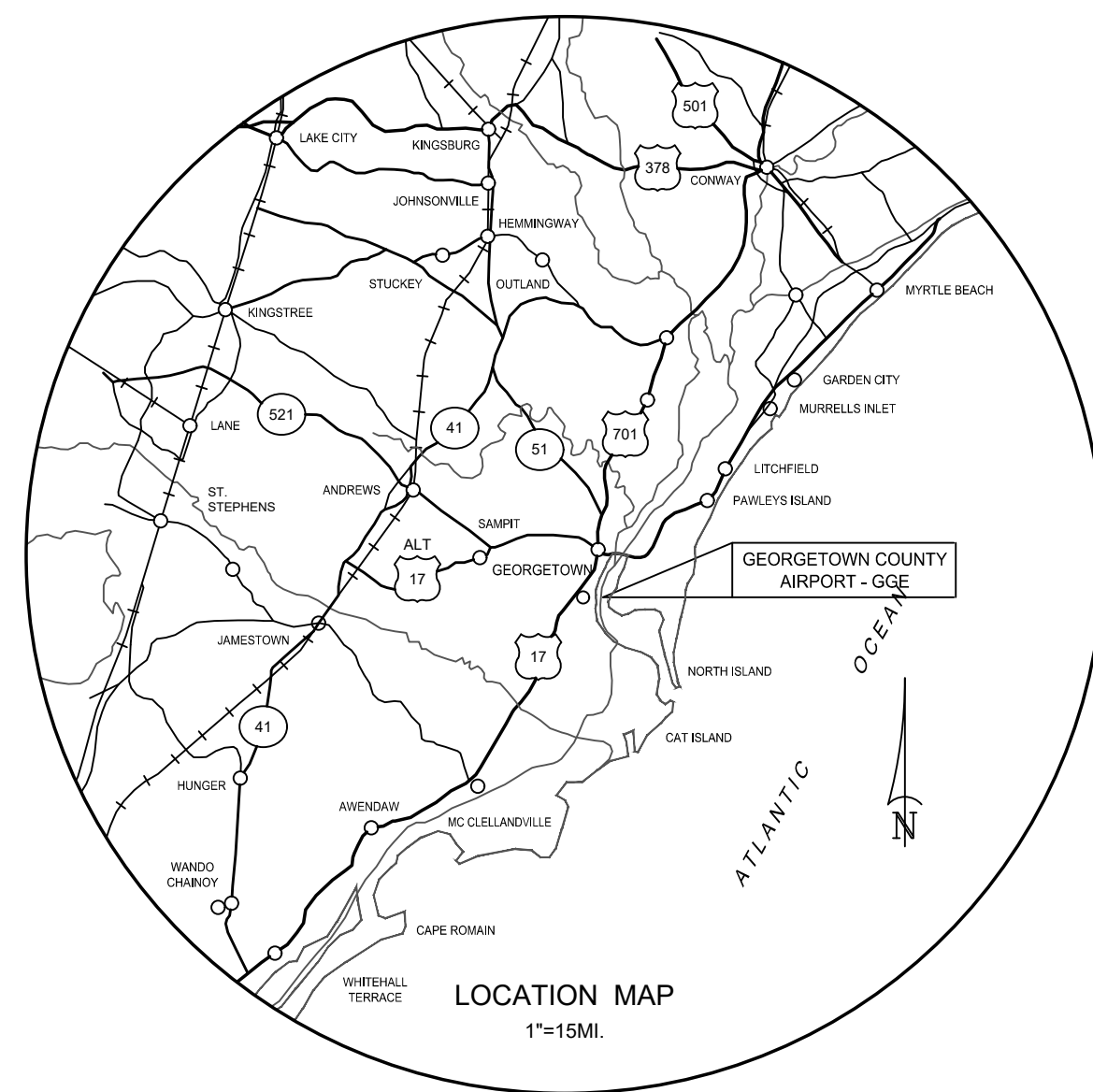


CONSTRUCTION PLANS FOR APRON RECONSTRUCTION (SCHEDULE I AND SCHEDULE II)

AIP NO.: 3-45-0025-027

BID NO.: 24-051

GEORGETOWN COUNTY AIRPORT (GGE) GEORGETOWN, SOUTH CAROLINA



LOCATION MAP

I HAVE PLACED MY SIGNATURE AND SEAL ON THE DESIGN DOCUMENTS SUBMITTED SIGNIFYING THAT I ACCEPT RESPONSIBILITY FOR THE DESIGN OF THE SYSTEM. FURTHER, I CERTIFY TO THE BEST OF MY KNOWLEDGE AND BELIEF THAT THE DESIGN IS CONSISTENT WITH THE REQUIREMENTS OF TITLE 48, CHAPTER 14 OF THE CODE OF LAWS OF SC, 1976 AS AMENDED, PURSUANT TO REGULATION 72-300 ET SEQ. (IF APPLICABLE), AND IN ACCORDANCE WITH THE TERMS AND CONDITIONS OF SCR100000.

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A2.3	PROJECT PHASING PLAN - WORK AREA 1	JUNE 2024	
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M2.1	PAVEMENT MARKING AND TIE-DOWN PLAN	JUNE 2024	
X2.1	CROSS SECTIONS	JUNE 2024	

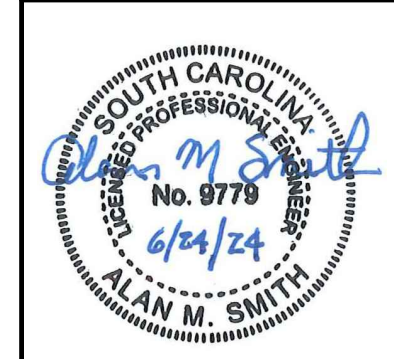


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILL@TBILL.COM
SC LICENSE NO. C00336

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REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION
(SCHEDULE I AND SCHEDULE II)
COVER SHEET



Date: JUNE 2024
Scale: NONE
Drawn: NT
Checked: AMS
Project No.: 2601-2105
Sheet No.:

A1

CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ELECTRICALLY OPERATED GATE SHALL BE TURNED OFF AND THE GATE OPENED AND CLOSED MANUALLY FOR CONSTRUCTION TRAFFIC. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RPR. DURING PROJECT ACCESS THROUGH GATE SHALL BE FOR CONSTRUCTION TRAFFIC ONLY. SEE NOTE 4 OF GENERAL NOTES.

NOTE B - AUTOMOBILE PARKING:
EXISTING AUTOMOBILE PARKING WILL BE CLOSED FOR DURATION OF THE PROJECT. CONTRACTOR SHALL INSTALL SIGN FOR CLOSED AUTOMOBILE PARKING. ANY DAMAGE CAUSED TO EXISTING PAVEMENT DUE TO CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ORIGINAL CONDITION BY THE CONTRACTOR. CONTRACTOR SHALL REMOVE THE CLOSED AUTOMOBILE PARKING SIGN AFTER COMPLETION OF THE PROJECT.

NOTE C - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS 'CONSTRUCTION ENTRANCE/EXIT' TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR, COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

CONTRACTOR'S STAGING AREA. INSTALL SILT FENCE ALONG OUTSIDE EDGE OF STAGING AREA.

INSTALL CLOSED AUTOMOBILE PARKING SIGN. SEE NOTE B.

CONCRETE WASHOUT AREA. SEE DETAIL ON SHEET EC1.2.

PORTABLE TOILETS

INSTALL LIGHTED BARRICADES FOR WORK DURATION OF THE PROJECT. SEE DETAIL ON SHEET A1.3.

ACCESS GATE (SEE NOTE A)

WORK AREA 1. SEE SHEET A1.3.

INSTALL LIGHTED BARRICADES OUTSIDE TAXIWAY 'B' OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A1.3.

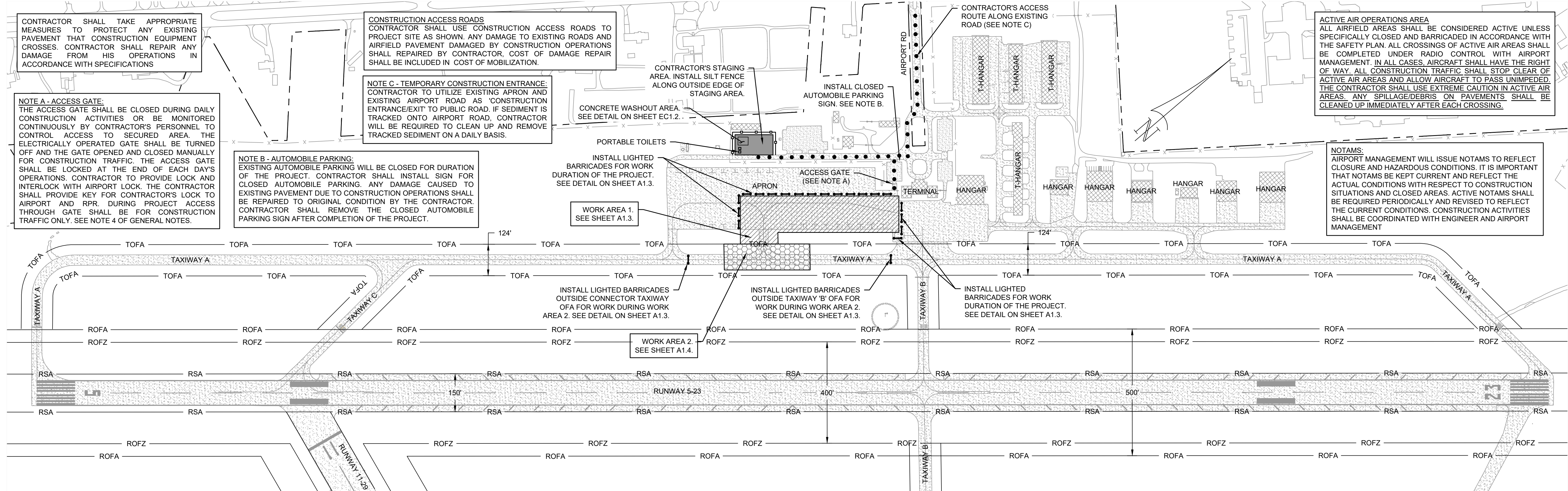
WORK AREA 2. SEE SHEET A1.4.

INSTALL LIGHTED BARRICADES OUTSIDE TAXIWAY 'B' OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A1.3.

INSTALL LIGHTED BARRICADES FOR WORK DURATION OF THE PROJECT. SEE DETAIL ON SHEET A1.3.

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT



SAFETY PLAN REQUIREMENTS

THE INTENT OF THIS PLAN IS TO ESTABLISH CERTAIN SAFETY REQUIREMENTS THAT MUST BE ADHERED TO BY THE CONTRACTOR DURING CONSTRUCTION OF THIS PROJECT. PORTION OF AIRCRAFT PARKING APRON WILL BE CLOSED TO AIR TRAFFIC AT ALL TIMES DURING THIS PROJECT. PARTIAL CLOSURE OF TAXIWAY 'A' WILL BE REQUIRED FOR WORK WITHIN TAXIWAY 'A' OBJECT FREE AREA. SEE PROJECT SPECIAL PROVISIONS FOR CLOSURE REQUIREMENTS AND DETAILS. THE AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. SEE SEQUENCE OF CONSTRUCTION ON PHASING PLANS.

- THE PROJECT AREA IS LOCATED WITHIN AND ADJACENT TO THE AIRCRAFT OPERATIONS AREA (AOA). THIS IS A CLOSELY MAINTAINED SECURITY AREA WITH RESTRICTED ACCESS. THE CONTRACTOR WILL BE REQUIRED TO MEET ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THIS AREA AT ALL TIMES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THE AOA. FURTHER, IT WILL REMAIN THE CONTRACTOR'S RESPONSIBILITY TO KEEP HIMSELF ADVISED OF ANY CHANGES IN REQUIREMENTS, TO ADHERE TO CURRENT REGULATIONS. THE CONTRACTOR SHALL ALSO HAVE PERSONNEL TRAINED TO OPERATE AND MONITOR AIRPORT SECURITY GATES USED DURING THE PROJECT.
- THE CONTRACTOR SHALL NOT BEGIN WORK UNLESS AND UNTIL 72 HOURS PRIOR NOTICE HAS BEEN GIVEN TO THE ENGINEER AND AIRPORT MANAGEMENT. CROSSING OF RUNWAYS OR TAXIWAYS IS ALLOWED ONLY IF THE RUNWAY OR TAXIWAY IS CLOSED AND SHOWN ON THIS PLAN. CONTRACTOR IS PROHIBITED FROM ENTERING THE RUNWAY 5-23 OBSTACLE FREE ZONE AT ANY TIME. SEE GENERAL NOTE 1.
- IN AN EMERGENCY SITUATION, THE CONTRACTOR SHALL CALL 911 AND NOTIFY THE AIRPORT MANAGEMENT IMMEDIATELY. THE AIRPORT CAN BE REACHED BY PHONE AT 843-545-3638.
- SEE GENERAL NOTE 4 FOR AIRPORT ENTRY AND DEPARTURE PROCEDURES AND FOR VEHICLE MARKING REQUIREMENTS.
- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL DESIGNATE A REPRESENTATIVE AND ALTERNATE TO CONTACT ON A 24 HOUR BASIS SHOULD PROBLEMS ARISE. THE CONTRACTOR SHALL PROVIDE A CONTACT LIST FOR ALL SUPERVISORY PERSONNEL AND ALL SUBCONTRACTORS TO THE ENGINEER.
- A DAILY START-UP AND SHUT-DOWN CHECKLIST WILL BE JOINTLY PREPARED BY THE CONTRACTOR AND AIRPORT MANAGEMENT. THE CHECKLIST WILL BE FOLLOWED THROUGHOUT THE PROJECT. THIS CHECKLIST SHALL INCLUDE, BUT NOT BE LIMITED TO BARRICADES, FLAGMEN, HAUL ROUTES, SECURING OF ACCESS GATES, CLEAN UP, ETC. THE CONTRACTOR'S SITE SUPERVISOR AND LABOR CREW SHALL NOT LEAVE THE WORK SITE UNTIL SUCH TIME AS THE AIRPORT HAS INSPECTED THE AREA AND SIGNED OFF ON THE DAILY CHECKLIST.
- UNDERGROUND UTILITIES ARE KNOWN TO BE LOCATED IN THE PROJECT AREAS. EXISTING UNDERGROUND UTILITIES INCLUDING BUT NOT LIMITED TO AIRFIELD LIGHTING, NAVAID POWER AND CONTROL CABLES MAY BE IN THE PATH OF CONSTRUCTION. LOCATIONS OF UTILITIES, IF SHOWN ON THE PLANS, ARE APPROXIMATE ONLY. ALL UTILITIES AND FACILITIES ARE NOT NECESSARILY INDICATED ON PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES FROM DAMAGE. SEE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGEMENT WHEN WORKING IN AREAS CONTAINING AIRFIELD LIGHTING, NAVAID CABLE, OR OTHER UTILITIES.

CONTACTS ARE:
AIRPORT DIRECTOR MR. JIM TAYLOR 843-545-3638

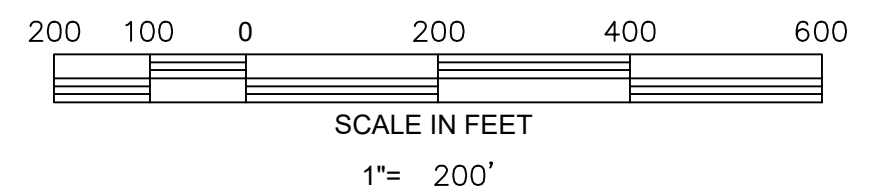
FOR ADDITIONAL REQUIREMENTS RELATED TO PROTECTION OF EXISTING UTILITIES, INCLUDING CABLES, CONTROLS, AND NAVAIDS SEE PROJECT SPECIAL PROVISIONS.

- THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS IN REGARD TO CONSTRUCTION NOISE AND EROSION CONTROL DURING CONSTRUCTION.

- THE CONTRACTOR SHALL CLEAN ALL CONSTRUCTION AREAS OF LITTER, LOOSE PAPERS, DEBRIS, ETC. ON A DAILY BASIS, OR AS DIRECTED BY THE ENGINEER. PRIOR TO THE CLOSE OF DAILY OPERATIONS, CONTRACTOR SHALL INSPECT ALL ACTIVE AIR OPERATIONS AREAS AND CONSTRUCTION AREA FOR FOD AND LITTER. ALL DEBRIS SHALL BE CLEANED UP AND PROPERLY DISPOSED OF PRIOR TO RELEASE OF CREWS FROM EACH SHIFT.
- MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL ARE NOT ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) OF AN OPEN RUNWAY AT ANY TIME UNLESS THE RUNWAY IS CLOSED OR THE TAXIWAY OBJECT FREE AREA (TOFA) UNLESS THE SECTION OF TAXIWAY IS CLOSED (SEE GENERAL NOTE 1). MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL WILL NOT BE PERMITTED CLOSER THAN 200 FEET FROM THE EDGE OF THE RUNWAY OR 50 FEET FROM THE EDGE OF ANY TAXIWAY WITHOUT PRIOR PERMISSION FROM THE AIRPORT MANAGEMENT.
- DURING CONSTRUCTION, ADJACENT TAXIWAYS AND THE RUNWAY WILL BE OPEN TO AIRCRAFT UNLESS OTHERWISE NOTED. AIRCRAFT HAVE THE RIGHT OF WAY AT ALL TIMES. THE CONTRACTOR SHALL BE AWARE OF THE AIRCRAFT MOVEMENTS AND THE JETBLAST AND/OR PROP-WASH ASSOCIATED WITH THESE AIRCRAFT. THE CONTRACTOR SHALL SECURE LOOSE ITEMS AT ALL TIMES AND SHALL LOCATE STOCKPILES OF MATERIALS OR EQUIPMENT AWAY FROM AIRCRAFT OPERATION AREAS.
- INSPECTION - FREQUENT INSPECTIONS WILL BE MADE BY AIRPORT MANAGEMENT DURING CRITICAL PHASES OF THE WORK TO ENSURE THAT THE CONTRACTOR IS FOLLOWING THE RECOMMENDED AIRFIELD SAFETY PROCEDURES.
- EXCAVATIONS - CONTRACTOR MUST PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE AIRPORT, AND BACKFILL OR LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN 250 FEET OF THE RUNWAY CENTERLINE WHILE THE RUNWAY IS OPEN. IF THE RUNWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, COVER OR BACKFILL THE EXCAVATIONS APPROPRIATELY. COVERINGS FOR OPEN TRENCHES OR EXCAVATIONS MUST BE OF SUFFICIENT STRENGTH TO SUPPORT THE WEIGHT OF THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY.
- ALL AIRFIELD LIGHTING AND LIGHTED SIGNS OUTSIDE AREAS CLOSED FOR CONSTRUCTION SHALL BE KEPT OPERATIONAL THROUGHOUT THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL DISABLE TAXIWAY LIGHTING IN CLOSED SECTIONS OF TAXIWAYS BY INSTALLING TEMPORARY JUMPER CABLE OR COVERING LIGHTS. ALL GUIDANCE SIGNS WITHIN CLOSED AREAS AND LEADING TO CLOSED AREAS SHALL BE COVERED.
- NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA OF AN ACTIVE RUNWAY OR TAXIWAY OBJECT FREE AREA OF AN ACTIVE TAXIWAY. ANY WORK WITHIN AN ACTIVE RUNWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE RUNWAY. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN A ACTIVE TAXIWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE EFFECTED SECTION OF TAXIWAY. PULLBACKS FOR MEN AND EQUIPMENT WITHIN THE RUNWAY OBSTACLE FREE ZONE WILL NOT BE ALLOWED (SEE GENERAL NOTE 1).

NOTAMS (NOTICE TO AIRMEN)

THE AIRPORT MANAGEMENT WILL ISSUE THE NECESSARY NOTAMS TO REFLECT HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS. ACTIVE NOTAMS SHALL BE REVIEWED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS.



GENERAL NOTES:

- IT IS THE INTENT OF THE OWNER THAT THE GEORGETOWN COUNTY AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. RUNWAY 5-23 AND RUNWAY 11-29 SHALL NOT BE CLOSED. PARTIAL CLOSURE OF TAXIWAY 'A' WILL BE REQUIRED FOR THE CONSTRUCTION OF PROJECT. THE CONTRACTOR SHALL PROVIDE A MINIMUM 72 HOURS NOTICE TO AIRPORT MANAGEMENT AND ENGINEER PRIOR TO ANY PROPOSED CLOSURE DATES. PRIOR TO REOPENING ANY PAVEMENT, THE CONTRACTOR MUST REMOVE BARRICADES AND PERFORM A WALK THROUGH OF THE CONSTRUCTION AREA WITH AIRPORT MANAGEMENT, THE RESIDENT PROJECT REPRESENTATIVE, OR ENGINEER TO CONFIRM THAT THE PAVEMENT AND SAFETY AREAS ARE FREE OF FOD OR OTHER HAZARDS.
 - PRIOR TO LEAVING WORK EACH DAY, CONTRACTOR SHALL RETURN HIS EQUIPMENT AND MATERIALS TO THE STAGING AREA IDENTIFIED ON THE PLANS.
 - ALL CONTRACTOR PERSONNEL, INCLUDING BUT NOT LIMITED TO, GENERAL LABORERS, SUBCONTRACTORS, DRIVERS, AND JOURNEYMEN WORKING WITHIN ACTIVE AIR OPERATIONS AREAS MUST AT ALL TIMES REMAIN WITHIN VISUAL AND VOICE RANGE OF CONTRACTOR SUPERVISORY PERSONNEL. FOR THE PURPOSES OF THIS PROJECT, THE AIR OPERATIONS AREA (AOA) REFERS TO ALL AREAS WITHIN THE AIRPORT SECURITY FENCE.
 - PRIOR TO ENTERING THE SECURED AOA OF THE AIRPORT EACH DAY, THE CONTRACTOR SHALL CHECK IN WITH THE AIRPORT. CLOSE COORDINATION FOR ACCESS TO WORK AREAS AND SCHEDULES BETWEEN THE CONTRACTOR, AND AIRPORT WILL BE REQUIRED THROUGHOUT THE PROJECT.
- THE CONTRACTOR SHALL COORDINATE INGRESS-EGRESS REQUIREMENTS WITH THE AIRPORT MANAGEMENT AND RESIDENT PROJECT REPRESENTATIVE (RPR). ALL OPEN GATES TO SECURED AIRPORT AREAS SHALL BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA OR SHALL BE CLOSED AND LOCKED. CONTRACTOR PERSONNEL SHALL NOT ALLOW ANY UNAUTHORIZED PERSONNEL TO ENTER THROUGH THE CONSTRUCTION GATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING AND LOCKING ALL GATES WHEN NOT IN USE AND AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR SHALL PROVIDE INTERLOCK AT PADLOCKED GATES. CONTRACTOR SHALL PROVIDE A COPY OF ALL GATE KEYS TO THE AIRPORT AND RPR. CONTRACTOR SHALL PROVIDE A LIST OF ALL KEY HOLDERS WHICH SHALL BE KEPT UPDATED THROUGHOUT THE PROJECT.
- ACCESS BY PRIVATE VEHICLES TO THE APRON SOUTH OF THE PROJECT AND IN FRONT OF THE TERMINAL BUILDING SHALL BE COORDINATED THROUGH AND ESCORTED BY REPRESENTATIVES OF THE FBO. ACCESS TO THE APRON SHALL BE THROUGH GATE LOCATED NEAR T-HANGARS.
- ALL CONSTRUCTION VEHICLES MUST BE CLEARED FOR ACCESS BY THE AIRPORT MANAGEMENT AND RESIDENT PROJECT REPRESENTATIVE. PERSONAL CARS SHALL BE PARKED IN STAGING AREA. ALL VEHICLES OPERATING IN THE AOA SHALL BE LIGHTED OR FLAGGED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G. COPIES OF THE ADVISORY CIRCULAR WILL BE MADE AVAILABLE UPON REQUEST.
- THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN SUCH A MANNER AS TO ASSURE THAT SUCH OPERATIONS DO NOT IMPEDE ACCESS TO ANY AREA OF THE AIRFIELD AT ANY TIME FOR THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) VEHICLES AND OTHER EMERGENCY VEHICLES. EMERGENCY VEHICLE ACCESS SHALL BE A STANDING AGENDA ITEM FOR ALL PROGRESS MEETINGS. THE CONTRACTOR SHALL COOPERATE FULLY AND IMMEDIATELY WITH ANY DIRECTIVES ISSUED BY AIRPORT MANAGEMENT RELATIVE TO EMERGENCY ACCESS.
- ACCESS ROADS TO BE USED UNDER THIS CONTRACT SHALL BE THOSE DESIGNATED AND APPROVED BY THE ENGINEER. IN GENERAL, THE CONTRACTOR SHALL CONFINE HIS EQUIPMENT AND HAULING WHERE PRACTICAL TO EXISTING ROADS ON THE AIRPORT. IF EXISTING PAVEMENT OR ROAD SURFACE IS DAMAGED BY THE CONTRACTOR'S HAULING OPERATIONS, IT SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. HAUL ROADS ACROSS TURFED AREAS SHALL BE REPAIRED, SCARIFIED, SEED, MULCHED, AND FERTILIZED AT THE CONTRACTOR'S EXPENSE. METAL TRACK VEHICLES WILL NOT BE PERMITTED TO OPERATE ON OR ACROSS EXISTING PAVEMENT WITHOUT PROTECTIVE MATTING TO PREVENT MARRING OF THE PAVEMENT SURFACE. ACCESS ROADS SHALL BE CONSTRUCTED BY CONTRACTOR AS REQUIRED. ALL COSTS ASSOCIATED WITH SUPPLYING, CONSTRUCTING, MAINTAINING AND RESTORING TEMPORARY HAUL AND ACCESS ROADS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR "TEMPORARY CONSTRUCTION ITEMS"

- ALL EXISTING FACILITIES WILL BE CAREFULLY PROTECTED BY THE CONTRACTOR. ANY FACILITIES DAMAGED BY THE CONTRACTOR WILL BE REPAIRED IMMEDIATELY AND RESTORED TO ORIGINAL CONDITION AT CONTRACTOR'S COST.
- CONTRACTOR WILL, BY WATERING, CHEMICALS, VEGETATION, OR OTHER MEANS, PREVENT THE OCCURRENCE OF DUST WHICH WILL BE OBJECTIONABLE TO THE RESIDENTS OF THE AREA, VIOLATE EXISTING LAWS OR REGULATION, OR CAUSE HAZARDS TO AIR TRAFFIC.
- CONTRACTOR MAY ENCOUNTER WET CONDITIONS DURING CONSTRUCTION. ALL COST FOR DEWATERING IS CONSIDERED INCIDENTAL TO COST OF ITEMS OF WORK BID UPON.
- SEE PROJECT SPECIAL PROVISIONS FOR PROTECTION OF UTILITIES.

CONSTRUCTION CONTRACTOR'S RESPONSIBILITIES

- CONTRACTOR SHALL HAVE AVAILABLE A COPY OF THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPF) AND PROJECT AIRPORT SAFETY PLAN ON SITE AT ALL TIMES. SEE APPENDIX 'G' OF THE SPECIFICATIONS.
- CONTRACTOR SHALL COMPLY WITH THE AIRPORT SAFETY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT.
- CONTRACTOR SHALL PROVIDE A POINT OF CONTACT WHO WILL COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION-RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT.
- CONTRACTOR SHALL PROVIDE A SAFETY/CONSTRUCTION INSPECTOR FAMILIAR WITH AIRPORT SAFETY TO MONITOR CONSTRUCTION ACTIVITIES AT ALL TIMES DURING CONSTRUCTION.
- CONTRACTOR SHALL RESTRICT MOVEMENT OF CONSTRUCTION VEHICLES TO CONSTRUCTION AREAS BY FLAGGING AND BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING FLAGMEN OR ESCORTS AS APPROPRIATE.
- CONTRACTOR SHALL ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE ACTIVE AIR OPERATIONS AREAS (AOA) FROM THE CONSTRUCTION SITE UNLESS AUTHORIZED.

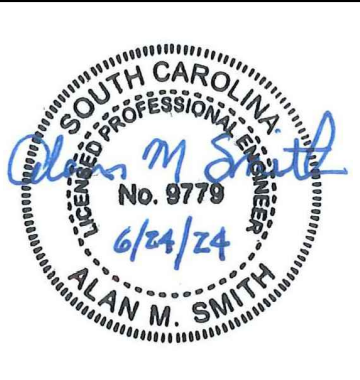
LEGEND	
---	APPROXIMATE AIRPORT PROPERTY LINE
---	RSA RUNWAY SAFETY AREA
---	ROFA RUNWAY OBJECT FREE AREA
---	ROFZ RUNWAY OBSTACLE FREE ZONE
---	TOFA TAXIWAY OBJECT AREA
▨	WORK AREA 1
▩	WORK AREA 2
●●●●●●●●	CONTRACTOR'S ACCESS ROUTE
■	CONTRACTOR'S STAGING/STOCKPILE AREA
---	LIGHTED BARRICADES

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLEY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C002396
EMAIL: TBILL@TBILL.COM

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REV. NO.	DESCRIPTION	DATE
	REVISIONS	

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
PROJECT LAYOUT AND SAFETY PLAN



Date	JUNE 2024
Scale	1" = 200'
Drawn	NT
Checked	AMS
Project No.	2601-2105
Sheet No.	

