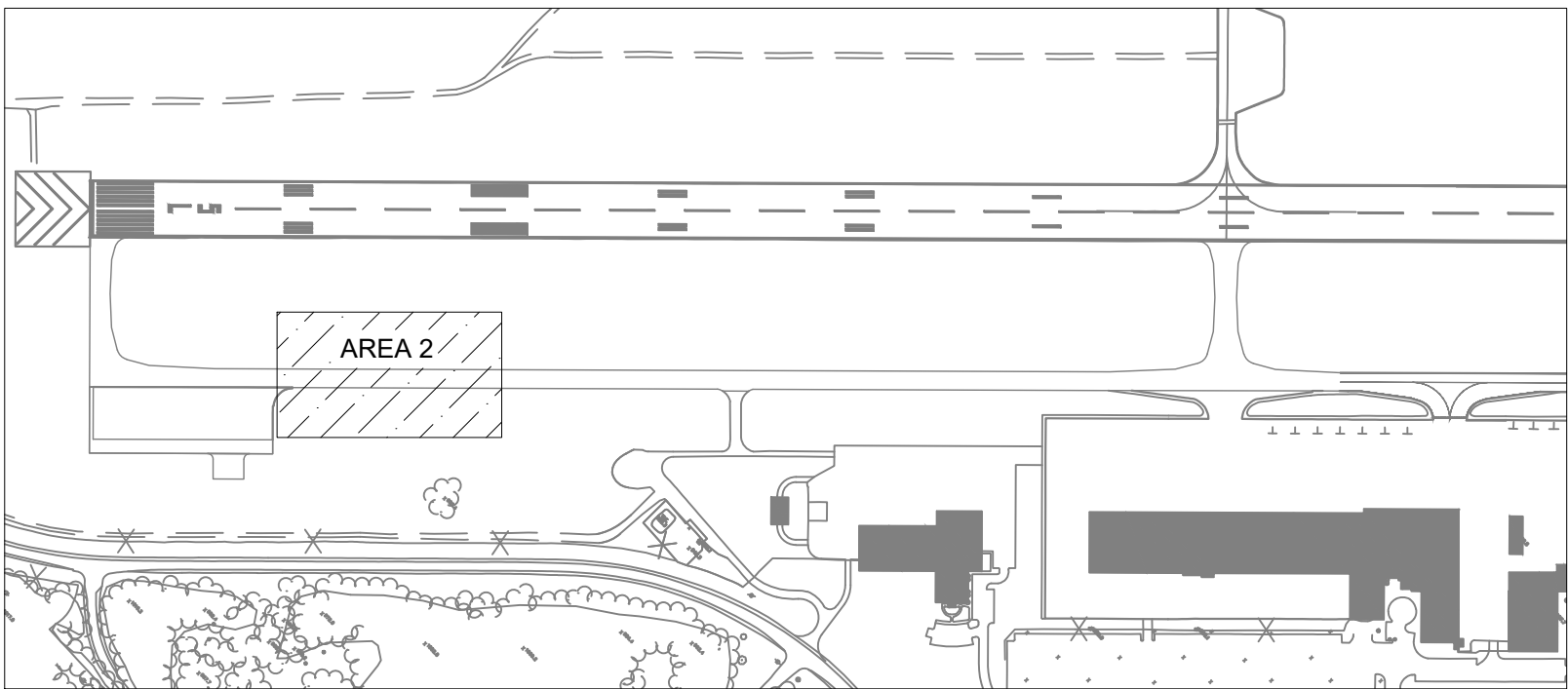
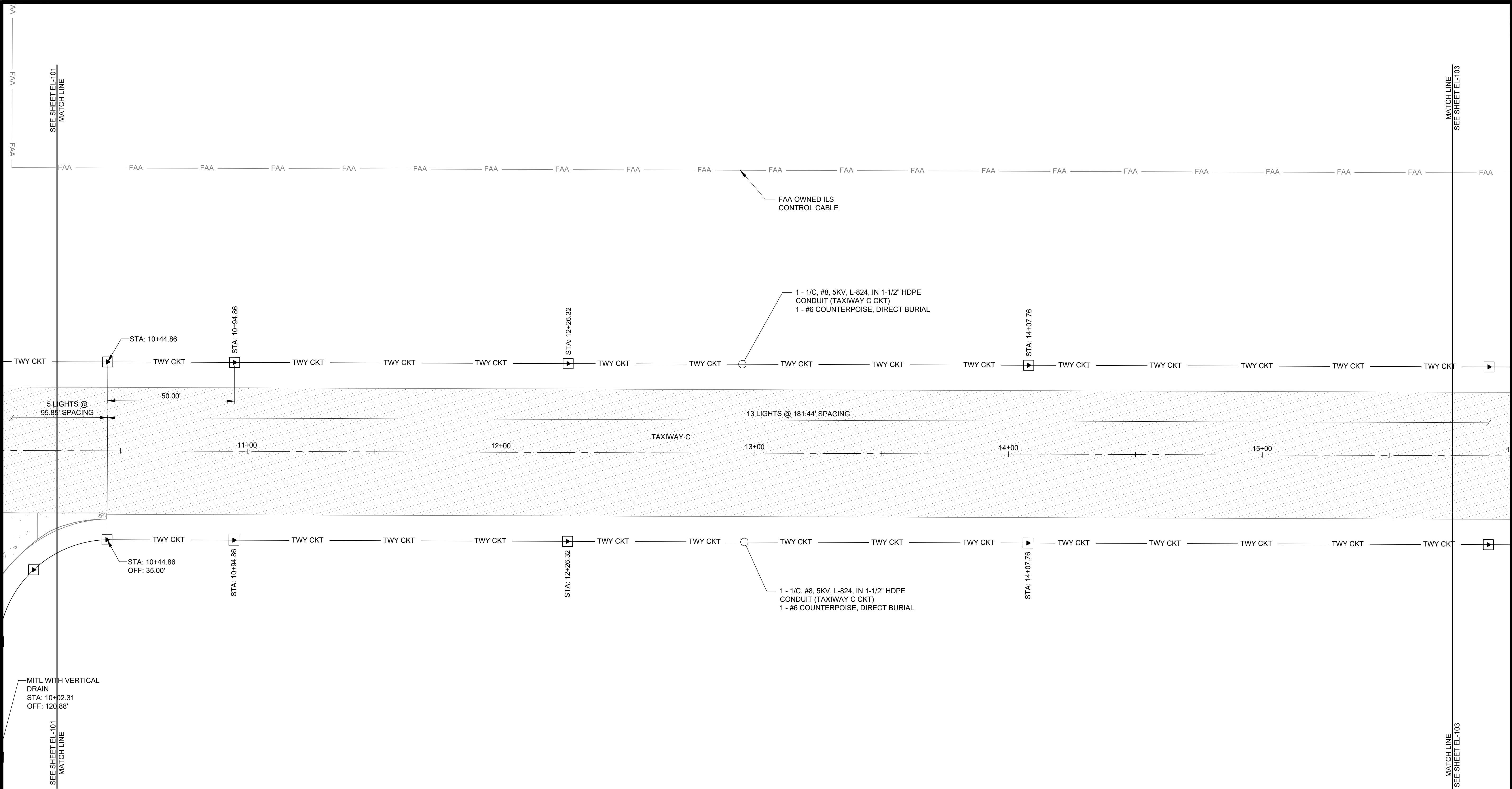
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 2

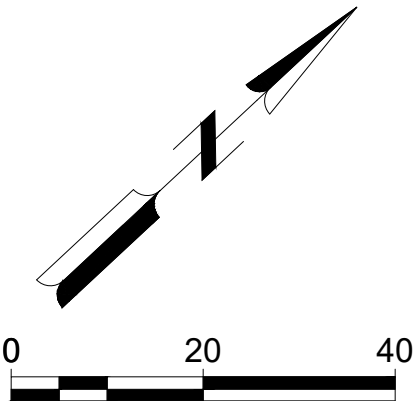
SHEET NO 52 of 76

EL-102



LEGEND					
	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			TWY CKT		PROPOSED TAXIWAY C CKT
			RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

- NOTES:**
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 - ALL PROPOSED MITLS SHALL BE OFFSET 35' FROM TAXIWAY CENTERLINE UNLESS OTHERWISE NOTED.



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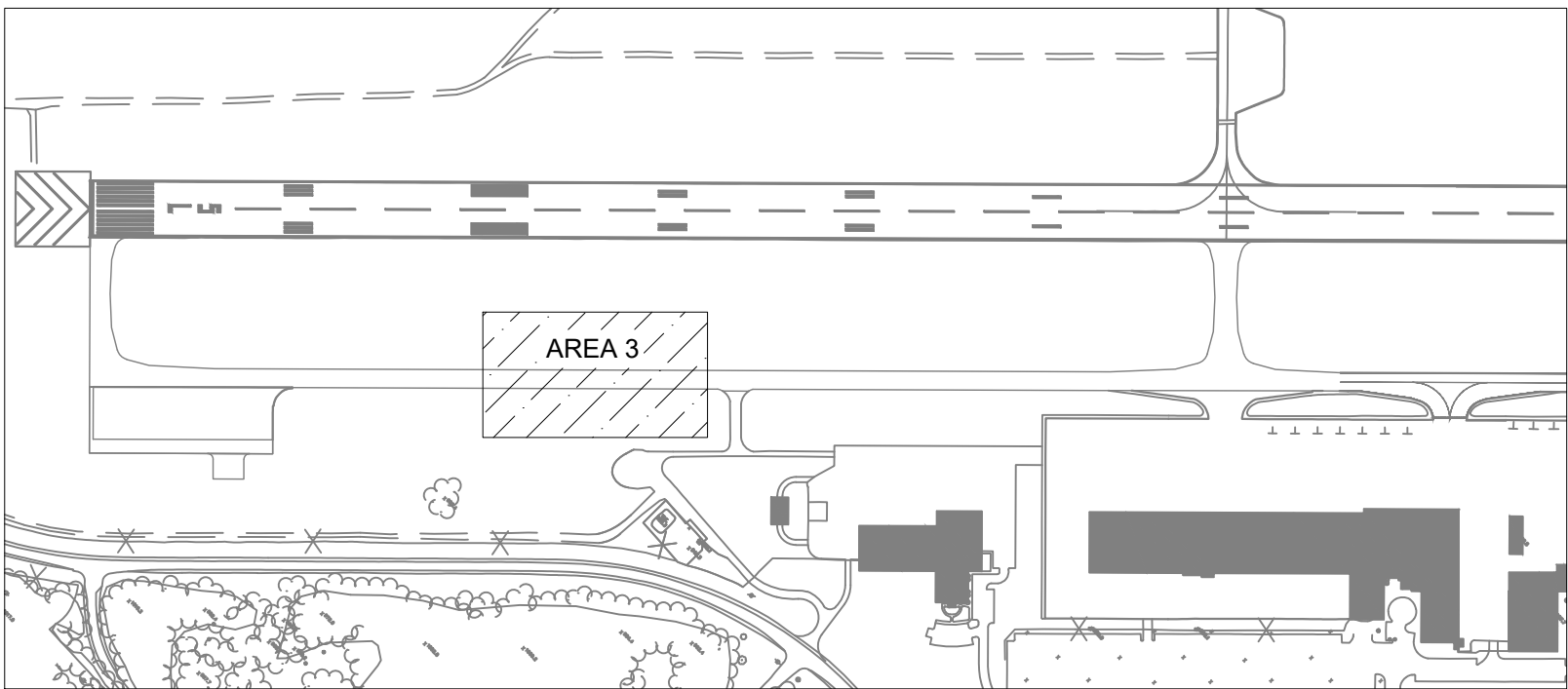
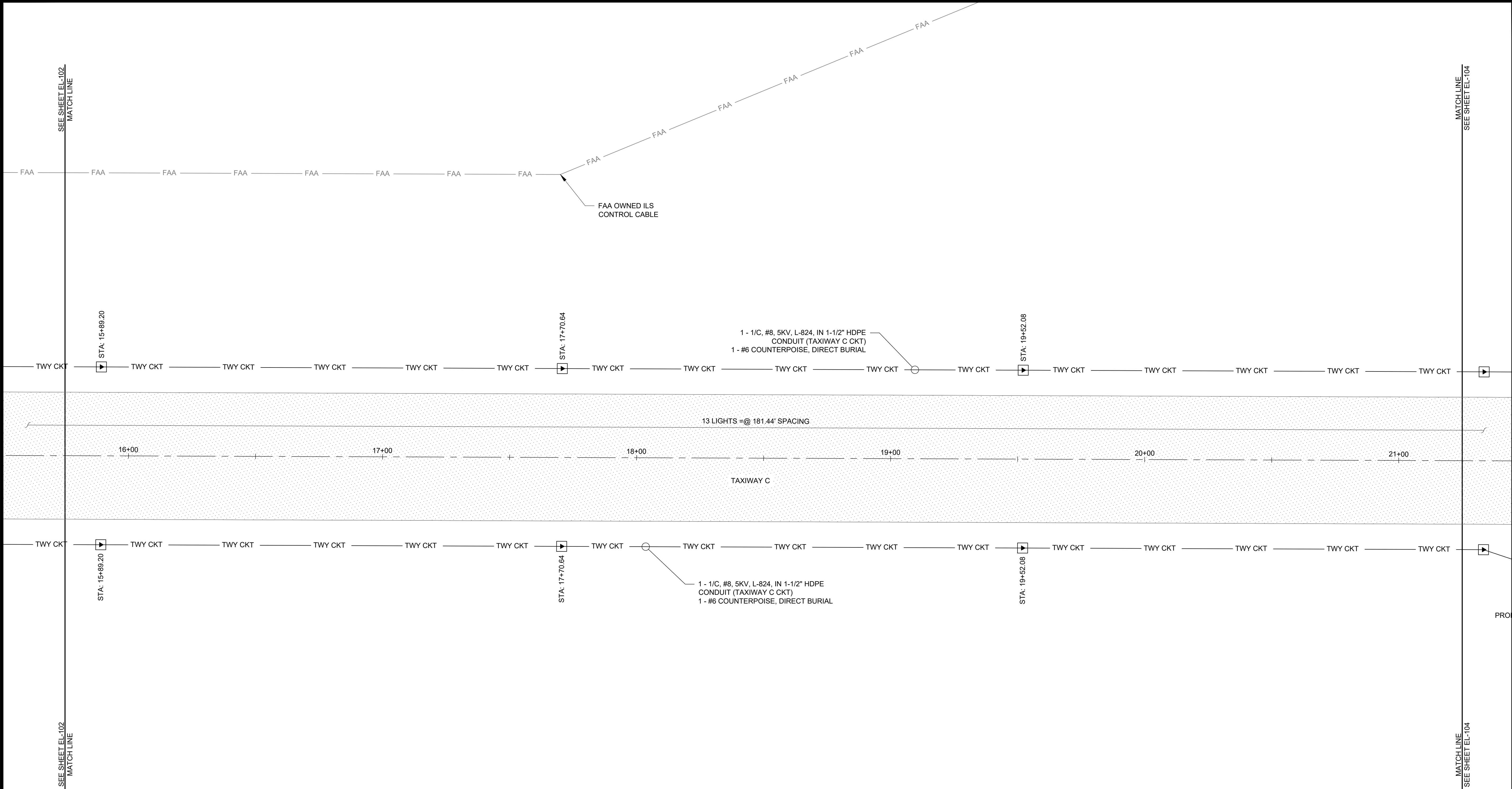
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 3

SHEET NO 53 of 76

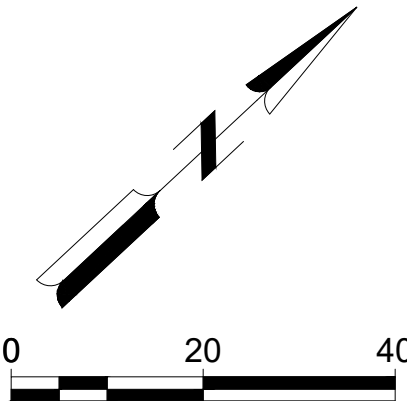
EL-103



LEGEND					
	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			TWY CKT		PROPOSED TAXIWAY C CKT
			RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

NOTES:

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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

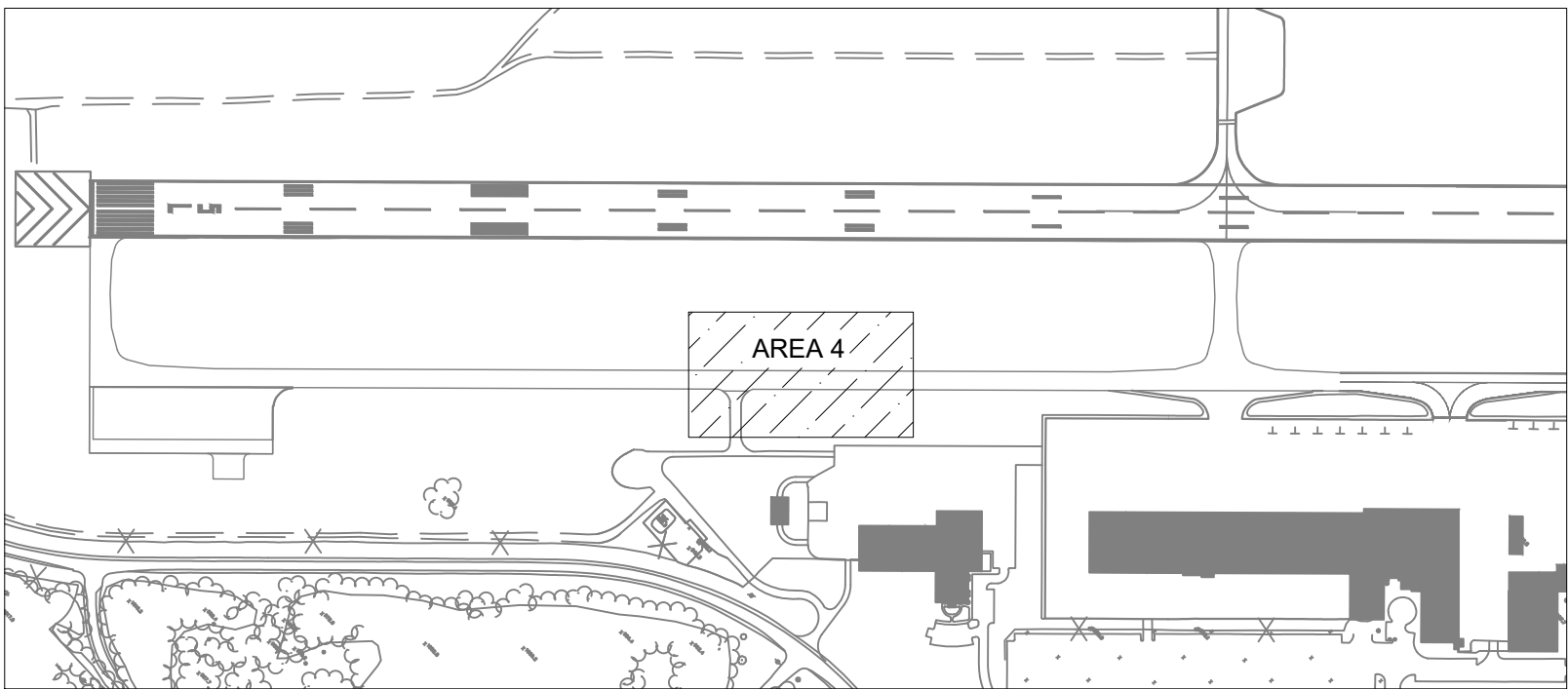
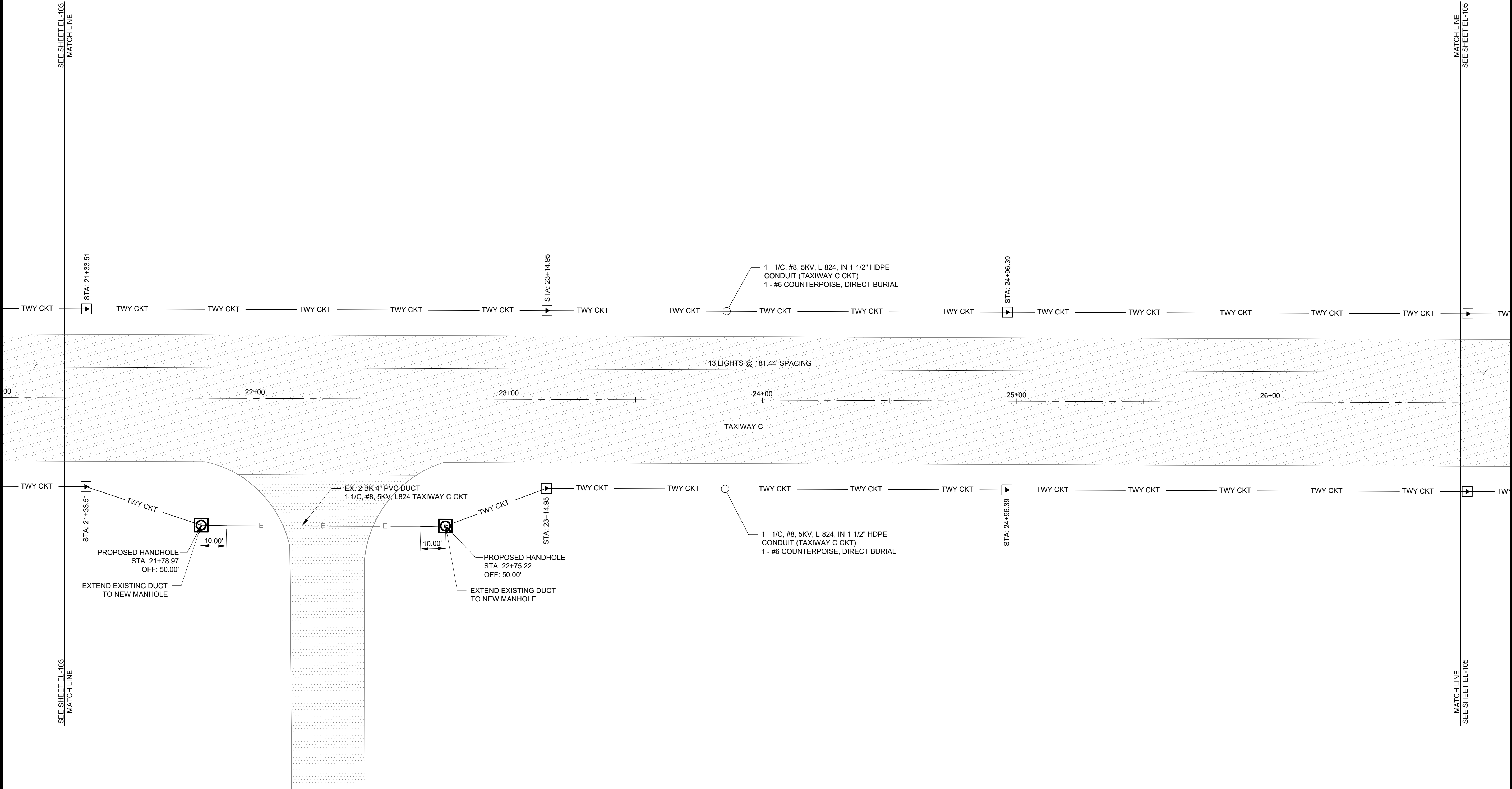
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DRAWN BY: ALP
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 4

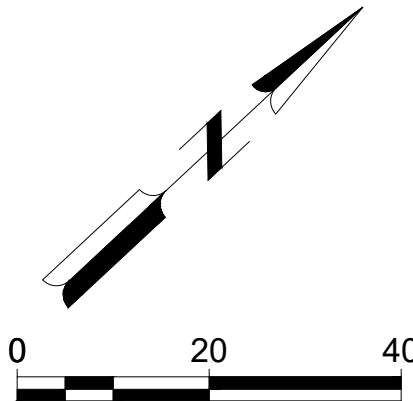
SHEET NO 54 of 76

EL-104



LEGEND					
	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			TWY CKT		PROPOSED TAXIWAY C CKT
			RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

- NOTES:**
- ALL PROPOSED MITLS SHALL HAVE HORIZONTAL DRAINS UNLESS OTHERWISE NOTED.
 - ALL PROPOSED MITLS SHALL BE OFFSET 35' FROM TAXIWAY CENTERLINE UNLESS OTHERWISE NOTED.



WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

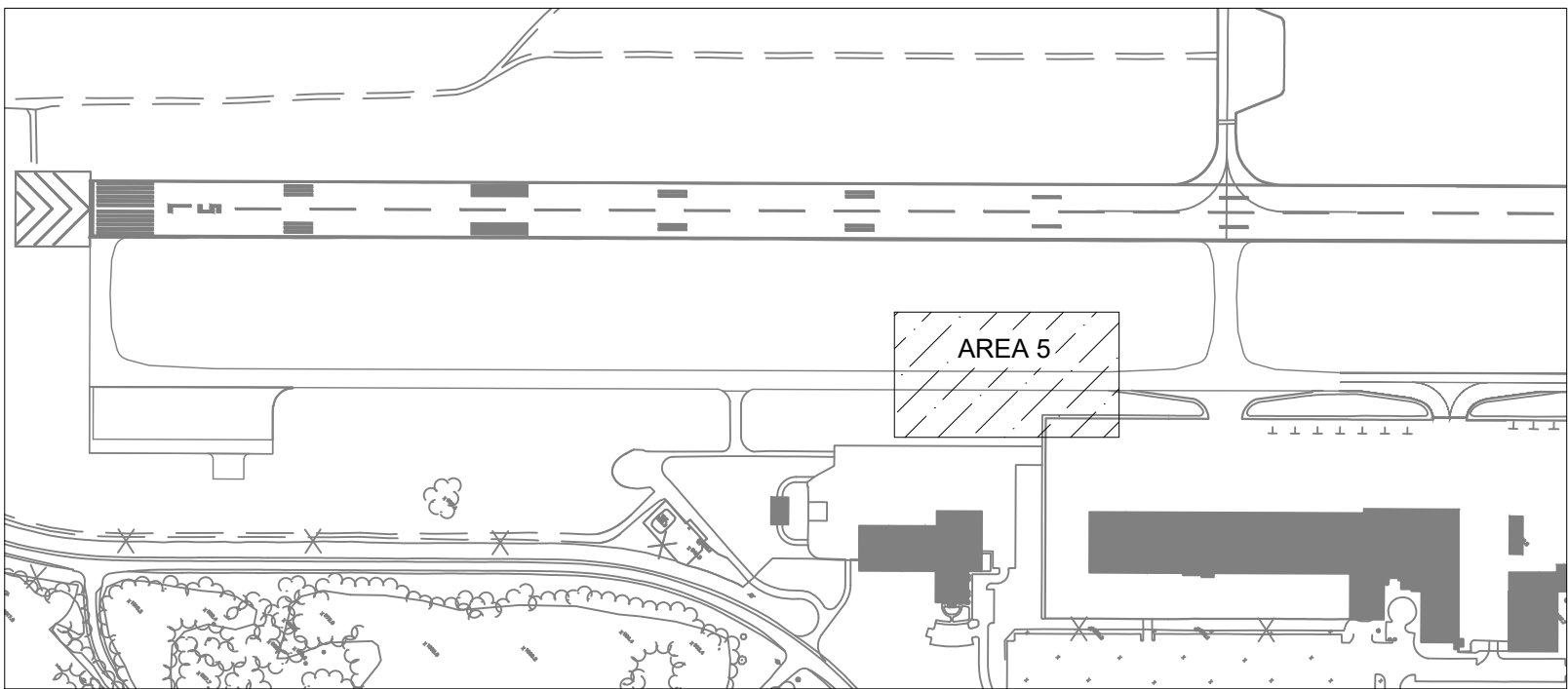
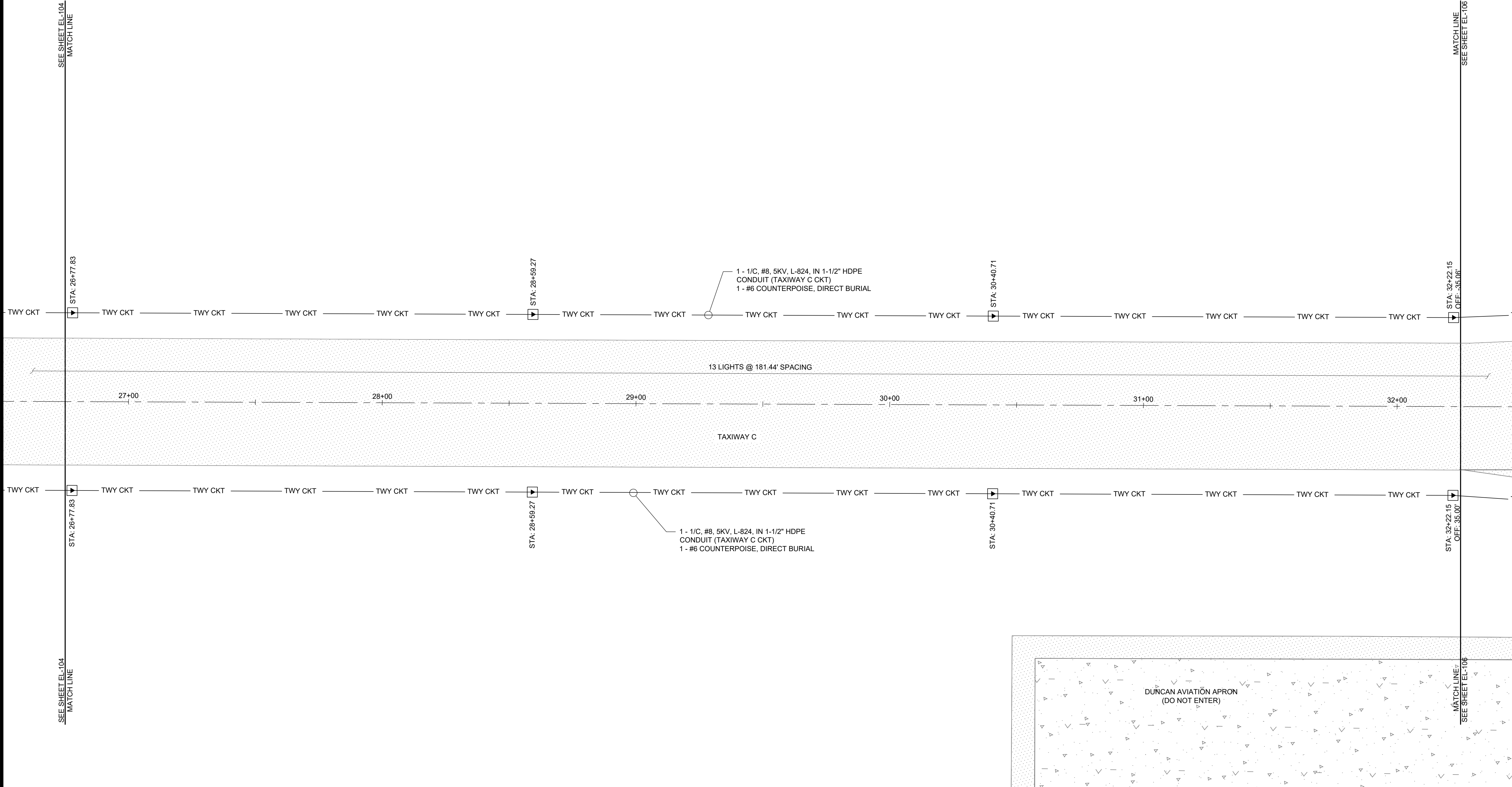
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 5

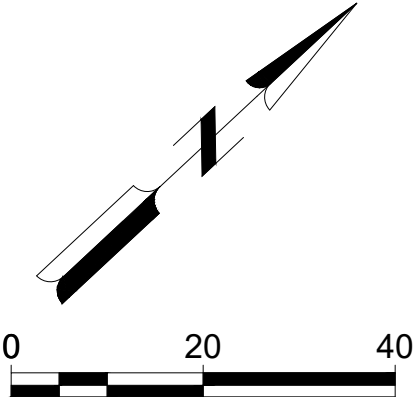
SHEET NO 55 of 76

EL-105



LEGEND					
	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL
			SD		EXISTING RCCP
			E		EXISTING DUCT BANK
			TWY CKT		PROPOSED TAXIWAY C CKT
			RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

- NOTES:
- ALL PROPOSED MITLS SHALL HAVE HORIZONTAL DRAINS UNLESS OTHERWISE NOTED.
 - ALL PROPOSED MITLS SHALL BE OFFSET 35' FROM TAXIWAY CENTERLINE UNLESS OTHERWISE NOTED.



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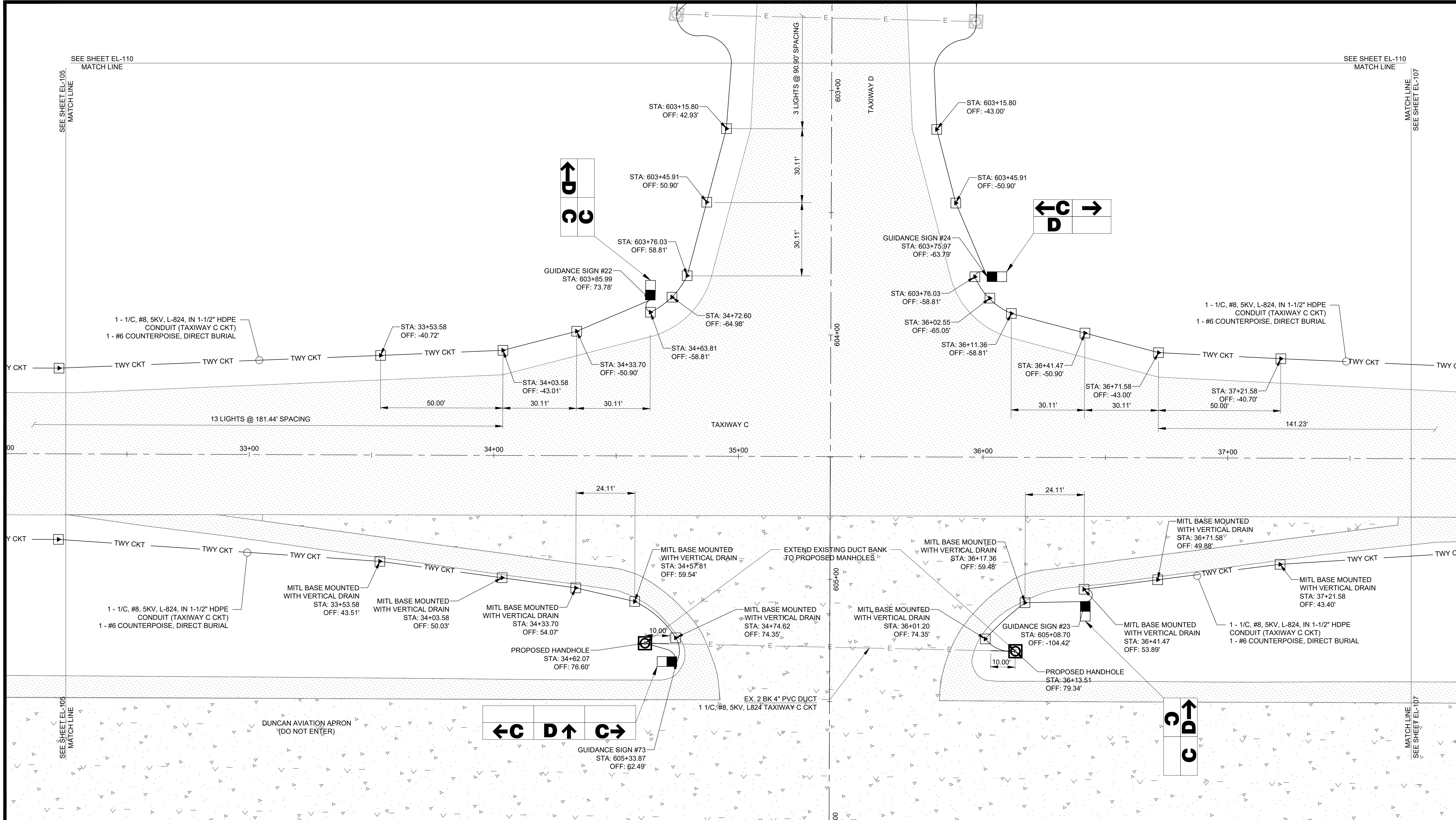
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 6

SHEET NO 56 of 76

EL-106

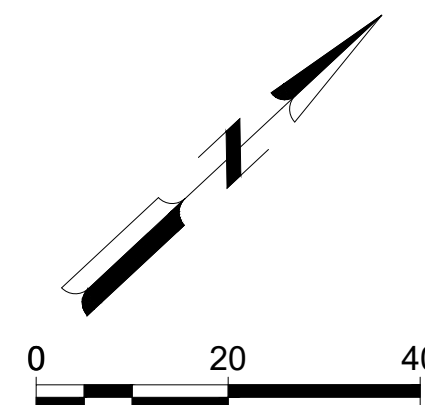


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN		EXISTING DUCT BANK
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL		PROPOSED TAXIWAY C CKT
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL		PROPOSED RWY 5L/23R SOUTH CKT

NOTES:

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- ALL PROPOSED MITLS SHALL BE OFFSET 35' FROM TAXIWAY CENTERLINE UNLESS OTHERWISE NOTED.



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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

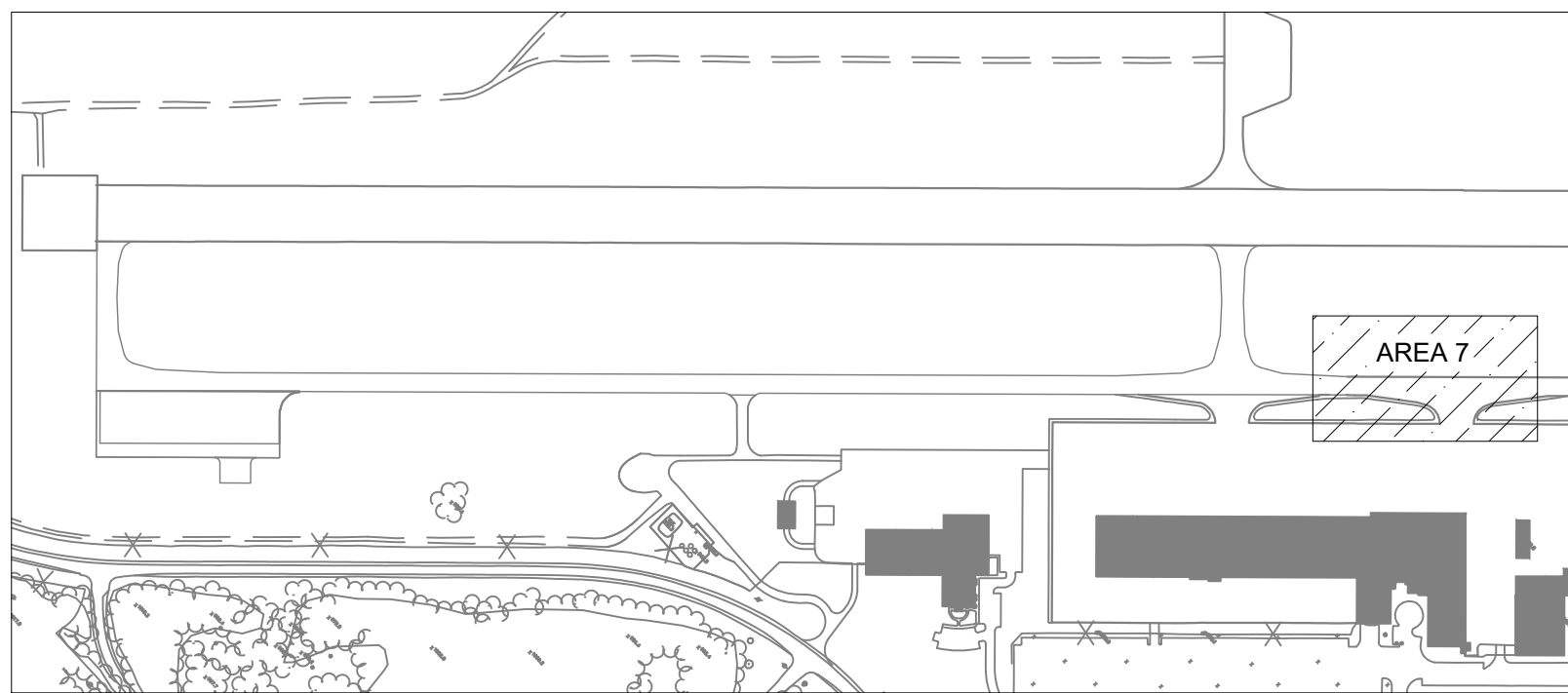
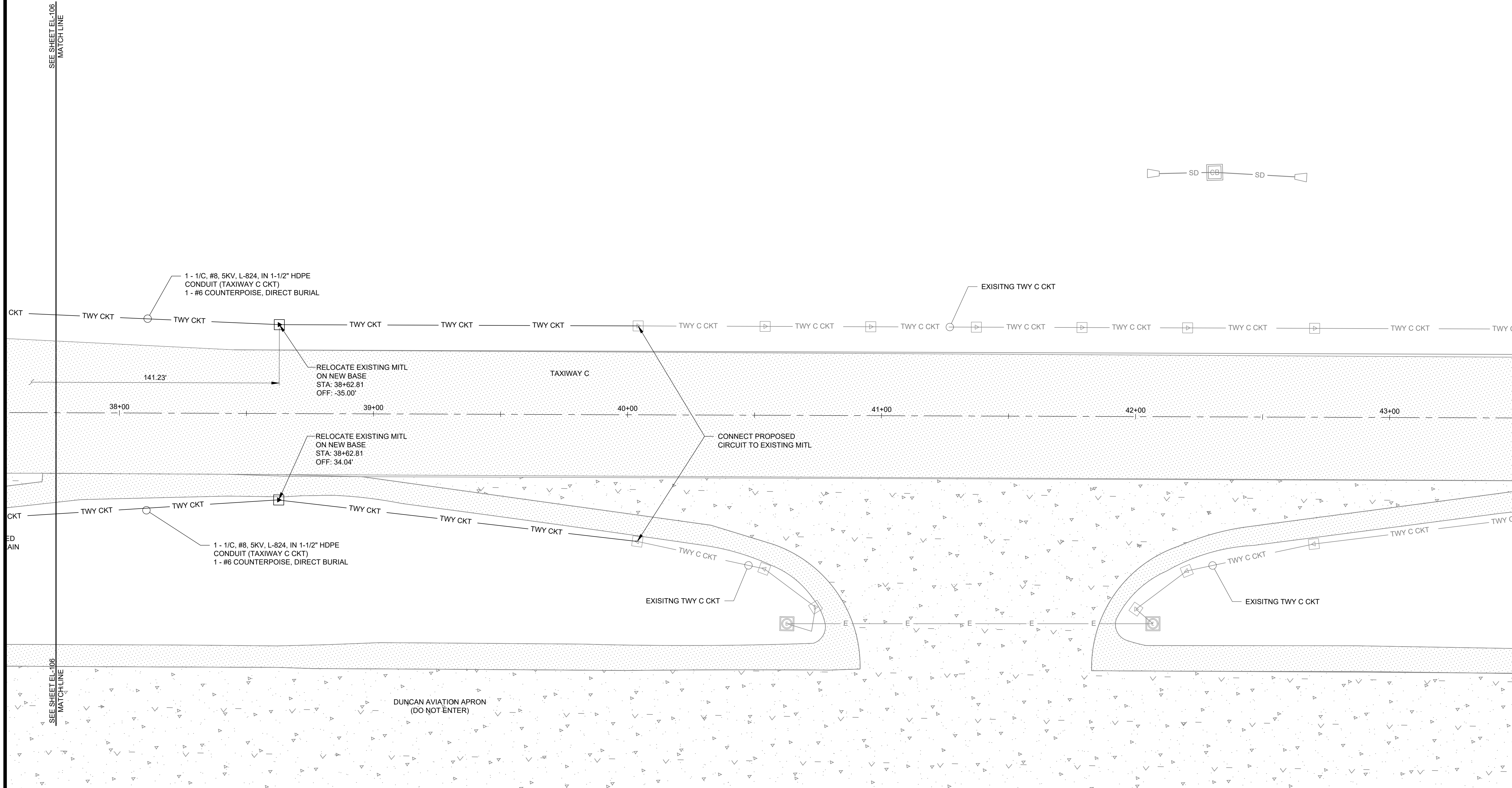
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 7

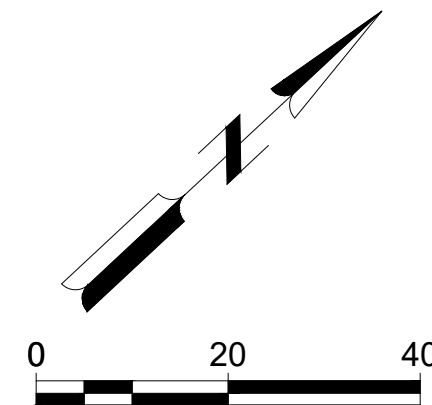
SHEET NO 57 of 76

EL-107



LEGEND					
	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL
					EXISTING RCCP
					EXISTING DUCT BANK
					PROPOSED TAXIWAY C CKT
					PROPOSED RWY 5L/23R SOUTH CKT