A1.2

CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR. COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

NOTE C - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS 'CONSTRUCTION ENTRANCE/EXIT' TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ELECTRICALLY OPERATED GATE SHALL BE TURNED OFF AND THE GATE OPENED AND CLOSED MANUALLY FOR CONSTRUCTION TRAFFIC. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RFD. DURING PROJECT ACCESS THROUGH GATE SHALL BE FOR CONSTRUCTION TRAFFIC ONLY. SEE NOTE 4 OF GENERAL NOTES ON SHEET A1.2.

NOTE B - AUTOMOBILE PARKING:
EXISTING AUTOMOBILE PARKING WILL BE CLOSED FOR DURATION OF THE PROJECT. CONTRACTOR SHALL INSTALL SIGN FOR CLOSED AUTOMOBILE PARKING. ANY DAMAGE CAUSED TO EXISTING PAVEMENT DUE TO CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ORIGINAL CONDITION BY THE CONTRACTOR. CONTRACTOR SHALL REMOVE THE CLOSED AUTOMOBILE PARKING SIGN AFTER COMPLETION OF THE PROJECT.

INSTALL LIGHTED BARRICADES 10' FROM PAVEMENT RECONSTRUCTION LIMITS FOR WORK DURATION OF THE PROJECT. SEE DETAIL THIS SHEET.

CONCRETE WASHOUT AREA. SEE DETAIL ON SHEET EC1.2.

PORTABLE TOILETS (SEE NOTE D)

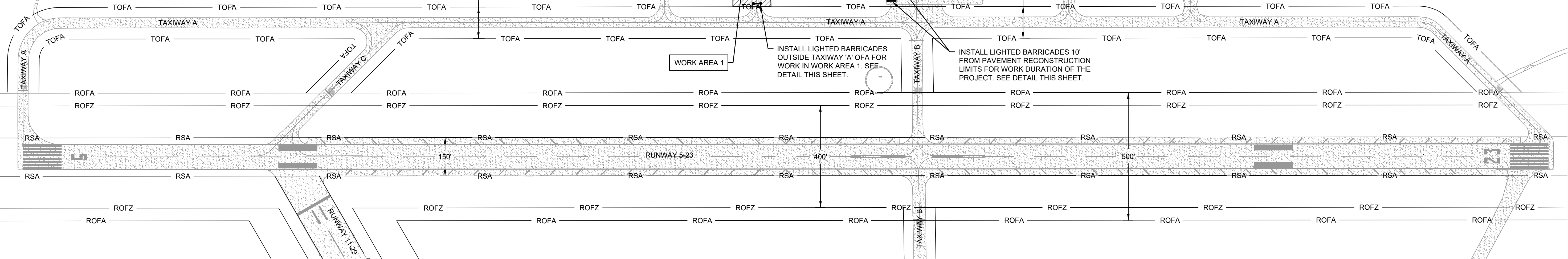
CONTRACTOR'S STAGING AREA. INSTALL SILT FENCE ALONG OUTSIDE EDGE OF STAGING AREA.

INSTALL CLOSED AUTOMOBILE PARKING SIGN. SEE NOTE B.

CONTRACTOR'S ACCESS ROUTE ALONG EXISTING ROAD (SEE NOTE C)

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT



THE SEQUENCE OF CONSTRUCTION FOR THIS PROJECT WILL FOLLOW TYPICAL PATTERN FOR PROJECTS OF THIS TYPE. INCLUDING ESTABLISHMENT OF STAGING AREA AND STOCKPILE AREA. INSTALLATION OF APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES. PAVEMENT REMOVAL, GRADING, PAVING, MARKING, LIGHTING, AND SEEDING AND MULCHING.

THE FOLLOW SEQUENCE OF CONSTRUCTION HAS BEEN DEVELOPED TO HELP THE CONTRACTOR UNDERSTAND THE OPERATIONAL NEEDS OF THE AIRPORT AND HELP ENSURE MINIMAL CLOSURE TIME. IN ACCORDANCE WITH THE SPECIFICATIONS, THE CONTRACTOR SHALL PROVIDE A DETAILED SCHEDULE OF CONSTRUCTION TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.

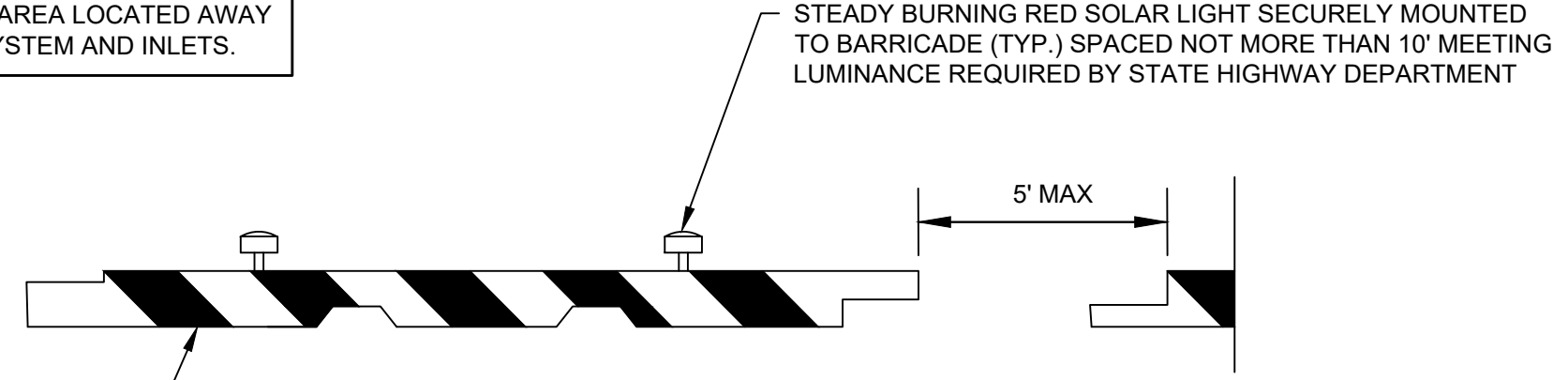
PRIOR TO BEGINNING PROJECT AND CONTRACT START TIME

1. DEVELOP DETAILED SCHEDULE TO ENSURE CONSTRUCTION CAN BE COMPLETED FOR ALL WORK WITHIN CONSTRUCTION TIME ALLOTTED FOR THE PROJECT.
2. MOBILIZE EQUIPMENT AND SET UP STAGING AND STOCKPILE AREA FOR THE PROJECT. LOCATION OF STAGING AND STOCKPILE AREA IS SHOWN ON THE SAFETY AND PHASING PLANS SHALL BE COORDINATED WITH AIRPORT MANAGEMENT. INSTALL TEMPORARY SILT FENCE AT STAGING AREA AS SHOWN.
3. CONTRACTOR MAY COMPLETE REQUIRED SURVEY WORK ON PROJECT AREA PRIOR TO CONSTRUCTION START TIME. SCHEDULING FOR THE SURVEY WORK WILL BE REQUIRED TO BE APPROVED BY AIRPORT MANAGEMENT. SEE PROJECT SPECIAL PROVISIONS.

WORK AREA 1 - CONSTRUCTION SHALL INCLUDE SEDIMENTATION AND EROSION CONTROL; DEMOLITION AND REMOVAL OF EXISTING PAVEMENT; LIGHTING DEMOLITION; EARTHWORK; AGGREGATE BASE COURSE; P-401 AND P-404 ASPHALT SURFACE COURSE; MARKING; LIGHTING INSTALLATION; SEEDING; SODDING; AND MULCHING. ALL WORK IN WORK AREA 1 SHALL BE COMPLETED WITHIN THE 75 CALENDAR DAYS CONTRACT TIME FOR THE PROJECT.

1. INSTALL LIGHTED BARRICADES AT LOCATIONS SHOWN ON THIS SHEET. CONTRACTOR WILL BE WORKING ADJACENT TO ACTIVE TAXIWAY AND SHALL UTILIZE CAUTION AS REQUIRED BY THE SAFETY PLAN.
2. INSTALL TEMPORARY SEDIMENT BASIN AND OTHER SEDIMENTATION AND EROSION CONTROL MEASURES AS SHOWN ON SHEET EC1.1.
3. COMPLETE AIRCRAFT PARKING TIE-DOWN REMOVAL, PAVEMENT REMOVAL, AND LIGHTING REMOVAL OPERATIONS. MILLINGS SHALL BE REMOVED FROM AIRPORT AND DISPOSED OF AT DESIGNATED OFF SITE LOCATION.
4. COMPLETE GRADING OPERATIONS FOR PROJECT SITE, FINE GRADE APRON AND CONNECTOR TAXIWAY SUBGRADE AND COMPACT SUBGRADE AS REQUIRED.
5. COMPLETE PLACEMENT, GRADING AND COMPACTION OF AGGREGATE BASE COURSE FOR APRON AND CONNECTOR TAXIWAY.
6. APPLY BITUMINOUS PRIME COAT TO AGGREGATE BASE IN ACCORDANCE WITH SPECIFICATIONS.
7. COMPLETE PAVING OPERATIONS ON APRON AND CONNECTOR TAXIWAY. PAVEMENT SHALL BE PLACED IN TWO 2" LIFTS OF P-401 ASPHALT SURFACE COURSE ON CONNECTOR TAXIWAY AND ONE 2" LIFT OF P-401 ASPHALT SURFACE COURSE AND ONE 2" LIFT OF P-404 FUEL RESISTANT ASPHALT SURFACE COURSE ON THE APRON.
8. COMPLETE INSTALLATION OF NEW TAXIWAY LIGHTING SYSTEM MODIFICATIONS AS SHOWN ON THE PLANS.
9. COMPLETE PLACEMENT OF SHOULDER FILL, COMPACT AND FINE GRADE SHOULDERS IN ACCORDANCE WITH DETAILS SHOWN ON THE PLANS.
10. COMPLETE FIRST APPLICATION OF PAVEMENT MARKING. MARKING SHALL BE INSTALLED WITHOUT BEADS.
11. COMPLETE INSTALLATION OF SOD ALONG THE EDGE OF PAVEMENT AND SEEDING AND MULCHING OPERATIONS FOR SITE. RESTORE ALL DISTURBED AREAS AND SEED AND MULCH ALL AREAS AS REQUIRED.
12. REMOVE ALL EQUIPMENT, MATERIALS AND DEBRIS.
13. REMOVE LIGHTED BARRICADES AND OPEN APRON AND CONNECTOR TAXIWAY TO AIR OPERATIONS.
14. AFTER 30 DAY CURING PERIOD, CONTRACTOR SHALL APPLY FINAL APPLICATION OF MARKING WITH BEADS FOR ALL MARKINGS.
15. AFTER GRASS IS ESTABLISHED AND SITE IS STABILIZED OR AS DIRECTED BY THE ENGINEER, REMOVE ALL TEMPORARY EROSION MEASURES.

NOTE D - PORTABLE TOILET FACILITIES:
PORTABLE TOILET FACILITIES SHALL BE LOCATED WITHIN STAGING AREA LOCATED AWAY FROM STORM DRAINAGE SYSTEM AND INLETS.



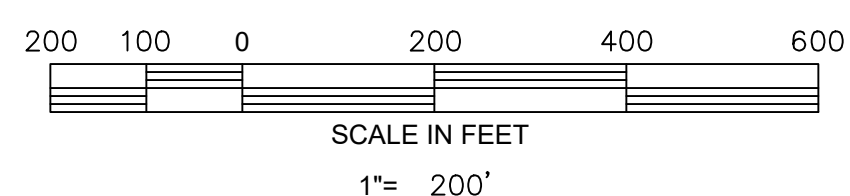
4" WIDE REFLECTIVE ORANGE TAPE AT 12" O.C. SET DIAGONAL TO BARRICADE TO CREATE ALTERNATING ORANGE/WHITE STRIPES (TYP.)
LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

AVIATION BARRICADE NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
2. LOW PROFILE LIGHTED BARRICADES SHALL BE MULTI-BARRIER AIRPORT RUNWAY BARRICADE AR10X96 AND SOLAR LIGHTS SHALL BE PROVIDED FOR THE BARRICADES AS MANUFACTURED BY OFF THE WALL PRODUCTS, LLC. OR APPROVED EQUAL. THE CONTRACTOR SHALL PROVIDE WATER NECESSARY TO FILL THE BARRICADES AND ENSURE ADEQUATE WATER HAS BEEN PROVIDED TO ANCHOR THE BARRICADES IN PLACE.
3. CONTRACTOR SHALL CHECK LIGHTS DAILY TO VERIFY THAT THEY ARE IN WORKING CONDITION AND SHALL REPLACE LIGHTS AS REQUIRED.
4. CONTRACTOR SHALL INSTALL BARRICADES AT LOCATIONS SHOWN ON INDIVIDUAL PLAN SHEETS. BARRICADES SHALL BE MOVED AND RELOCATED AS REQUIRED. BARRICADES SHALL BE INSTALLED WITH MAXIMUM 5' SPACE BETWEEN BARRICADE AND CONTRACTOR SHALL LEAVE ONE 15' SPACE FOR EMERGENCY VEHICLES. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST.

LEGEND

---	APPROXIMATE AIRPORT PROPERTY LINE
---	RSA RUNWAY SAFETY AREA
---	ROFA RUNWAY OBJECT FREE AREA
---	ROFZ RUNWAY OBSTACLE FREE ZONE
---	TOFA TAXIWAY OBJECT AREA
▨	WORK AREA 1
●●●●●●●●	CONTRACTOR'S ACCESS ROUTE
■	CONTRACTOR'S STAGING/STOCKPILE AREA
- - - - -	LIGHTED BARRICADES



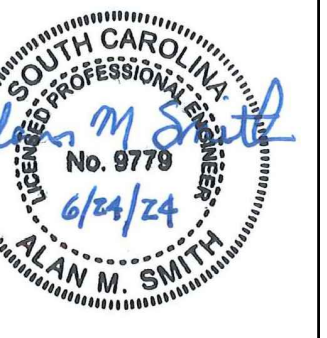
TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLEY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C00296
EMAIL: TBILL@TBILL.COM

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1	ADDED NOTE D	02/29/24

REVISIONS

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
PROJECT PHASING PLAN - WORK AREA 1



Date	JUNE 2024
Scale	1" = 200'
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Sheet No.	

A1.3

CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR. COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

NOTE C - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS CONSTRUCTION ENTRANCE/EXIT TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

NOTE B - AUTOMOBILE PARKING:
EXISTING AUTOMOBILE PARKING WILL BE CLOSED FOR DURATION OF THE PROJECT. CONTRACTOR SHALL INSTALL SIGN FOR CLOSED AUTOMOBILE PARKING. ANY DAMAGE CAUSED TO EXISTING PAVEMENT DUE TO CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ORIGINAL CONDITION BY THE CONTRACTOR. CONTRACTOR SHALL REMOVE THE CLOSED AUTOMOBILE PARKING SIGN AFTER COMPLETION OF THE PROJECT.

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ELECTRICALLY OPERATED GATE SHALL BE TURNED OFF AND THE GATE OPENED AND CLOSED MANUALLY FOR CONSTRUCTION TRAFFIC. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RPR. DURING PROJECT ACCESS THROUGH GATE SHALL BE FOR CONSTRUCTION TRAFFIC ONLY. SEE NOTE 4 OF GENERAL NOTES ON SHEET A1.2.

INSTALL LIGHTED BARRICADES 10' FROM PAVEMENT RECONSTRUCTION LIMITS FOR WORK DURATION OF THE PROJECT. SEE DETAIL ON SHEET A1.3.

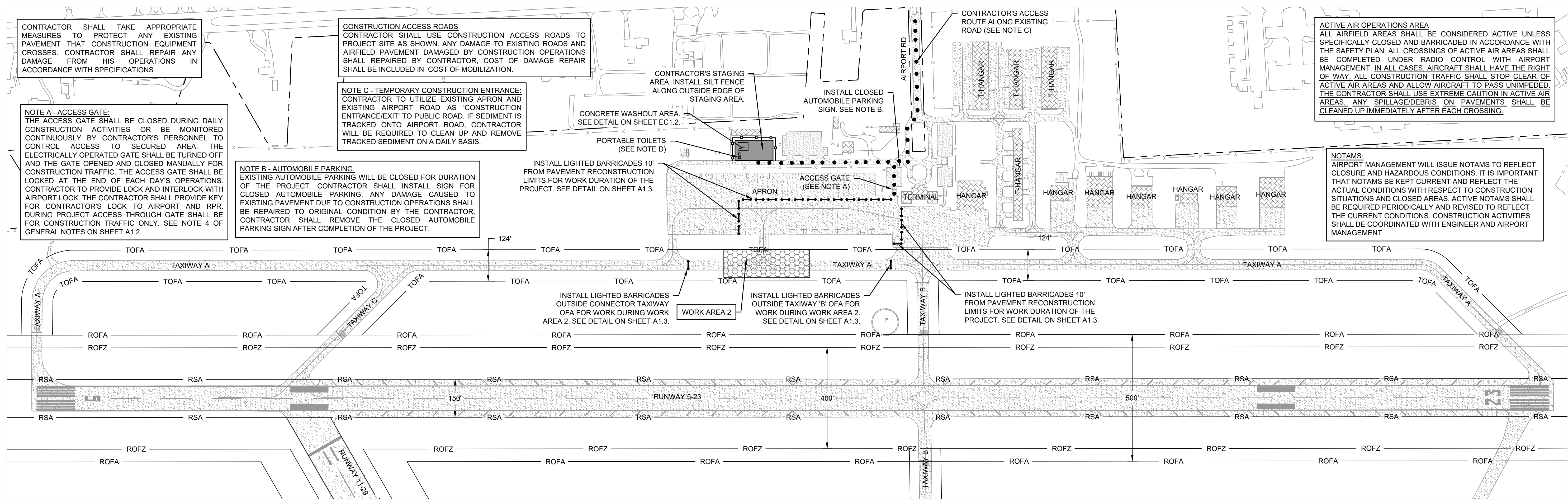
INSTALL LIGHTED BARRICADES OUTSIDE CONNECTOR TAXIWAY OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A1.3.

INSTALL LIGHTED BARRICADES OUTSIDE TAXIWAY 'B' OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A1.3.

INSTALL LIGHTED BARRICADES 10' FROM PAVEMENT RECONSTRUCTION LIMITS FOR WORK DURATION OF THE PROJECT. SEE DETAIL ON SHEET A1.3.

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT

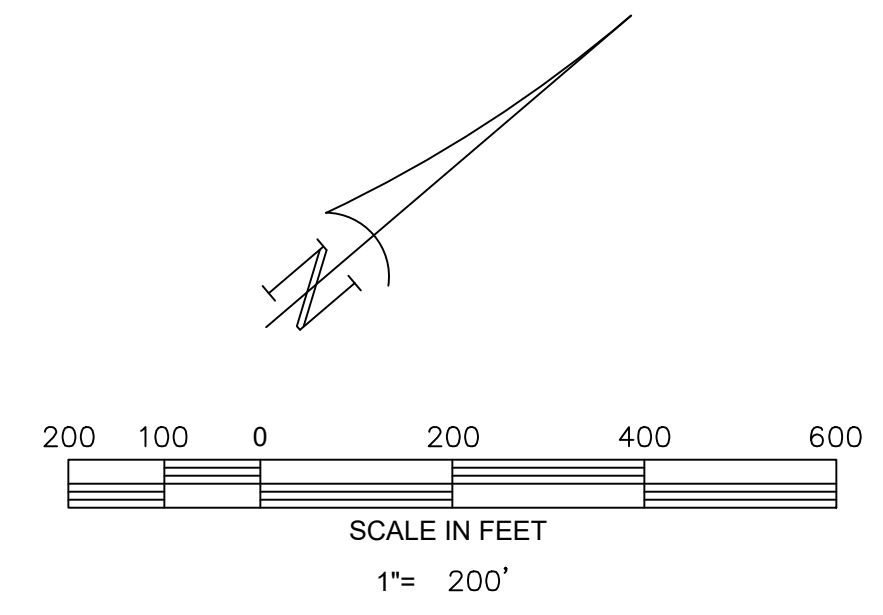


NOTE D - PORTABLE TOILET FACILITIES:
PORTABLE TOILET FACILITIES SHALL BE LOCATED WITHIN STAGING AREA LOCATED AWAY FROM STORM DRAINAGE SYSTEM AND INLETS.

WORK AREA 2 SEQUENCE OF CONSTRUCTION - CONSTRUCTION IN WORK AREA 2 SHALL REQUIRE PARTIAL CLOSURE OF TAXIWAY 'A' AS SHOWN ON THIS PLAN. WORK ELEMENTS SHALL INCLUDE SEDIMENTATION AND EROSION CONTROL, DEMOLITION AND REMOVAL OF EXISTING PAVEMENT, LIGHTING DEMOLITION, EARTHWORK, AGGREGATE BASE COURSE, P-401 ASPHALT SURFACE COURSE, MARKING, LIGHTING INSTALLATION, SEEDING, SODDING, AND MULCHING. ALL WORK IN WORK AREA 2 SHALL BE COMPLETED IN 12 CALENDAR DAYS WITHIN THE TOTAL CONTRACT TIME FOR THE PROJECT OF 75 CALENDAR DAYS.

- INSTALL LIGHTED BARRICADES AT LOCATIONS SHOWN ON THIS SHEET. CONTRACTOR WILL BE WORKING ADJACENT TO ACTIVE TAXIWAY AND SHALL UTILIZE CAUTION AS REQUIRED BY THE SAFETY PLAN. DE-ENERGIZE THE TAXIWAY LIGHTS WITHIN THE CLOSED PORTION OF TAXIWAY USING TEMPORARY JUMPER CABLE.
- INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS SHOWN ON SHEET EC1.1.
- COMPLETE PAVEMENT REMOVAL AND LIGHTING REMOVAL OPERATIONS. MILLINGS SHALL BE REMOVED FROM AIRPORT AND DISPOSED OF AT DESIGNATED OFF SITE LOCATION.
- COMPLETE GRADING OPERATIONS FOR PROJECT SITE, FINE GRADE CONNECTOR TAXIWAY SUBGRADE AND COMPACT SUBGRADE AS REQUIRED.
- COMPLETE PLACEMENT, GRADING AND COMPACTION OF AGGREGATE BASE COURSE FOR CONNECTOR TAXIWAY.
- APPLY BITUMINOUS PRIME COAT TO AGGREGATE BASE IN ACCORDANCE WITH SPECIFICATIONS.
- COMPLETE PAVING OPERATIONS ON CONNECTOR TAXIWAY. PAVEMENT SHALL BE PLACED IN TWO 2" LIFTS OF P-401 ASPHALT SURFACE COURSE.
- COMPLETE INSTALLATION OF NEW TAXIWAY LIGHTING SYSTEM MODIFICATIONS AS SHOWN ON THE PLANS.
- COMPLETE PLACEMENT OF SHOULDER FILL, COMPACT AND FINE GRADE SHOULDERS IN ACCORDANCE WITH DETAILS SHOWN ON THE PLANS.
- COMPLETE FIRST APPLICATION OF PAVEMENT MARKING. MARKING SHALL BE INSTALLED WITHOUT BEADS.
- COMPLETE INSTALLATION OF SOD ALONG THE EDGE OF PAVEMENT AND SEEDING AND MULCHING OPERATIONS FOR SITE. RESTORE ALL DISTURBED AREAS AND SEED AND MULCH ALL AREAS AS REQUIRED.
- REMOVE ALL EQUIPMENT, MATERIALS AND DEBRIS.
- REMOVE LIGHTED BARRICADES AND OPEN TAXIWAY 'A' TO AIR OPERATIONS.
- AFTER 30 DAY CURING PERIOD, CONTRACTOR SHALL APPLY FINAL APPLICATION OF MARKING WITH BEADS FOR ALL MARKING.
- AFTER GRASS IS ESTABLISHED AND SITE IS STABILIZED OR AS DIRECTED BY THE ENGINEER, REMOVE ALL TEMPORARY EROSION MEASURES.

- LEGEND
- APPROXIMATE AIRPORT PROPERTY LINE
 - RSA — RUNWAY SAFETY AREA
 - ROFA — RUNWAY OBJECT FREE AREA
 - ROFZ — RUNWAY OBSTACLE FREE ZONE
 - TOFA — TAXIWAY OBJECT AREA
 - ▨ WORK AREA 2
 - CONTRACTOR'S ACCESS ROUTE
 - CONTRACTORS STAGING/STOCKPILE AREA
 - - - - - LIGHTED BARRICADES



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
4810 SHELLEY DRIVE
SC LICENSE NO. C002986
EMAIL: TBILL@TBILL.COM

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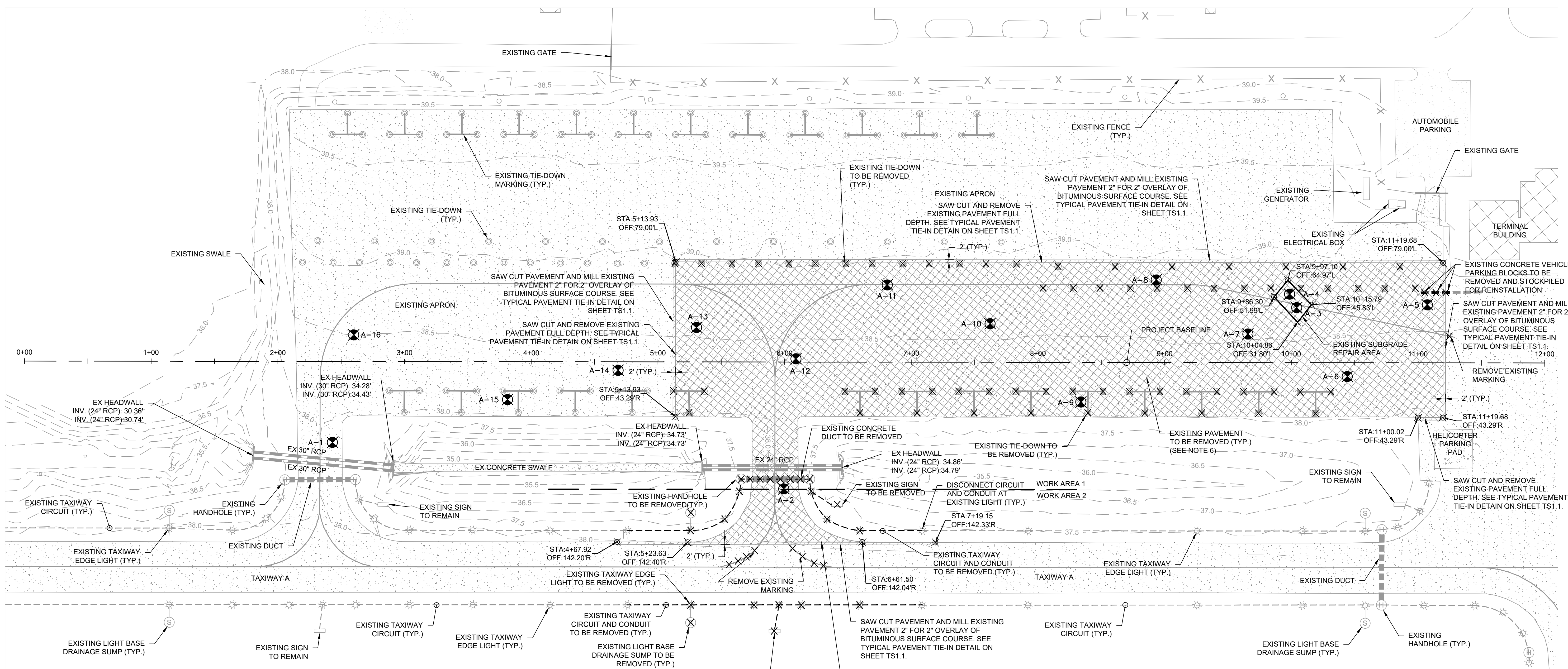
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1	ADDED NOTE D	02/29/24

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
PROJECT PHASING PLAN - WORK AREA 2



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Checked: AMS
Project No.: 2601-2105
Sheet No.:

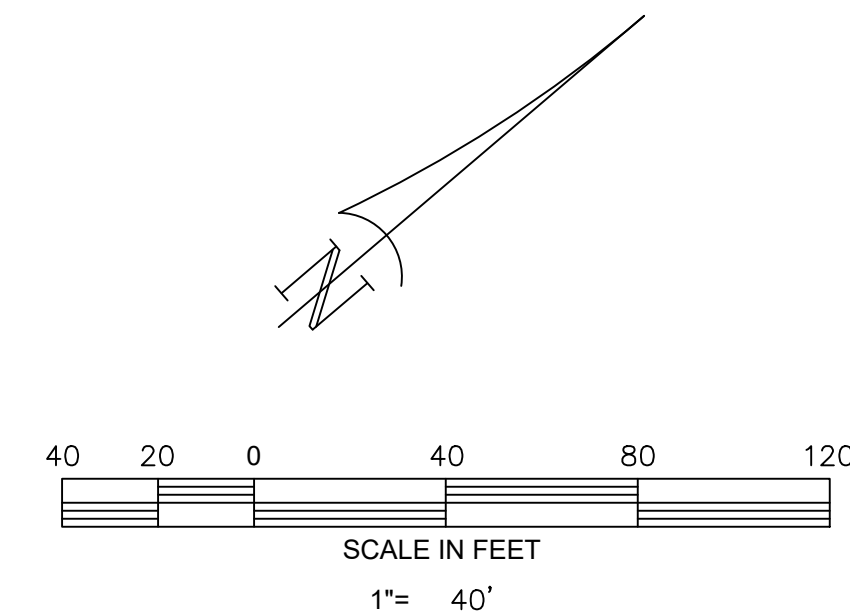
A1.4



GENERAL NOTES:

- SOIL BORING LOGS AND TEST DATA PERFORMED BY GEOTECHNOLOGIES, INC. ARE CONTAINED IN APPENDIX "F" OF THE PROJECT SPECIFICATIONS AND ARE FOR INFORMATIONAL PURPOSES ONLY. SOIL BORING LOCATIONS SHOWN ARE APPROXIMATE.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON SHEET SL1.1.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES, AND TAXIWAY LIGHTS TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022.
- ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- ALL MILLINGS SHALL BE DISPOSED OFF AIRPORT PROPERTY AT GEORGETOWN COUNTY SITE LOCATED AT 1324 AVIATION BLVD. CONTRACTOR SHALL DUMP MILLINGS AT SPECIFIED SITE. COUNTY STAFF SHALL BE RESPONSIBLE FOR STOCKPILE MAINTENANCE.
- LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE OR SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
- AFTER EXISTING BITUMINOUS PAVEMENT HAS BEEN REMOVED AND EXCAVATED TO PROPOSED SUBGRADE, THE PROPOSED SUBGRADE SHALL BE PROOFROLLED TO LOCATE STUMPS AND LOOSE SOILS. STUMPS, ROOTS, AND OTHER ORGANIC MATERIAL ARE NOT ACCEPTABLE IN THE SUBGRADE AND SHALL BE REMOVED. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE PROJECT AND DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- TAXIWAY LIGHTS AND TRANSFORMERS REMOVED FROM PROJECT SHALL REMAIN PROPERTY OF THE AIRPORT. CONTRACTOR SHALL TAKE CARE DURING REMOVAL OPERATIONS TO NOT DAMAGE THE LIGHTING EQUIPMENT. THE CONTRACTOR SHALL STOCKPILE SALVAGED LIGHTS AND TRANSFORMERS AT LOCATION ON AIRFIELD DESIGNATED BY AIRPORT MANAGEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE DISPOSED OFF AIRPORT PROPERTY AT A PROPERLY PERMITTED LOCATION.
- SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.4. CONTRACTOR SHALL INSTALL SEDIMENTATION AND EROSION CONTROL MEASURES AS REQUIRED PRIOR TO BEGINNING OF REMOVAL AND DEMOLITION ITEMS.