- NOTES:**
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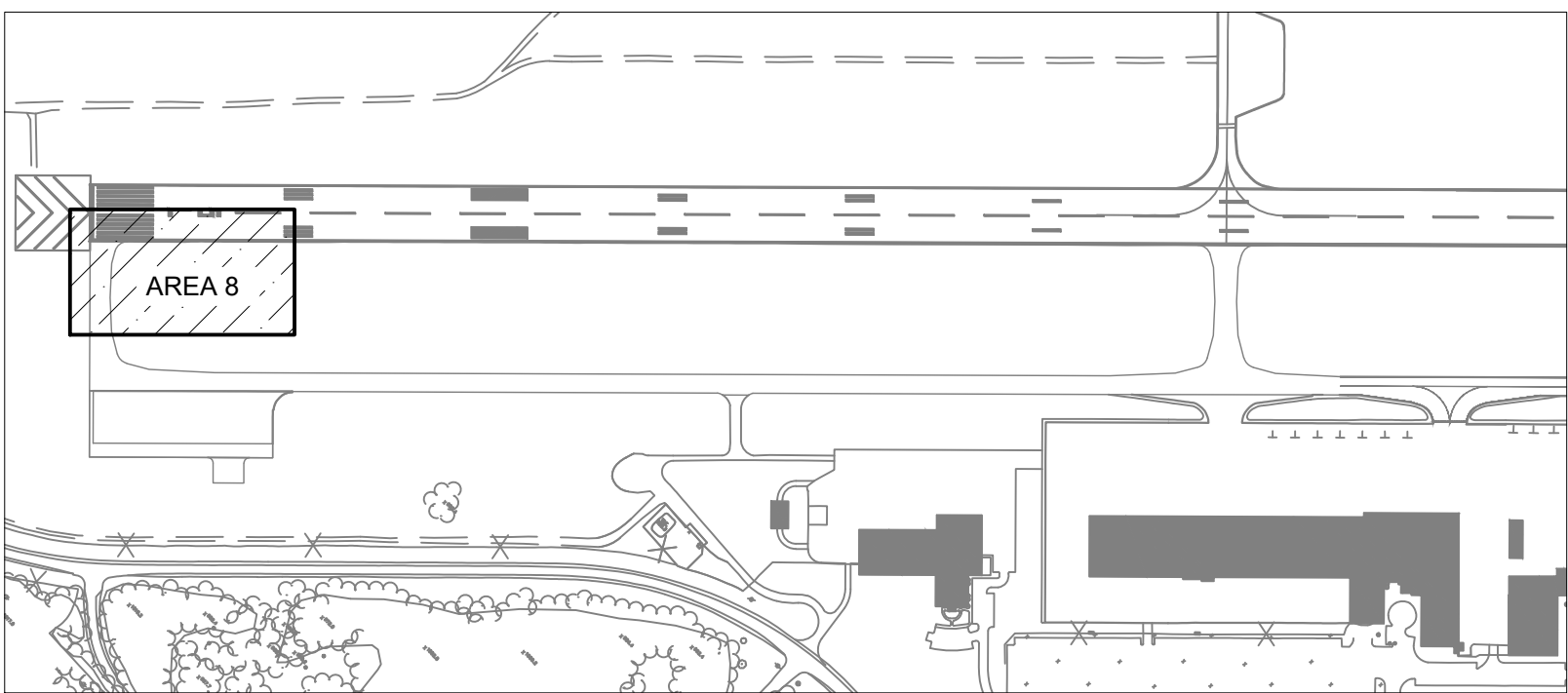
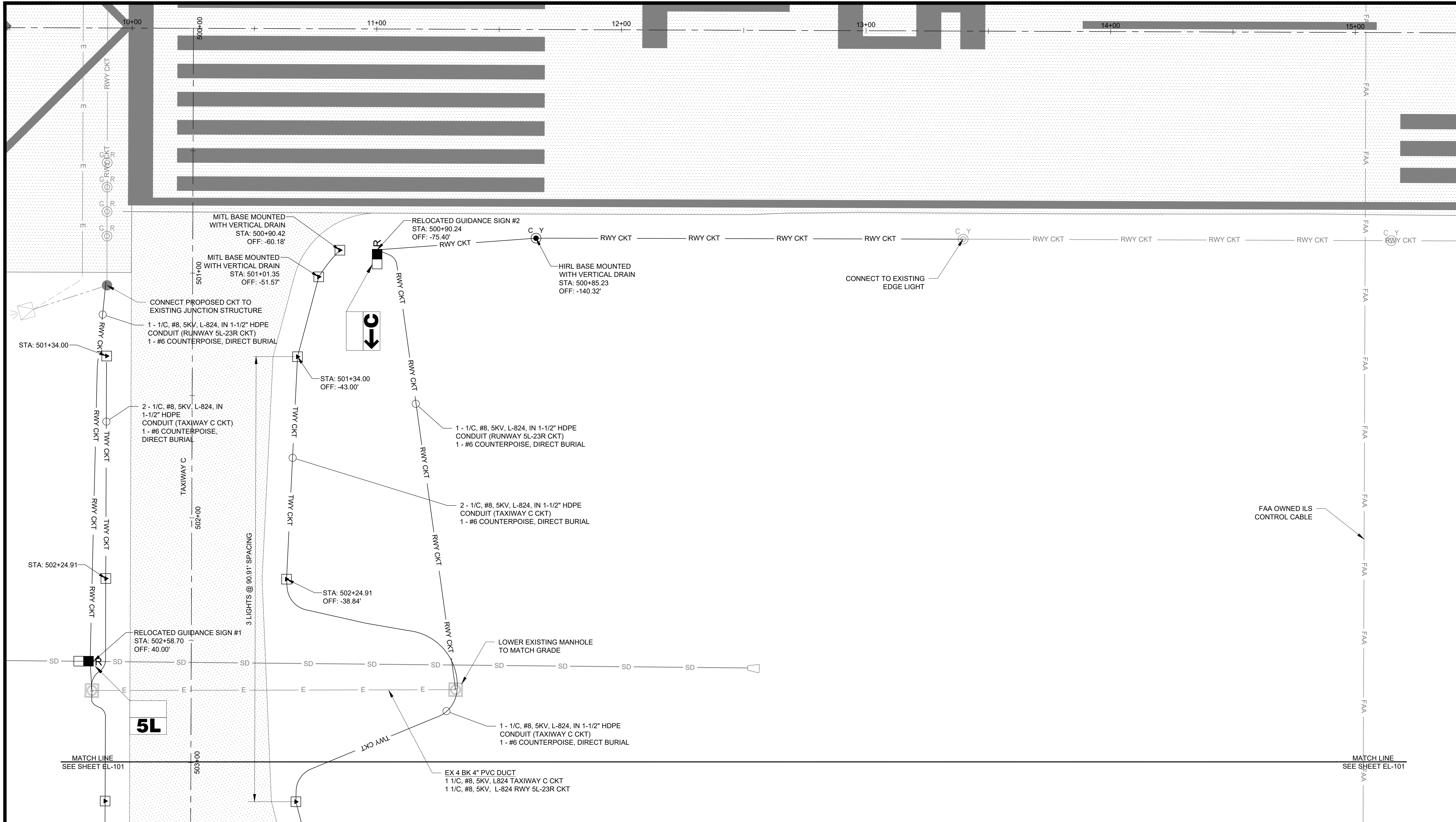
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 8

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EL-108

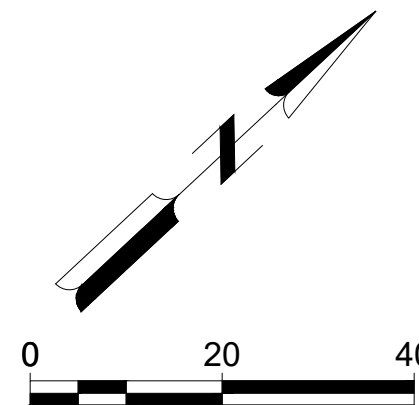


LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN		EXISTING DUCT BANK
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL		PROPOSED TAXIWAY C CKT
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL		PROPOSED RWY 5L/23R SOUTH CKT

NOTES:

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2
BATTLE CREEK, MI

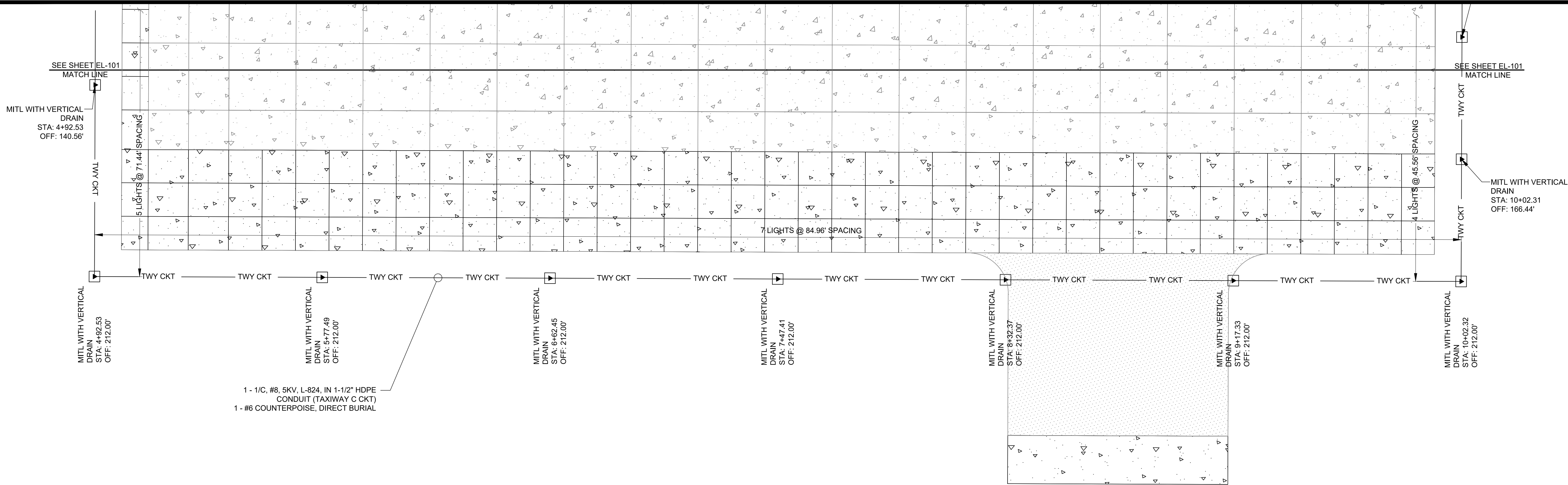
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 9

SHEET NO 59 of 76

EL-109



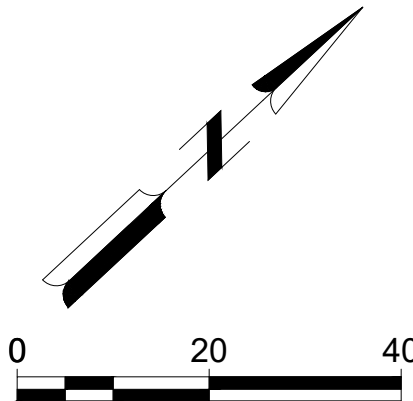
1 - 1/C, #8, 5KV, L-824, IN 1-1/2" HDPE
CONDUIT (TAXIWAY C CKT)
1 - #6 COUNTERPOISE, DIRECT BURIAL

LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN		SD		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN		E		EXISTING DUCT BANK
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL		TWY CKT		PROPOSED TAXIWAY C CKT
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL		RWY CKT		PROPOSED RWY 5L/23R SOUTH CKT

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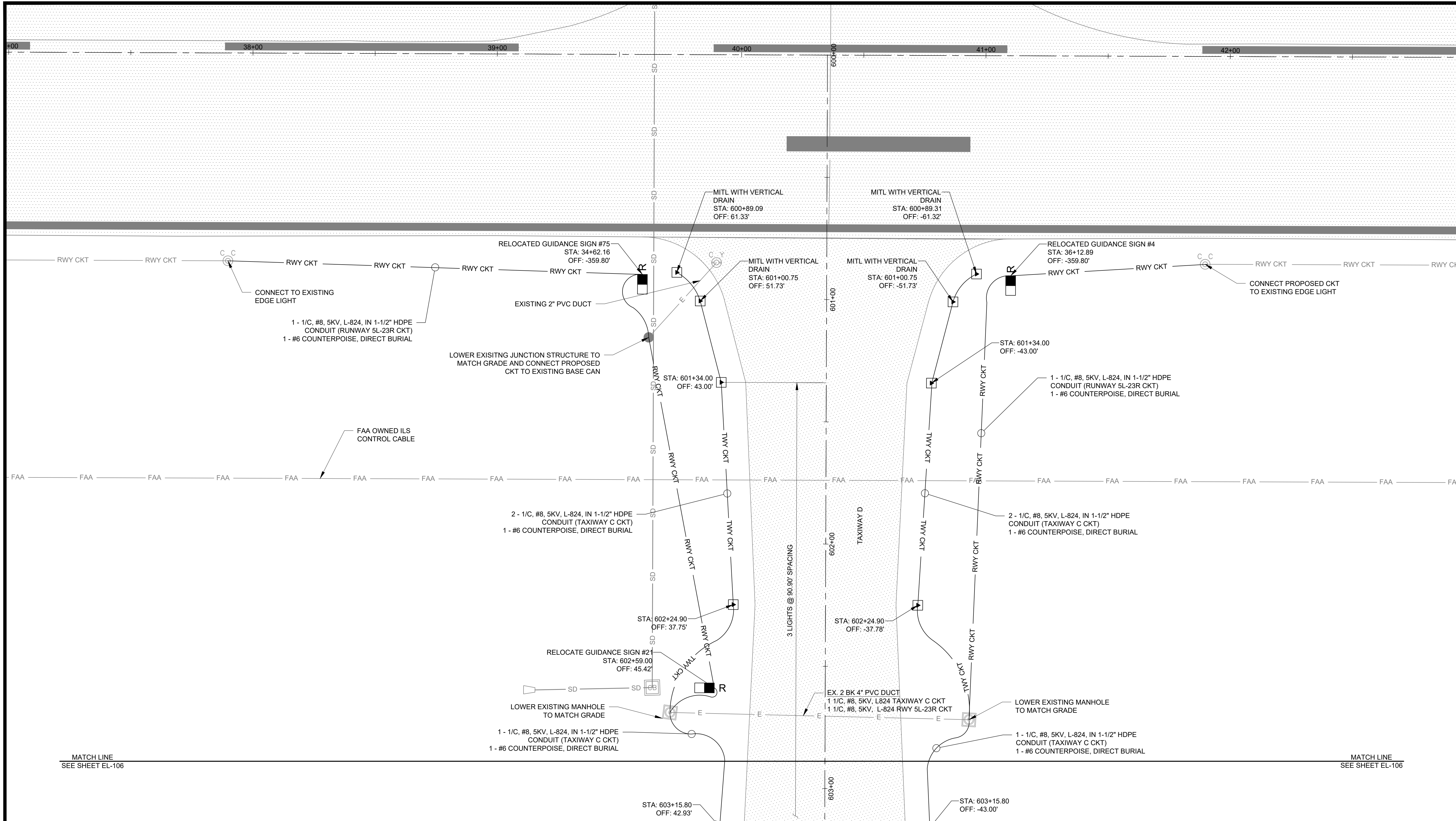
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SHEET CONTENTS
ELECTRICAL LAYOUT
PLAN - AREA 10

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EL-110

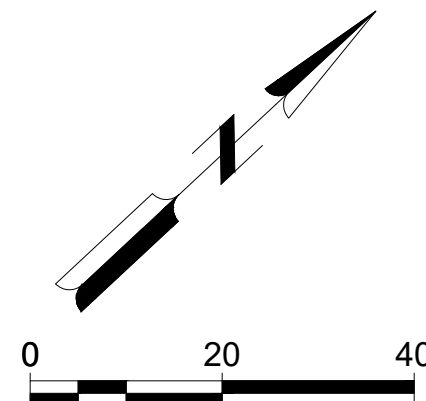


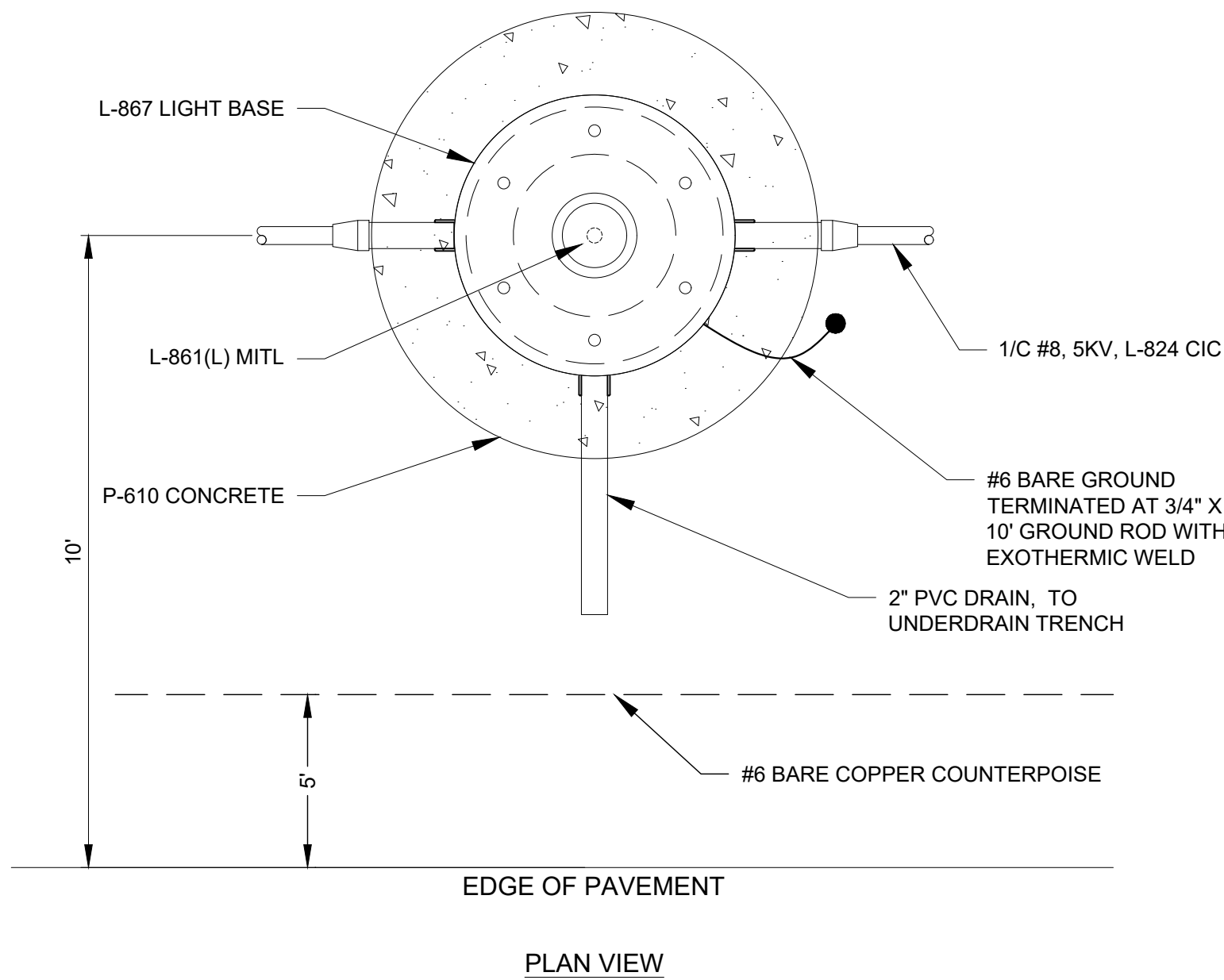
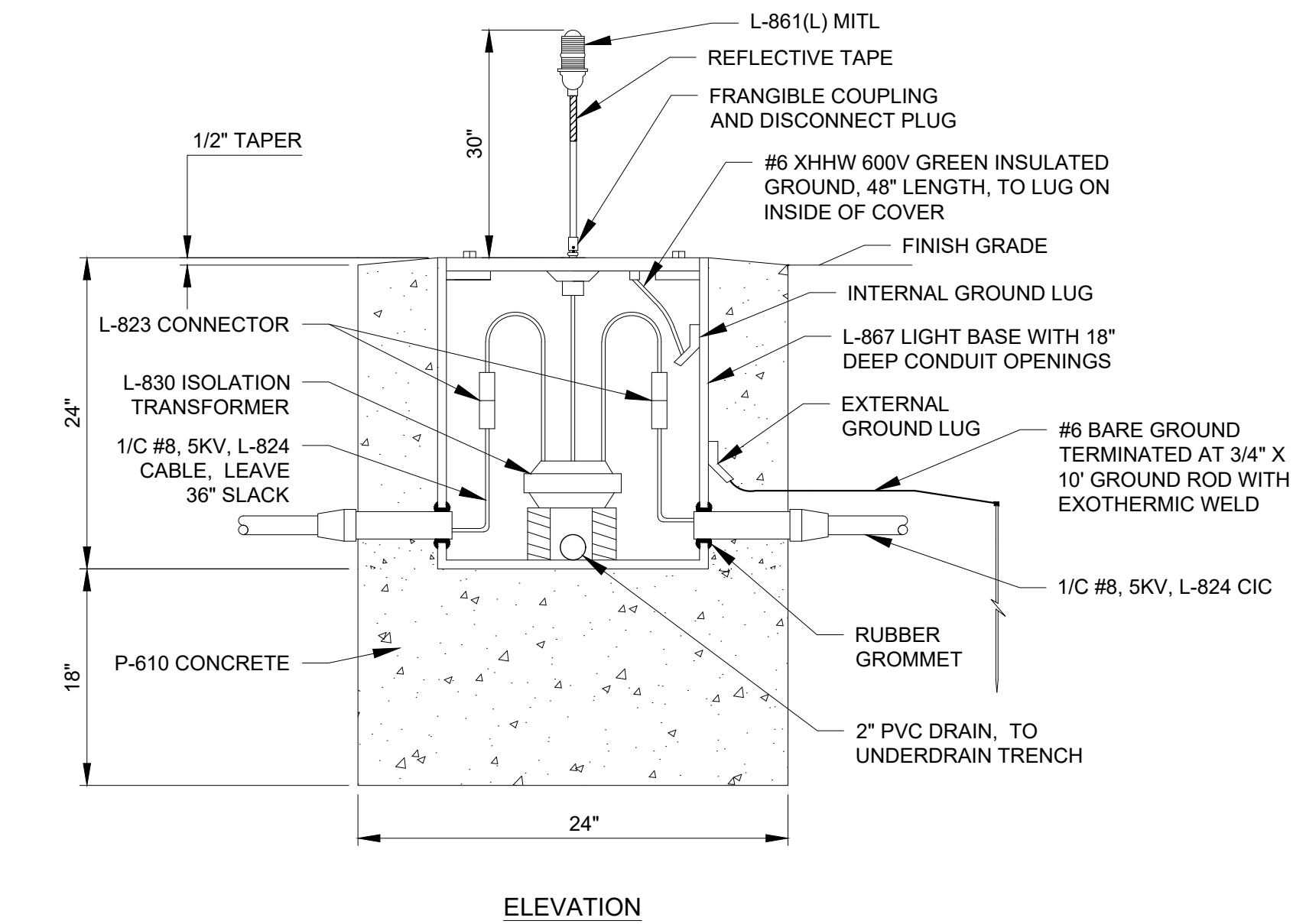
LEGEND

	EXISTING ASPHALT PAVEMENT		EXISTING ELECTRIC MANHOLE		PROPOSED GUIDANCE SIGN		EXISTING RCCP
	EXISTING PCC PAVEMENT		EXISTING BASE MOUNTED HIRL		RELOCATED GUIDANCE SIGN		EXISTING DUCT BANK
	EXISTING BASE MOUNTED MITL		EXISTING INSET HIRL		PROPOSED BASE MOUNTED MITL		PROPOSED TAXIWAY C CKT
	EXISTING FLARED END SECTION		PROPOSED ELECTRIC MANHOLE		PROPOSED BASE MOUNTED HIRL		PROPOSED RWY 5L/23R SOUTH CKT

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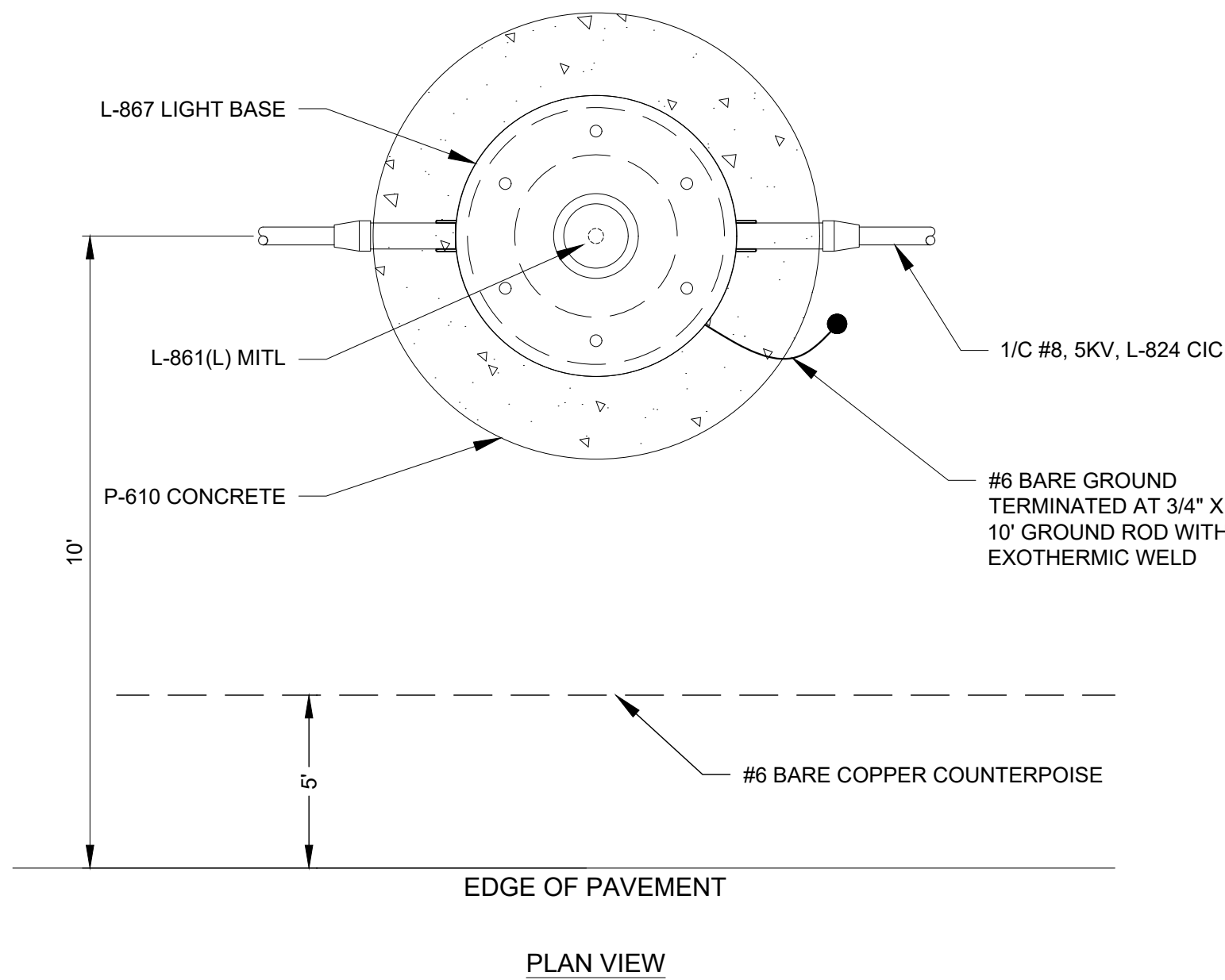
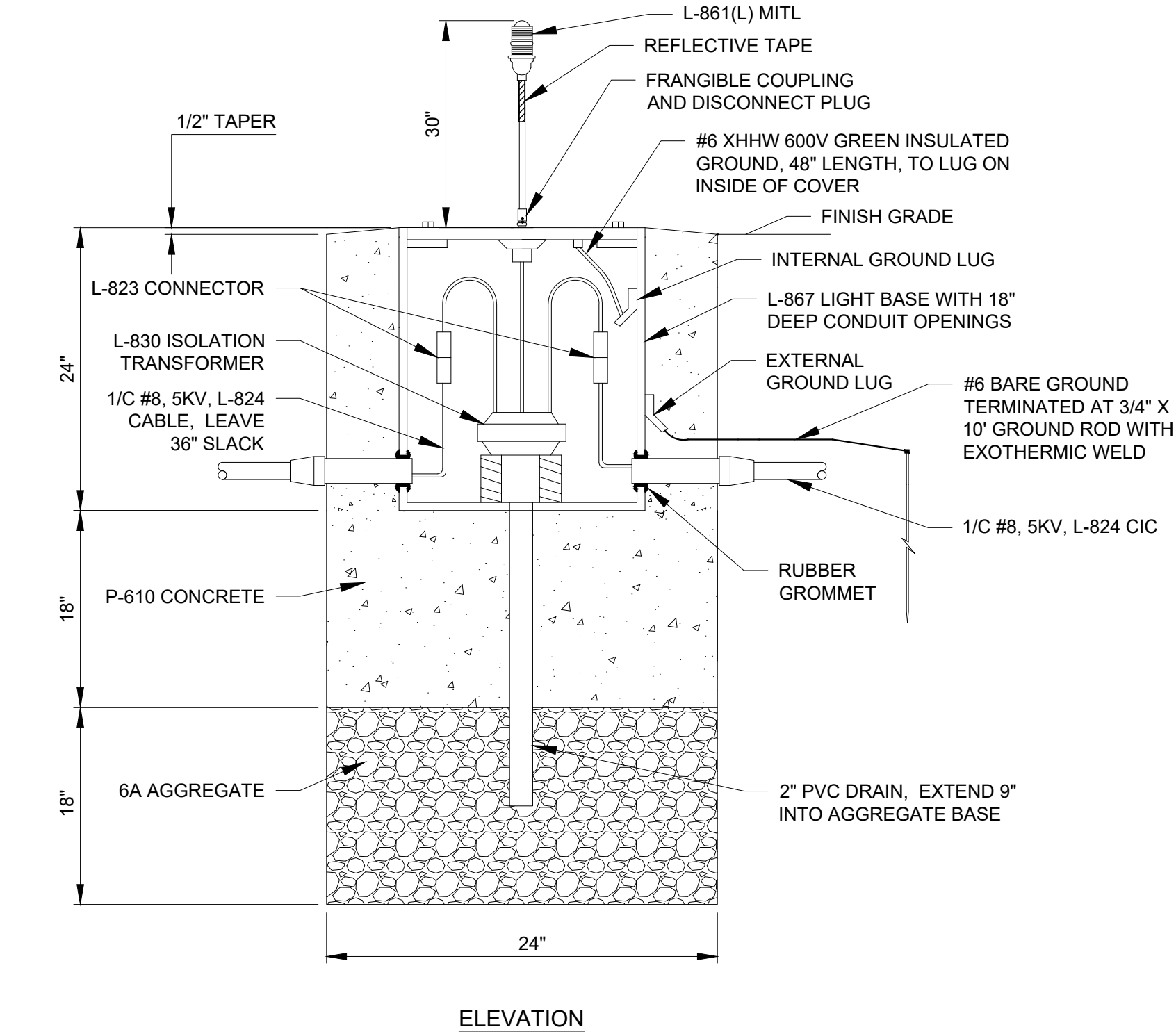




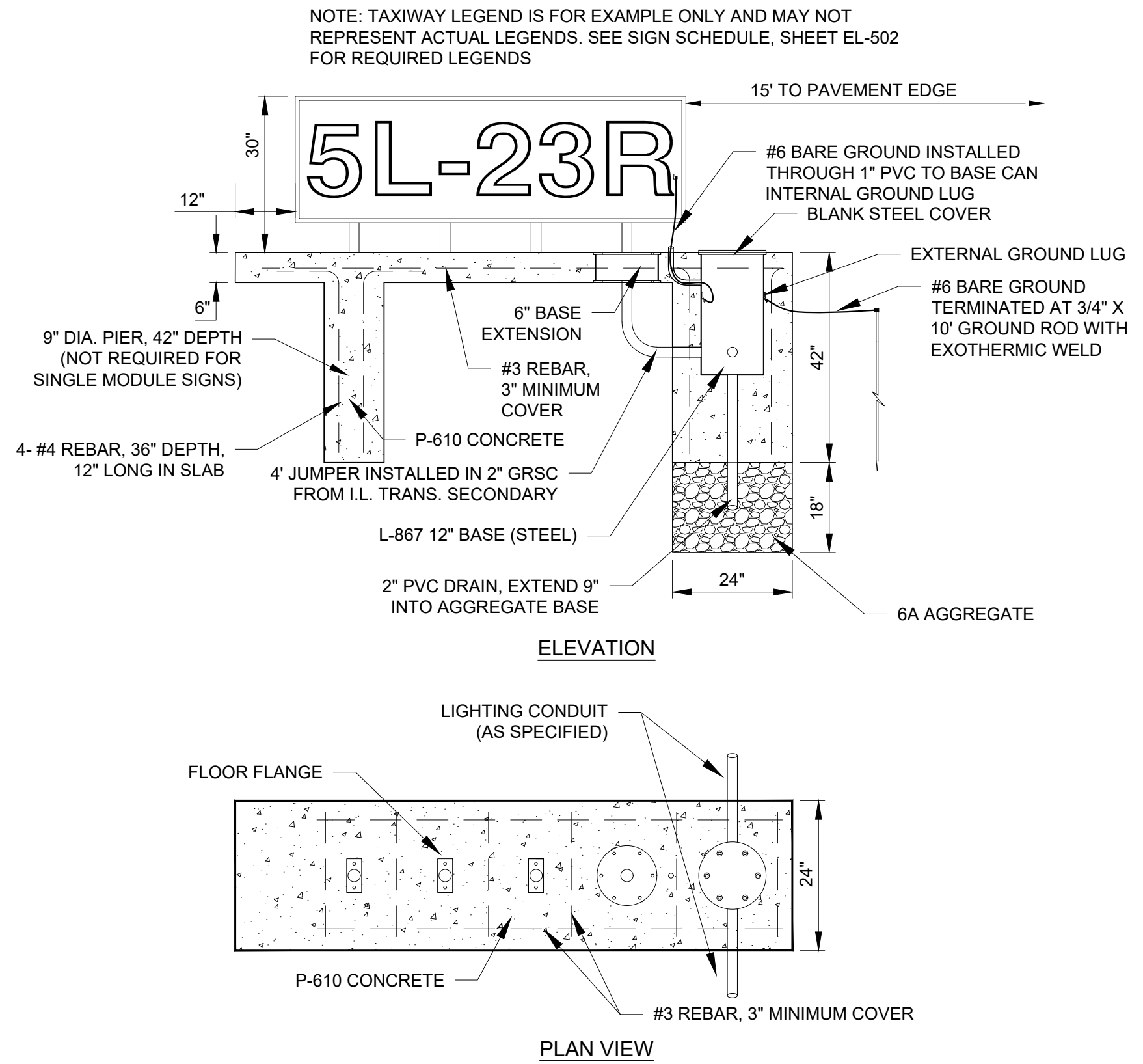
3 BASE MOUNTED TAXIWAY LIGHT WITH HORIZONTAL DRAIN
NOT TO SCALE

NOTES (ALL LIGHT BASES):

1. ALL BOLTS SHALL BE STAINLESS STEEL.
2. APPLY ANTI-SIZE COMPOUND TO ALL MALE THREADS.
3. PROVIDE FORMS AS NECESSARY FOR CONCRETE PLACEMENT. COST INCIDENTAL TO THE INSTALLATION.
4. ALL NEW BASE CANS SHALL HAVE INTERNAL AND EXTERNAL GROUND LUGS.



2 BASE MOUNTED TAXIWAY LIGHT WITH VERTICAL DRAIN
NOT TO SCALE



1 TAXIWAY GUIDANCE SIGN INSTALLATION
NOT TO SCALE

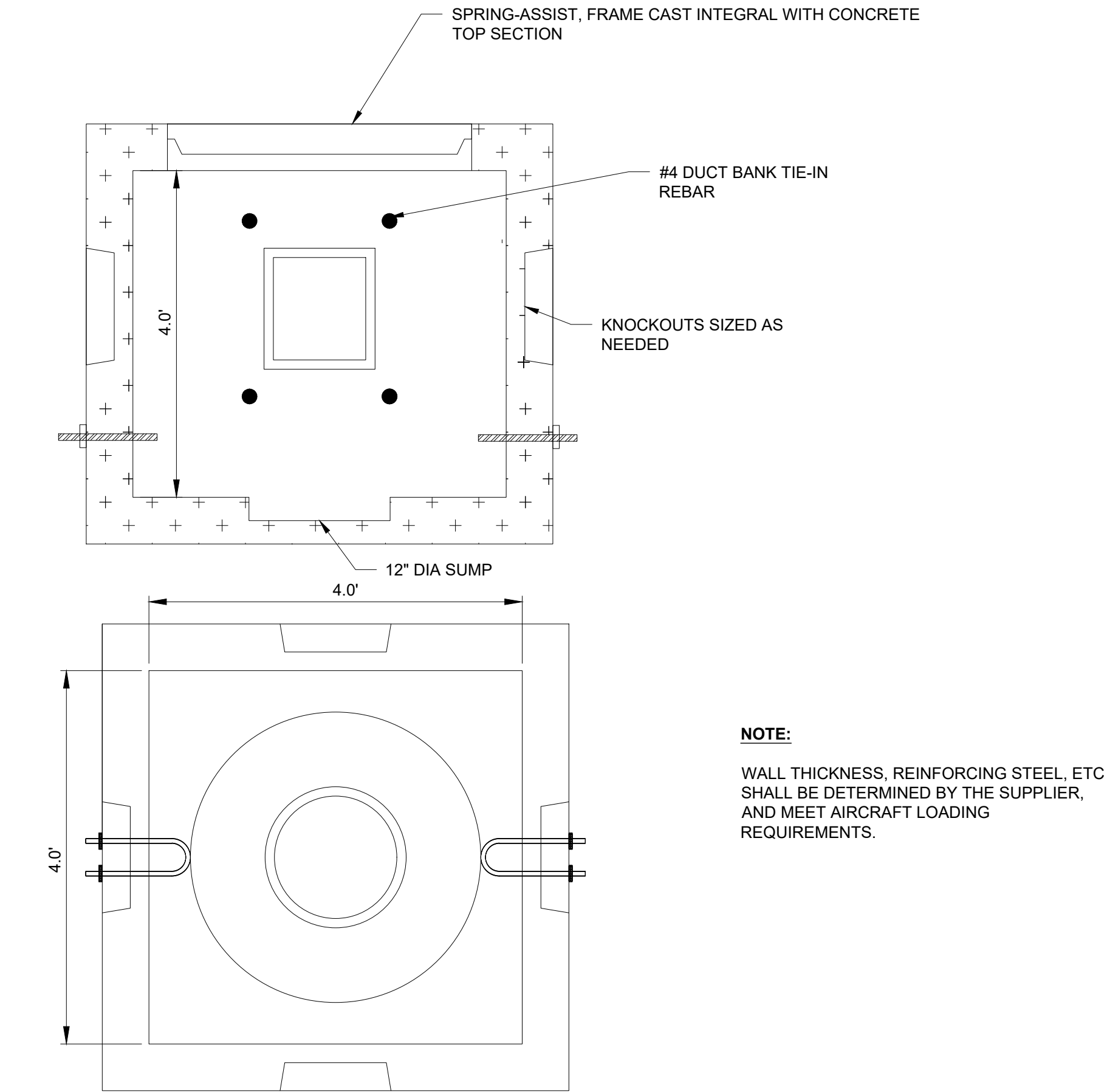
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3/21/2018 9:05:01 AM

FAA APPROVED EQUIPMENT SPECIFICATIONS REQUIREMENTS SOURCE: AC 150/5345-53, (ADDENDUM) CURRENT EDITION				AS CONSTRUCTED		
DESCRIPTION	FAA SPEC. NO.	EFFECTIVE FAA AC	DATE OF AC	MFR.	CATALOG NO.	REMARKS
CONNECTORS, CABLE	L-823	150/5345-26D	09/30/08	TBD		
UNDERGROUND ELECTRICAL CABLE FOR AIRPORT LIGHTING CIRCUITS	L-824	150/5345-7F	08/19/13	TBD		
SIGNS, RUNWAY AND TAXIWAY	L-858	150/5345-44K	10/08/15	TBD		
LIGHTS, RUNWAY & TAXIWAY EDGE, MEDIUM INTENSITY	L-861	150/5345-46E	03/02/16	TBD		
LIGHT BASE, NON-LOAD BEARING	L-867	150/5345-42H	11/06/15	TBD		

3

EQUIPMENT
MANUFACTUER INFORMATION

NOT TO SCALE



1

4'X4'X4'
ELECTRICAL MANHOLE DETAIL

NOT TO SCALE

PROPOSED AIRPORT SIGNS					
LEGEND	LEGEND		COLOR		SIZE
	SIDE A	SIDE B	SIDE A	SIDE B	
22					1
23					1
24					1
73					1

2

AIRPORT GUIDANCE SIGN DETAIL

NOT TO SCALE

KEY TO SIGN SCHEDULE	
	=BLACK LEGEND ON YELLOW BACKGROUND
	=YELLOW LEGEND ON BLACK BACKGROUND
	=WHITE LEGEND ON RED BACKGROUND
	=WHITE LEGEND ON BLACK BACKGROUND
	=BLANK BLACK BACKGROUND, NO LETTERING

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WK KELLOGG AIRPORT
RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2

BATTLE CREEK, MI
BATTLE CREEK, MI

ISSUED
1 03.29.18 ISSUED FOR
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SHEET CONTENTS
ELECTRICAL DETAILS
SHEET 2 OF 2

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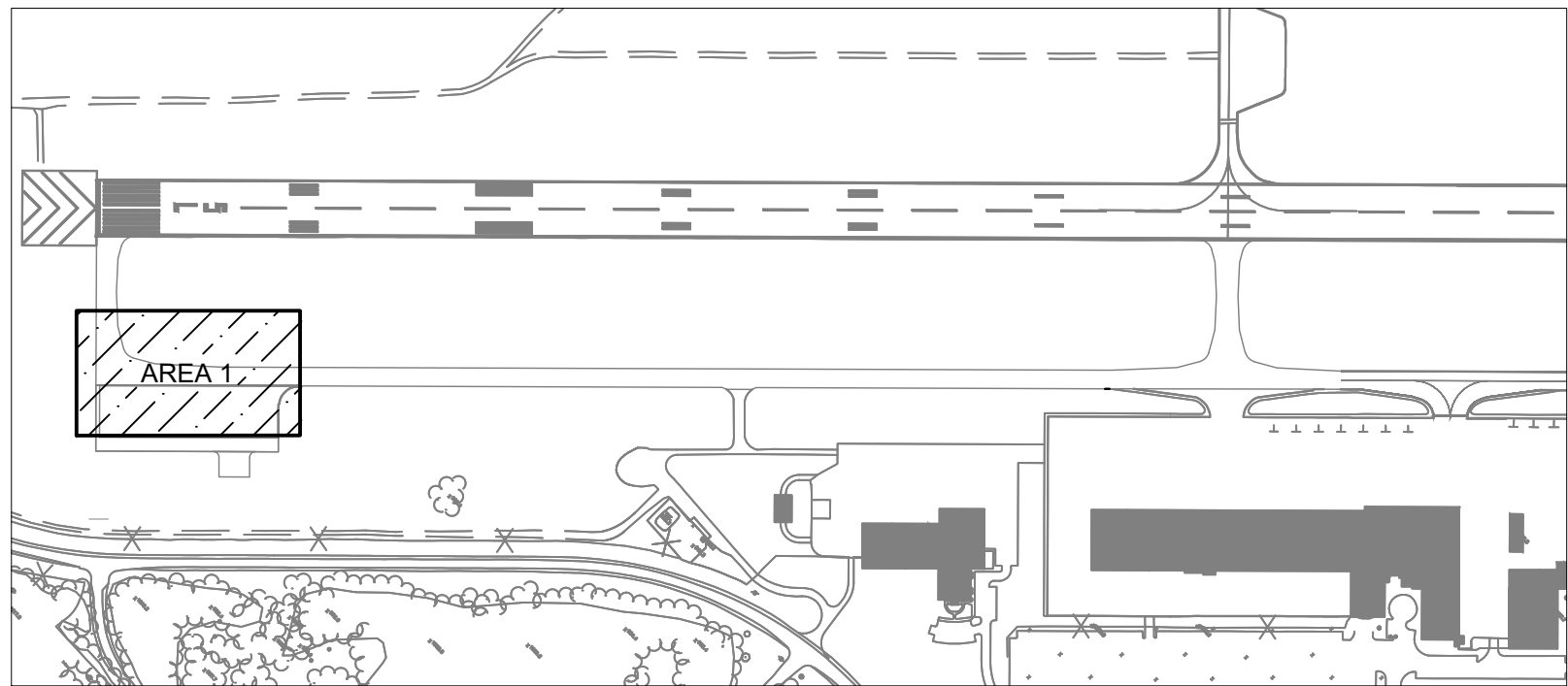
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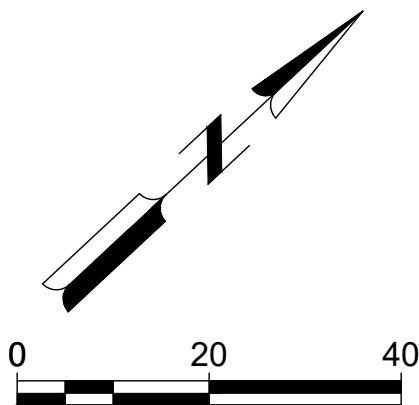
PAVEMENT MARKING AND RESTORATION - AREA 1

SHEET NO. 63 of 76

XM-101



1. ALL RADIUS POINTS ARE CENTERLINE MARKING UNLESS OTHERWISE NOTED.
2. REFLECTIVE MEDIA SHALL BE APPLIED TO ALL FULL RATE MARKINGS EXCEPT THE FOLLOWING AREAS. UNLESS OTHERWISE DETAILED IN THE PLANS.
 - TAXIWAY EDGE MARKING
 - BLACK OUTLINE
3. REDUCED RATE MARKINGS SHALL BE APPLIED TO THE FOLLOWING AREAS PRIOR TO OPENING FOR AIRCRAFT USE (BLACK OUTLINES SHALL NOT BE MARKED FOR REDUCED RATE MARKINGS, REDUCED MARKINGS SHALL NOT INCLUDE BEADS).
 - TAXIWAY CENTERLINE
 - ENHANCED CENTERLINE
 - HOLD LINE
 - NON-MOVEMENT AREA BOUNDARY
 - INTERMEDIATE HOLD LINE.