LEGEND	
	EXISTING ASPHALT PAVEMENT
	EXISTING PAVEMENT TO BE REMOVED
	EXISTING CONTOUR
	EXISTING TAXIWAY CIRCUIT AND CONDUIT TO BE REMOVED
	EXISTING DUCT
	EXISTING TAXIWAY CIRCUIT
	EXISTING STORM DRAIN PIPE
	EXISTING TAXIWAY EDGE LIGHT
	EXISTING HANDHOLE
	EXISTING LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	EXISTING TIE-DOWN
	APPROXIMATE BORING LOCATION



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
4810 SHELLEY DRIVE
SC LICENSE NO. C003296
EMAIL: TBILL@TBILL.COM

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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
EXISTING CONDITIONS AND DEMOLITION PLAN



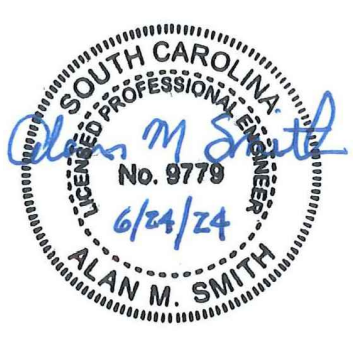
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EX1.1

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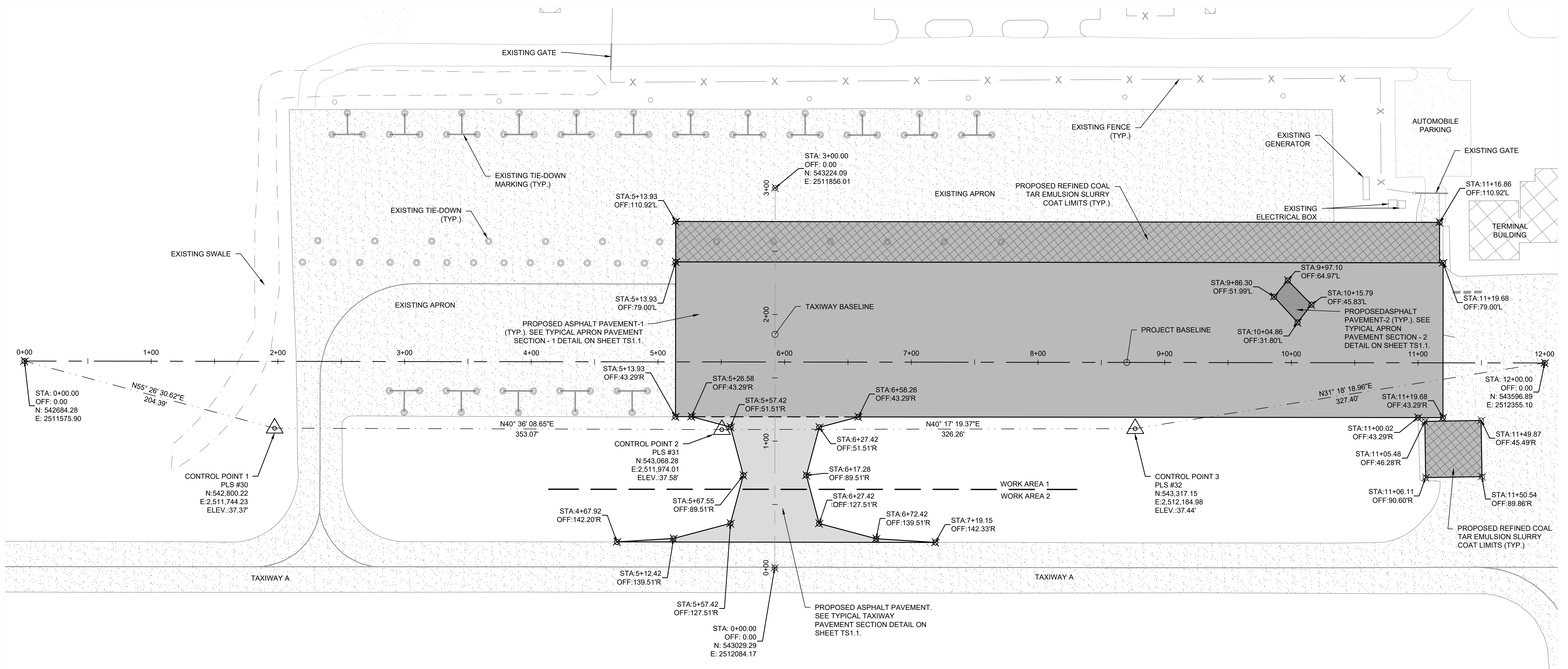
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
SURVEY LAYOUT PLAN



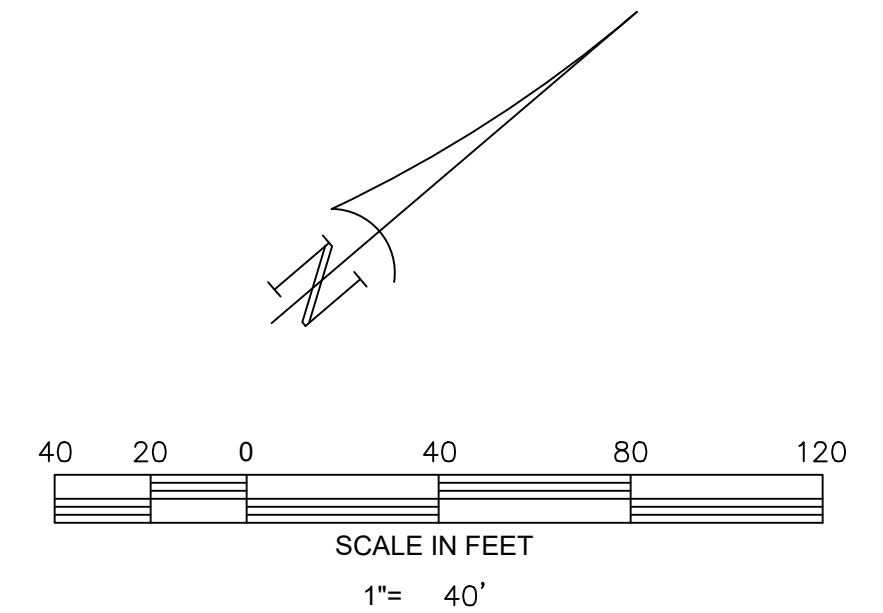
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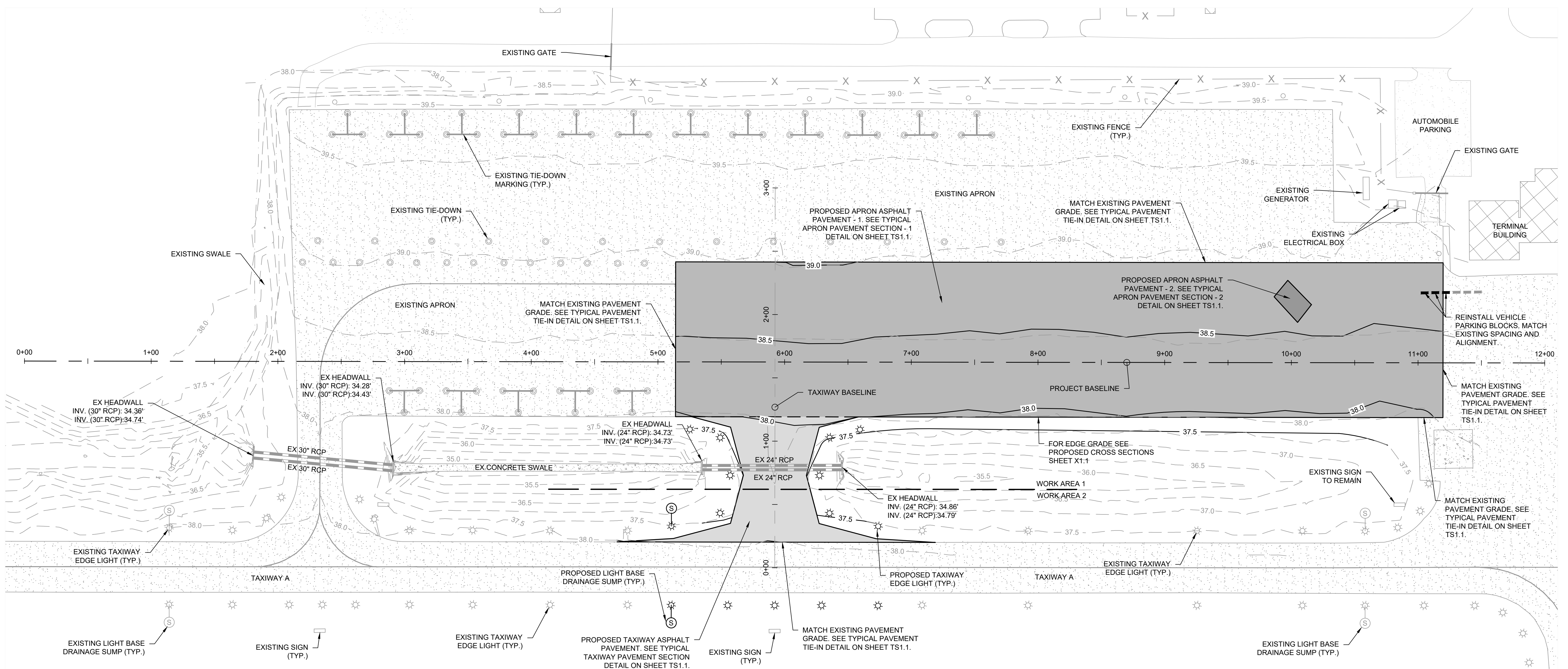
SL1.1



- GENERAL NOTES:**
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022. ALL STATIONS AND OFFSETS SHOWN ON THE PLANS ARE BASED ON 'PROJECT BASELINE'.
 - ALL BEARINGS AND COORDINATES ARE BASED ON SOUTH CAROLINA STATE PLANE COORDINATE SYSTEM 1983. ELEVATIONS ARE BASED ON NAVD88.
 - SEE SHEET EX1.1 FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
 - SEE SHEET G1.1 FOR GRADING AND DRAINAGE PLAN.
 - SEE SHEET TS1.1 FOR TYPICAL PAVEMENT SECTIONS AND MISCELLANEOUS DETAILS.

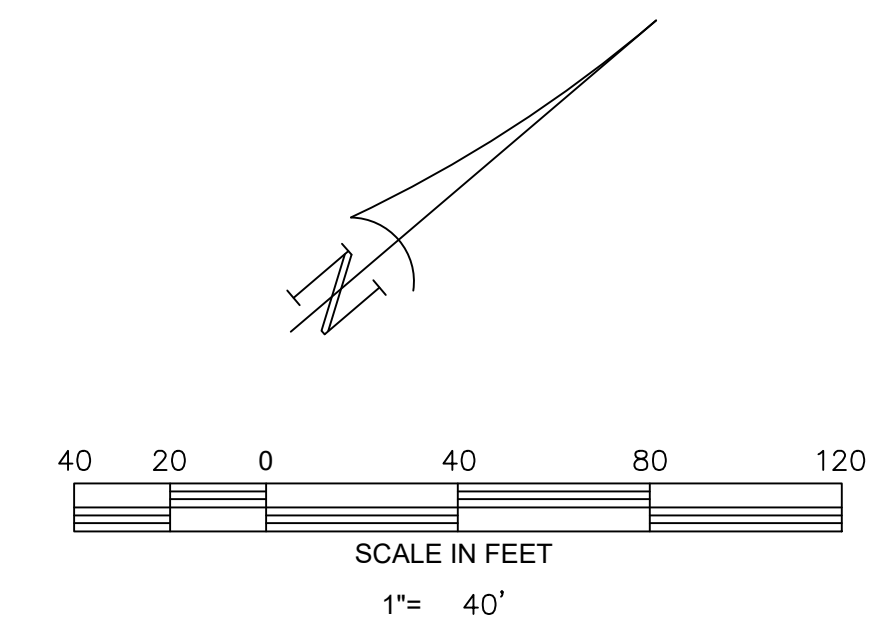
LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT - 1
	PROPOSED APRON ASPHALT PAVEMENT - 2
	PROPOSED SLURRY COAT LIMITS
	EXISTING SWALE
	EXISTING TIE-DOWN





- GENERAL NOTES:**
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES, AND TAXIWAY LIGHTS TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022.
 - ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
 - LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE OR SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
 - SEE SHEET EX1.1 FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
 - SEE SHEET SL1.1 FOR SURVEY CONTROL AND LAYOUT.
 - FOR TYPICAL SECTIONS AND MISCELLANEOUS DETAIL SEE SHEET TS1.1.
 - SEE TAXIWAY BASELINE PROFILE ON SHEET TS1.1.
 - SEE PHASING PLANS SHEET A1.3 AND A1.4 FOR SEQUENCE OF CONSTRUCTION.
 - SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
 - SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.4. CONTRACTOR SHALL INSTALL SEDIMENTATION AND EROSION CONTROL MEASURES AS ITEMS ARE COMPLETED.

LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT - 1
	PROPOSED APRON ASPHALT PAVEMENT - 2
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING SWALE
	EXISTING TAXIWAY EDGE LIGHT
	PROPOSED TAXIWAY EDGE LIGHT
	EXISTING LIGHT BASE DRAINAGE SUMP
	PROPOSED LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	EXISTING TIE-DOWN

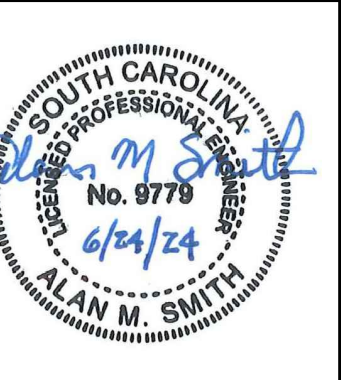


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILLM@TALBERT.COM
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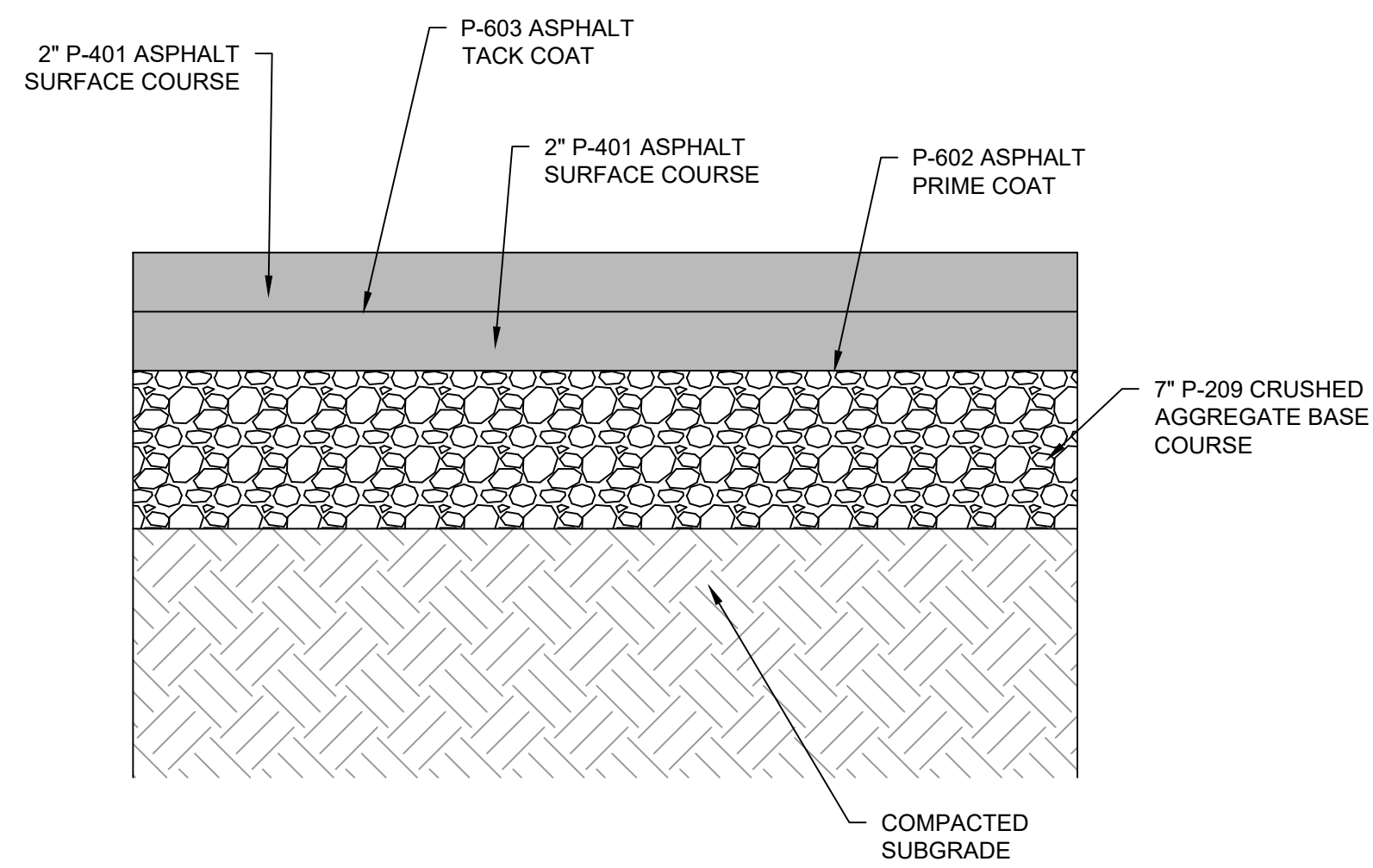
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
GRADING AND PAVING PLAN

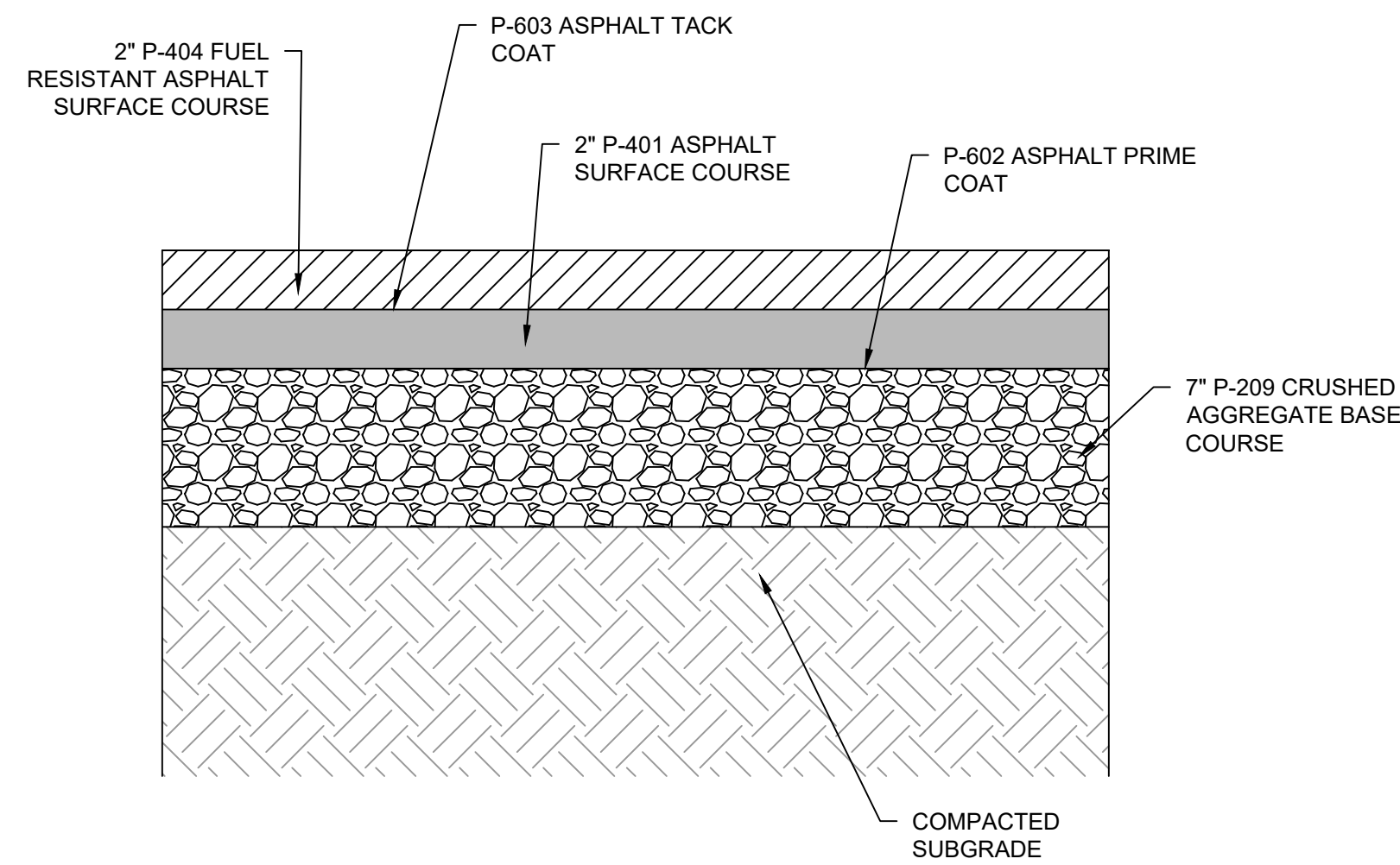


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Project No.	2601-2105
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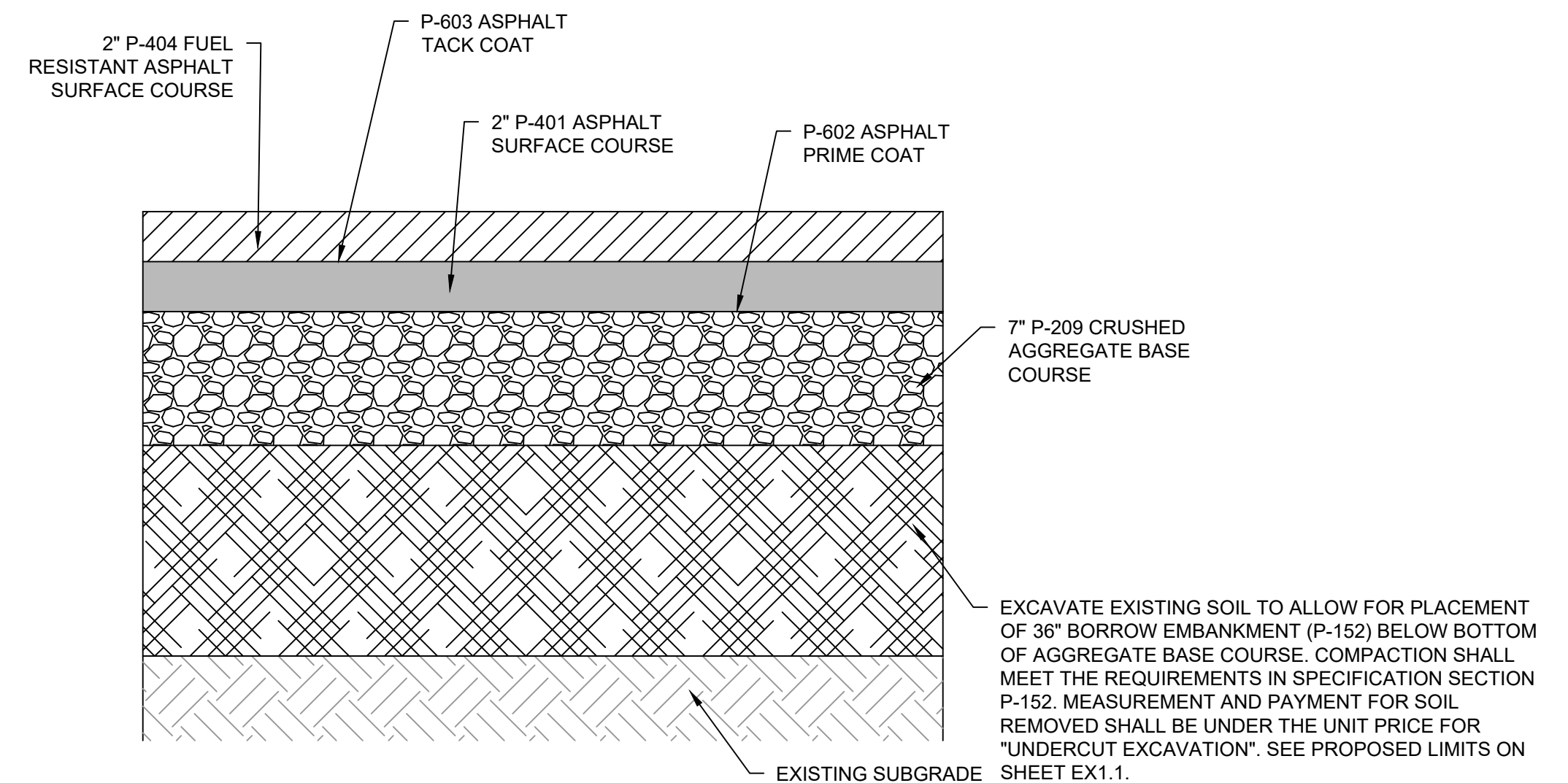
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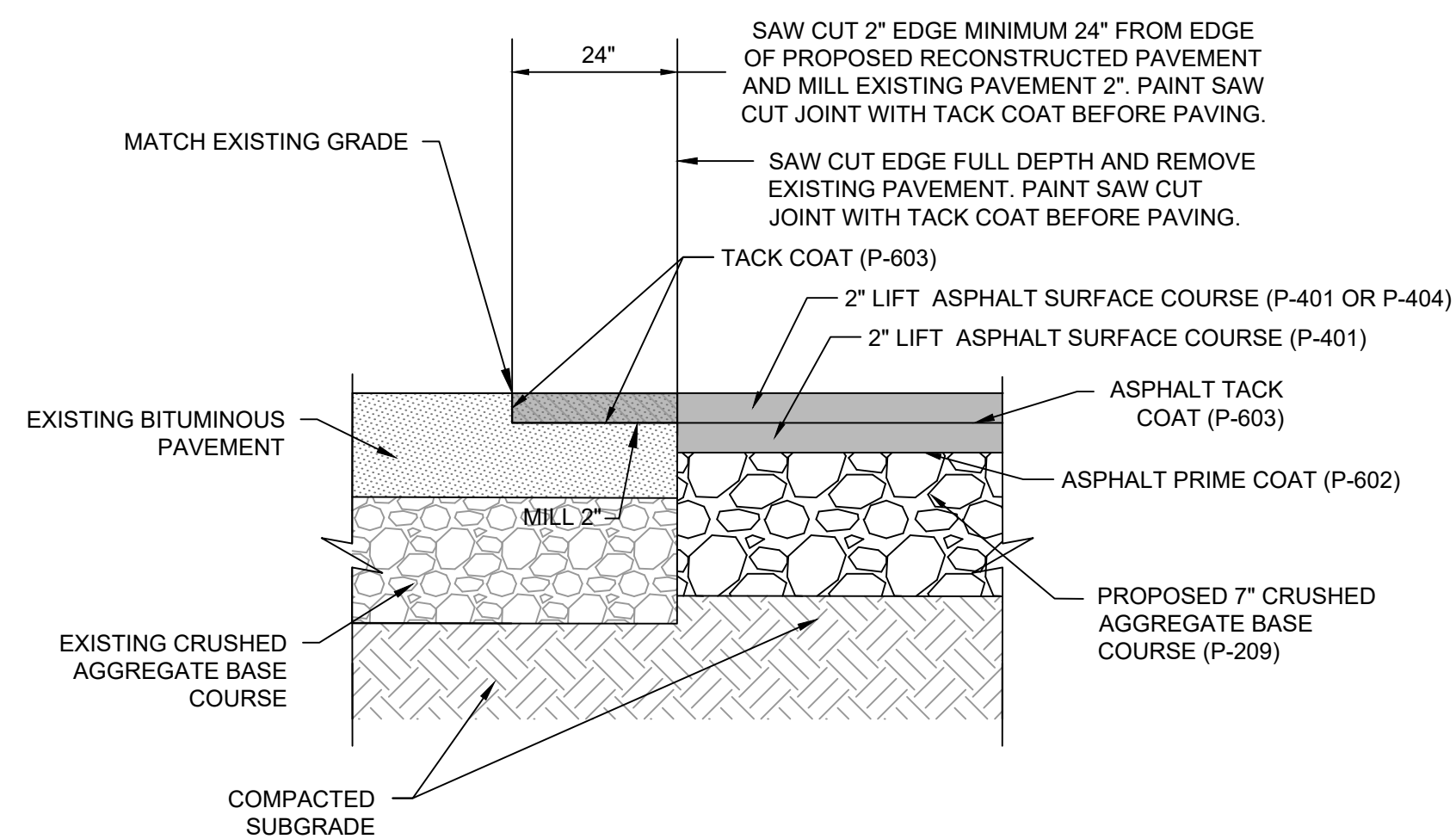
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NOT TO SCALE



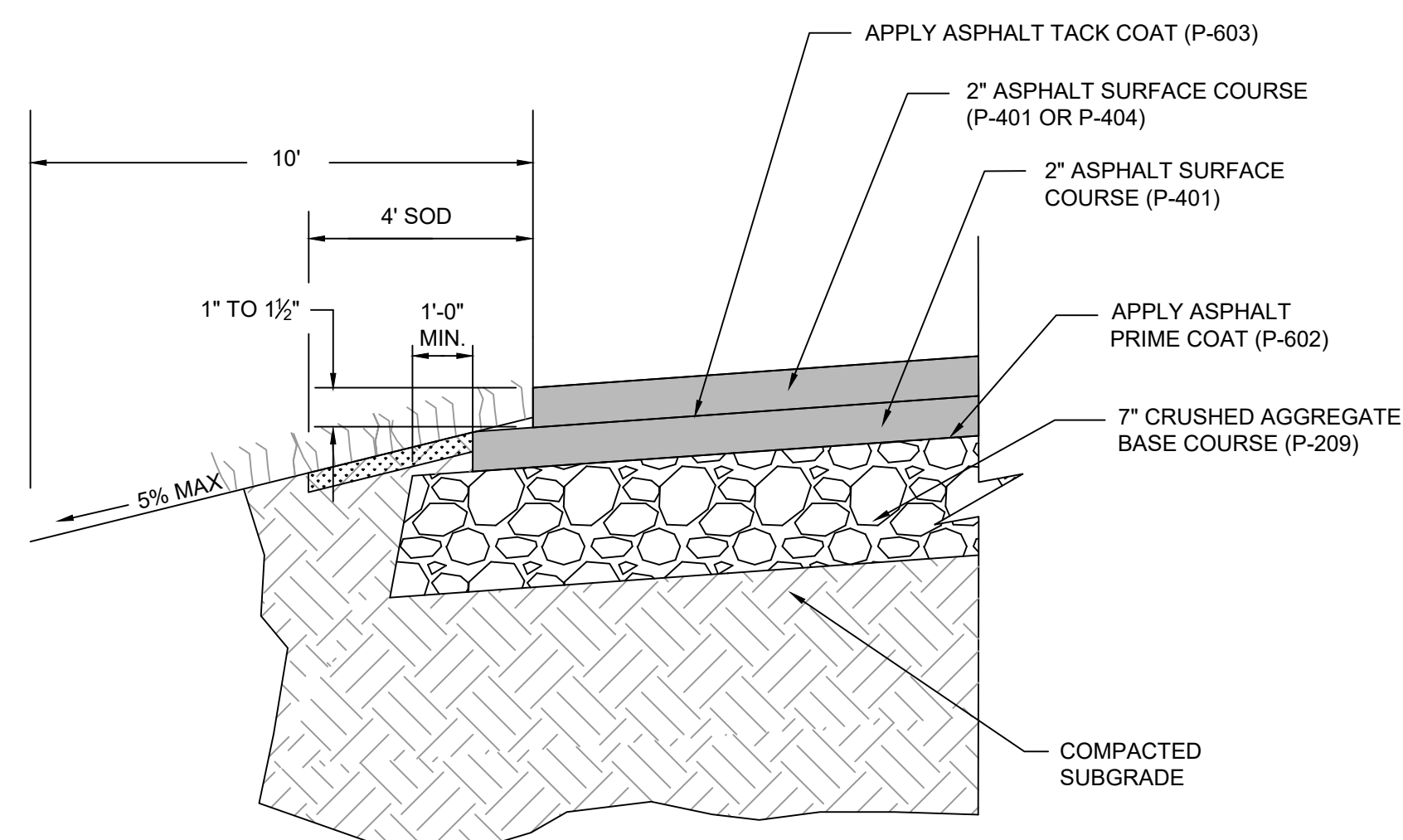
TYPICAL APRON PAVEMENT SECTION - 1
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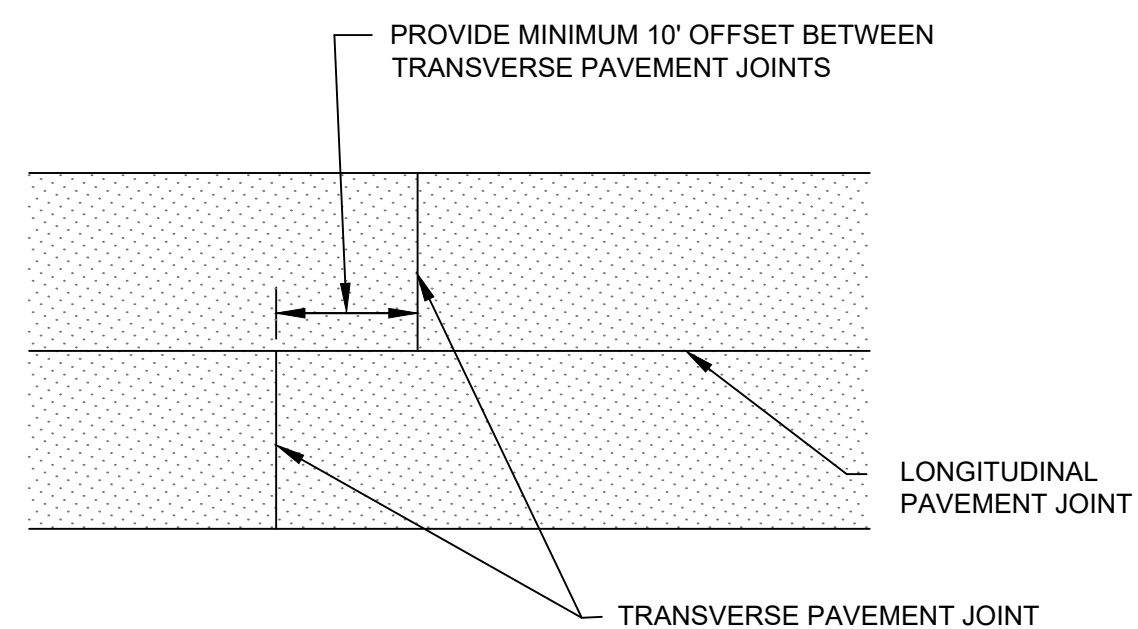
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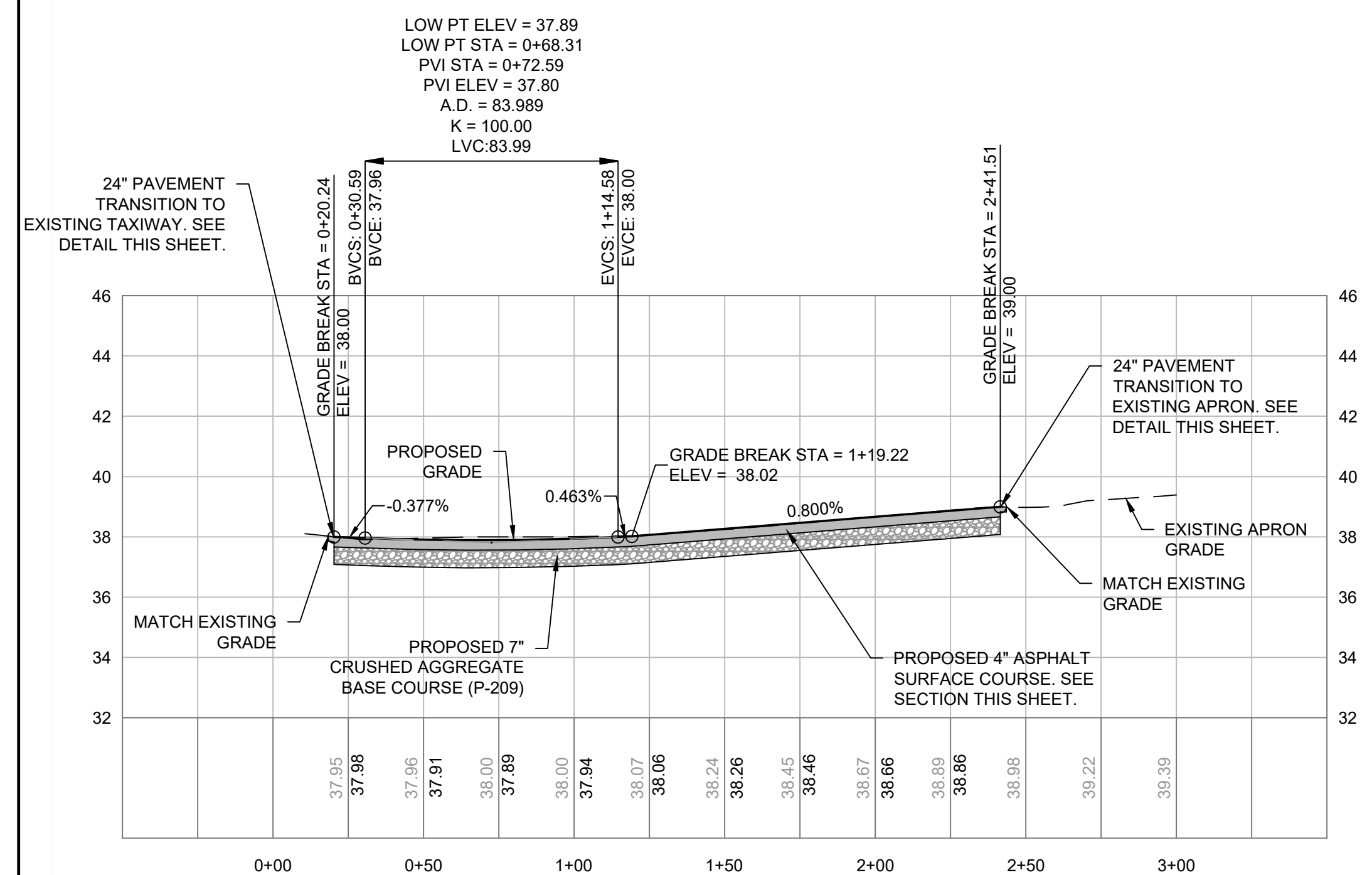
TYPICAL PAVEMENT TIE-IN DETAIL
NOT TO SCALE



TYPICAL SHOULDER DETAIL
NOT TO SCALE



TRANSVERSE PAVEMENT JOINTS
NOT TO SCALE

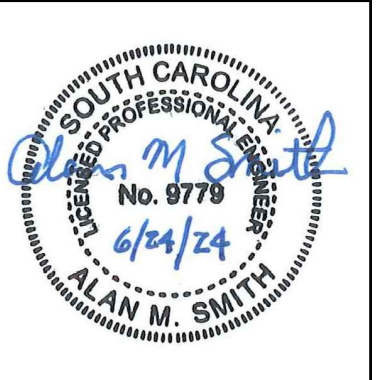


TAXIWAY BASELINE PROFILE
SCALE - H: 1" = 40', V: 1" = 4'

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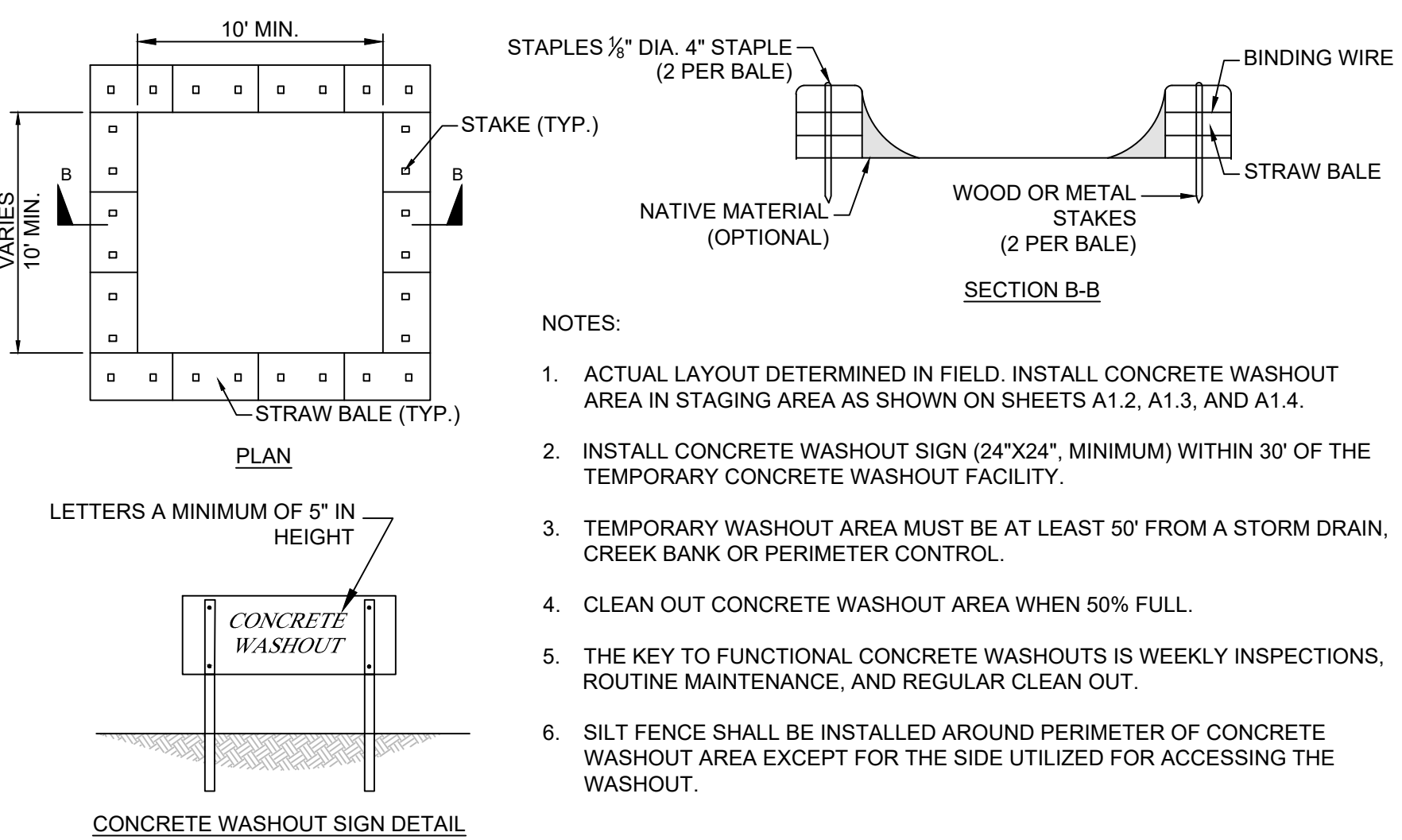
GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
TYPICAL PAVEMENT SECTIONS AND MISCELLANEOUS DETAILS



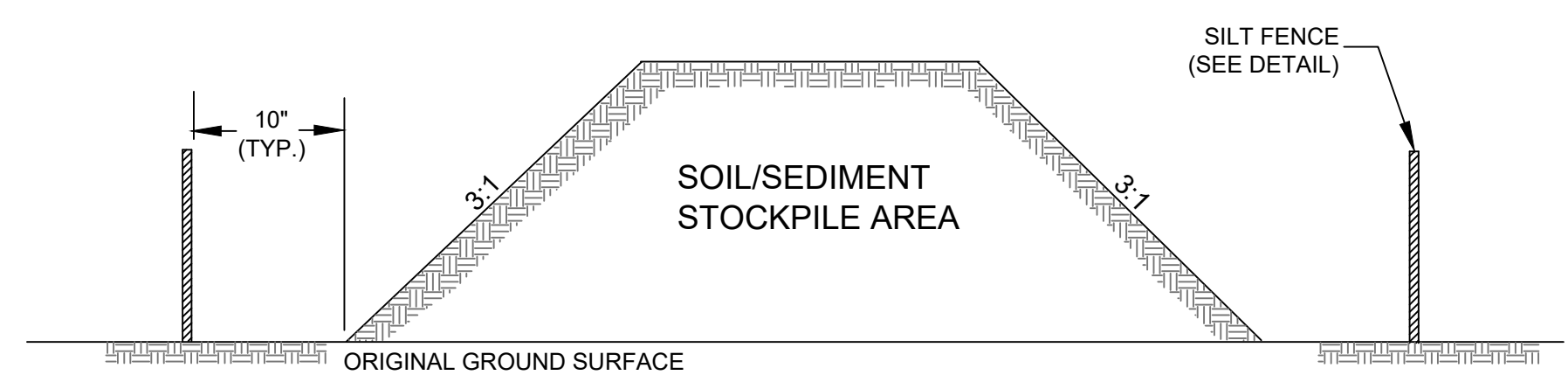
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Checked	AMS
Project No.	2601-2105
Sheet No.	

EROSION AND SEDIMENT CONTROL STANDARD NOTES:

- IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS. IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
 - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED ONCE A CALENDAR WEEK. IF PERIODIC INSPECTIONS OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY, OR INCORRECTLY INSTALLED, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION. FILL, COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE ANY SEDIMENTS BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAY(S) FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UP SLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CAN'T BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
- A COPY OF THE SWPPP, INSPECTIONS RECORDS, AND RAINFALL DATA MUST BE RETAINED AT THE CONSTRUCTION SITE OR A NEARBY LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS, FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE THAT FINAL STABILIZATION IS REACHED.
- INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (3H:1V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL NOT RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
- MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BMPS (SEDIMENT BASIN, FILTER BAG, ETC.).
- THE FOLLOWING DISCHARGES FROM SITES ARE PROHIBITED:
 - WASTEWATER FROM WASHOUT OF CONCRETE, UNLESS MANAGED BY AN APPROPRIATE CONTROL;
 - WASTEWATER FROM WASHOUT AND CLEANOUT OF STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS, AND OTHER CONSTRUCTION MATERIALS;
 - FUELS, OILS, OR OTHER POLLUTANTS USED IN VEHICLE AND EQUIPMENT OPERATION AND MAINTENANCE; AND
 - SOAPS OR SOLVENTS USED IN VEHICLE AND EQUIPMENT WASHING.
- AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE.
- IF EXISTING BMPs NEED TO BE MODIFIED OR IF ADDITIONAL BMPs ARE NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT AND/OR SC'S WATER QUALITY STANDARDS, IMPLEMENTATION MUST BE COMPLETED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE. IF IMPLEMENTATION BEFORE THE NEXT STORM EVENT IS IMPRACTICAL, THE SITUATION MUST BE DOCUMENTED IN THE SWPPP AND ALTERNATIVE BMPs MUST BE IMPLEMENTED AS SOON AS REASONABLY POSSIBLE.
- A PRE-CONSTRUCTION CONFERENCE MUST BE HELD FOR EACH CONSTRUCTION SITE WITH AN APPROVED ON-SITE SWPPP PRIOR TO THE IMPLEMENTATION OF CONSTRUCTION ACTIVITIES. FOR NON-LINEAR PROJECTS THAT DISTURB 10 ACRES OR MORE THIS CONFERENCE MUST BE HELD ON-SITE UNLESS THE DEPARTMENT HAS APPROVED OTHERWISE.



CONCRETE WASHOUT AREA DETAIL
NOT TO SCALE



- NOTES:**
- SILT FENCE TO EXTEND AROUND ENTIRE PERIMETER OF STOCKPILE, OR IF STOCKPILE AREA IS LOCATED ON/NEAR A SLOPE THE SILT FENCE IS TO EXTEND ALONG CONTOURS OF THE DOWN-GRADIENT AREA.
 - IF STOCKPILE IS TO REMAIN FOR MORE THAN 14 DAYS, TEMPORARY STABILIZATION MEASURES MUST BE IMPLEMENTED.
 - SILT FENCE SHALL BE MAINTAINED UNTIL STOCKPILE AREA HAS EITHER BEEN REMOVED OR PERMANENTLY STABILIZED.
 - THE KEY TO FUNCTIONAL TEMPORARY STOCKPILE AREAS IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.

TEMPORARY STOCKPILE AREA
NOT TO SCALE

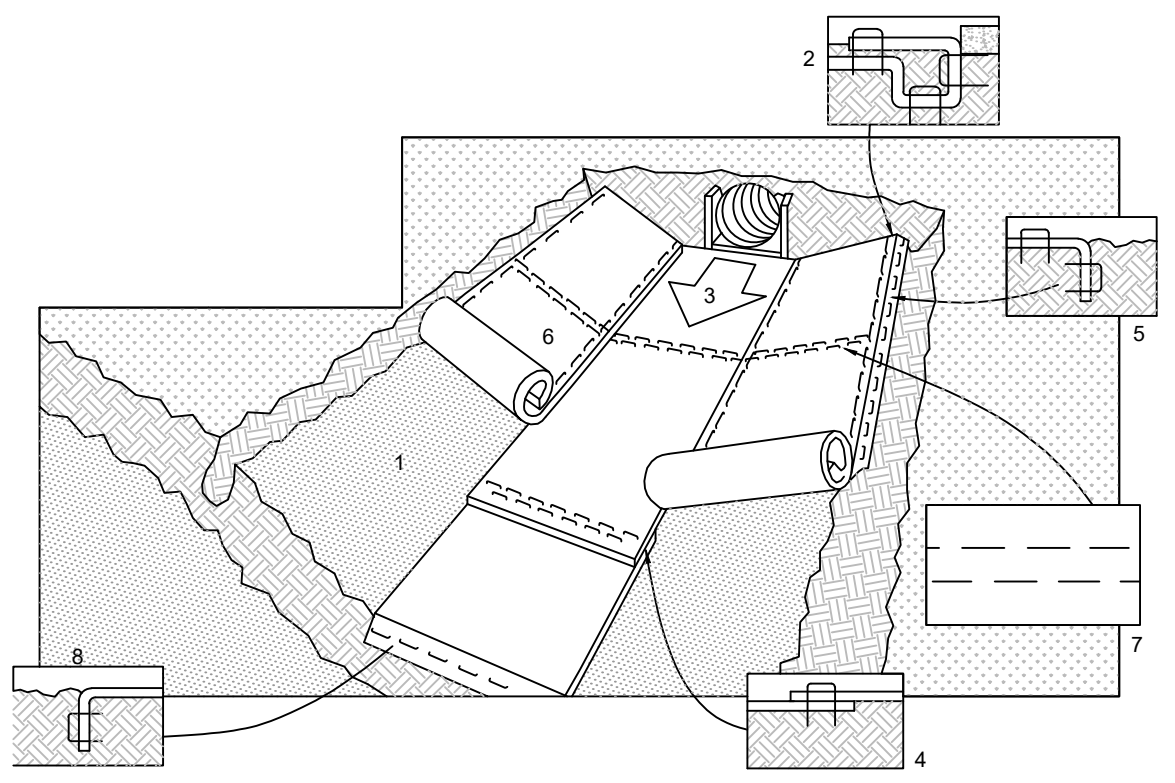
PERMANENT SEED MIXES SHALL BE APPLIED AS FOLLOWS:

SEED	MINIMUM SEED PURITY (PERCENT)	MINIMUM GERMINATION (PERCENT)	RATE OF APPLICATION (LBS/ACRE)	SEEDING DATES
HULLED COMMON BERMUDA GRASS	80%	70%	70	MARCH 1 - JULY 31
UNHULLED COMMON BERMUDA GRASS	80%	70%	70	AUGUST 1 - FEB. 28
RYE (GRAIN)	80%	70%	120	

TEMPORARY SEED MIXES SHALL BE APPLIED AS FOLLOWS:

SEED	MINIMUM SEED PURITY (PERCENT)	MINIMUM GERMINATION (PERCENT)	RATE OF APPLICATION (LBS/ACRE)	SEEDING DATES
HULLED COMMON BERMUDA GRASS	80%	70%	70	MARCH 1 - JULY 31
RYE (GRAIN)	76%	70%	120	AUGUST 1 - FEB. 28

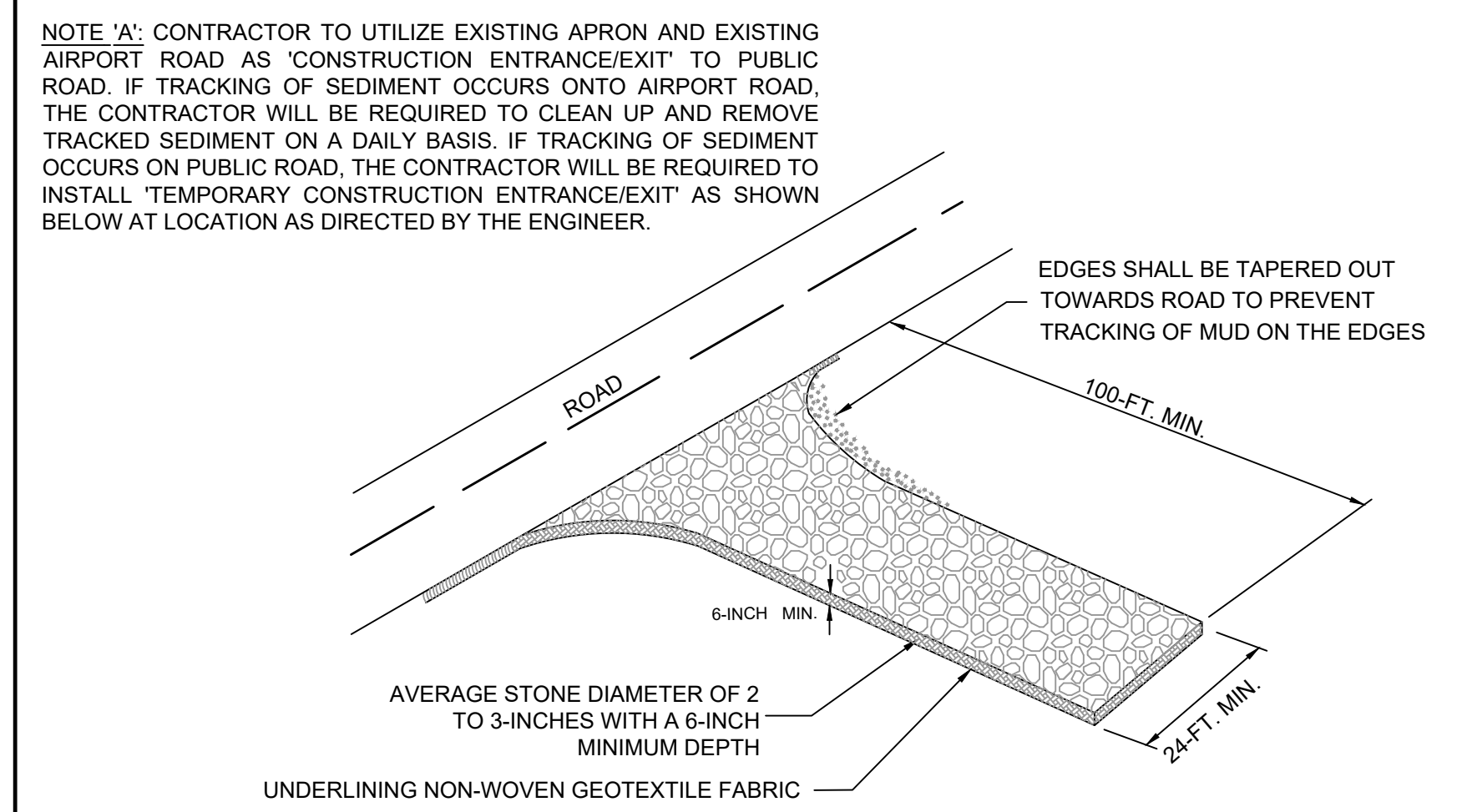
SEED MIXTURE AND PLANTING DATES
NOT TO SCALE



GENERAL INSTALLATION DETAIL
NOT TO SCALE

- NOTE:** SYNTHETIC MATTING TO BE AMERICAN EXCELSIOR COMPANY CURLEX I EXCELSIOR EROSION CONTROL BLANKET OR ENGINEER APPROVED EQUAL.
- EROSION CONTROL MATTING NOTES:**
- HORIZONTAL STAPLE SPACING MAY BE ALTERED IF NECESSARY TO ALLOW STAPLES TO SECURE THE CRITICAL POINTS ALONG THE CHANNEL SURFACE. REFER TO MANUFACTURE'S GENERAL STAPLE PATTERN GUIDE FOR CORRECT STAPLE RECOMMENDATIONS FOR CHANNELS.
- PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF LIME, FERTILIZER, AND SEED.
 - BEGIN AT THE TOP OF THE CHANNEL BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
 - ROLL CENTER BLANKET IN DIRECTION OF WATER FLOW ON BOTTOM OF CHANNEL.
 - PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH A 6" OVERLAP. USE A DOUBLE ROW OF STAGGERED STAPLES 4" APART TO SECURE BLANKETS.
 - BLANKETS ON SIDE SLOPES MUST BE OVERLAPPED 4" OVER THE CENTER BLANKET AND STAPLED.
 - THE TERMINAL END OF THE BLANKETS MUST BE ANCHORED IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

EXCELSIOR MATTING DETAIL
NOT TO SCALE



WHEN AND WHERE TO USE IT

STABILIZED CONSTRUCTION ENTRANCES SHOULD BE USED AT ALL POINTS WHERE TRAFFIC WILL BE LEAVING A CONSTRUCTION SITE AND MOVING DIRECTLY ONTO A PUBLIC ROAD. SEE NOTE 'A'.