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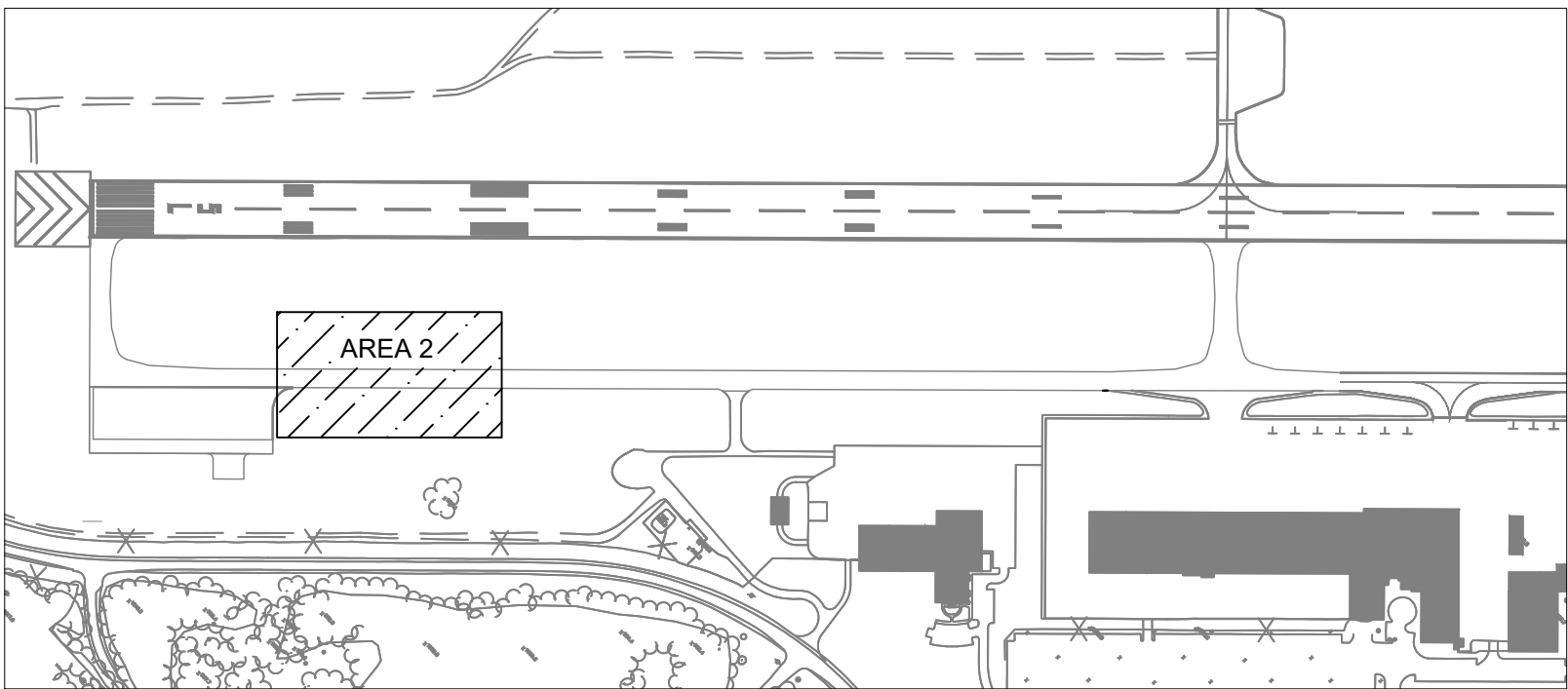
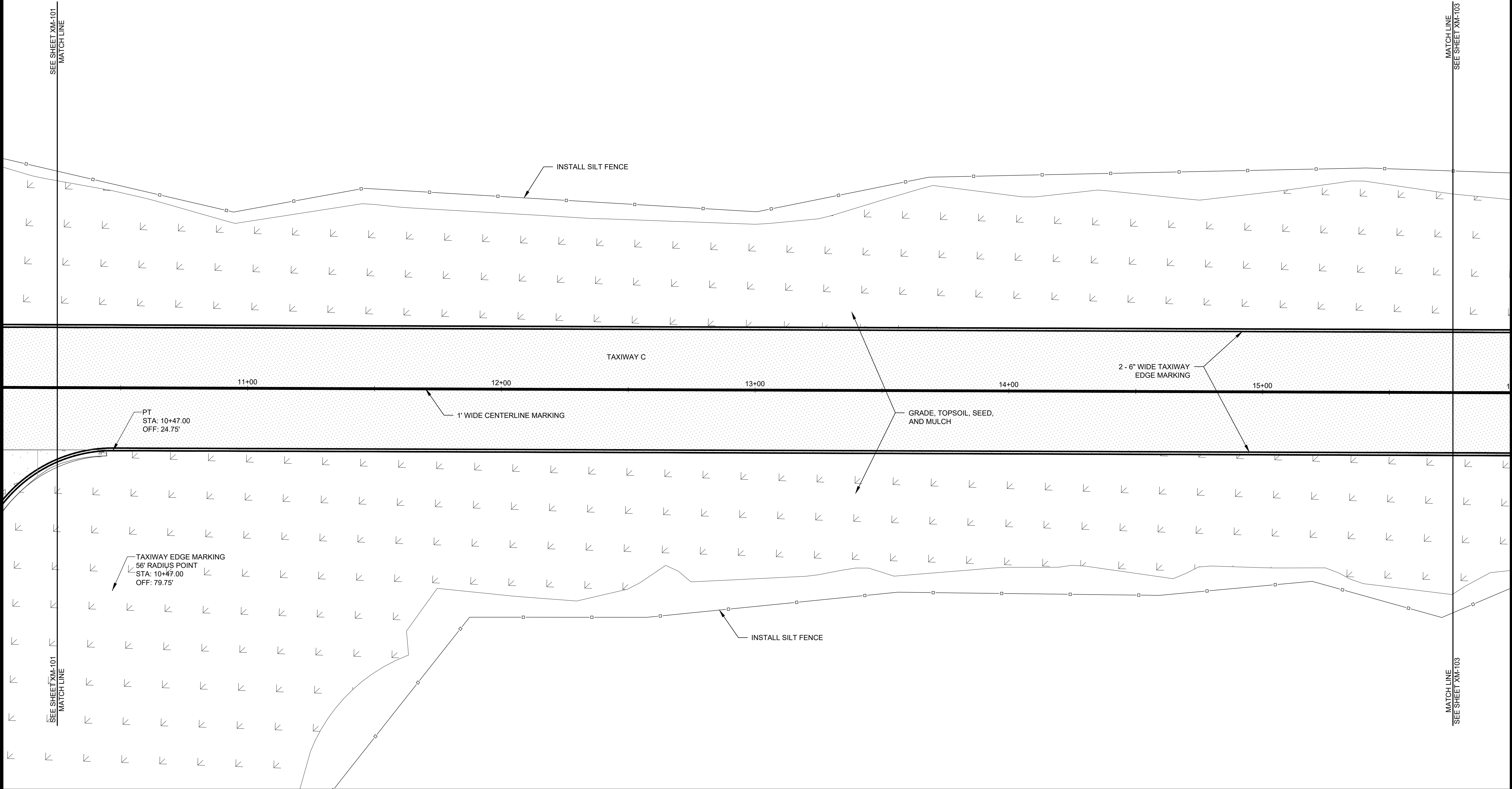
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 2

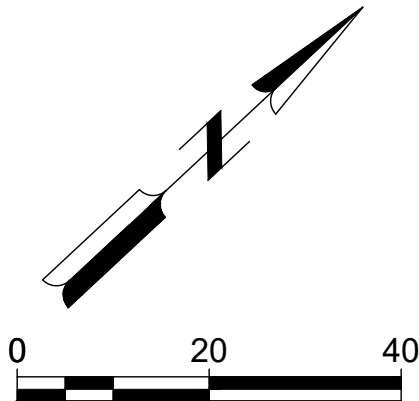
SHEET NO 64 of 76

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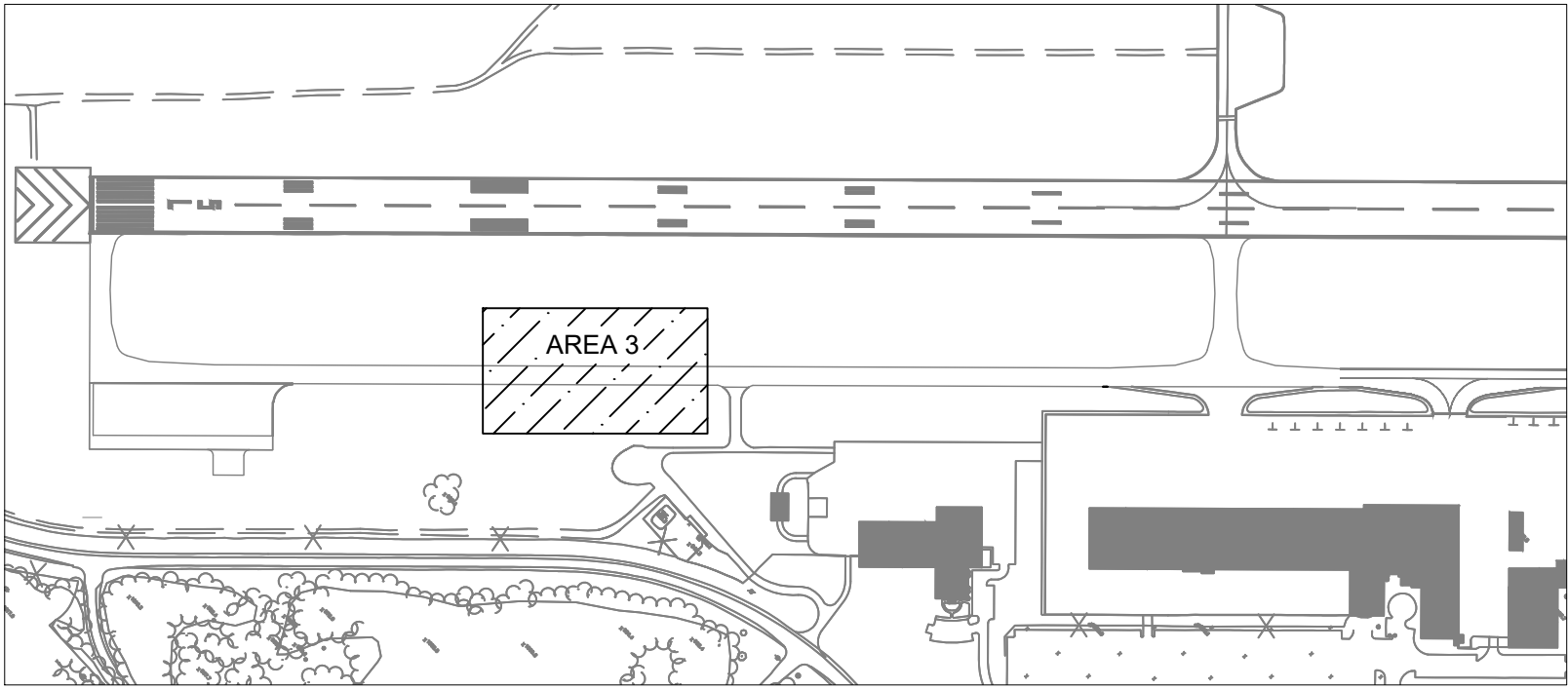
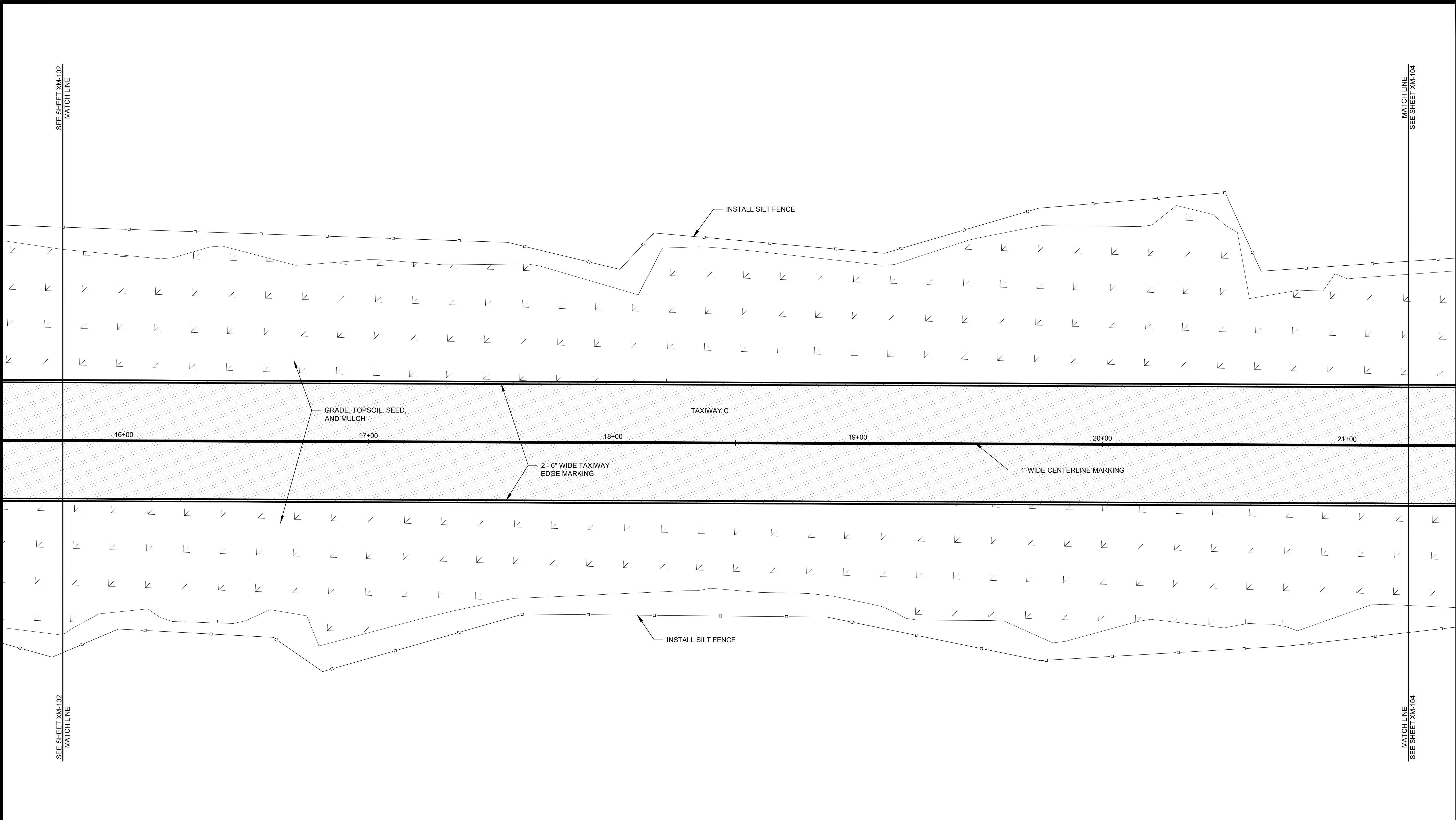


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 - BLACK OUTLINE
- REDUCED RATE MARKINGS SHALL BE APPLIED TO THE FOLLOWING AREAS PRIOR TO OPENING FOR AIRCRAFT USE (BLACK OUTLINES SHALL NOT BE MARKED FOR REDUCED RATE MARKINGS, REDUCED MARKINGS SHALL NOT INCLUDE BEADS).
 - TAXIWAY CENTERLINE
 - ENHANCED CENTERLINE
 - HOLD LINE
 - NON-MOVEMENT AREA BOUNDARY
 - INTERMEDIATE HOLD LINE.

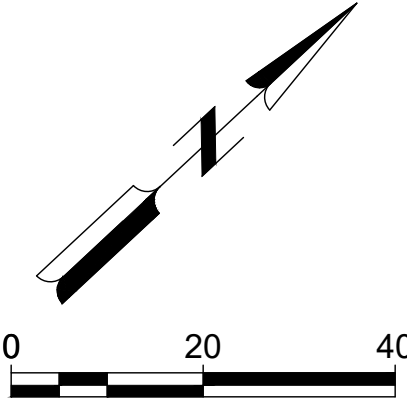


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RECONSTRUCT TAXIWAY C
PAVEMENT & LIGHTING PHASE 2**
BATTLE CREEK, MI

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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 3

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BATTLE CREEK, MI

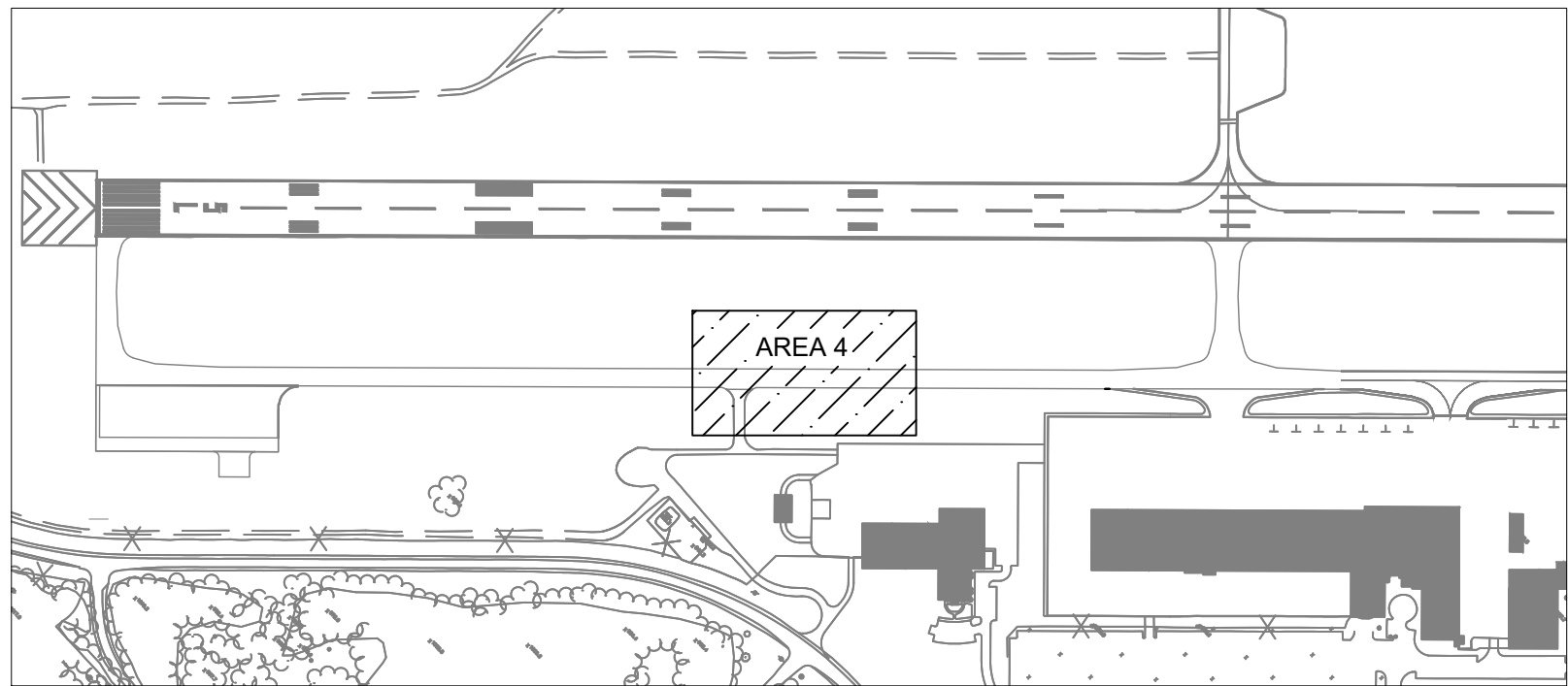
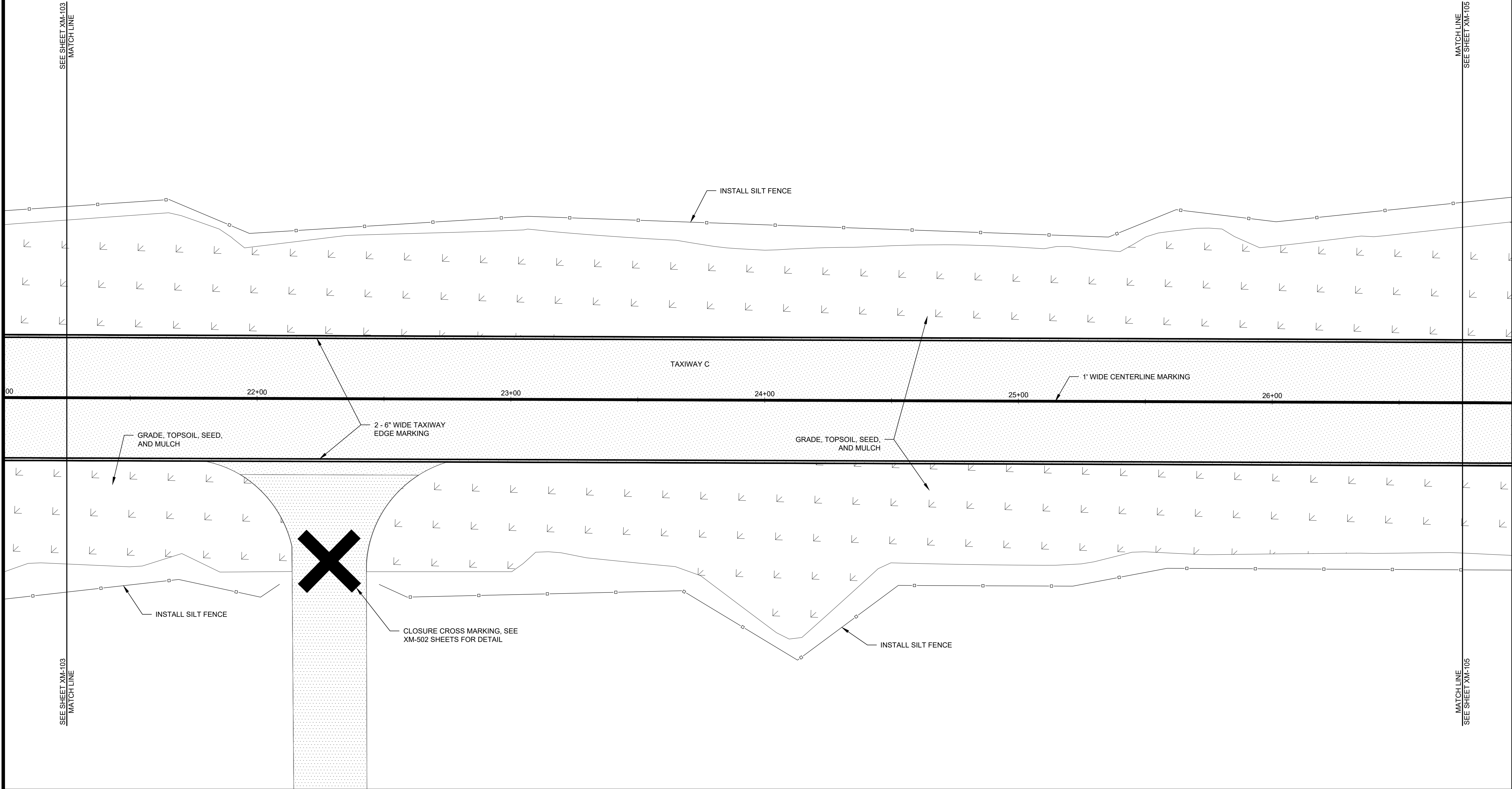
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 4