IMPORTANT CONSIDERATIONS

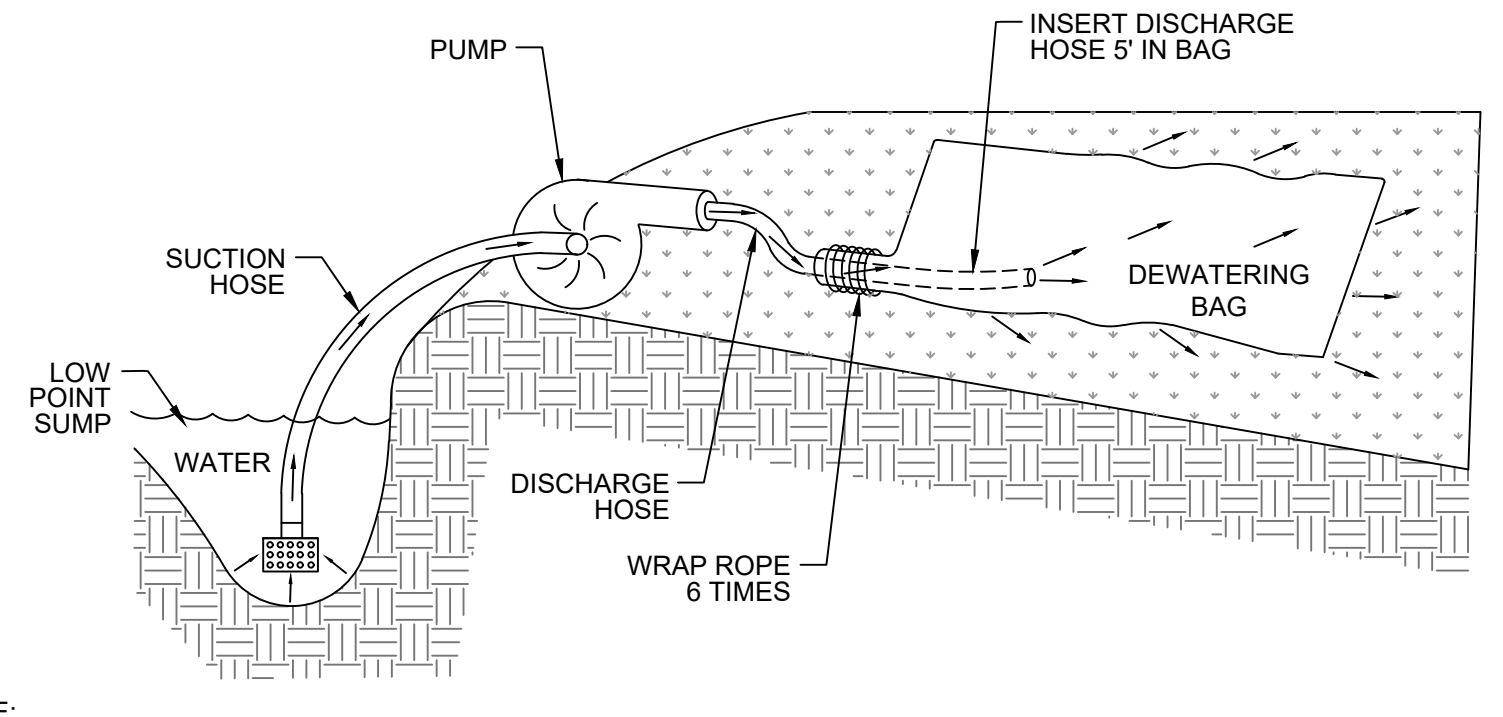
IF WASHING IS USED, PROVISIONS MUST BE MADE TO INTERCEPT THE WASH WATER AND TRAP THE SEDIMENT BEFORE IT IS CARRIED OFFSITE. WASHDOWN FACILITIES SHALL BE REQUIRED AS DIRECTED BY SCDHEC AS NEEDED. WASHDOWN AREAS IN GENERAL MUST BE ESTABLISHED WITH CRUSHED GRAVEL AND DRAIN INTO A SEDIMENT TRAP OR SEDIMENT BASIN. CONSTRUCTION ENTRANCES SHOULD BE USED IN CONJUNCTION WITH THE STABILIZATION OF CONSTRUCTION ROADS TO REDUCE THE AMOUNT OF MUD PICKED UP BY VEHICLES.

INSTALLATION:

REMOVE ALL VEGETATION AND ANY OBJECTIONABLE MATERIAL FROM THE FOUNDATION AREA. DIVERT ALL SURFACE RUNOFF AND DRAINAGE FROM STONES TO A SEDIMENT TRAP OR BASIN. INSTALL A NON-WOVEN GEOTEXTILE FABRIC PRIOR TO PLACING ANY STONE. THE ENTRANCE SHALL CONSIST OF 2-INCH TO 3-INCH D50 STONE PLACED AT A MINIMUM DEPTH OF 6-INCHES. MINIMUM DIMENSIONS OF THE ENTRANCE SHALL BE 24-FEET WIDE BY 100-FEET LONG, AND MAY BE MODIFIED AS NECESSARY TO ACCOMMODATE SITE CONSTRAINTS. THE EDGES OF THE ENTRANCE SHALL BE TAPERED OUT TOWARDS THE ROAD TO PREVENT TRACKING OF MUD AT THE EDGE OF THE ENTRANCE.

- INSPECTION AND MAINTENANCE:**
- INSPECT CONSTRUCTION ENTRANCES EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24-HOURS AFTER EACH RAINFALL EVENT THAT PRODUCES 1/2-INCHES OR MORE OF PRECIPITATION, OR AFTER HEAVY USE.
 - CHECK FOR MUD AND SEDIMENT BUILDUP AND PAD INTEGRITY.
 - MAKE DAILY INSPECTIONS DURING PERIODS OF WET WEATHER. MAINTENANCE IS REQUIRED MORE FREQUENTLY IN WET WEATHER CONDITIONS. RESHAPE THE STONE PAD AS NEEDED FOR DRAINAGE AND RUNOFF CONTROL.
 - WASH OR REPLACE STONES AS NEEDED AND AS DIRECTED BY THE ENGINEER. THE STONE IN THE ENTRANCE SHOULD BE WASHED OR REPLACED WHENEVER THE ENTRANCE FAILS TO REDUCE MUD BEING CARRIED OFF-SITE BY VEHICLES. FREQUENT WASHING WILL EXTEND THE USEFUL LIFE OF STONE.
 - IMMEDIATELY REMOVE MUD AND SEDIMENT TRACKED OR WASHED ONTO PUBLIC ROADS BY BRUSHING OR SWEEPING.
 - FLUSHING SHOULD ONLY BE USED WHEN THE WATER CAN BE DISCHARGED TO A SEDIMENT TRAP OR BASIN.
 - REPAIR ANY BROKEN PAVEMENT IMMEDIATELY.

TEMPORARY CONSTRUCTION ENTRANCE/EXIT
NOT TO SCALE



- NOTE:**
- A SEDCATCH DEWATERING BAG OR APPROVED EQUAL SHOULD BE USED ANYTIME WATER IS PUMPED FROM EXCAVATED AREAS ON SITE.
 - PLACE DEWATERING BAG ON THE GROUND OR ON A TRAILER OVER A LEVEL STABILIZED AREA.
 - INSERT DISCHARGE PIPE A MINIMUM OF 5 FEET INSIDE DEWATERING BAG AND SECURE WITH A ROPE WRAPPED 6 TIMES AROUND THE SNOUT OVER A 6 INCH WIDTH OF THE BAG.
 - REPLACE DEWATERING BAG WHEN HALF FULL OF SEDIMENT OR WHEN THE SEDIMENT HAS REDUCED THE FLOW RATE OF THE PUMP DISCHARGE TO AN IMPRACTICAL AMOUNT.
- MAINTENANCE AND DISPOSAL:**
- REMOVE AND DISPOSE OF ACCUMULATED SEDIMENT AWAY FROM WATERWAYS OR ENVIRONMENTALLY SENSITIVE AREAS. SLIT OPEN SEDIMENT BAG AND REMOVE ACCUMULATED SEDIMENT AND DISPERSE IN GRADED AREAS AND STABILIZE. DISPOSE OF BAG AT AN APPROPRIATE RECYCLING OR SOLID WASTE FACILITY.

DEWATERING BAG
NOT TO SCALE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLEY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C000296
EMAIL: TBILL@TBILL.COM

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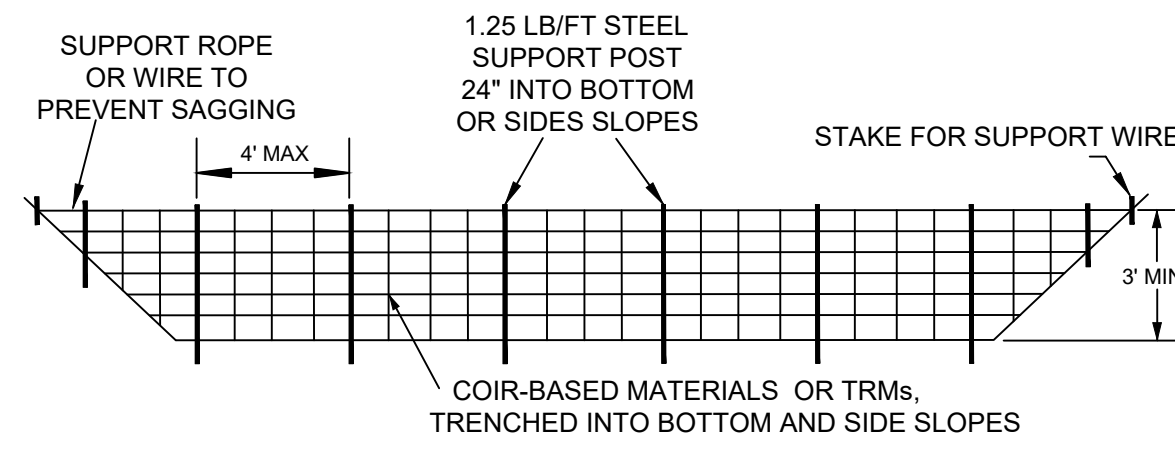
REV. NO.	DATE	DESCRIPTION

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
SEDIMENTATION AND EROSION CONTROL DETAILS
(SHEET 1 OF 3)

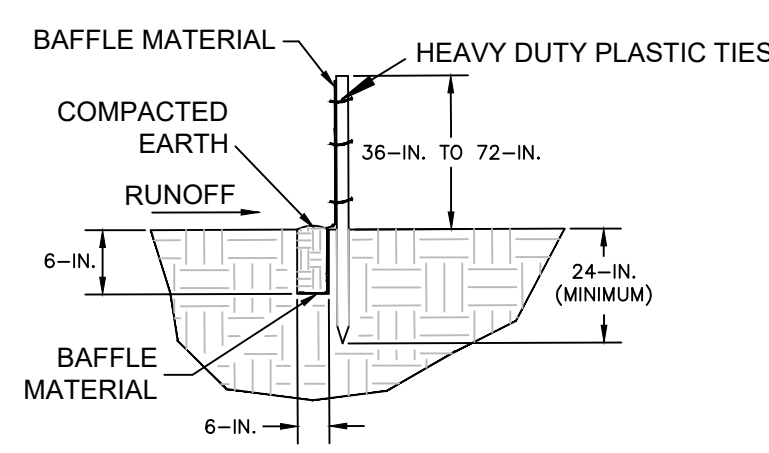


Date	JUNE 2024
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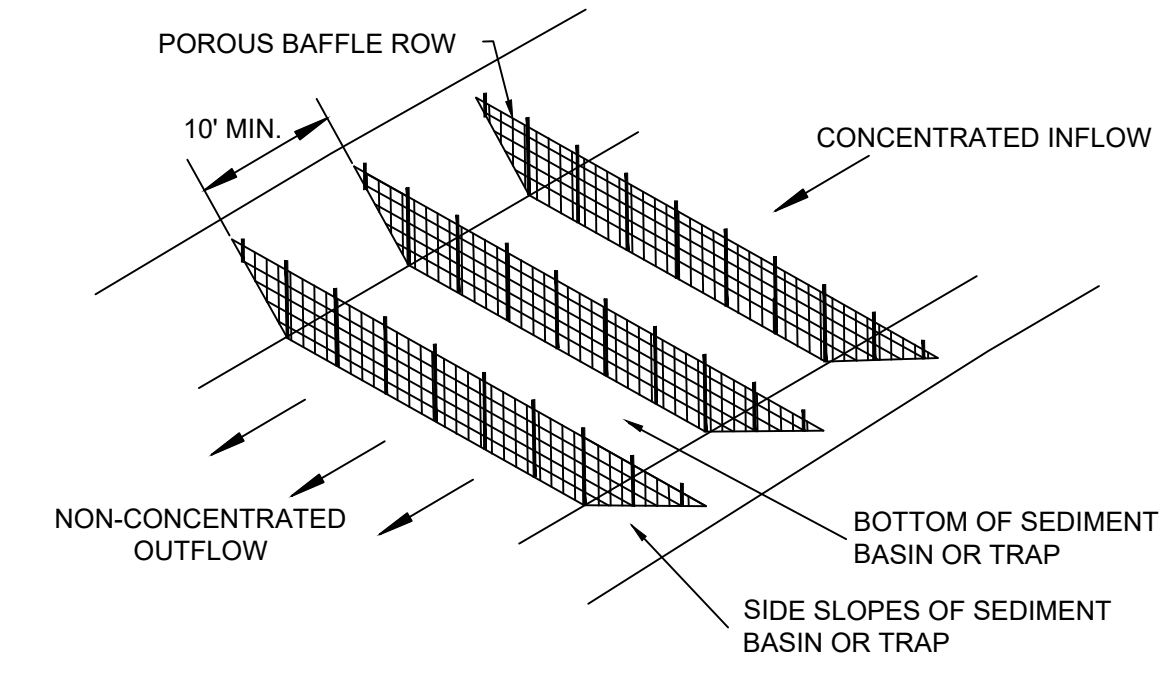
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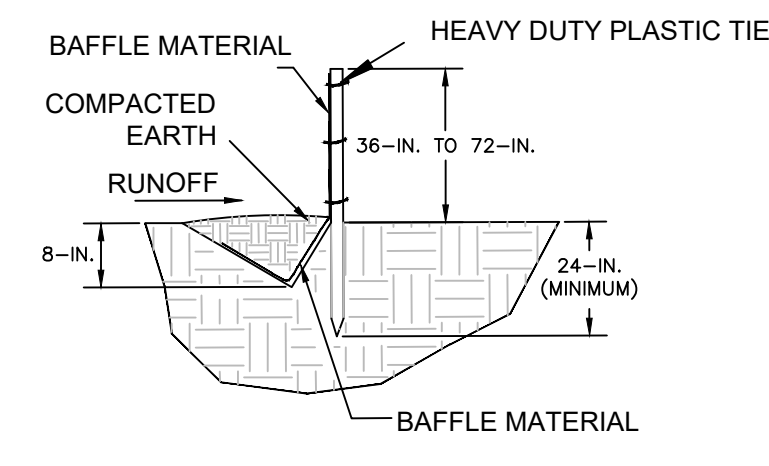
CROSS SECTION VIEW



FLAT-BOTTOM TRENCH DETAIL



PERSPECTIVE VIEW



V-SHAPED TRENCH DETAIL

BAFFLES - POST REQUIREMENTS

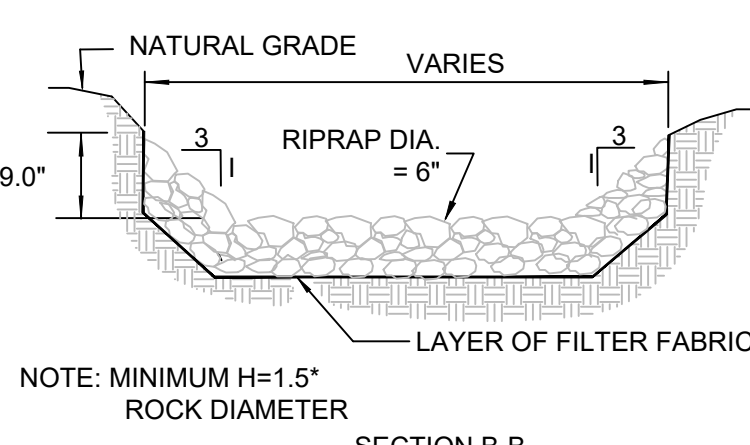
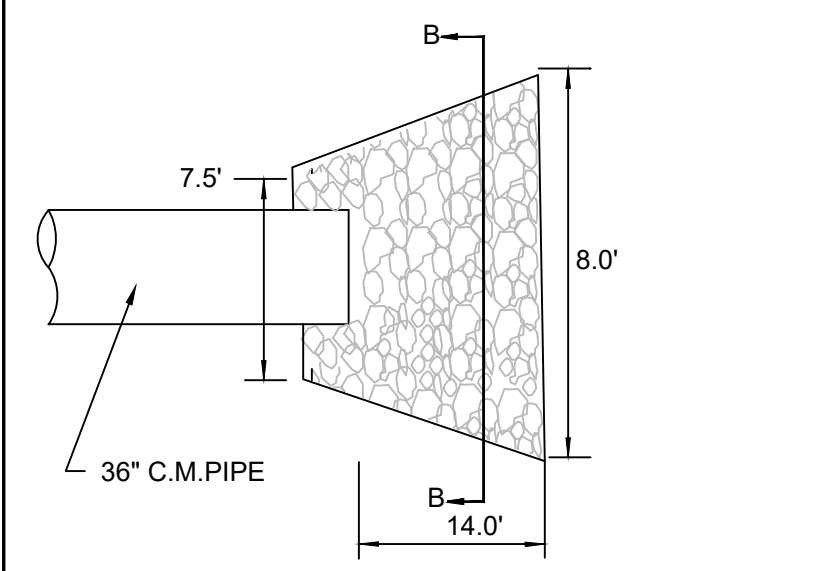
- POROUS BAFFLE POSTS MUST BE 60-INCH TO 96-INCH LONG STEEL POSTS THAT MEET, AT A MINIMUM, THE FOLLOWING PHYSICAL CHARACTERISTICS.
 - COMPOSED OF A HIGH STRENGTH STEEL WITH A MINIMUM YIELD STRENGTH OF 50,000 PSI.
 - INCLUDE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND A NOMINAL "T" LENGTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (± 8%)
- POSTS SHALL BE EQUIPPED WITH PROJECTIONS TO AID IN FASTENING OF BAFFLE MATERIAL.
- INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. A MINIMUM HEIGHT OF 1- TO 2- INCHES ABOVE THE FABRIC SHALL BE MAINTAINED, AND A MAXIMUM HEIGHT OF 3 FEET SHALL BE MAINTAINED ABOVE THE GROUND.
- POST SPACING SHALL BE AT A MAXIMUM OF 4- FEET ON CENTER.

BAFFLES - MATERIAL REQUIREMENTS

- BAFFLE MATERIAL MUST BE COMPOSED OF COIR-BASED MATERIALS OR TURF REINFORCEMENT MATTING (TRM) THAT CONSISTS OF THE FOLLOWING REQUIREMENTS:
 - HAVE A LIGHT PENETRATION (% OPENINGS) BETWEEN 10-35%;
 - FREE OF LOOSE STRAW MATERIAL;
 - HAVE A MINIMUM TENSILE STRENGTH OF 145 LB/FT. AND,
 - HAVE A MINIMUM WIDTH OF 48-INCHES.
- 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED OR BAFFLE MATERIAL MAY BE STAPLED INTO GROUND BY USING 12-INCH STAPLES WITH A MAXIMUM SPACING OF 12-INCHES.
- BAFFLE MATERIAL SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE WIDTH OF THE SEDIMENT BASIN OR TRAP TO AVOID JOINTS.

POROUS BAFFLES

NOT TO SCALE



RIPRAP APRON OUTLET PROTECTION FOR BASIN OUTLET PIPE

NOTES:

- RIPRAP SHOULD EXTEND UP BOTH SIDES OF THE APRON AND AROUND THE END OF THE PIPE OR CULVERT AT THE DISCHARGE OUTLET AT A MAXIMUM SLOPE OF 3:1 AND A HEIGHT NOT LESS THAN ONE HALF THE ROCK DIAMETER.
- THERE SHALL BE NO OVERFLOW FROM THE END OF THE APRON TO THE SURFACE OF THE RECEIVING CHANNEL. THE AREA TO BE PAVED OR RIPRAPPED SHALL BE UNDERCUT SO THAT THE INVERT OF THE APRON SHALL BE AT THE SAME GRADE (FLUSH) WITH THE SURFACE OF THE RECEIVING CHANNEL. THE APRON SHALL HAVE A CUTOFF OR TOE WALL AT THE DOWNSTREAM END.
- THE WIDTH OF THE END OF THE APRON SHALL BE EQUAL TO THE BOTTOM WIDTH OF THE RECEIVING CHANNEL. MAXIMUM TAPER TO RECEIVING CHANNEL 5:1
- ALL SUBGRADE FOR STRUCTURE TO BE COMPACTED TO 95% OR GREATER.
- THE PLACING OF FILL, EITHER LOOSE OR COMPACTED IN THE RECEIVING CHANNEL SHALL NOT BE ALLOWED.
- NO BENDS OR CURVES IN THE HORIZONTAL ALIGNMENT OF THE APRON WILL BE PERMITTED.

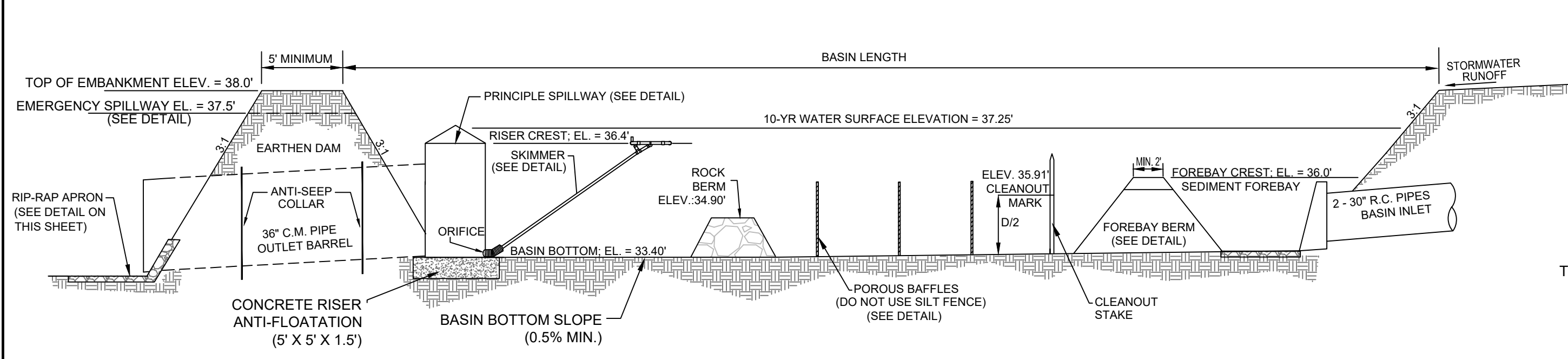
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BAFFLES - INSPECTION & MAINTENANCE

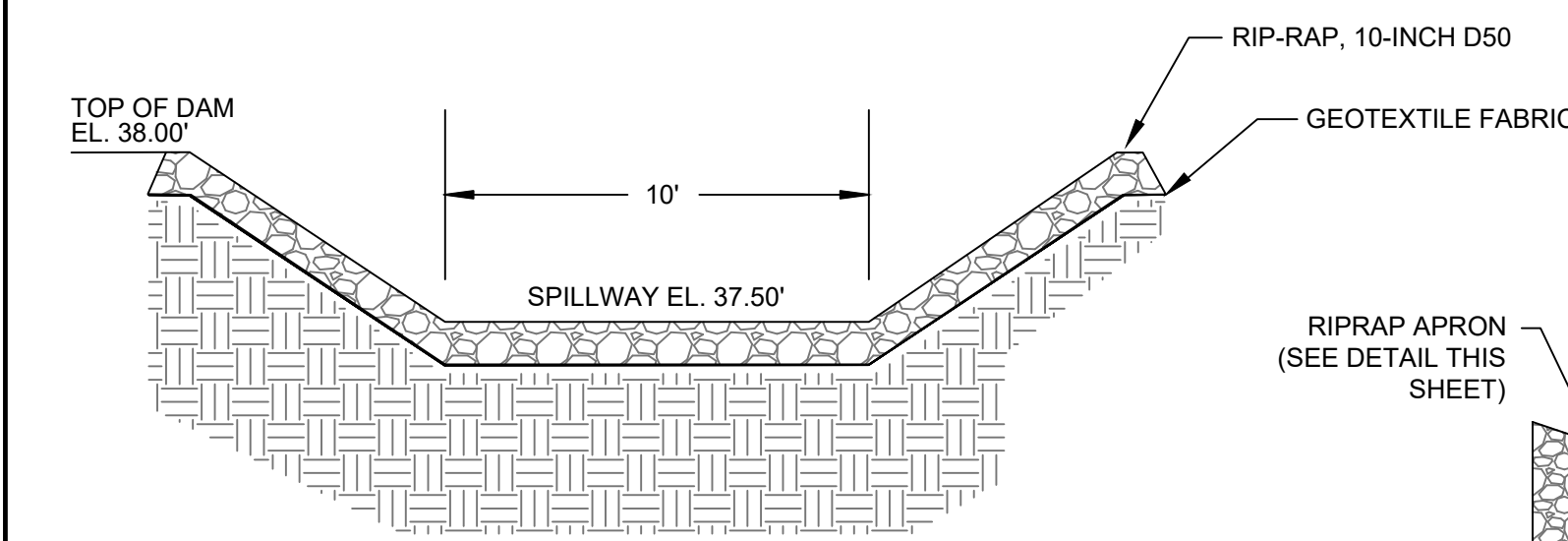
- THE KEY TO FUNCTIONAL POROUS BAFFLES IS WEEKLY INSPECTION, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- REGULAR INSPECTIONS OF POROUS BAFFLES SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVEN THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ATTENTION TO SEDIMENT ACCUMULATIONS ALONG EACH ROW OF BAFFLES IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE BAFFLE ROW OR WHEN IT REACHES THE CLEAN-OUT HEIGHT OF THE SEDIMENT BASIN OR TRAP, WHICHEVER IS REACHED FIRST.
- REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH EACH ROW OF BAFFLES, OR WHERE THE BAFFLE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE BAFFLE.
- CHECK FOR TEARS/RIPS WITHIN THE BAFFLES, AREAS WHERE THE BAFFLE HAS BEGUN TO DECOMPOSE, AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE BAFFLE INEFFECTIVE. REMOVED DAMAGED BAFFLES AND REINSTALL NEW BAFFLES IMMEDIATELY.
- POROUS BAFFLES SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

BAFFLES - GENERAL NOTES

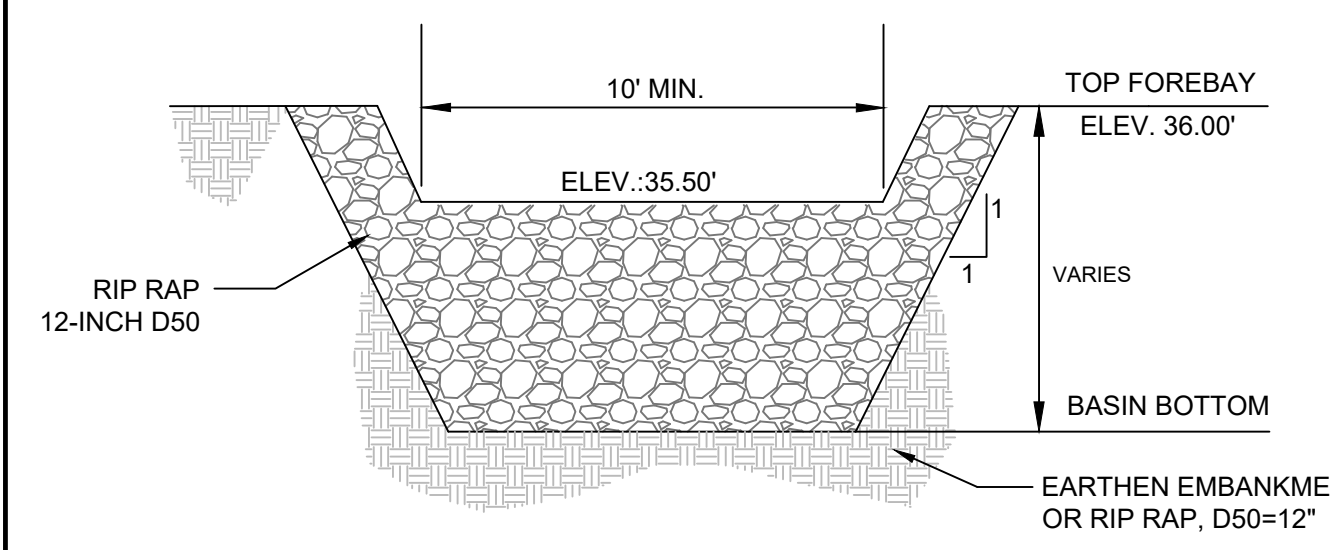
- ATTACH BAFFLE TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED ALONG THE ABOVE GROUND PORTION OF EACH POST.
- INSTALL THE BAFFLE ROWS PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE EACH BAFFLE THE PROPER DISTANCE FROM INLET AND OUTLETS TO ALLOW ACCESS FOR MAINTENANCE AND CLEAN-OUT.



DRY SEDIMENT BASIN SECTIONAL VIEW

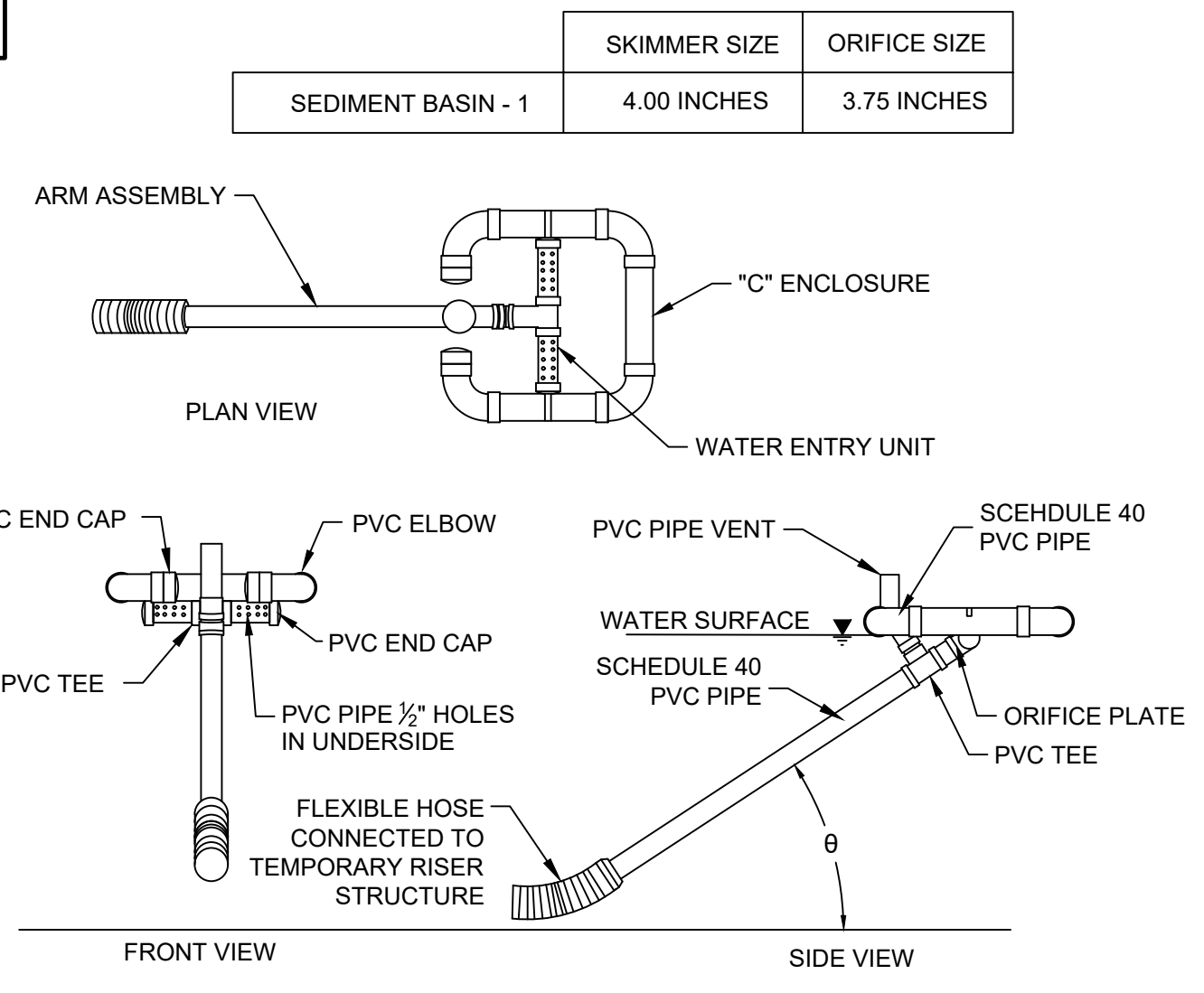


EMERGENCY SPILLWAY DETAIL



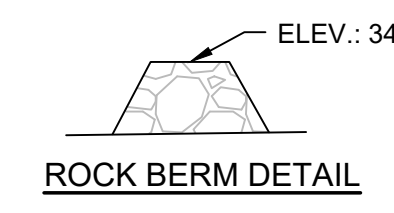
FOREBAY BERM DETAIL

NOTE: THE "TEMPORARY SKIMMER BASIN (COMPLETE AND IN PLACE) WILL BE PAID FOR ON A PER EACH BASIS. THE PRICE SHALL INCLUDE ALL COMPONENTS AS SHOWN ABOVE FOR A COMPLETE INSTALLATION. THE PRICE SHALL ALSO INCLUDE REMOVAL OF ALL ITEMS INSTALLED FOR THE BASIN AND RESTORATION OF ALL DISTURBED AREAS.

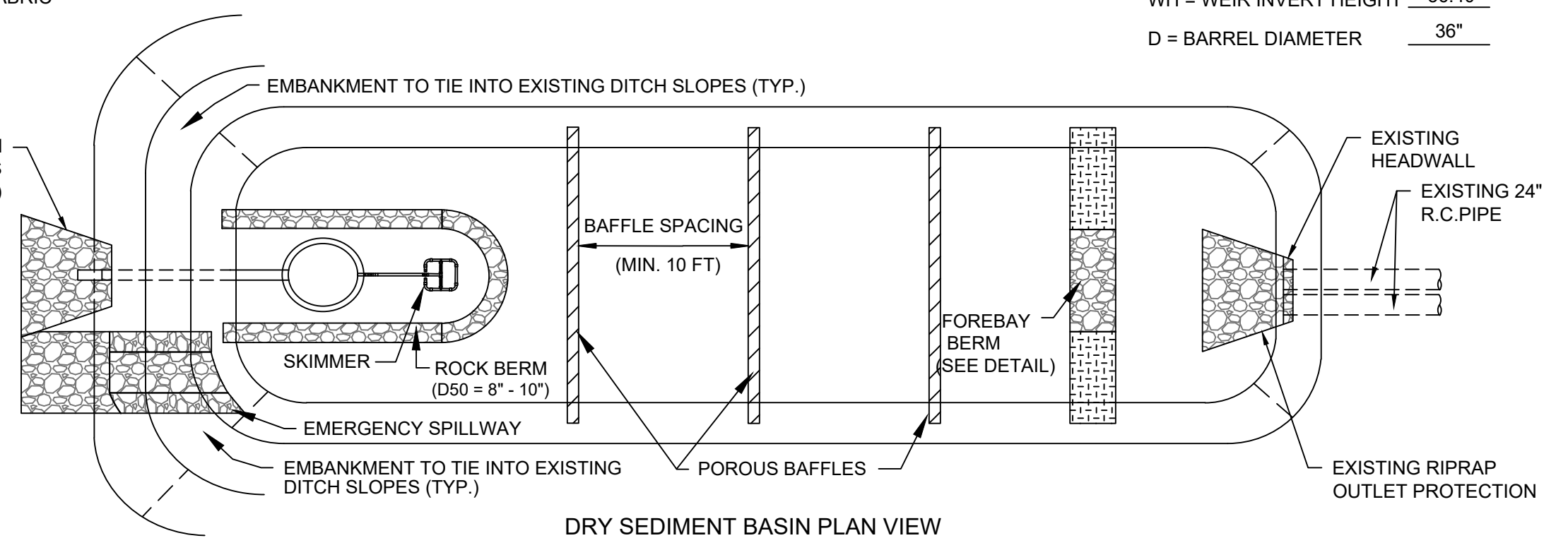


FAIRCLOTH SKIMMER DETAIL

NOTE: ORIFICE SIZES ARE FOR THE FAIRCLOTH SKIMMER. IF ANOTHER TYPE OF SKIMMER IS USED THE DEVICE MUST BE ABLE TO DRAIN THE BASIN WITHIN 48 HOURS.



ROCK BERM DETAIL



DRY SEDIMENT BASIN PLAN VIEW

DRY SEDIMENT BASIN - GENERAL NOTES

- SEDIMENT BASIN'S SIDE SLOPES SHALL BE SEEDED AND, WHEN NECESSARY, STABILIZED WITH VEGETATIVE OR SYNTHETIC MATTING TO PREVENT THE FORMATION OF RILLS AND GULLIES.
- INSTALL THREE (3) ROWS OF POROUS BAFFLES WITH A MINIMUM SPACING OF 10 FEET. BAFFLES SHOULD ULTIMATELY BE PLACED TO MAXIMIZE THE SPACE BETWEEN EACH ROW OF BAFFLES AND THE BASIN'S INLETS/OUTLETS. ONLY TWO (2) ROWS OF BAFFLES ARE NECESSARY FOR BASINS THAT ARE LESS THAN 50 FEET IN LENGTH.
- POROUS BAFFLES SHOULD BE COMPOSED OF COIR-BASED MATERIALS OR TRMS WITH A LIGHT PENETRATION (OPEN SPACES) BETWEEN 10-35%. THESE MATERIALS SHOULD NOT HAVE LOOSE STRAW. SILT FENCE MAY NOT BE USED AS POROUS BAFFLES.
- EACH POROUS BAFFLE SHALL BE INSTALLED ACROSS THE ENTIRE WIDTH OF THE BASIN AND ALONG THE BASIN'S SIDE SLOPE UNTIL THE HEIGHT OF THE BAFFLE INTERSECTS THE SLOPE.
- INSTALL SKIMMER AND COUPLING (AS NECESSARY) TO RISER STRUCTURE AT ORIFICE ALONG BOTTOM OF THE PRINCIPLE SPILLWAY'S RISER STRUCTURE. (REFER TO SKIMMER MANUFACTURER FOR INSTALLATION PROCEDURES AND SKIMMER SPECIFICATIONS.)
- SKIMMER SHOULD BE EQUIPPED WITH A MECHANISM, SUCH AS A ROPE, TO ALLOW EASY ACCESS TO SKIMMER TO UNCLOGGED ORIFICE OR PERFORM OTHER NECESSARY MAINTENANCE.
- STORMWATER RUNOFF ENTERING THE BASIN MUST BE DIRECTED INTO PROPER BMPS TO PREVENT EROSION ALONG SIDE SLOPES AND TO PREVENT SCOUR AT THE BASIN'S INLETS.
- THE FOREBAY BERM SHOULD CONSIST OF RIPRAP, GABION, OR AN EARTHEN BERM WITH A ROCK FILLED OUTLET THAT IS CONSTRUCTED ACROSS THE BOTTOM OF THE BASIN'S WIDTH.
- AN ADDITIONAL CLEANOUT STAKE FOR THE FOREBAY AREA IS RECOMMENDED AND SHOULD BE MARKED FOR CLEANOUT AT 50% OF PROVIDED SEDIMENT STORAGE.
- THE ELEVATION OF THE EMERGENCY SPILLWAY SHOULD BE AT LEAST 1 FOOT BELOW THE TOP OF THE EMBANKMENT. THE EMERGENCY SPILLWAY SHOULD NOT BE LOCATED ON FILL MATERIAL, WHEN POSSIBLE. RIPRAP AND GEOTEXTILE LINER SHOULD BE PLACED ON ALL SPILLWAYS THAT MUST BE LOCATED ON FILL MATERIAL.

DRY SEDIMENT BASIN - INSPECTION AND MAINTENANCE

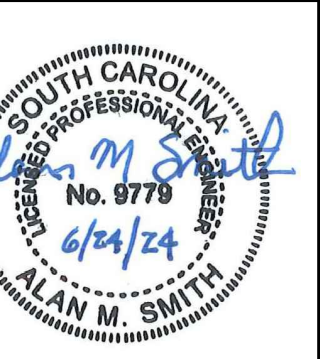
- THE KEY TO A FUNCTIONAL SEDIMENT BASIN IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- ATTENTION TO SEDIMENT ACCUMULATIONS WITHIN THE BASIN IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT DEPOSITION SHOULD BE CONTINUALLY CHECKED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 50% OF THE DESIGN SEDIMENT STORAGE VOLUME OR 1/2 THE HEIGHT OF THE RISER STRUCTURE, WHICHEVER IS REACHED FIRST.
- REMOVED SEDIMENT FROM THE BASIN SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS THE DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- INSPECTIONS OF SEDIMENT BASINS SHOULD BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS OF EACH RAINFALL EVENT THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ALL TEMPORARY SEDIMENT BASINS, WHICH ARE NOT TO BE CONVERTED TO A DETENTION BASIN POST-CONSTRUCTION, SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED.
- DISTURBED AREAS RESULTING FROM THE REMOVAL OF THE SEDIMENT BASIN SHOULD BE PERMANENTLY STABILIZED AND ADDITIONAL BMPS, SUCH AS SILT FENCE, SHOULD BE UTILIZED TO ACCEPT STORMWATER RUNOFF FROM THIS DISTURBED AREA UNTIL FINAL STABILIZATION IS REACHED.

TEMPORARY SEDIMENT BASIN DETAIL

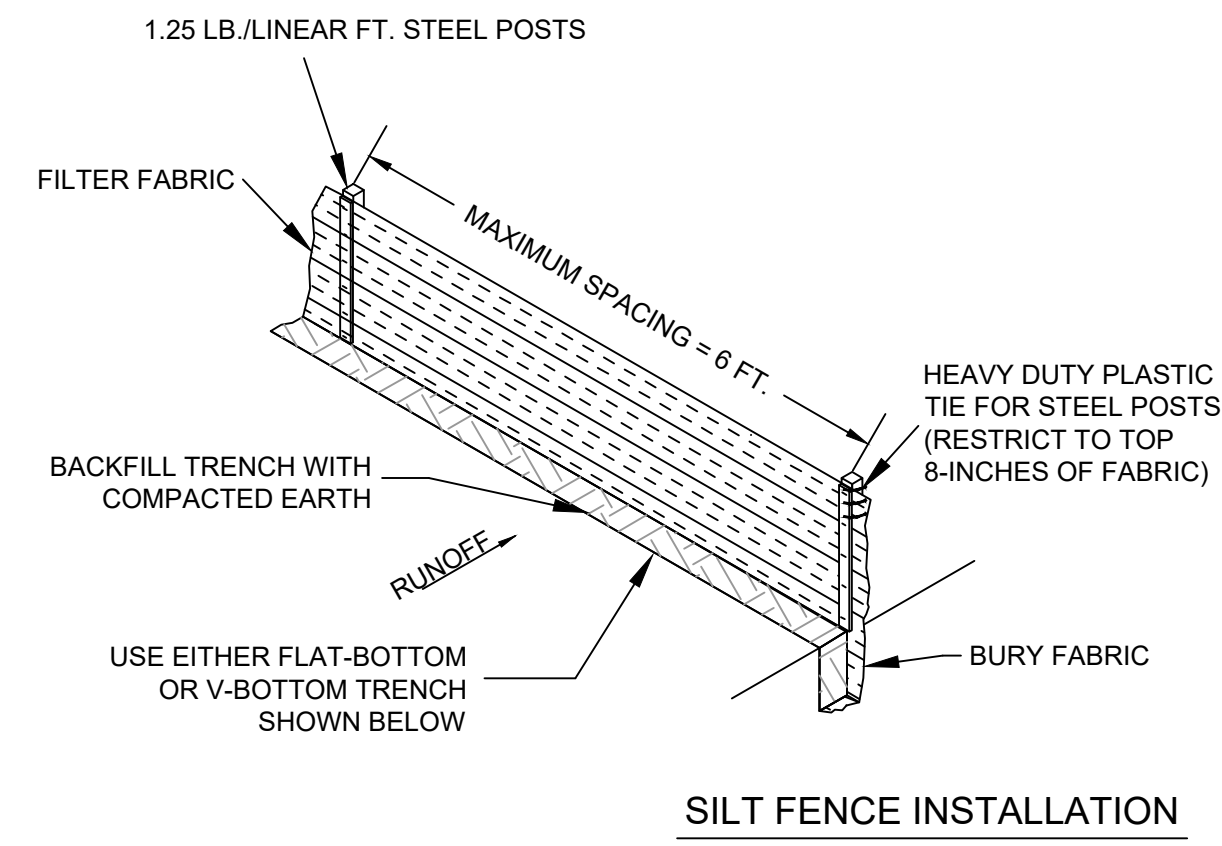
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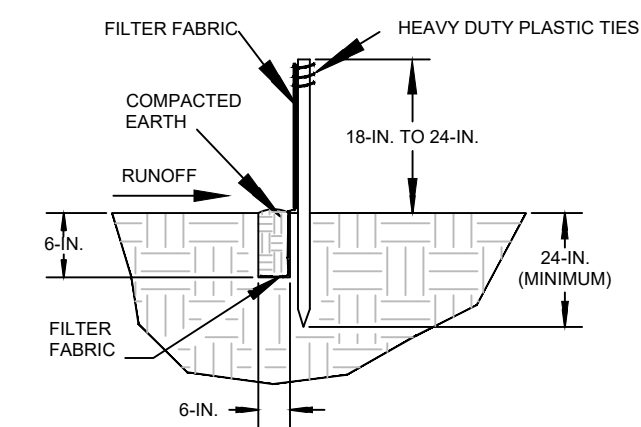
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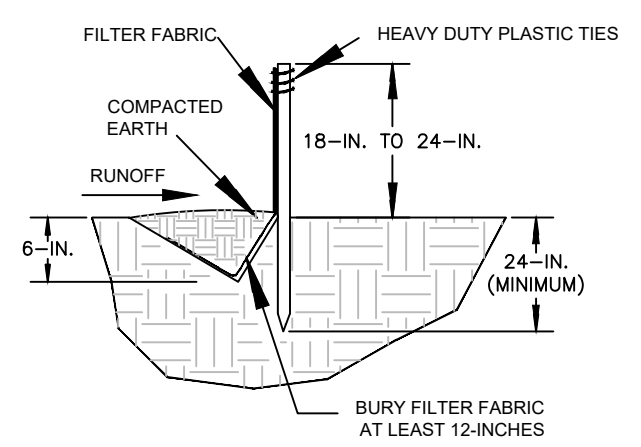
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FLAT-BOTTOM TRENCH DETAIL



V-SHAPED TRENCH DETAIL



SILT FENCE - GENERAL NOTES

- DO NOT PLACE SILT FENCE ACROSS CHANNELS OR IN OTHER AREAS SUBJECT TO CONCENTRATED FLOWS. SILT FENCE SHOULD NOT BE USED AS A VELOCITY CONTROL BMP. CONCENTRATED FLOWS ARE ANY FLOWS GREATER THAN 0.5 CFS.
- MAXIMUM SHEET OR OVERLAND FLOW PATH LENGTH TO THE SILT FENCE SHALL BE 100-FEET.
- MAXIMUM SLOPE STEEPNESS (NORMAL [PERPENDICULAR] TO THE FENCE LINE) SHALL BE 2:1.
- SILT FENCE JOINTS, WHEN NECESSARY, SHALL BE COMPLETED BY ONE OF THE FOLLOWING OPTIONS:
 - WRAP EACH FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 1-FOOT MINIMUM OVERLAP;
 - OVERLAP SILT FENCE BY INSTALLING 3-FEET PASSED THE SUPPORT POST TO WHICH THE NEW SILT FENCE ROLL IS ATTACHED. ATTACH OLD ROLL TO NEW ROLL WITH HEAVY-DUTY PLASTIC TIES; OR,
 - OVERLAP ENTIRE WIDTH OF EACH SILT FENCE ROLL FROM ONE SUPPORT POST TO THE NEXT SUPPORT POST.
- ATTACH FILTER FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED WITHIN THE TOP 8-INCHES OF THE FABRIC.
- INSTALL THE SILT FENCE PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE THE SILT FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.
- INSTALL SILT FENCE CHECKS (TIE-BACKS) EVERY 50-100 FEET, DEPENDENT ON SLOPE, ALONG SILT FENCE THAT IS INSTALLED WITH SLOPE AND WHERE CONCENTRATED FLOWS ARE EXPECTED OR ARE DOCUMENTED ALONG THE PROPOSED/INSTALLED SILT FENCE.

MATERIALS - STEEL POSTS

- USE 48-INCH LONG STEEL POSTS THAT MEET THE FOLLOWING MINIMUM PHYSICAL REQUIREMENTS:
- COMPOSED OF HIGH STRENGTH STEEL WITH MINIMUM YIELD STRENGTH OF 50,000 PSI.
 - HAVE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND NOMINAL "T" LENGTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (± 8%).
 - HAVE A SOIL STABILIZATION PLATE WITH A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES ATTACHED TO THE STEEL POSTS.
 - PAINTED WITH A WATER BASED BAKED ENAMEL PAINT.

- USE STEEL POSTS WITH A MINIMUM LENGTH OF 48 INCHES, WEIGHING 1.25 POUNDS PER LINEAR FOOT (± 8%) WITH PROJECTIONS TO AID IN FASTENING THE FABRIC. WHEN HEAVY CLAY SOILS ARE PRESENT ON SITE, STEEL POSTS WILL HAVE A METAL SOIL STABILIZATION PLATE WELDED NEAR THE BOTTOM SUCH THAT WHEN THE POST IS DRIVEN TO THE PROPER DEPTH, THE PLATE WILL BE BELOW THE GROUND LEVEL FOR ADDED STABILITY. THE SOIL PLATES SHOULD HAVE THE FOLLOWING CHARACTERISTICS:
- BE COMPOSED OF MINIMUM 15 GAUGE STEEL.
 - HAVE A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES.

GEOTEXTILE FILTER FABRIC

- SILT FENCE MUST BE COMPOSED OF WOVEN GEOTEXTILE FILTER FABRIC THAT CONSISTS OF THE FOLLOWING REQUIREMENTS:
 - COMPOSED OF FIBERS CONSISTING OF LONG CHAIN SYNTHETIC POLYMERS OF AT LEAST 85% BY WEIGHT OF POLYESTERS, POLYESTERS, OR POLYAMIDES THAT ARE FORMED INTO A NETWORK SUCH THAT THE FILAMENTS OR YARNS RETAIN DIMENSIONAL STABILITY RELATIVE TO EACH OTHER;
 - FREE OF ANY TREATMENT OR COATING WHICH MIGHT ADVERSELY ALTER ITS PHYSICAL PROPERTIES AFTER INSTALLATION; FREE OF ANY DEFECTS OR FLAWS THAT SIGNIFICANTLY AFFECT ITS PHYSICAL AND/OR FILTERING PROPERTIES; AND HAVE A MINIMUM WIDTH OF 36-INCHES.
- USE ONLY FABRIC APPEARING ON SC DOT'S QUALIFIED PRODUCTS LISTING (QPL), APPROVAL SHEET #34, MEETING THE REQUIREMENTS OF THE MOST CURRENT EDITION OF THE SC DOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
- 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED.
- FILTER FABRIC SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS.
- FILTER FABRIC SHALL BE INSTALLED AT A MINIMUM OF 24-INCHES ABOVE THE GROUND.

INSTALLATION

- EXCAVATE A TRENCH APPROXIMATELY 6-INCHES WIDE AND 6-INCHES DEEP WHEN PLACING FABRIC BY HAND. PLACE 12-INCHES OF GEOTEXTILE FABRIC INTO THE 6-INCH DEEP TRENCH, EXTENDING THE REMAINING 6-INCHES TOWARDS THE UPSLOPE SIDE OF THE TRENCH. BACKFILL THE TRENCH WITH SOIL OR GRAVEL AND COMPACT.
- BURY 12-INCHES OF FABRIC INTO THE GROUND WHEN PNEUMATICALLY INSTALLING SILT FENCE WITH A SLICING METHOD.
- PURCHASE FABRIC IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS. WHEN JOINTS ARE NECESSARY, WRAPPED THE FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 6-INCH MINIMUM OVERLAP.
- INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. INSTALL POSTS A MINIMUM OF 1 TO 2 INCHES ABOVE THE FABRIC, WITH NO MORE THAN 3-FEET OF THE POST ABOVE THE GROUND. SPACE POSTS TO MAXIMUM 6-FEET CENTERS.
- ATTACH FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED AND PLACED IN A MANNER TO PREVENT SAGGING OR TEARING OF THE FABRIC. IN ALL CASES, TIES SHOULD BE AFFIXED IN NO LESS THAN 4 PLACES.
- INSTALL THE FABRIC A MINIMUM OF 24-INCHES ABOVE THE GROUND. WHEN NECESSARY, THE HEIGHT OF THE FENCE ABOVE GROUND MAY BE GREATER THAN 24-INCHES. IN TIDAL AREAS, EXTRA SILT FENCE HEIGHT MAY BE REQUIRED.
- THE POST HEIGHT WILL BE TWICE THE EXPOSED POST HEIGHT. POST SPACING WILL REMAIN THE SAME AND EXTRA HEIGHT FABRIC WILL BE 4-, 5-, OR 6-FEET TALL.
- LOCATE SILT FENCE CHECKS EVERY 100 FEET MAXIMUM AND AT LOW POINTS.
- INSTALL THE FENCE PERPENDICULAR TO THE DIRECTION OF FLOW AND PLACE THE FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.