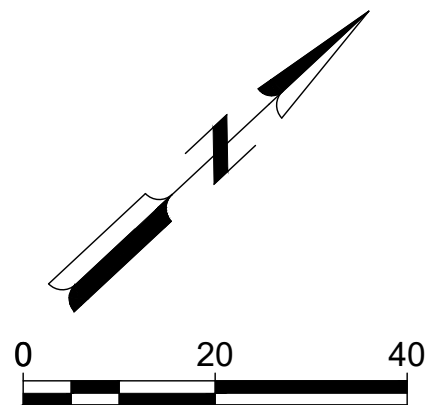
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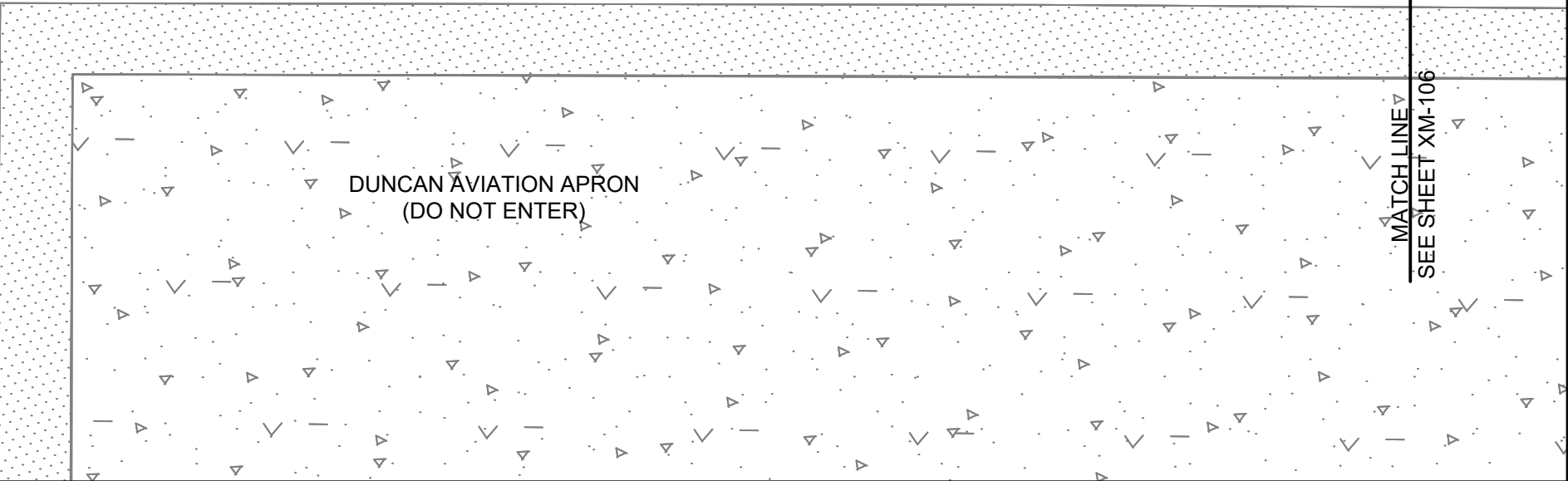
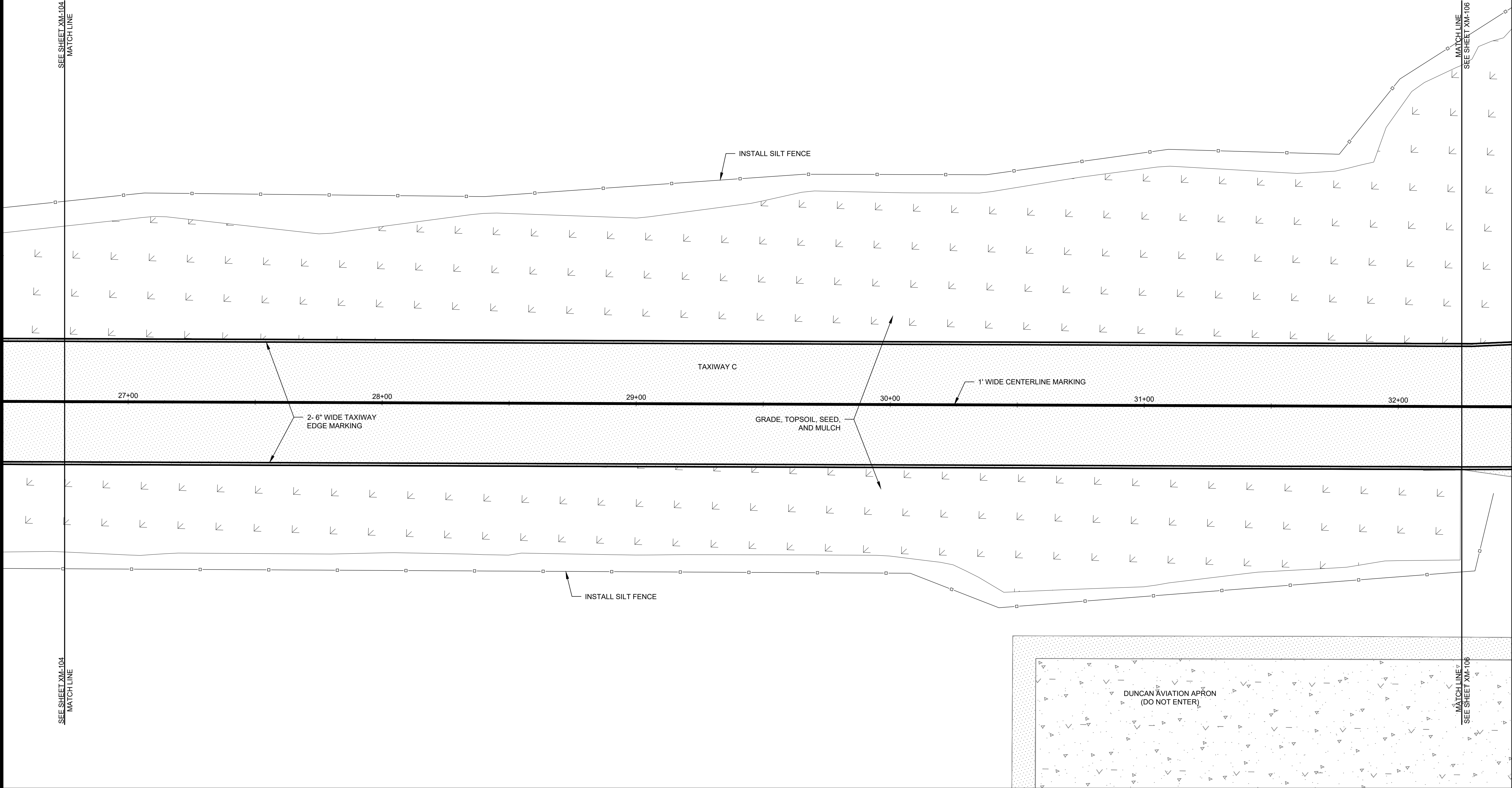
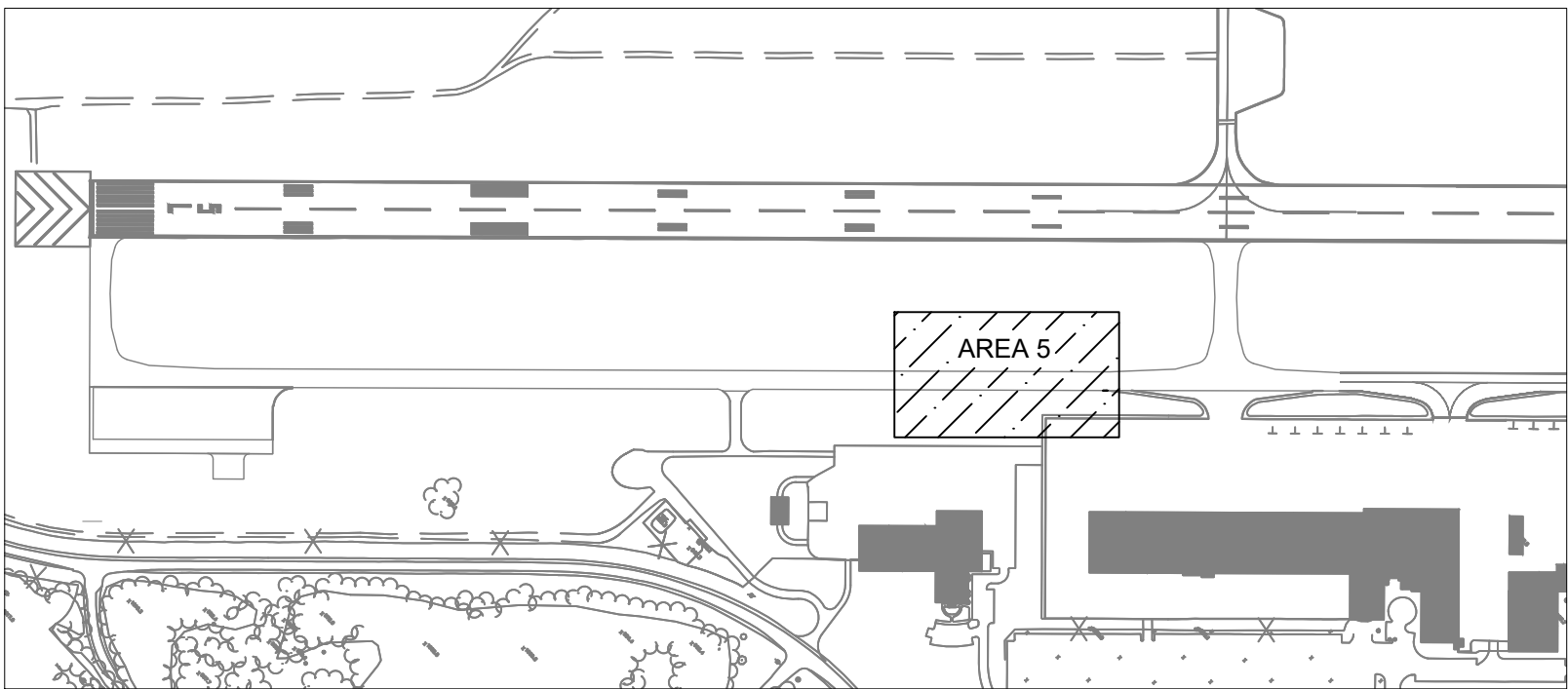


NOTES:

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 - ENHANCED CENTERLINE
 - HOLD LINE
 - NON-MOVEMENT AREA BOUNDARY
 - INTERMEDIATE HOLD LINE.

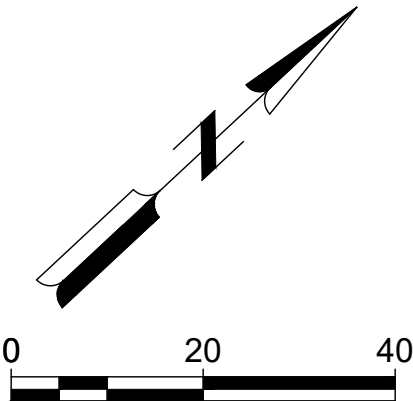


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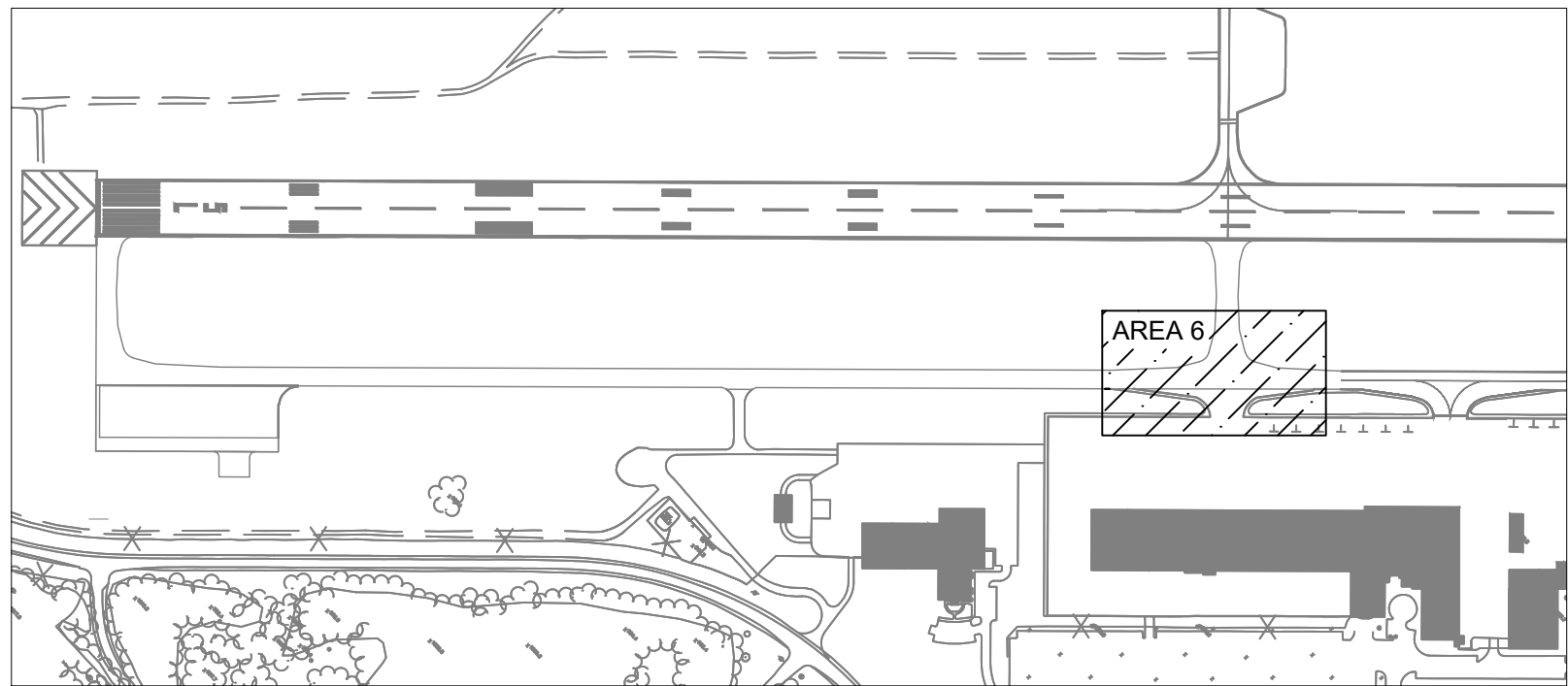
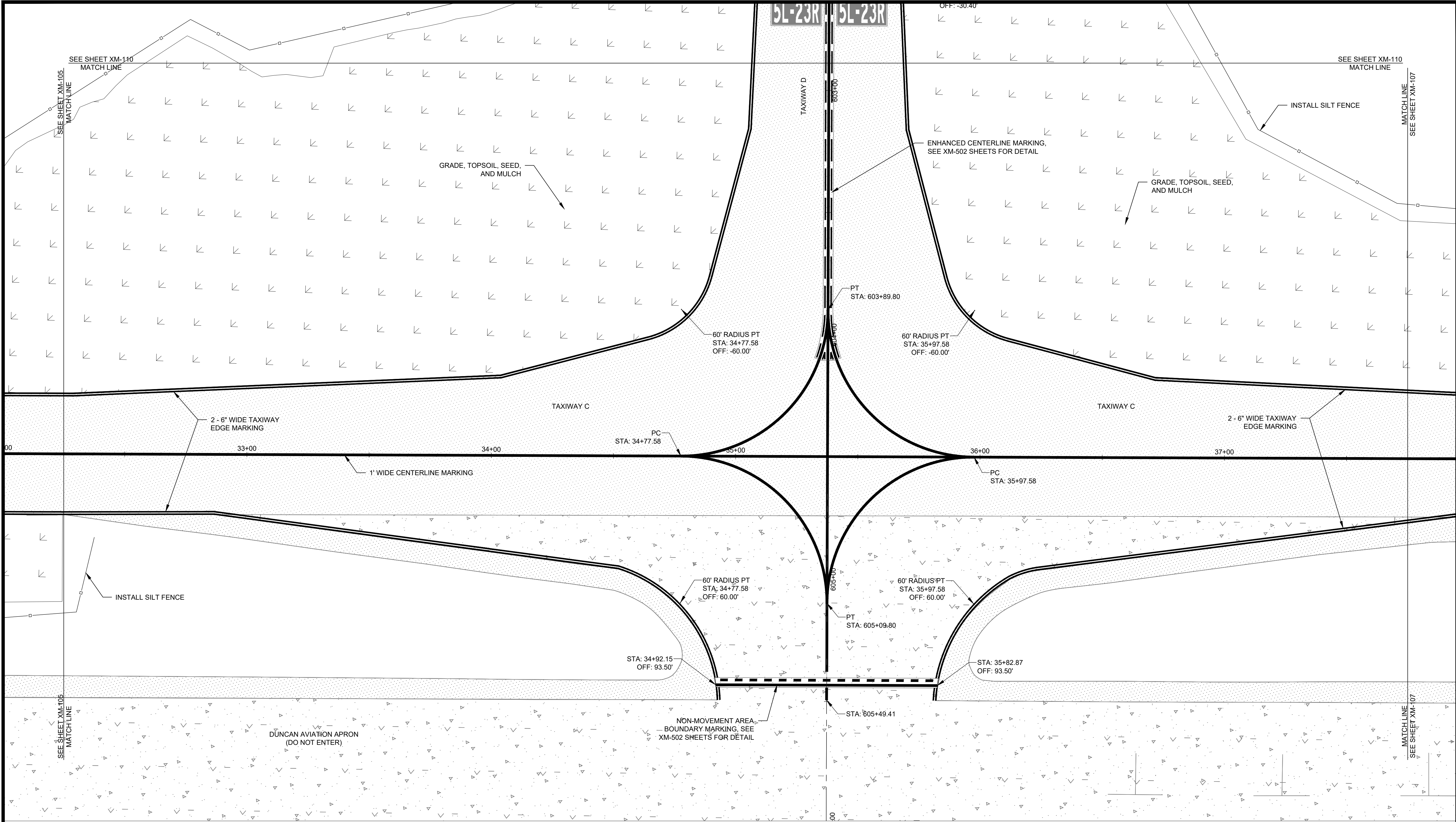
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 5

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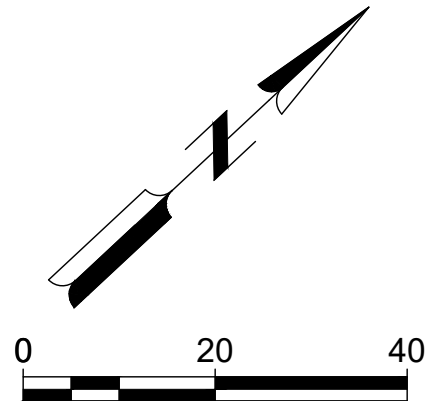
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
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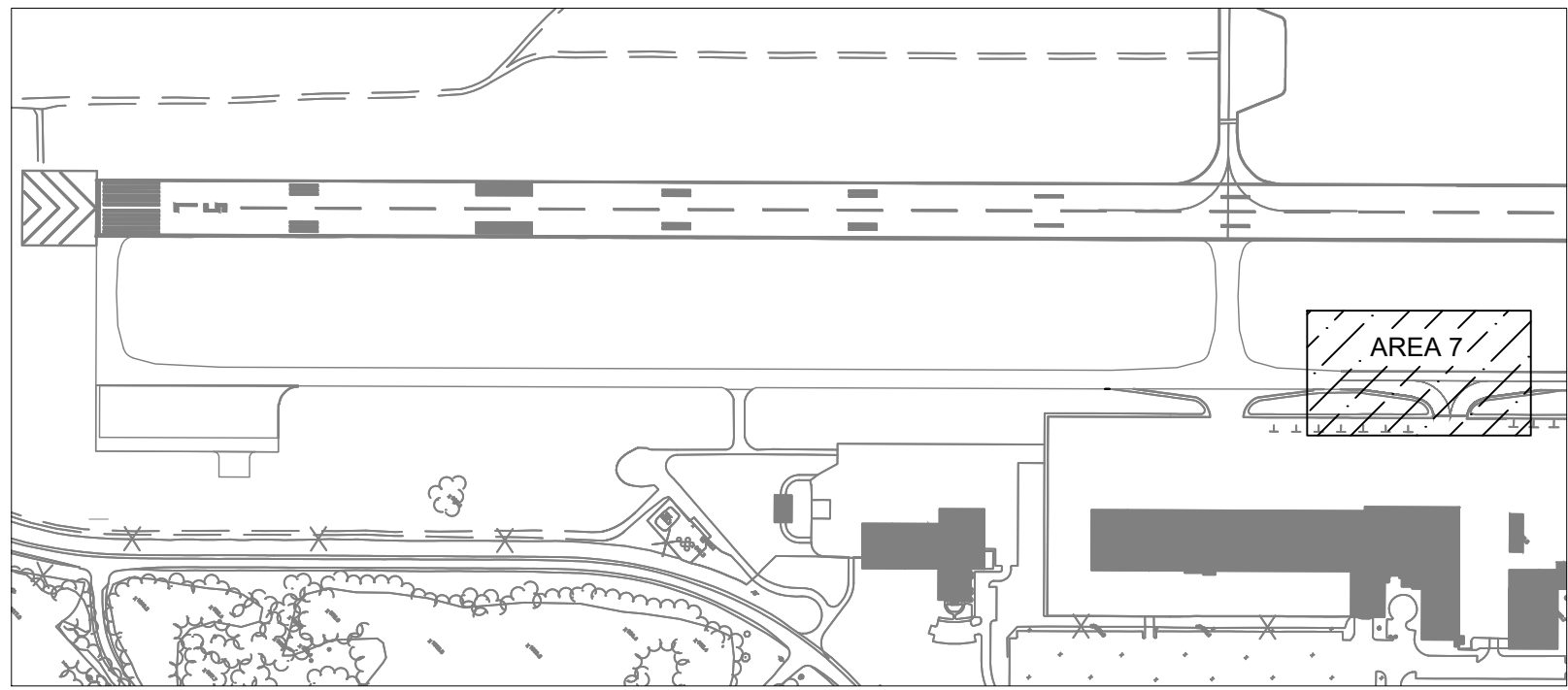
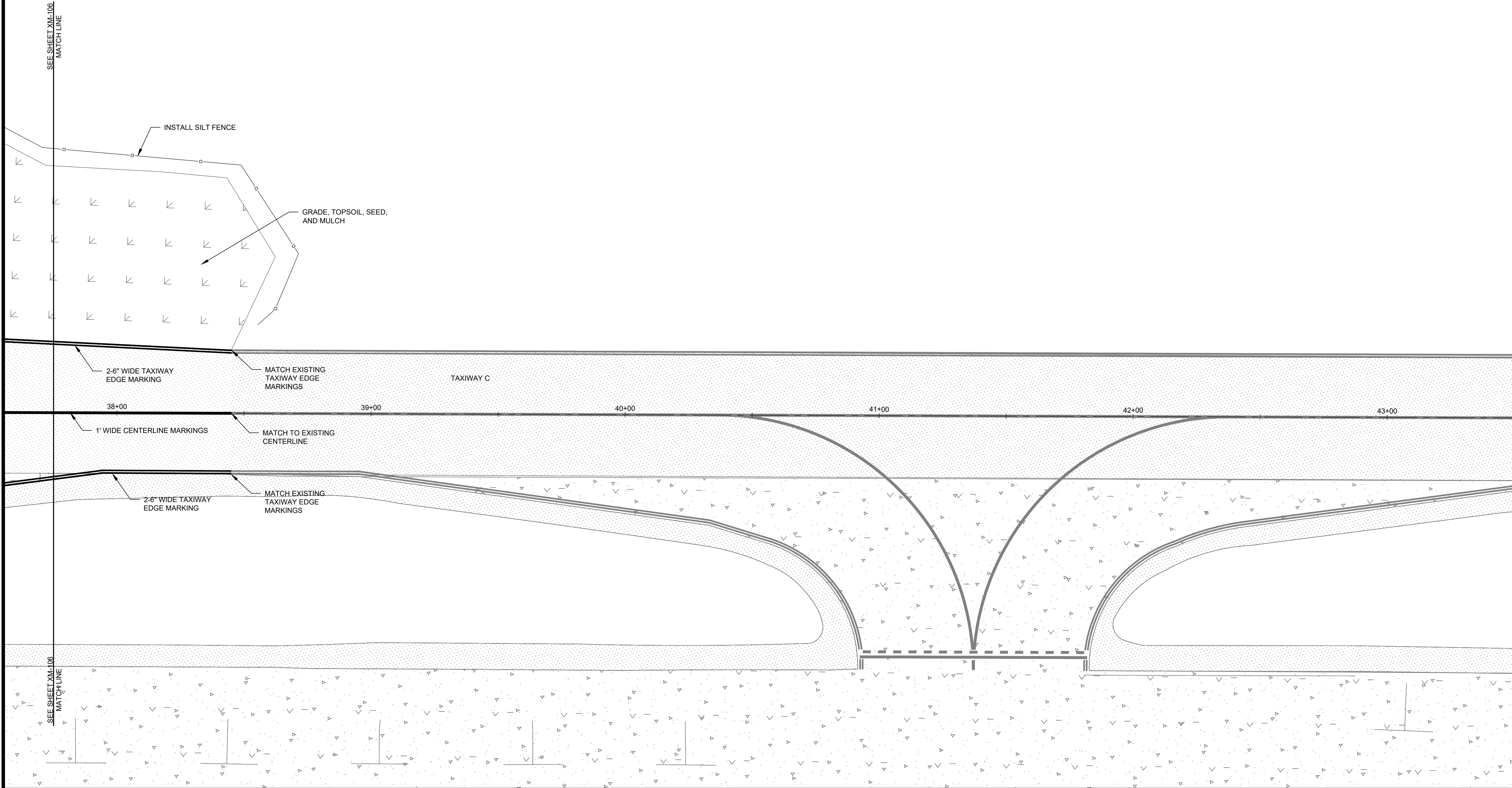
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 7

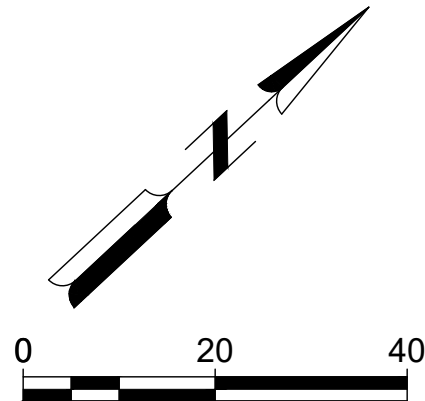
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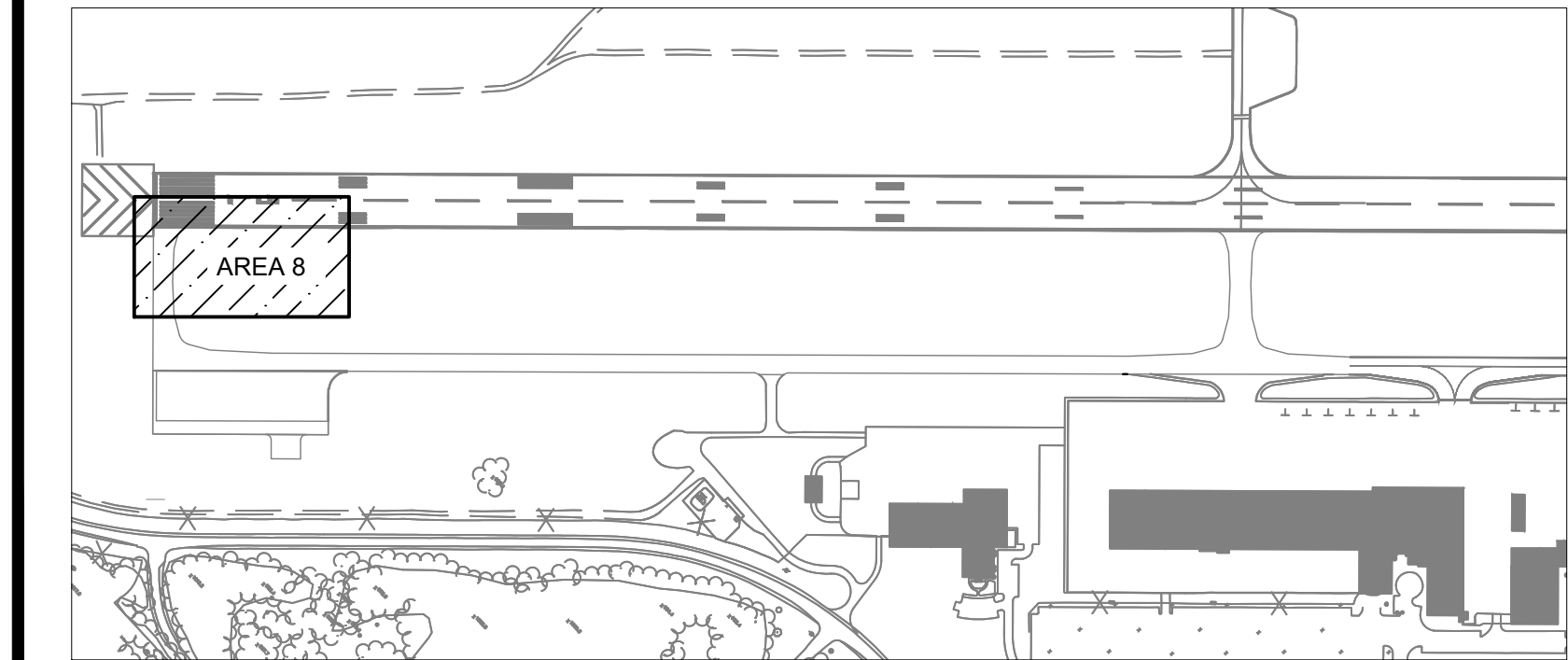
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PAVEMENT MARKING AND RESTORATION - AREA 8

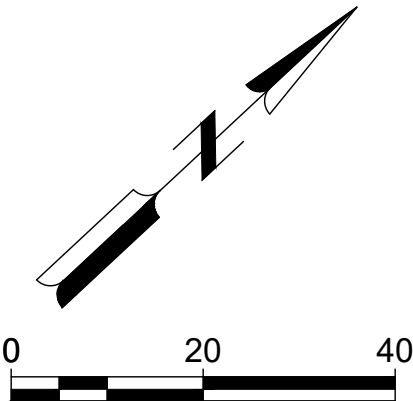
SHEET NO. 70 of 76

KM-108

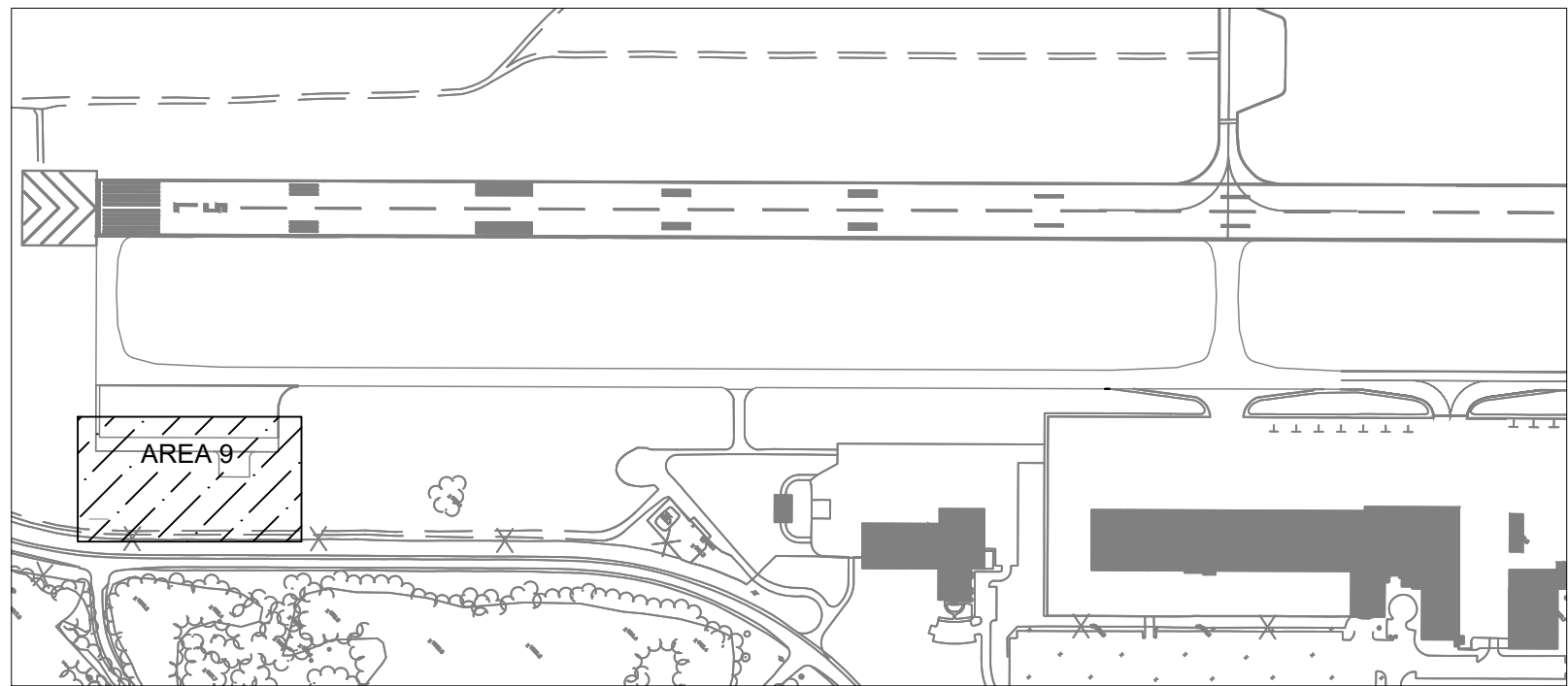
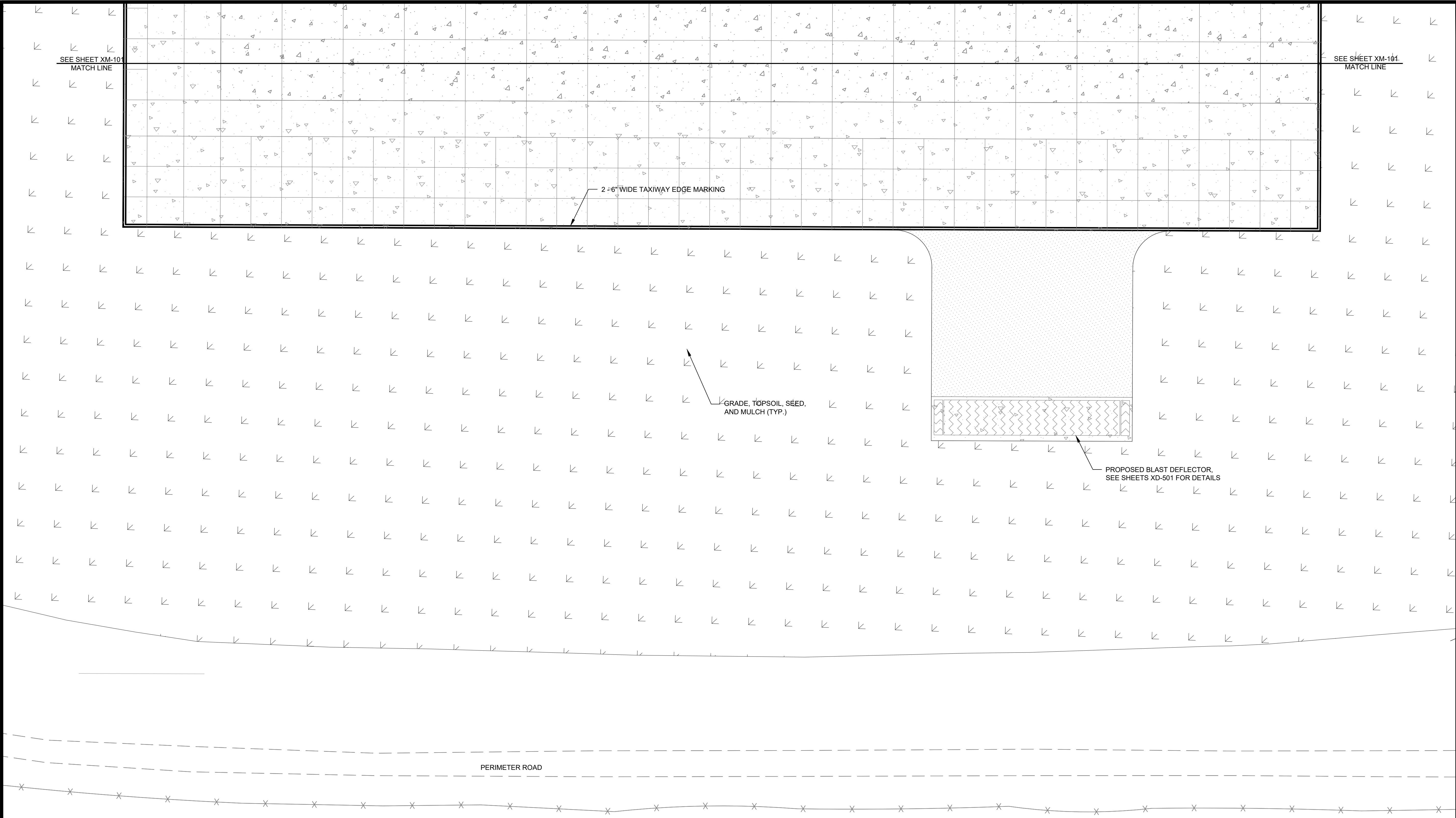


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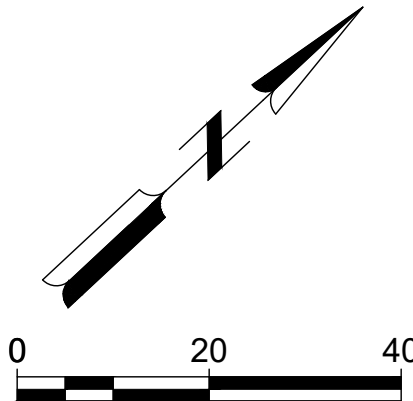


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BATTLE CREEK, MI

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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 9

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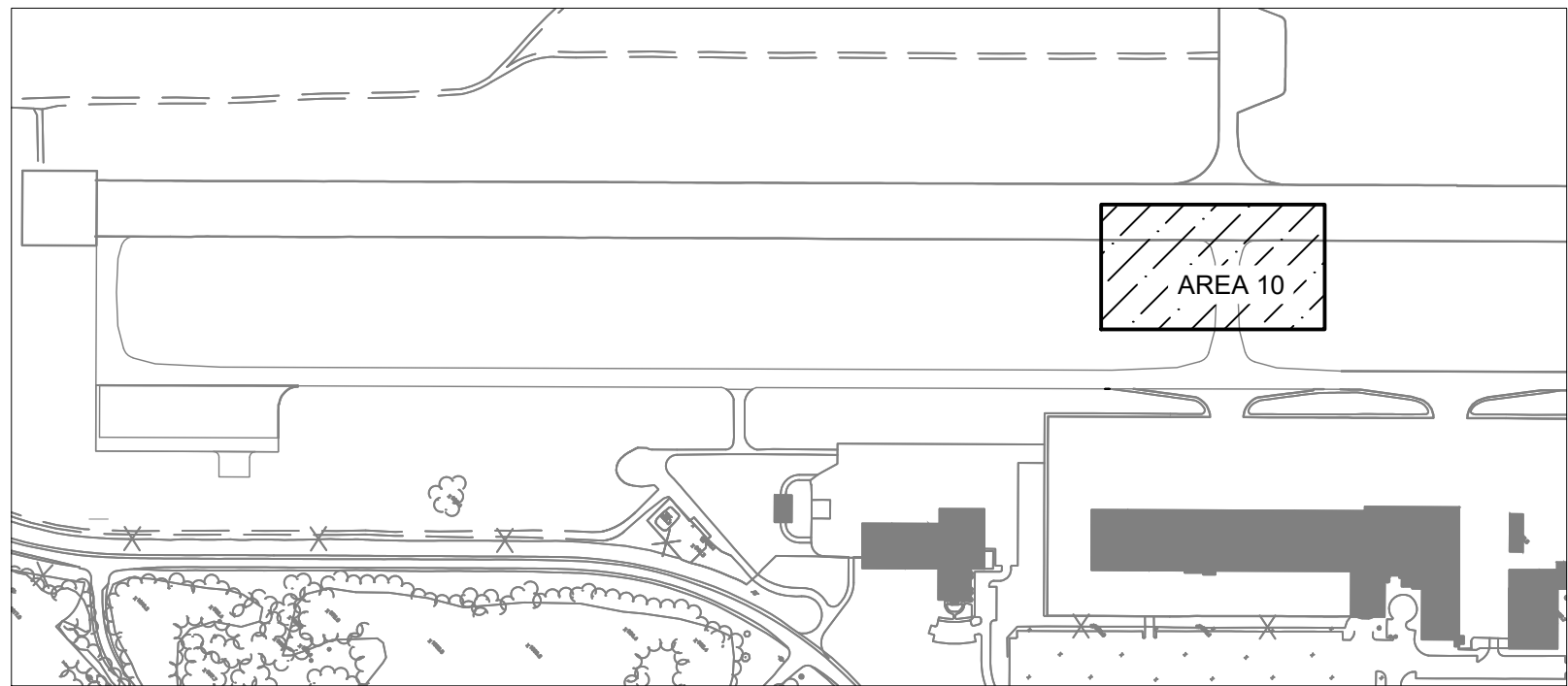
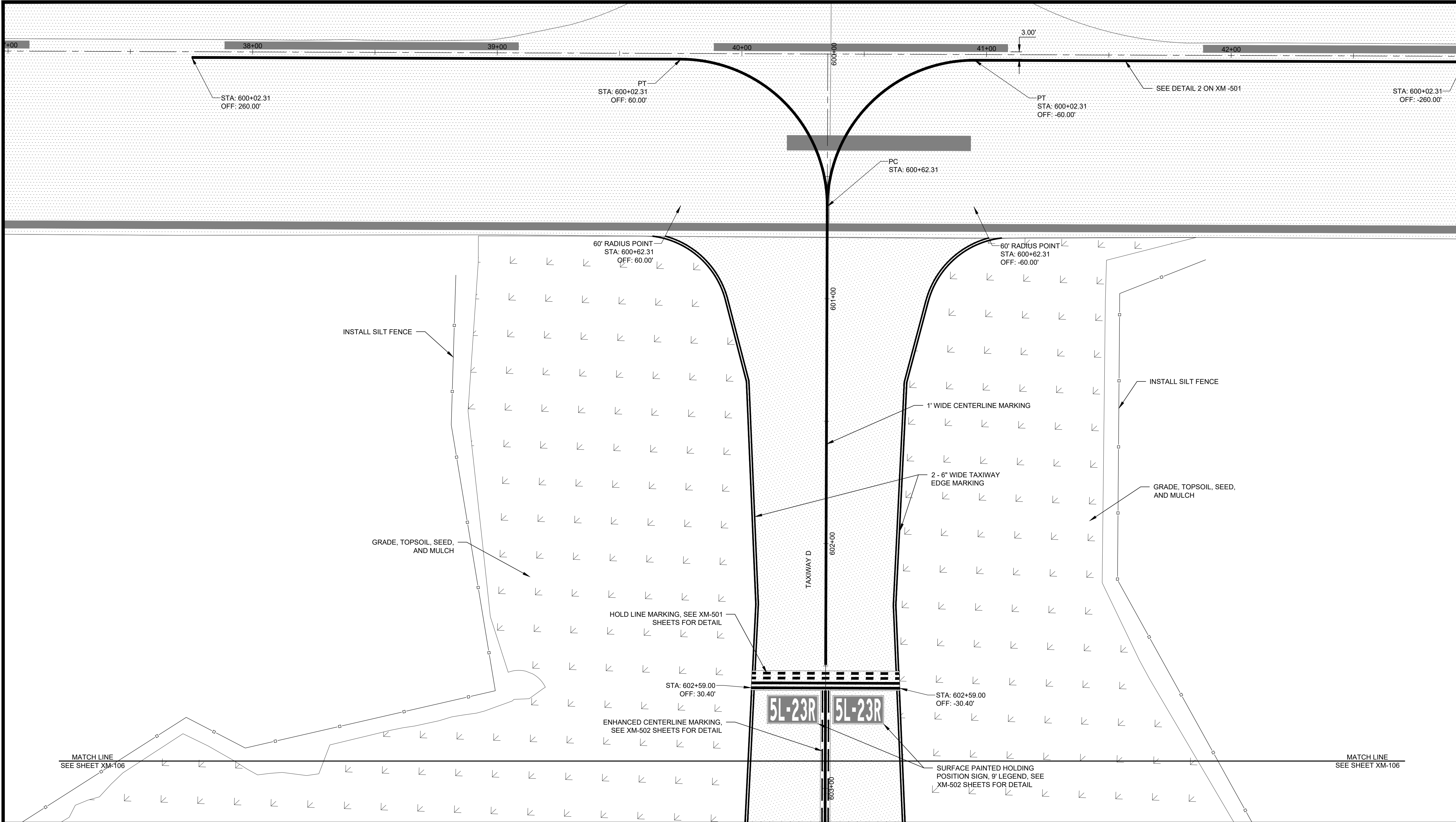
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SHEET CONTENTS
PAVEMENT MARKING
AND RESTORATION -
AREA 10

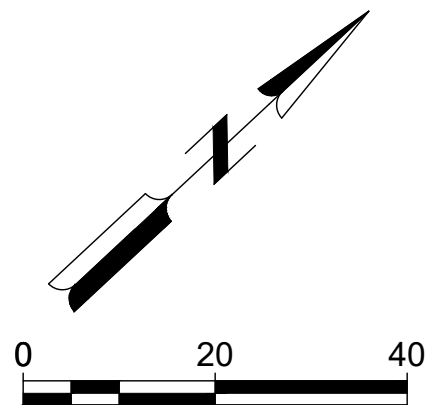
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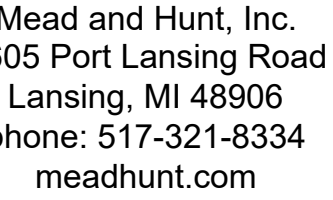
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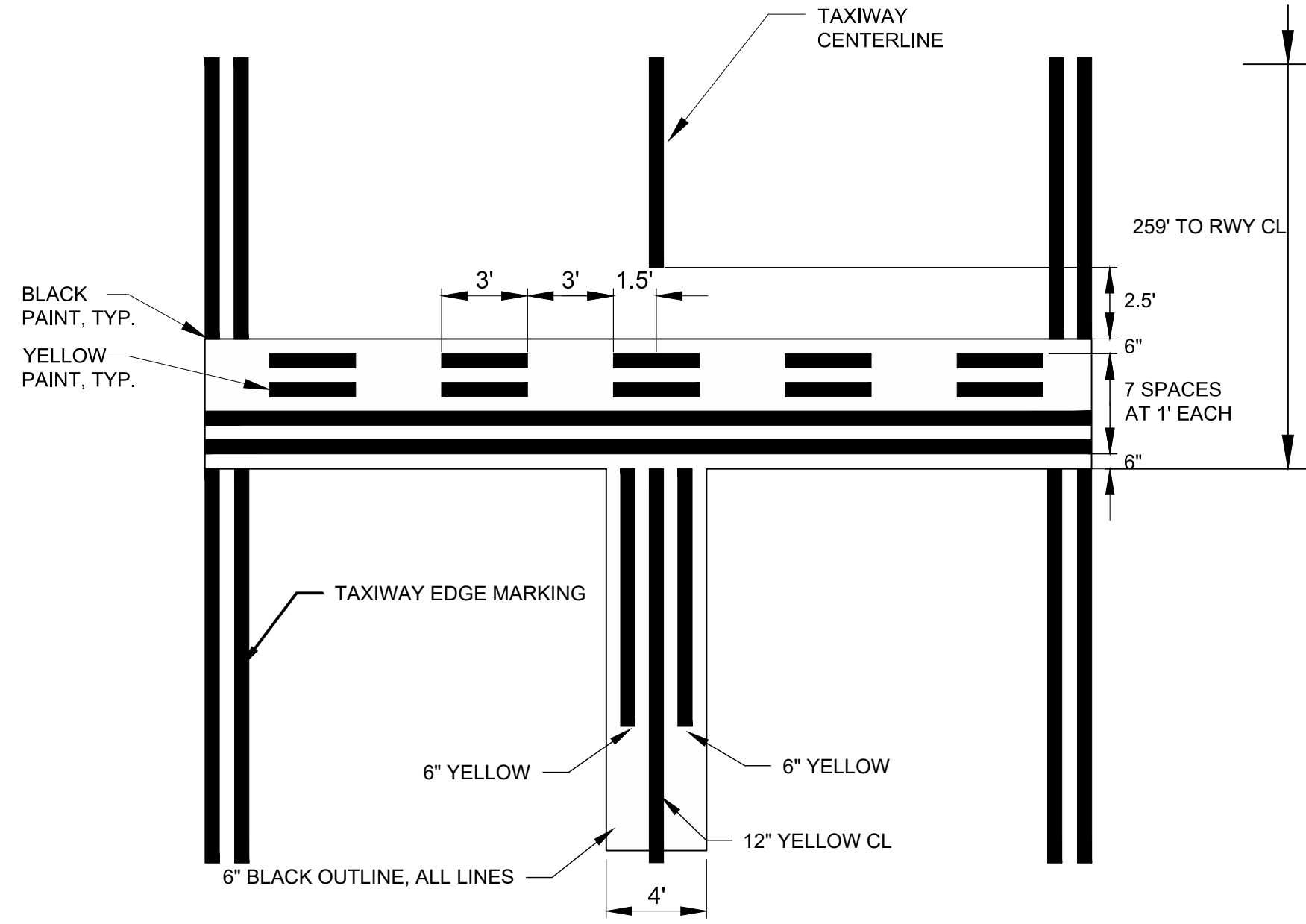
PAVEMENT MARKING AND RESTORATION - AREA 11

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2 HOLD-LINE DETAIL
NO SCALE



3 TAXIWAY CENTERLINE MARKING
NO SCALE

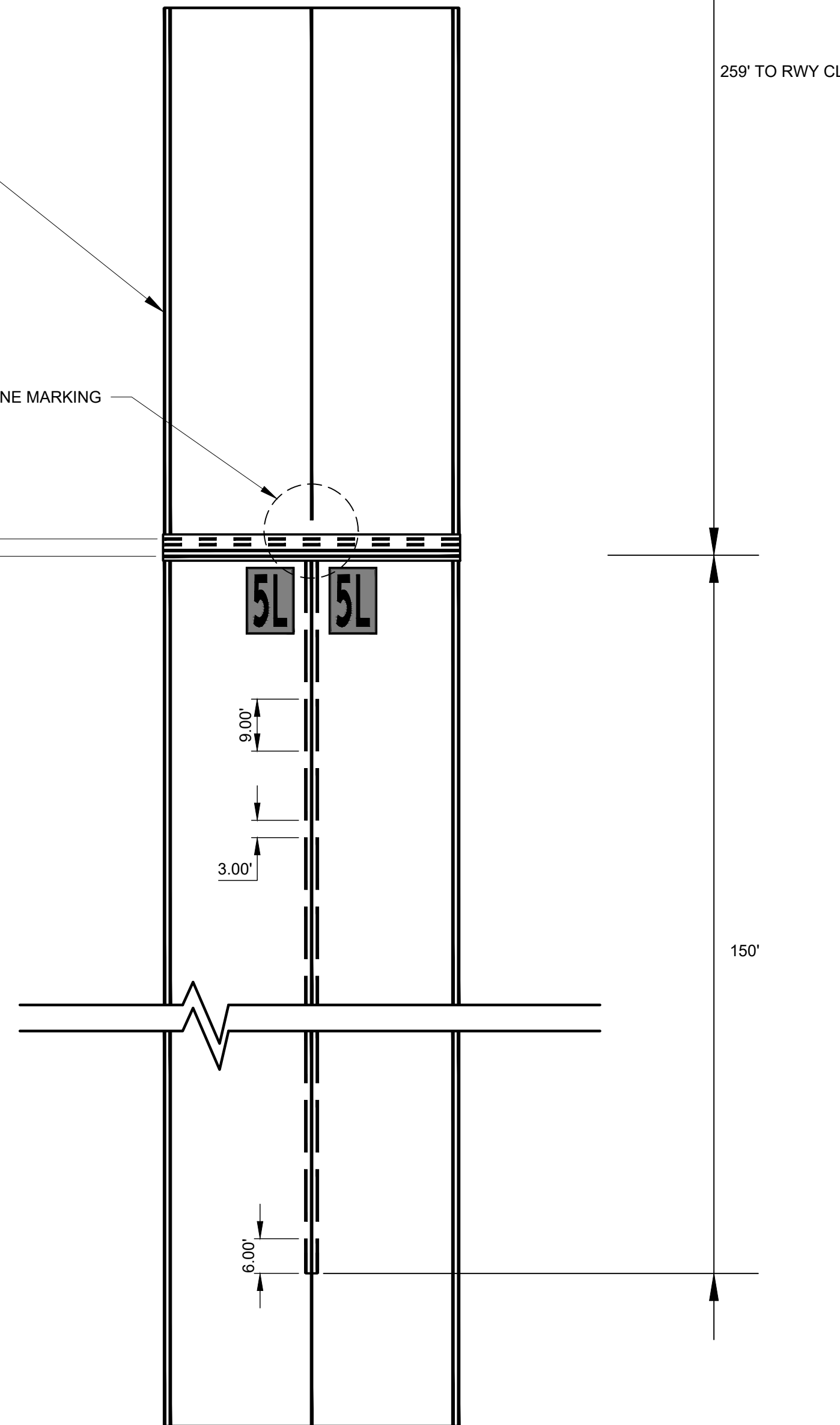


4 TAXIWAY EDGE MARKING, SOLID
NO SCALE

PROVIDE NEW MARKING WHERE
INDICATED ON THE MARKING
PLANS.