INSPECTION AND MAINTENANCE

- THE KEY TO FUNCTIONAL SILT FENCE IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- REGULAR INSPECTIONS OF SILT FENCE SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVEN THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ATTENTION TO SEDIMENT ACCUMULATIONS ALONG THE SILT FENCE IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE SILT FENCE.
- REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH THE SILT FENCE, OR WHERE THE FENCE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE SILT FENCE. INSTALL CHECKS/TIE-BACKS AND/OR REINSTALL SILT FENCE, AS NECESSARY.
- CHECK FOR TEARS WITHIN THE SILT FENCE. AREAS WHERE SILT FENCE HAS BEGUN TO DECOMPOSE, AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE SILT FENCE INEFFECTIVE. REMOVED DAMAGED SILT FENCE AND REINSTALL NEW SILT FENCE IMMEDIATELY.
- SILT FENCE SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

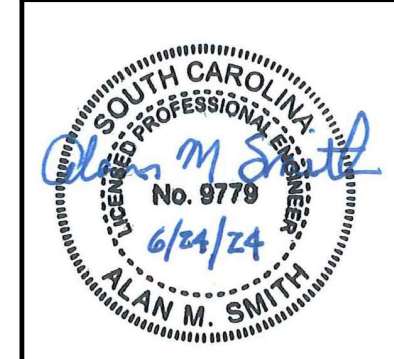
TEMPORARY SILT FENCE

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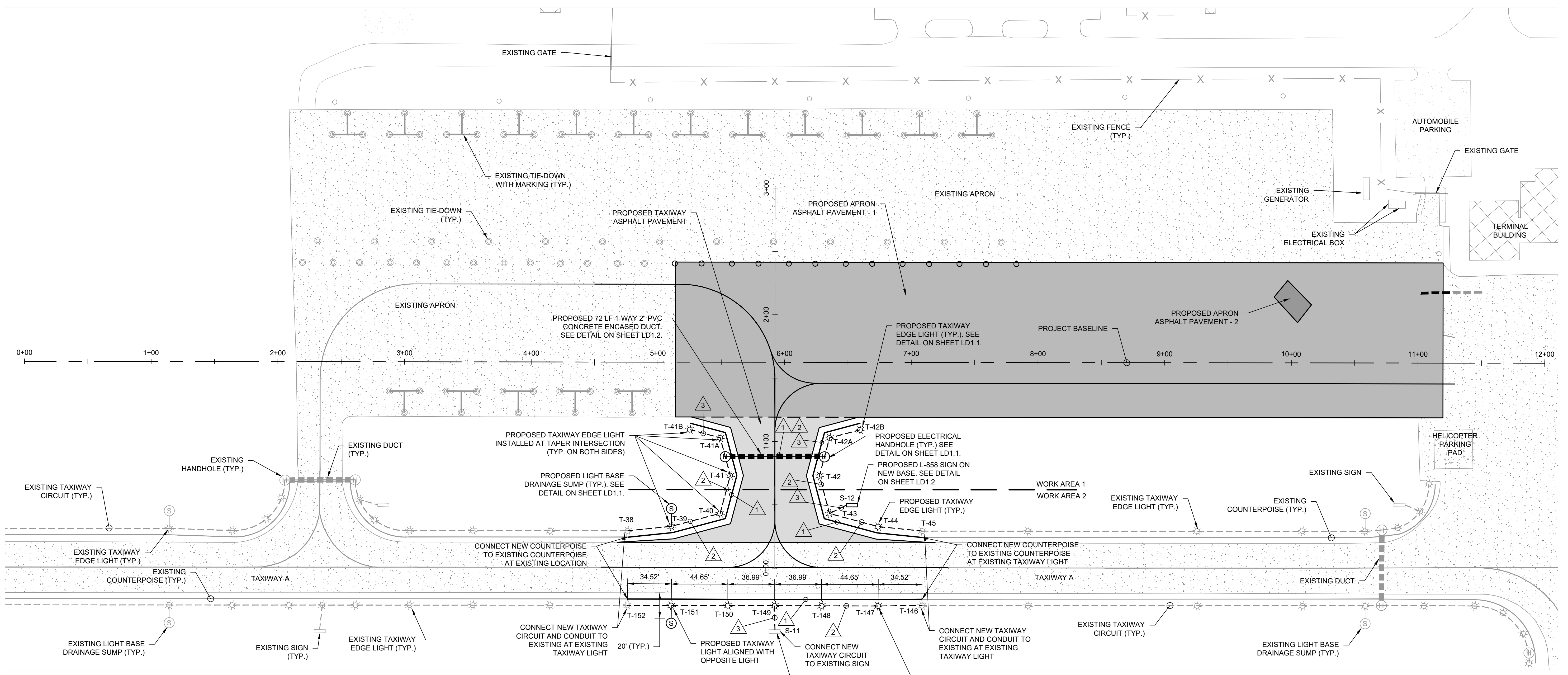
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
SEDIMENTATION AND EROSION CONTROL DETAILS
(SHEET 3 OF 3)



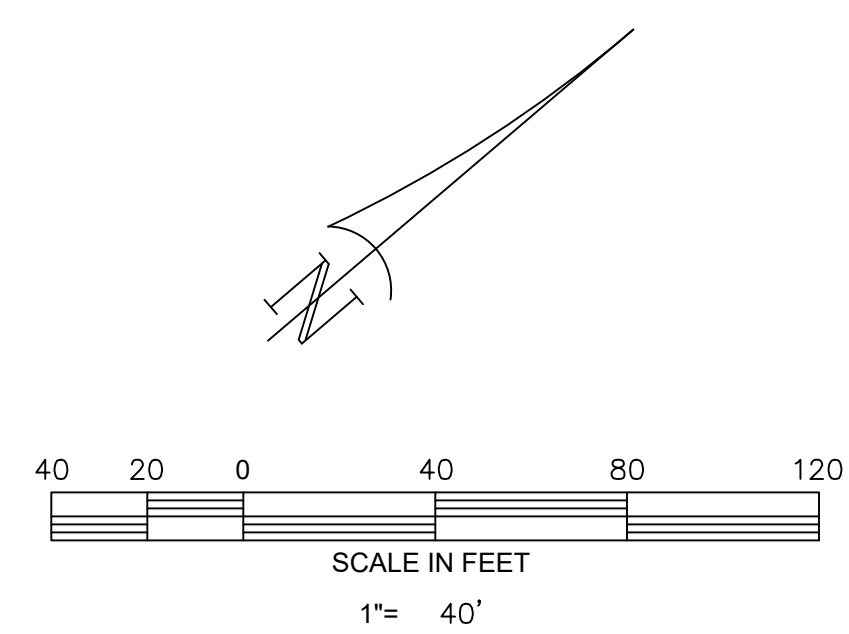
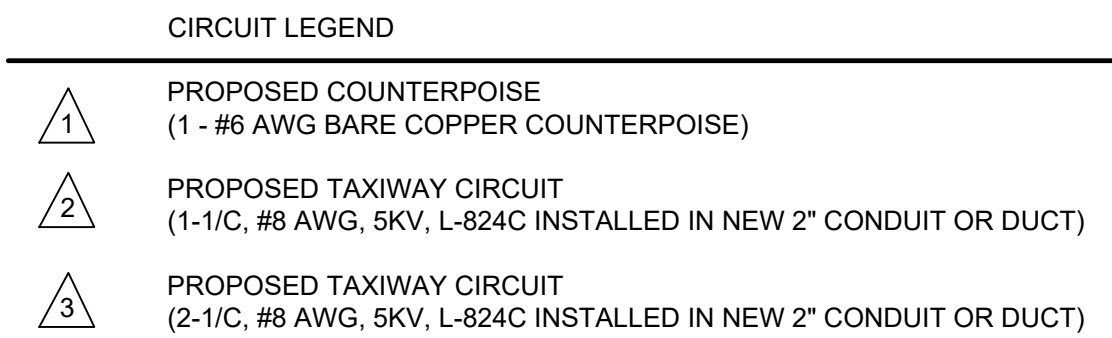
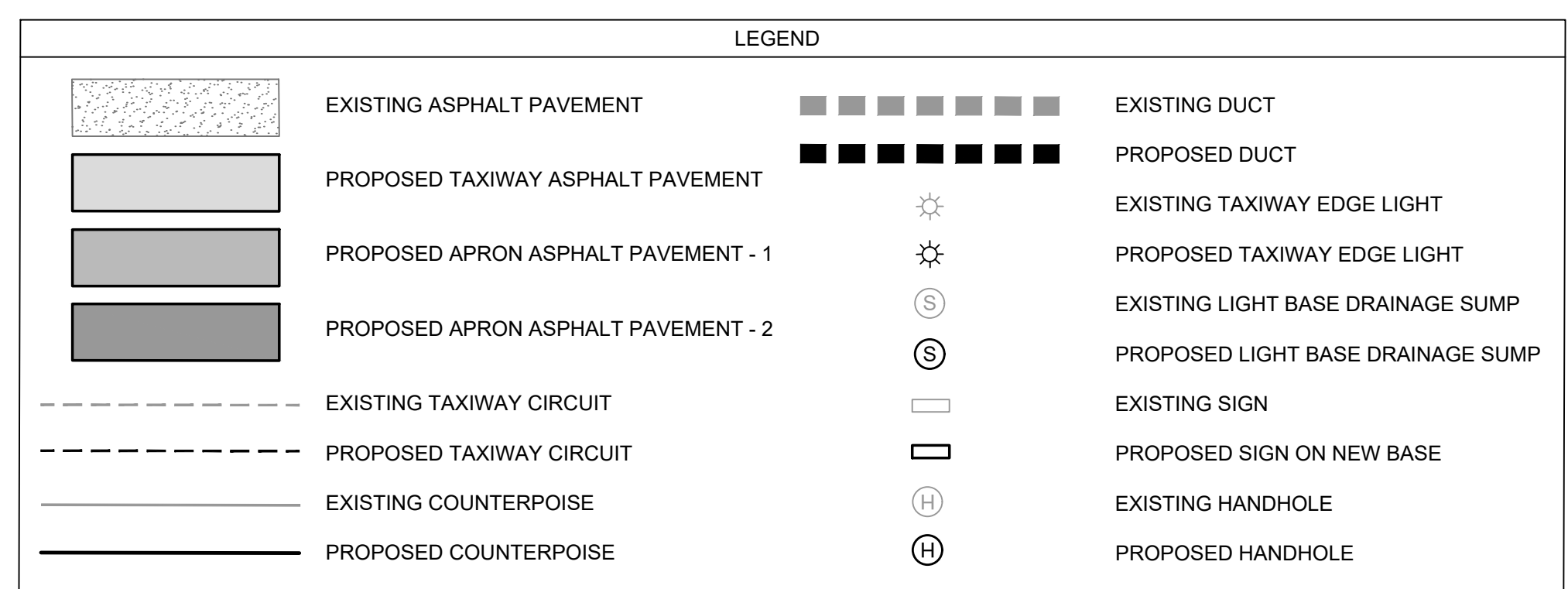
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GENERAL NOTES

1. LOCATIONS OF EXISTING CIRCUITS/CONDUCTORS ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, HANDHOLES, AND DUCT BANKS ARE BASED ON AVAILABLE INFORMATION. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. ANY EXISTING LIGHTING FACILITIES DAMAGED BY THE CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
2. THE EXISTING TAXIWAY LIGHTING SYSTEMS IN ACTIVE AIR OPERATIONS AREAS SHALL BE OPERATIONAL AT THE END OF EACH DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING ANY DAMAGE TO EXISTING LIGHTING SYSTEMS/CIRCUITS TO RESTORE LIGHTING OPERATIONS.
3. NEW INSTALLATIONS WHICH CROSS EXISTING INSTALLATIONS SHALL BE SEPARATED BY A MINIMUM OF 2 INCHES.
4. CONCRETE FOR BASE CAN INSTALLATION AND DUCT ENCASEMENT SHALL BE IN ACCORDANCE WITH SECTION P-610, STRUCTURAL PORTLAND CEMENT CONCRETE.
5. DUCT MARKERS SHALL BE INSTALLED AT THE ENDS OF ALL NEW DUCTS, AND WHERE THE NEW CABLE HOME RUN CHANGES DIRECTION AND EVERY 200' ALONG HOMERUN.
6. COPPER COUNTERPOISE GROUND WIRE SHALL BE CONNECTED TO 3/4" x 10' COPPER CLAD GROUND RODS SPACED AT MINIMUM 500' ALONG COUNTERPOISE TRENCH. WHERE DUCT RUNS CONTINUE BENEATH PAVEMENT, INSTALL THE COUNTERPOISE A MINIMUM OF 4" ABOVE DUCTS ALONG ENTIRE RUN. INSTALL GROUND RODS AT ENDS OF EACH DUCT. DO NOT INSTALL COUNTERPOISE THROUGH DUCT.
7. CONTRACTOR SHALL INSTALL NEW TAXIWAY LIGHTS AND CIRCUITS AS SHOWN. PROPOSED TAXIWAY LIGHTS SHALL BE INSTALLED 10' FROM PAVEMENT EDGE.
8. SEE SHEETS LD1.1 AND LD1.2 FOR LIGHTING INSTALLATION DETAILS.
9. CONTRACTOR TO INSTALL #8 AWG TEMPORARY JUMPER CABLE TO MAINTAIN EXISTING TAXIWAY LIGHTING CIRCUIT IN OPERATION AT ALL TIMES.

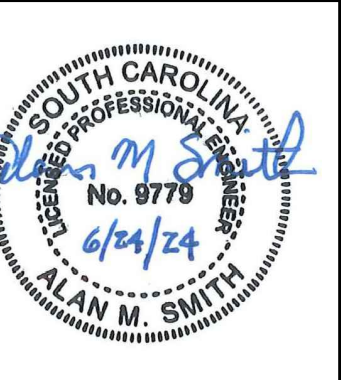


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLEY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C003296
EMAIL: TBILL@TBILL.COM

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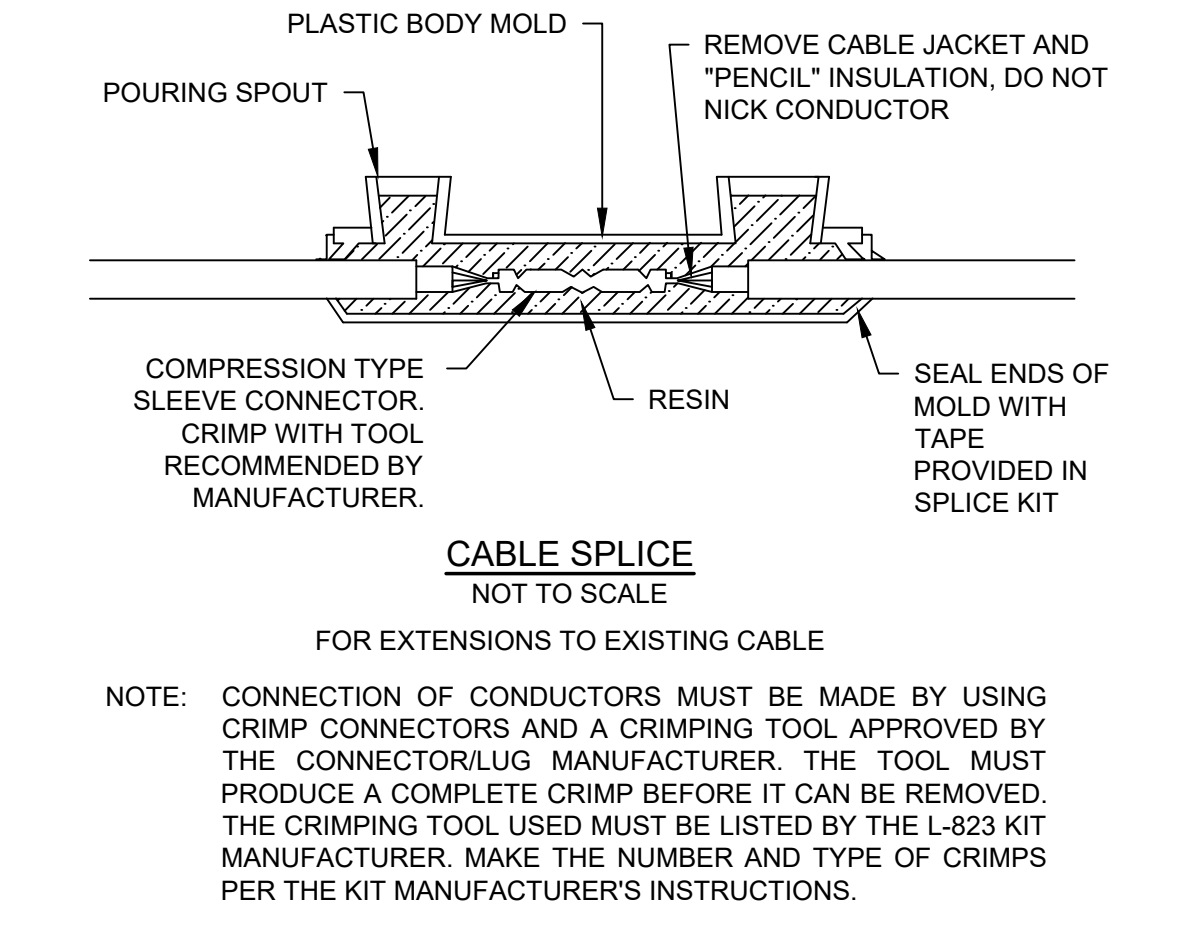
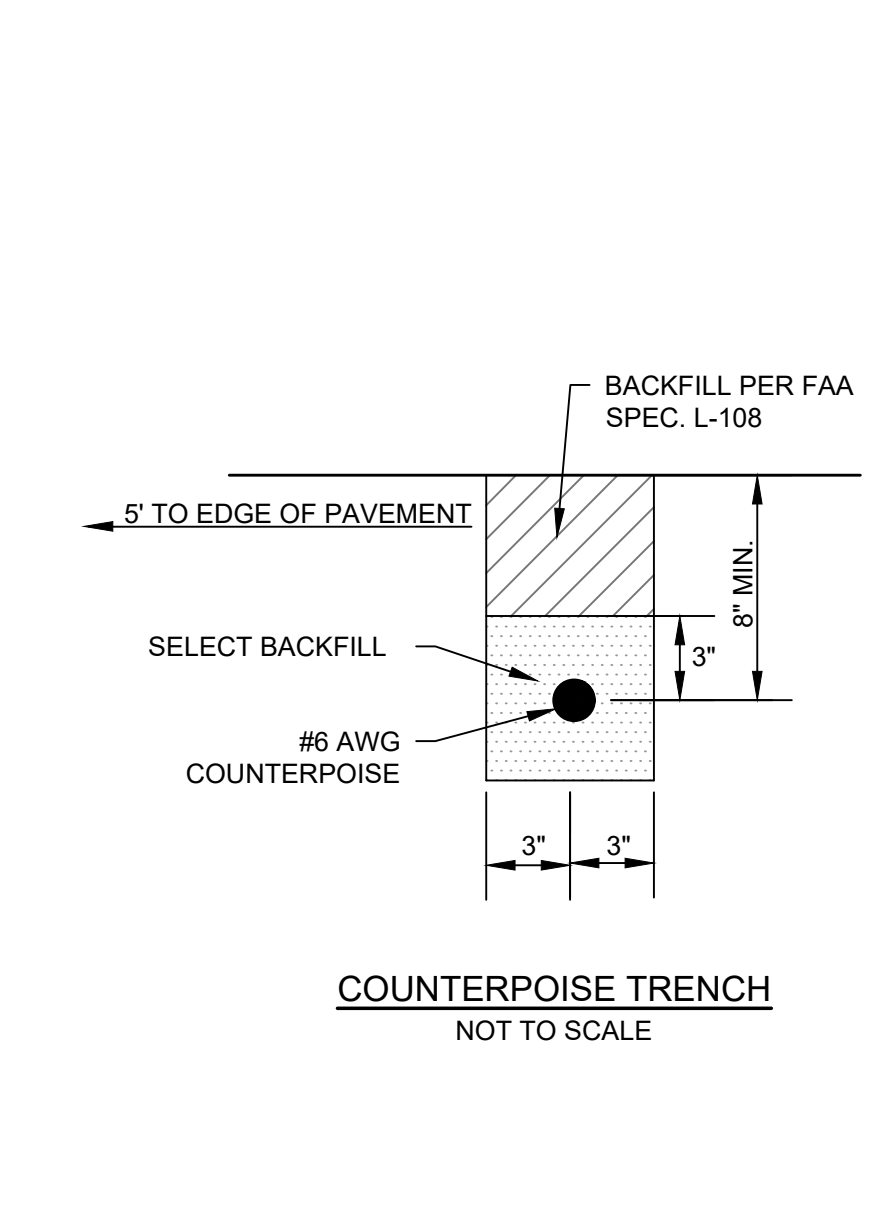
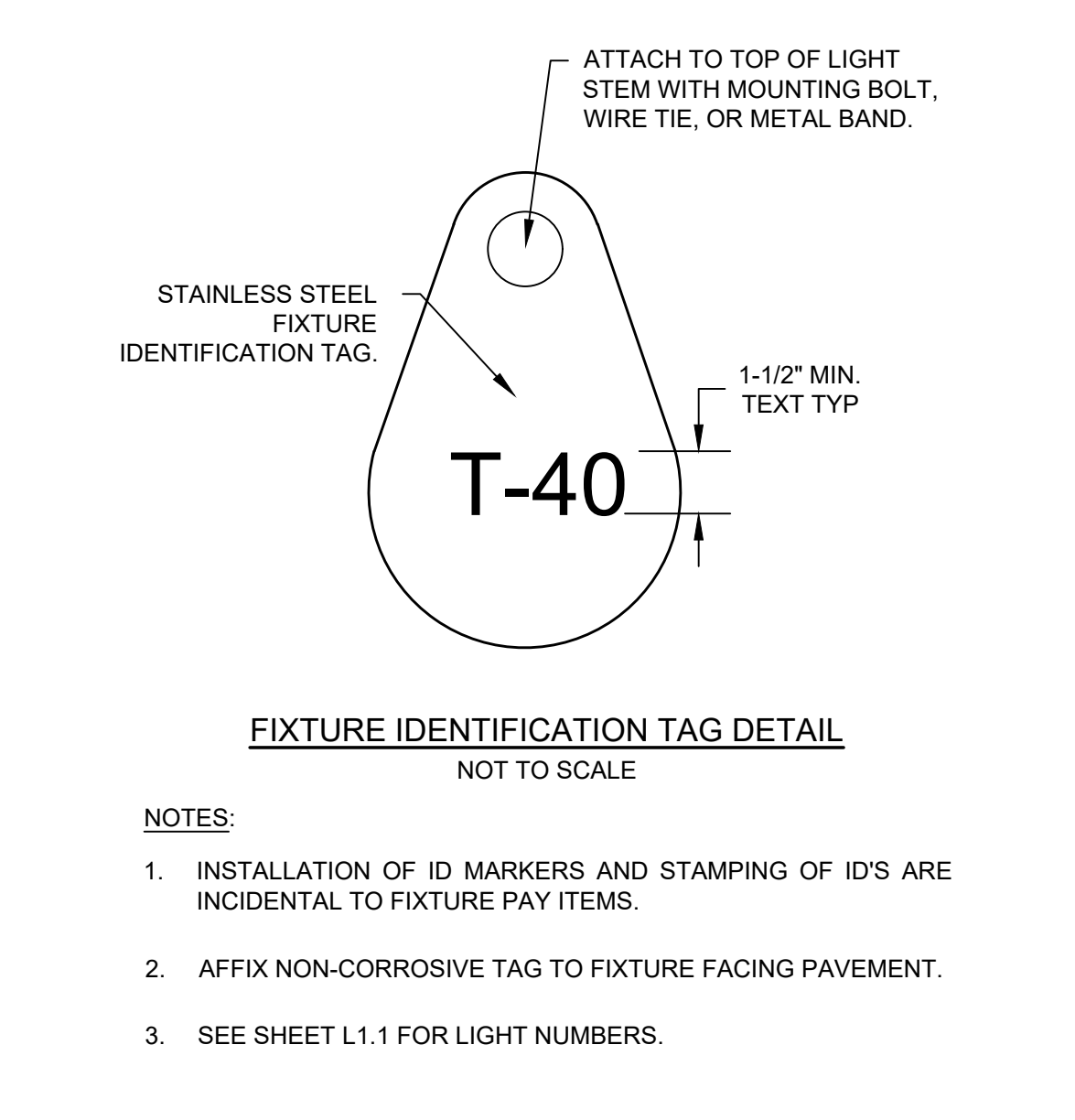
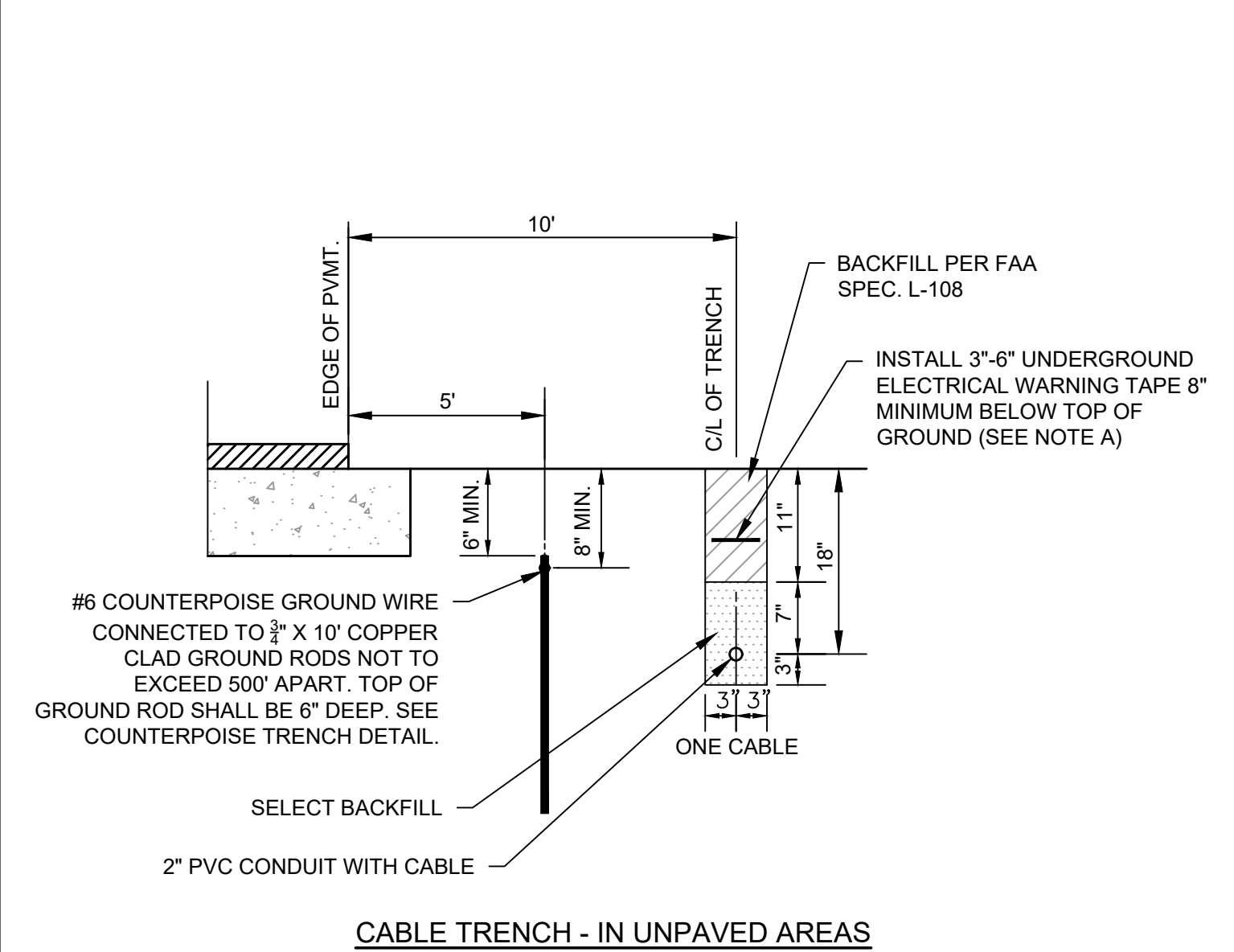
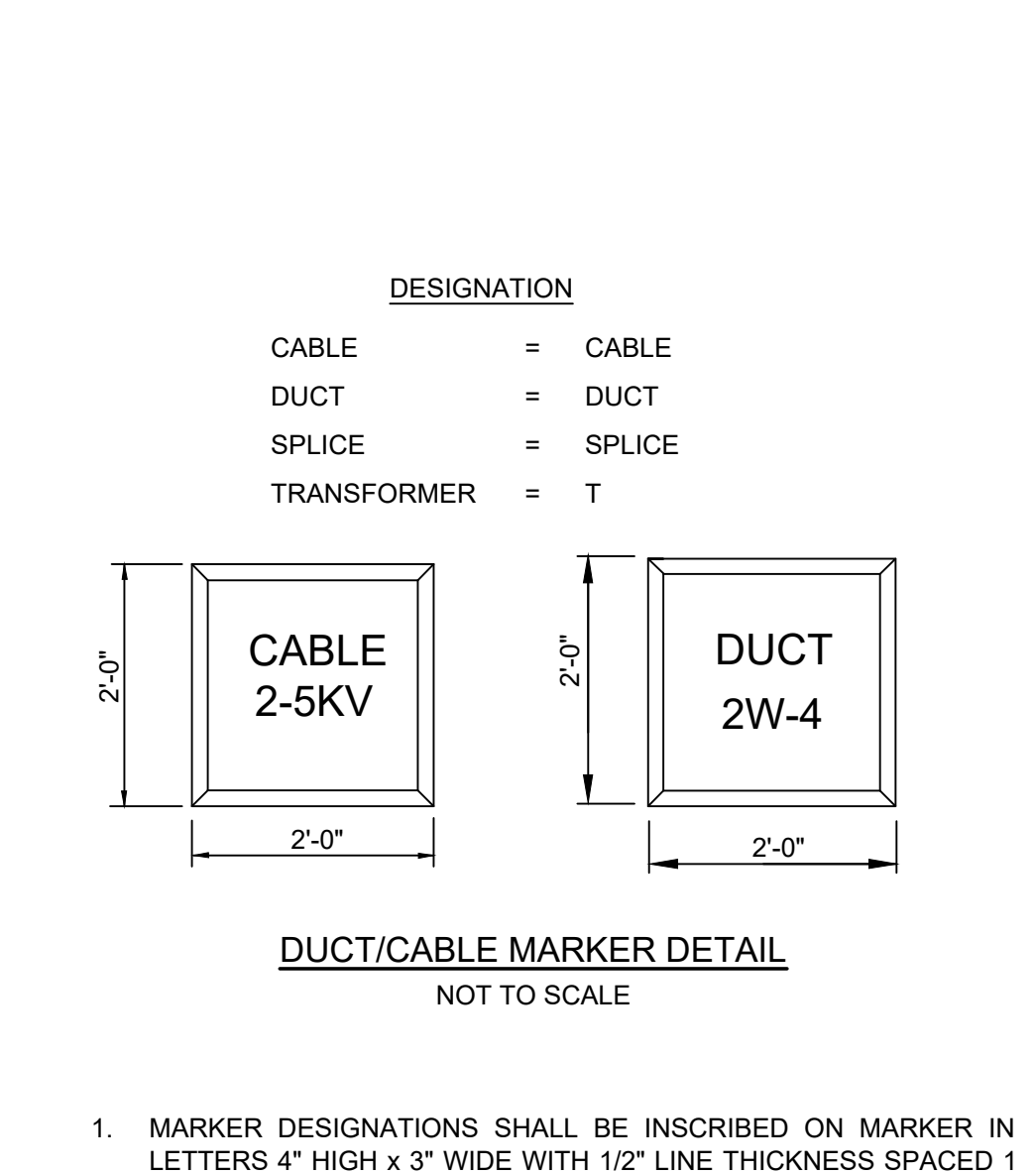
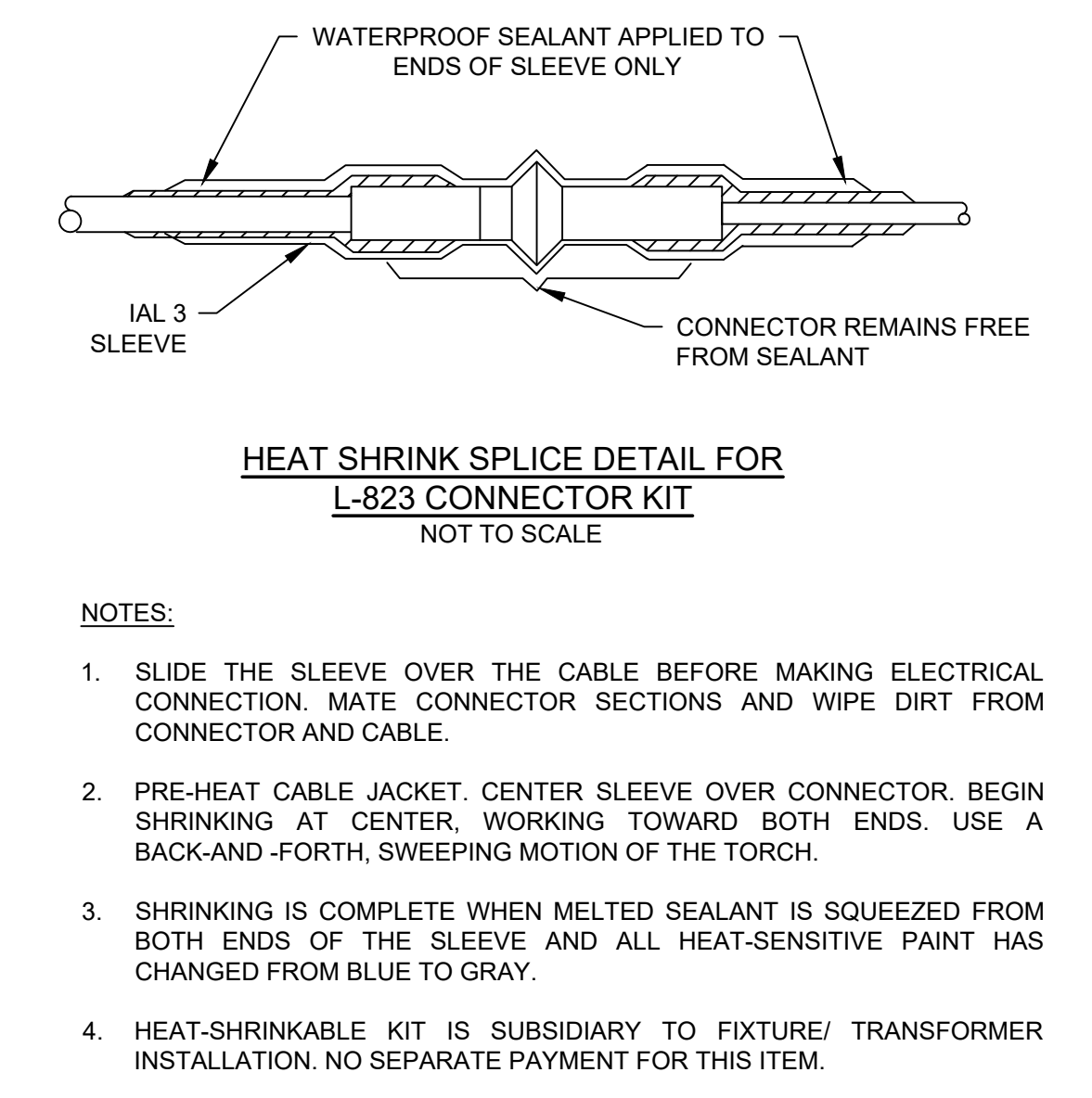
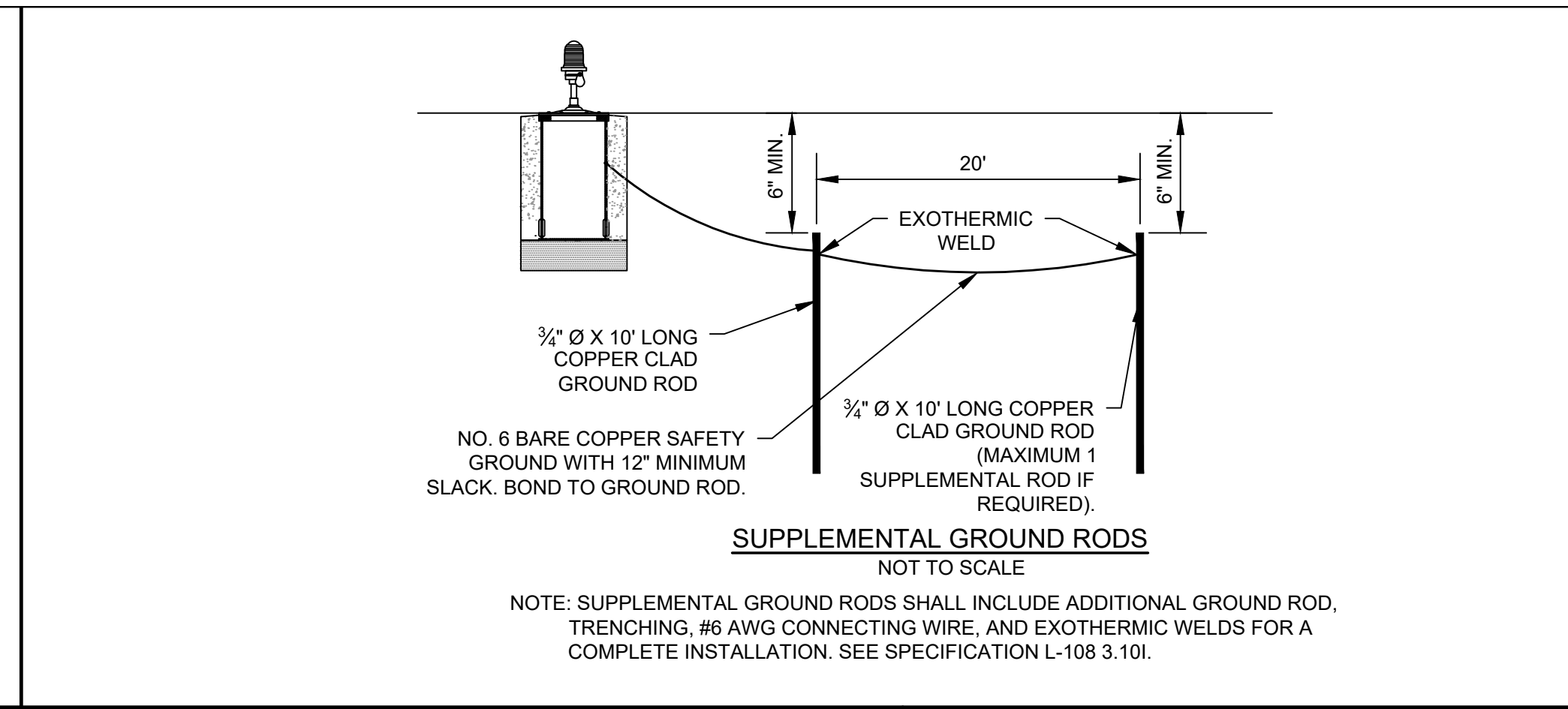
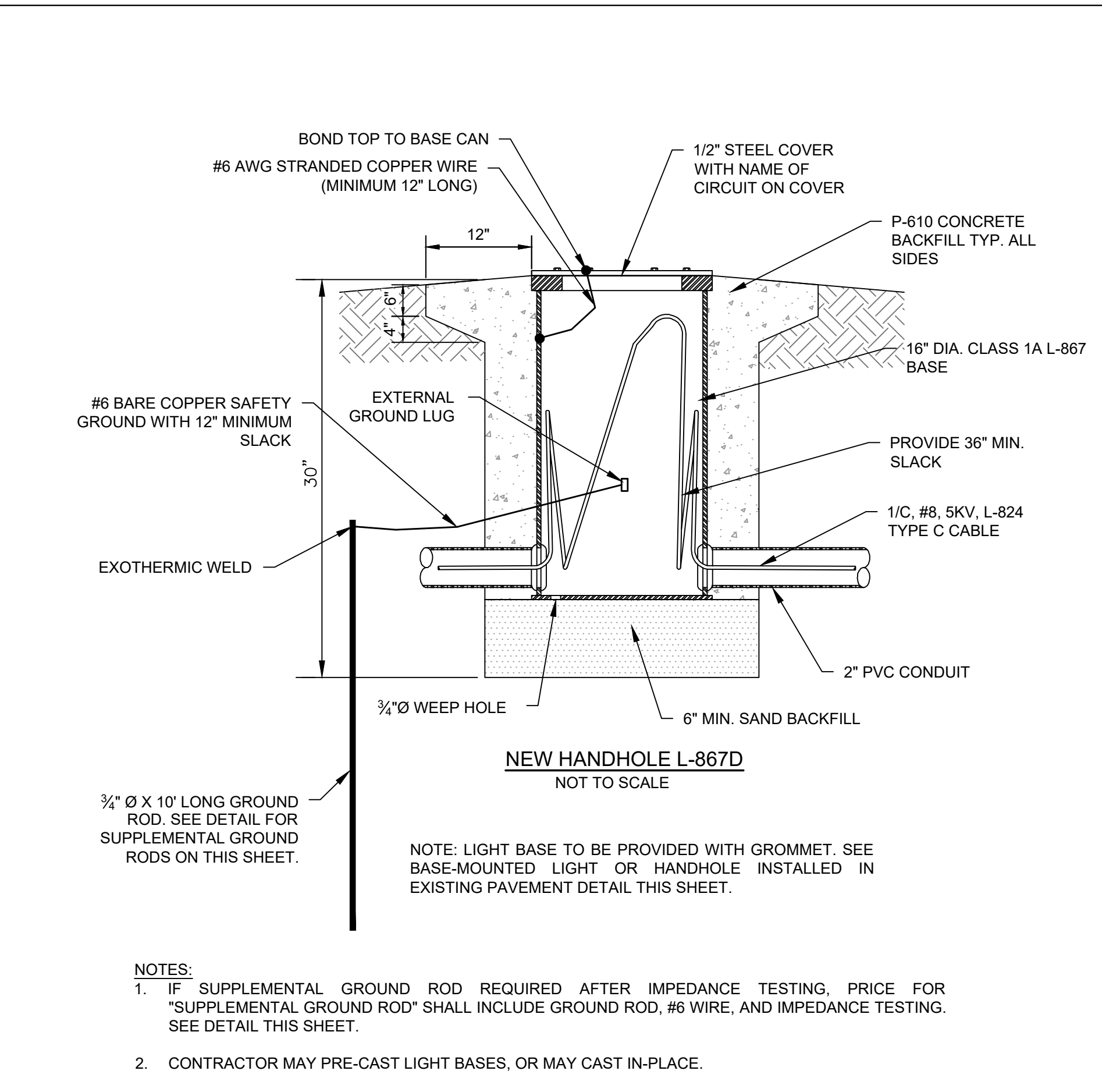
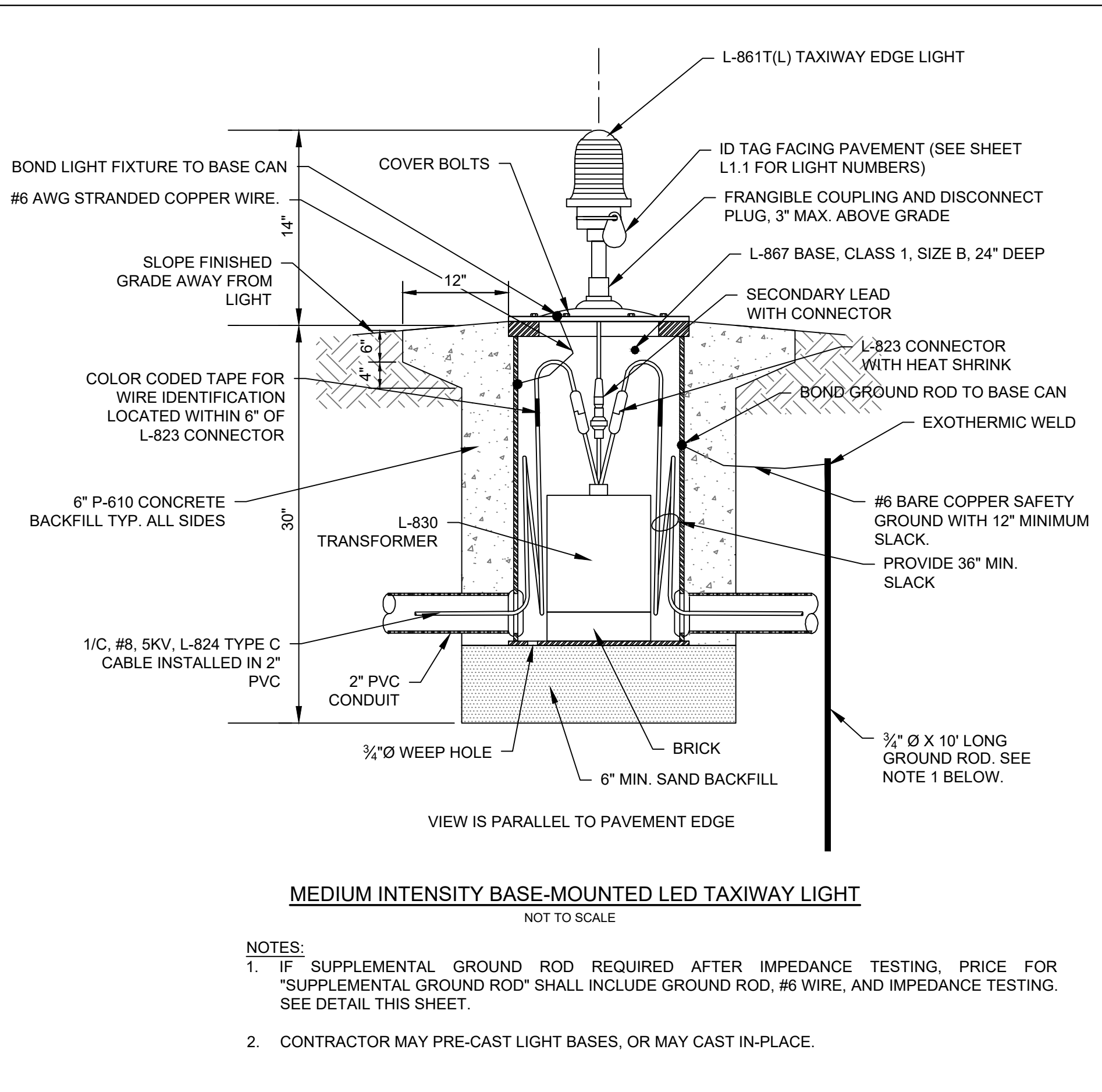
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
LIGHTING LAYOUT AND CIRCUIT PLAN