4 LINES (YELLOW) AND
SPACES AT 12" EACH

HOLD LINE MARKING



1 ENHANCED RUNWAY
HOLD LINE MARKING DETAIL
1"=20'

**Mead
& Hunt**

Mead and Hunt, Inc.
2605 Port Lansing Road
Lansing, MI 48906
phone: 517-321-8334
meadhunt.com

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SHEET CONTENTS
PAVEMENT MARKING
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DETAIL SHEET 1 OF 2

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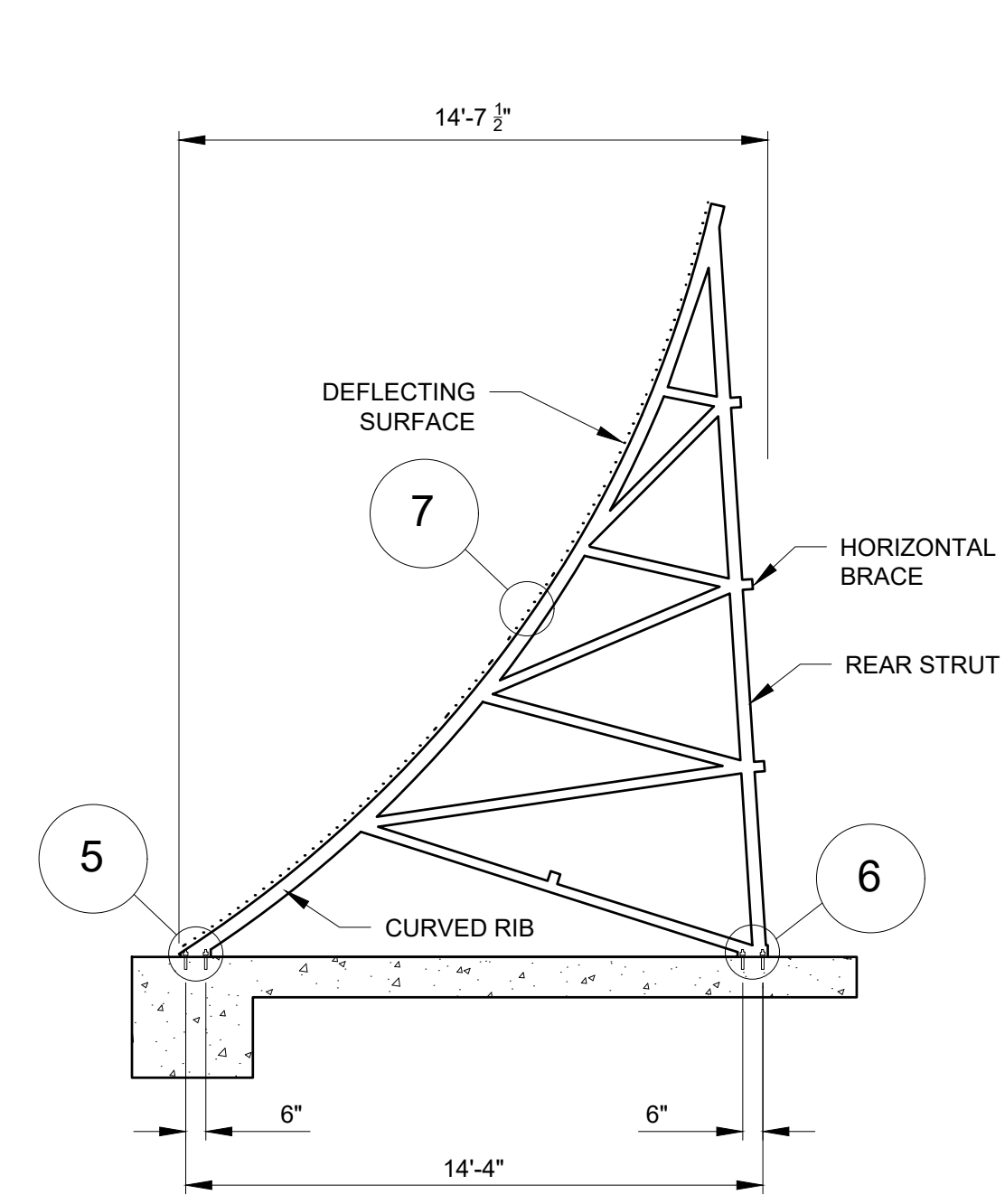
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SHEET CONTENTS
 PAVEMENT MARKING
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 DETAIL SHEET 2 OF 2

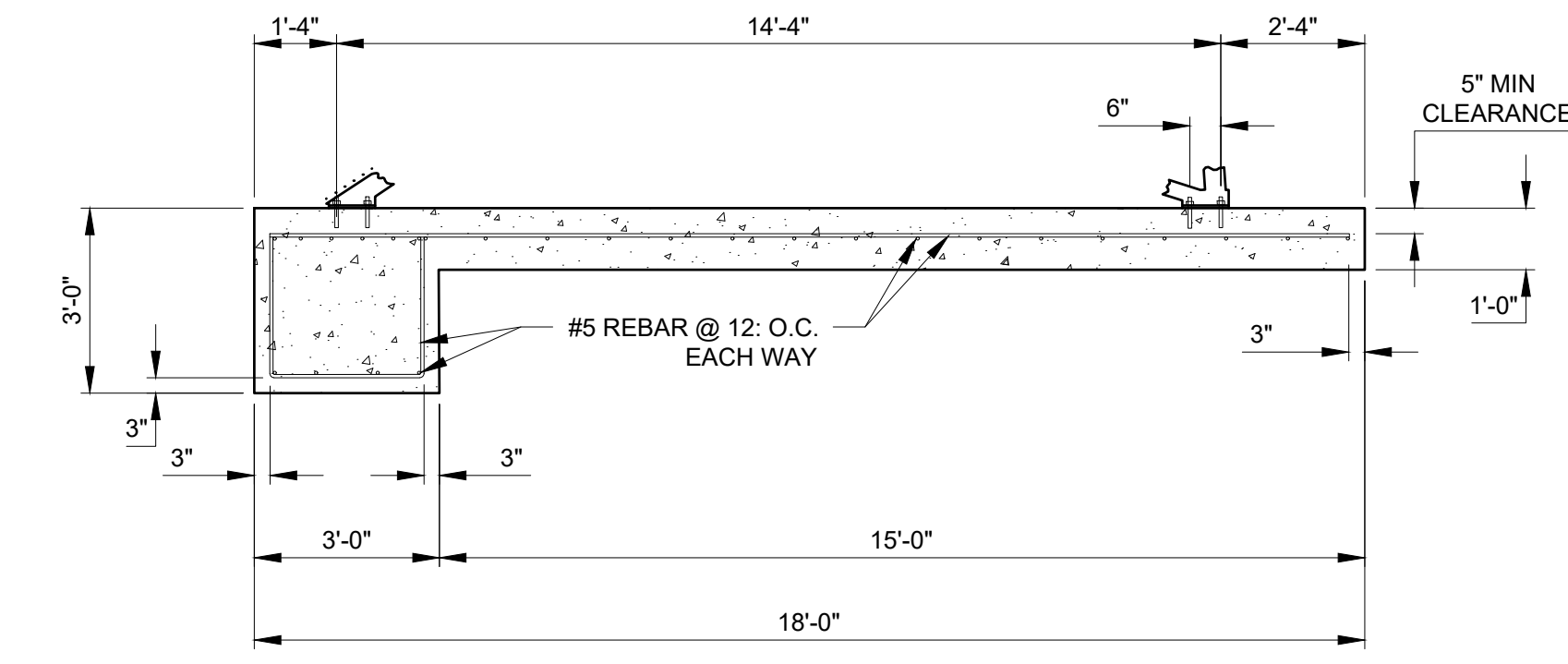
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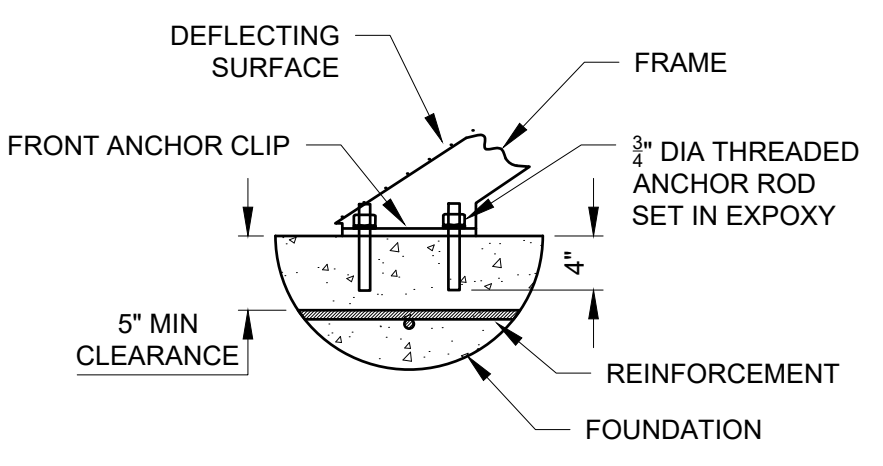




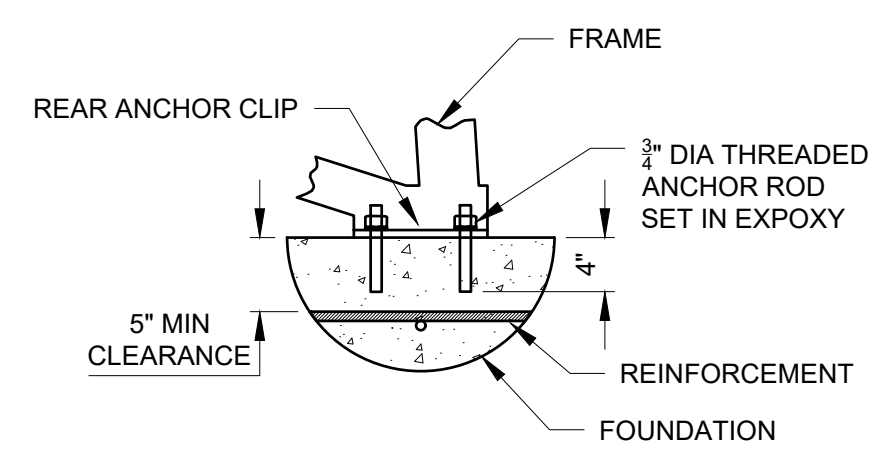
3 SECTION DETAIL
NOT TO SCALE



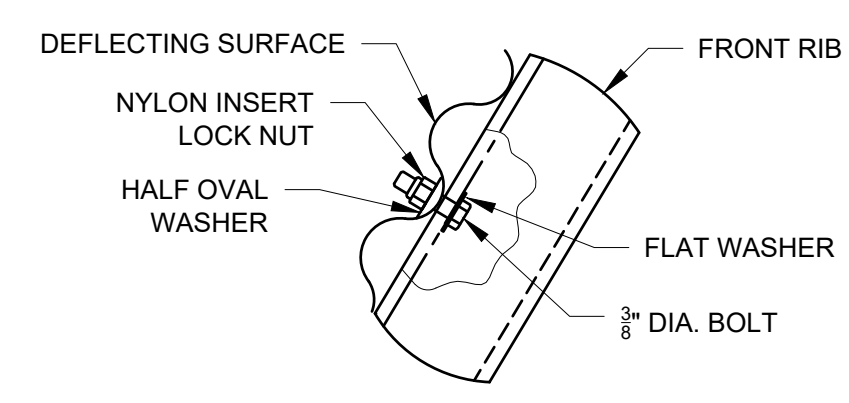
4 FOUNDATION SECTION DETAIL
NOT TO SCALE



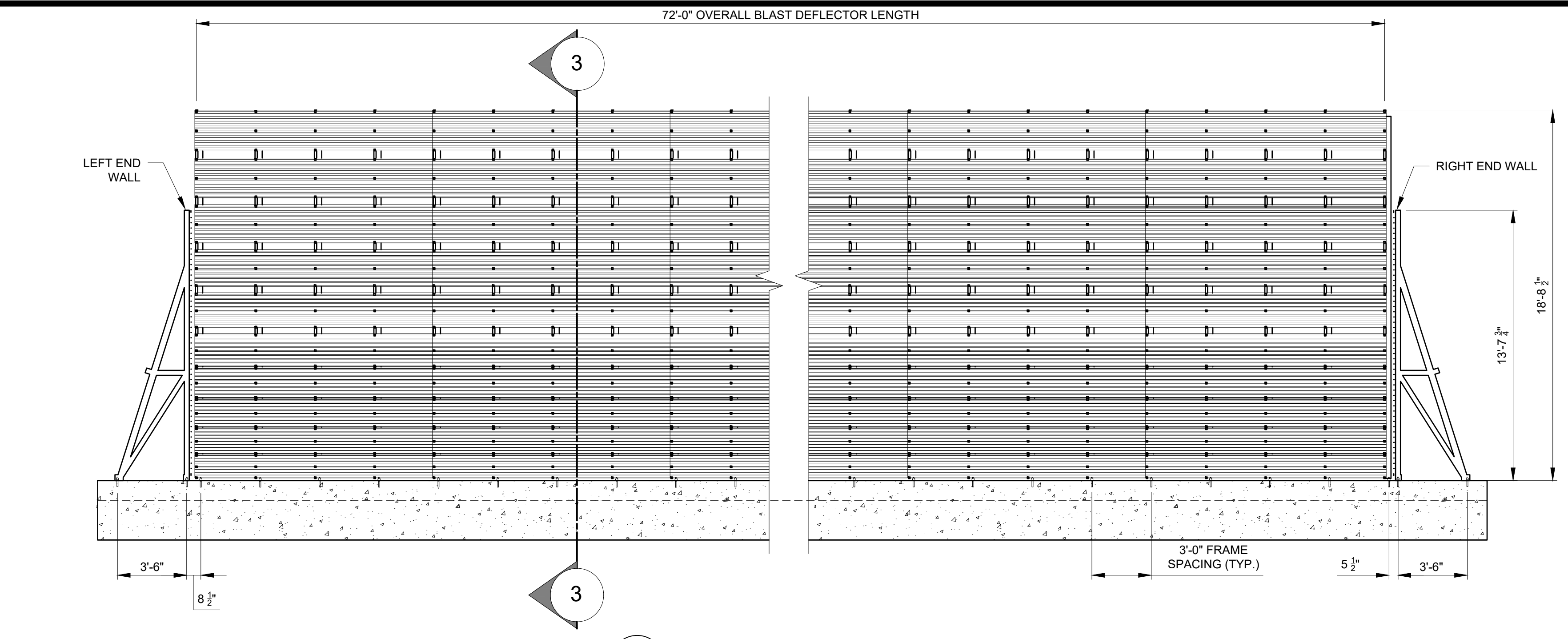
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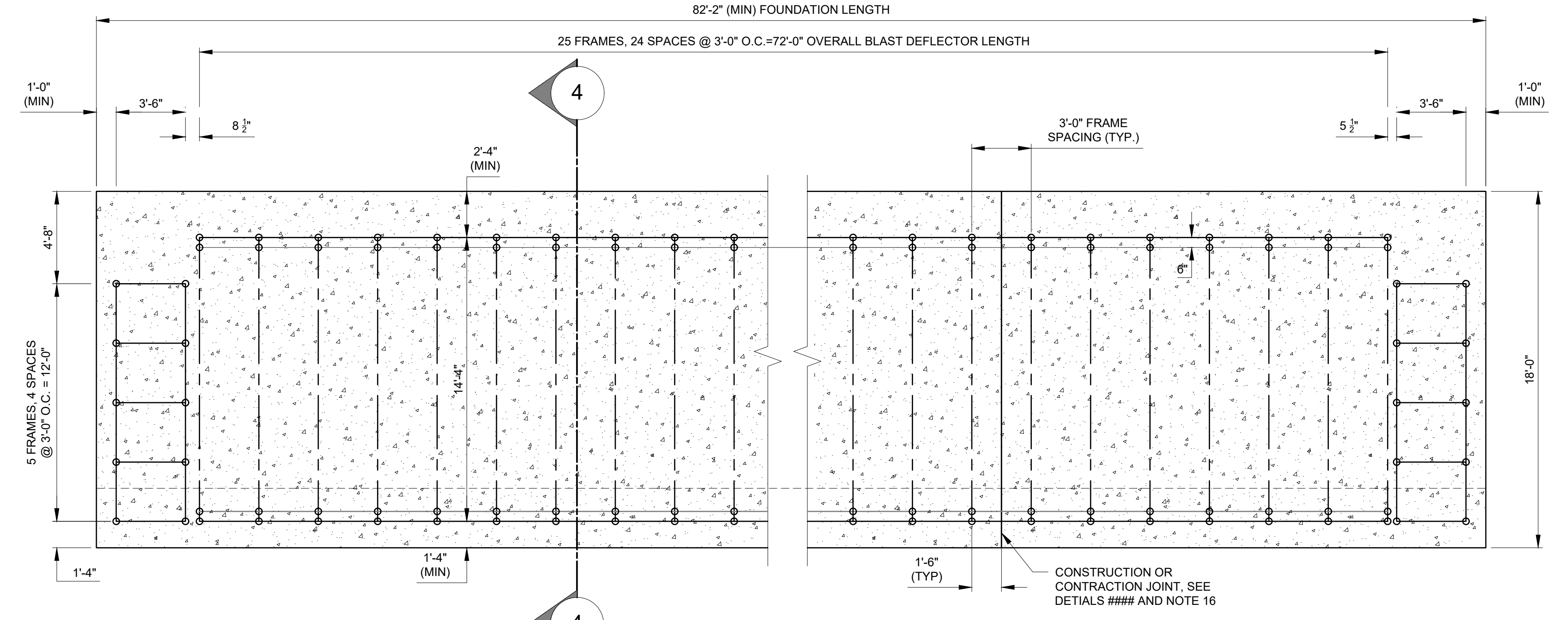
6 DETAIL
NOT TO SCALE



7 DETAIL
NOT TO SCALE



1 FRONT ELEVATION VIEW
NOT TO SCALE



2 ANCHOR BOLT LAYOUT PLAN
NOT TO SCALE

- NOTES:
- ALL DETAILS SHOWN ARE FOR SUGGESTION ONLY. FINAL DEFLECTOR AND FOUNDATION DESIGN AND DETAILS (SEALED) SHALL BE PROVIDED BY THE CONTRACTOR BASED ON GOVERNING CODES, SITE CONDITIONS, DEFLECTOR MANUFACTURER AND THE FOLLOWING NOTES:
- BLAST DEFLECTOR SHALL WITHSTAND FULL POWER EXHAUST VELOCITIES OF ALL NARROW BODY GROUP III BUSINESS JET AIRCRAFT WITH TAIL/FUSELAGE MOUNTED ENGINES WHEN OPERATING WITH ENGINE NOZZLE NO CLOSER THAN 60' FROM LEADING EDGE OF THE DEFLECTOR.
 - BLAST DEFLECTOR SHALL HAVE A MINIMUM NOMINAL HEIGHT OF 19' AND WIDTH OF 72'.
 - FRAME MEMBERS SHALL BE ASTM A36 STEEL AND HOT-DIP GALVANIZED TO 2 OZ/FT² PER ASTM A123.
 - DEFLECTING SURFACES SHALL BE CORRUGATED STEEL SHEETS DESIGNED TO SUPPORT LOADS IN A TWO-SPAN CONDITION. SHEET THICKNESS SHALL BE 16 GA WITH A MINIMUM 2.10 OZ/FT² (G210) HOT-DIP GALVANIZED FINISH PER ASTM A653. SHEET SECTION MODULUS SHALL BE A MINIMUM OF 0.196 IN²/FT.
 - ALL FIELD CONNECTIONS SHALL BE BOLTED. NO WELDING WILL BE PERMITTED. FASTENERS SHALL BE SAE J429 GRADES OR ASTM A449 WITH APPROPRIATE COATING FOR CORROSION RESISTANCE. ADEQUATE LOCKING PROPERTIES SHALL BE PROVIDED TO PREVENT FASTENERS FROM WORKING LOOSE DURING OPERATION.
 - THE CONTRACTOR SHALL BE REQUIRED TO HAVE AN ONSITE SUPERVISOR FROM THE DEFLECTOR MANUFACTURER DURING INSTALLATION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR FOUNDATION DESIGN AND SHALL PROVIDE SEALED FOUNDATION DRAWINGS BY A REGISTERED PROFESSIONAL ENGINEER, LICENSED ON THE STATE OF MICHIGAN. THE ANTICIPATED LOADS ARE SHOWN IN DETAIL #4 ON THIS SHEET. CONTRACTOR SHALL VERIFY LOADS WITH THE DEFLECTOR MANUFACTURER.
 - FOUNDATION CONCRETE SHALL BE P-610 WITH A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI.

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SHEET CONTENTS
**BLAST DEFLECTOR
DETAILS**

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