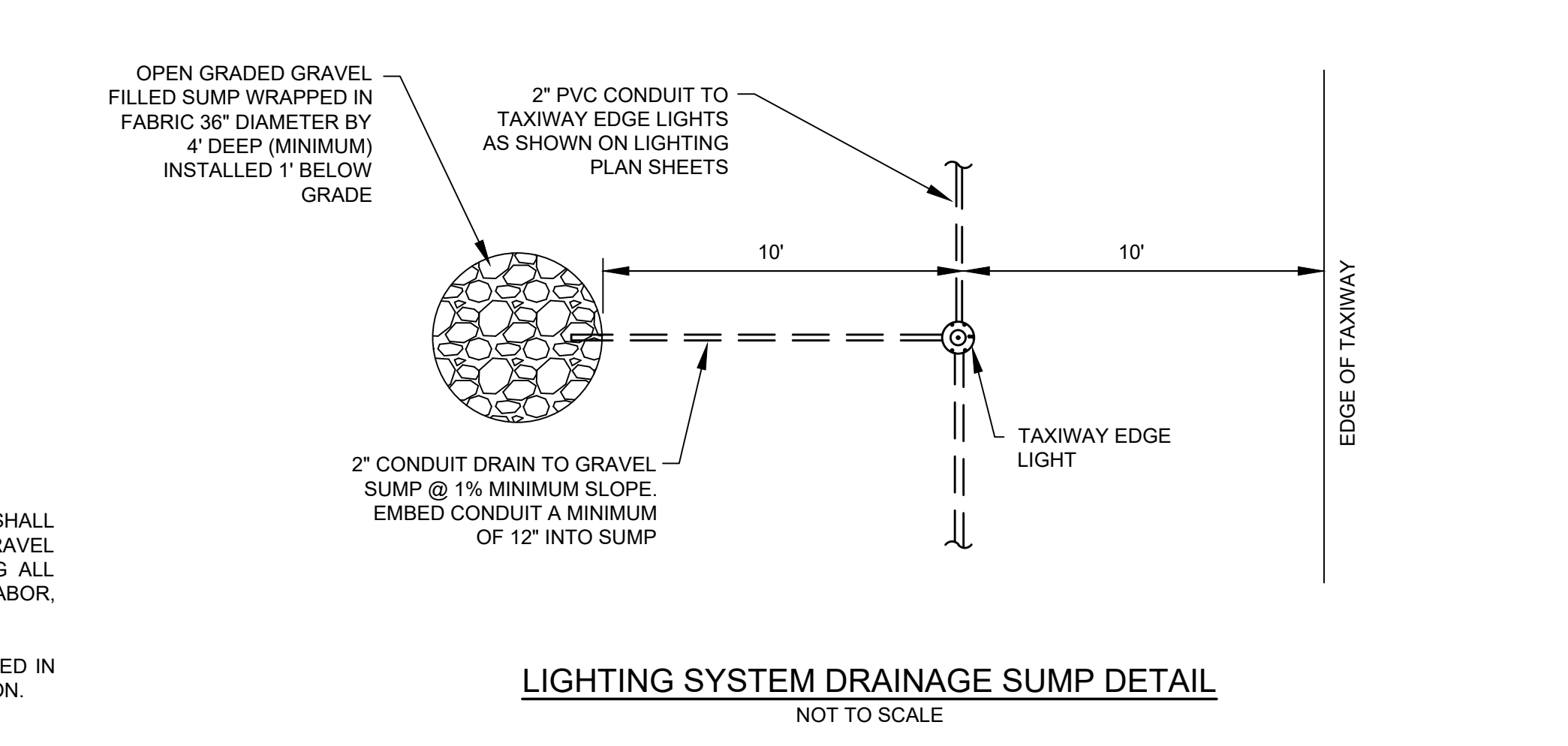
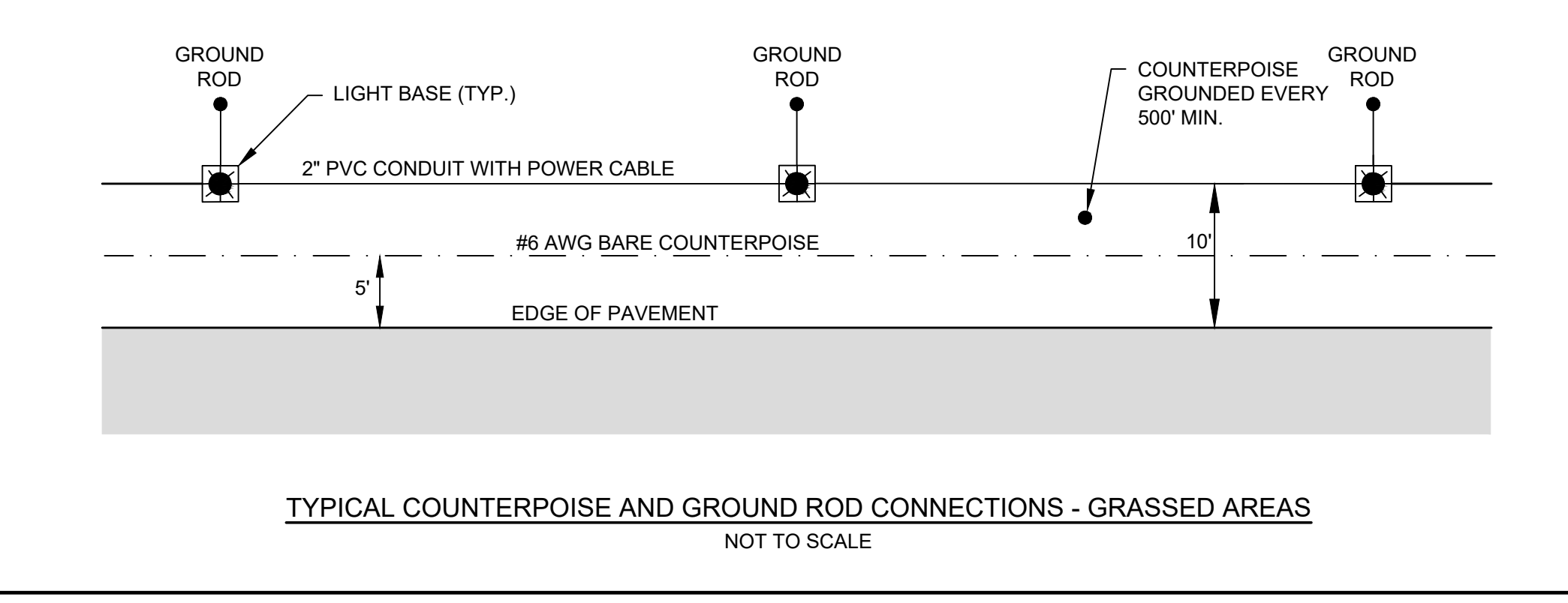
Date	JUNE 2024
Scale	1" = 40'
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Checked	AMS
Project No.	2601-2105
Sheet No.	

L1.1



- MARKER DESIGNATIONS SHALL BE INSCRIBED ON MARKER IN LETTERS 4\"/>

- SUMP NOTES:**
- FABRIC SHALL BE 8 OZ., NON-WOVEN GEOTEXTILE FABRIC.
 - STONE SHALL BE OPEN GRADED STONE, AASHTO #57 STONE OR EQUIVALENT.
 - "LIGHTING SYSTEM DRAINAGE SUMP" SHALL BE MEASURED AND PAID FOR ON A PER EACH BASIS. ITEM SHALL INCLUDE TRENCHING AND INSTALLATION OF 2\"/>

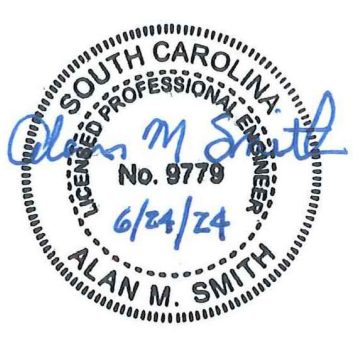


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C003296
EMAIL: TBILL@TBILL.COM

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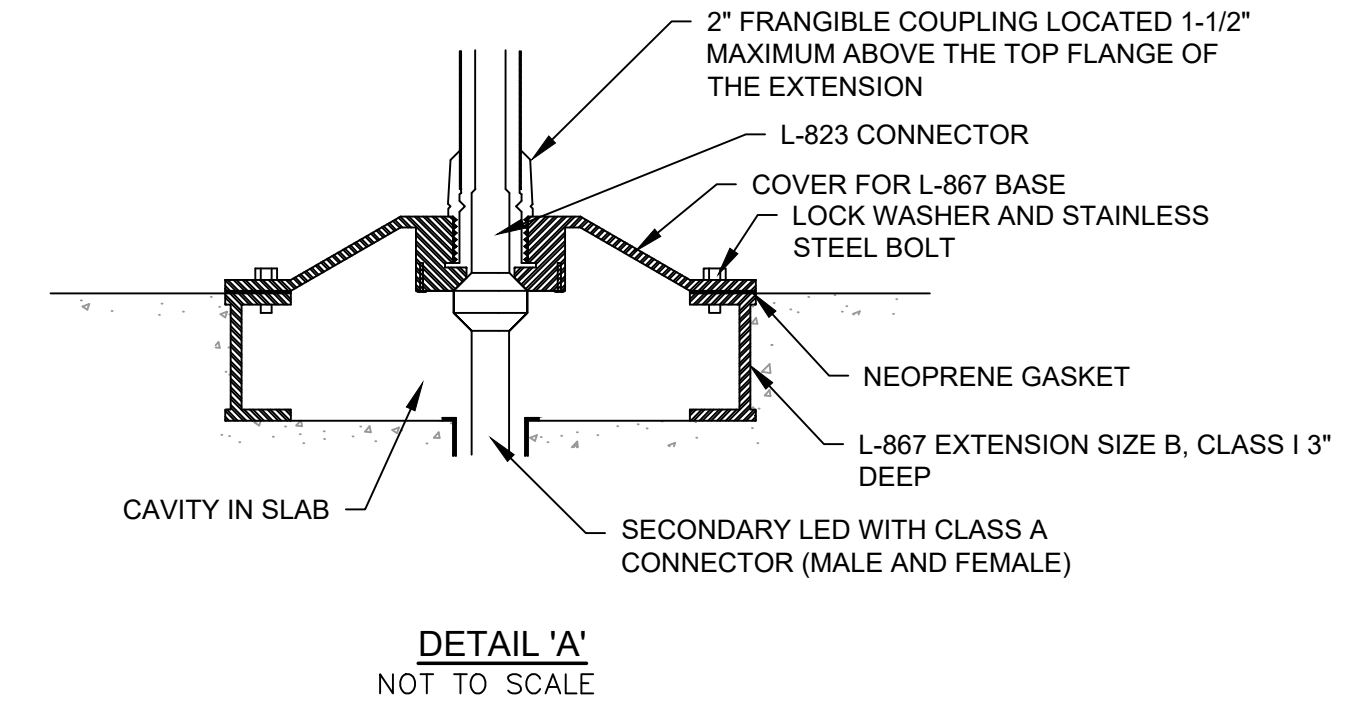
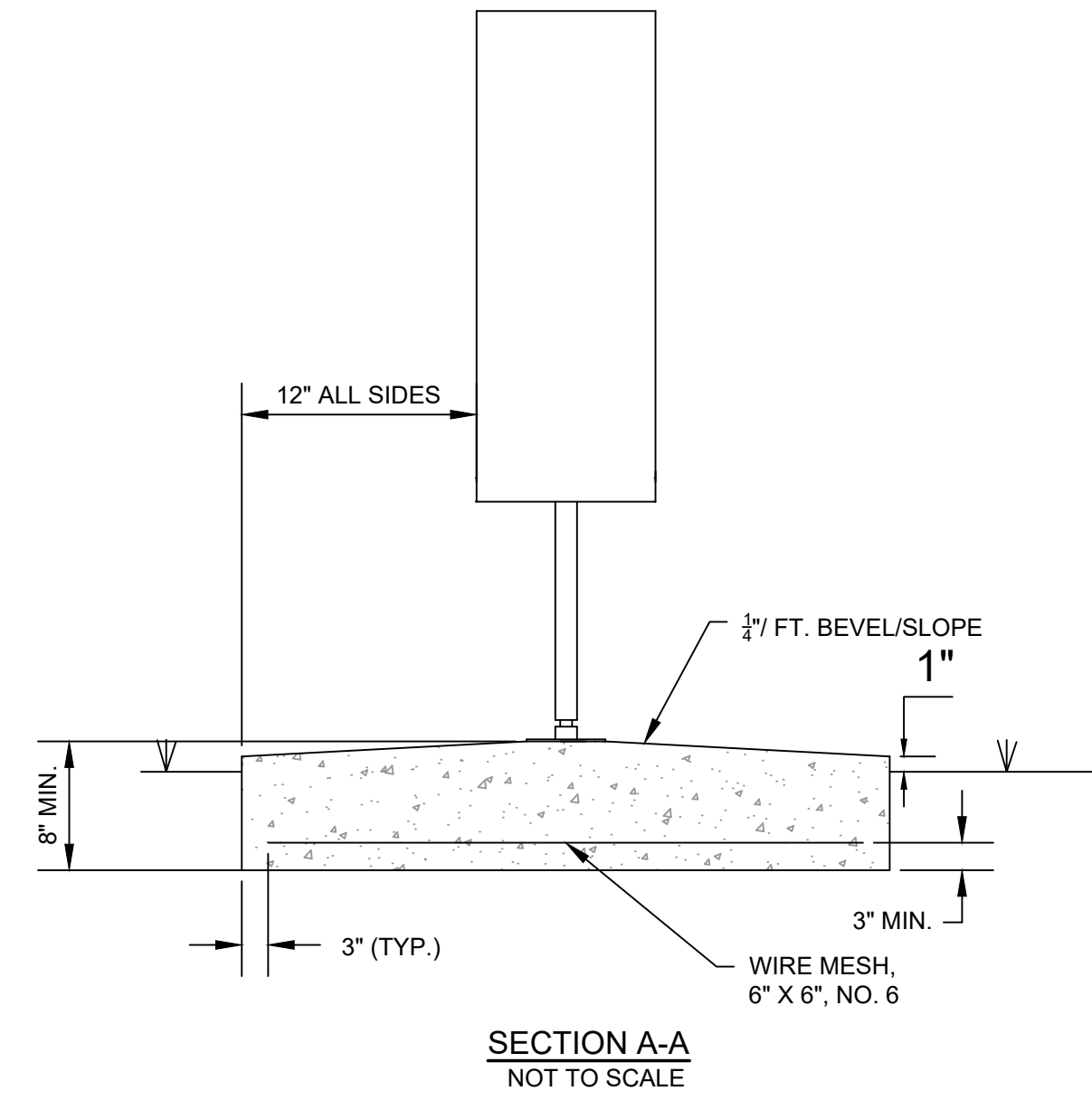
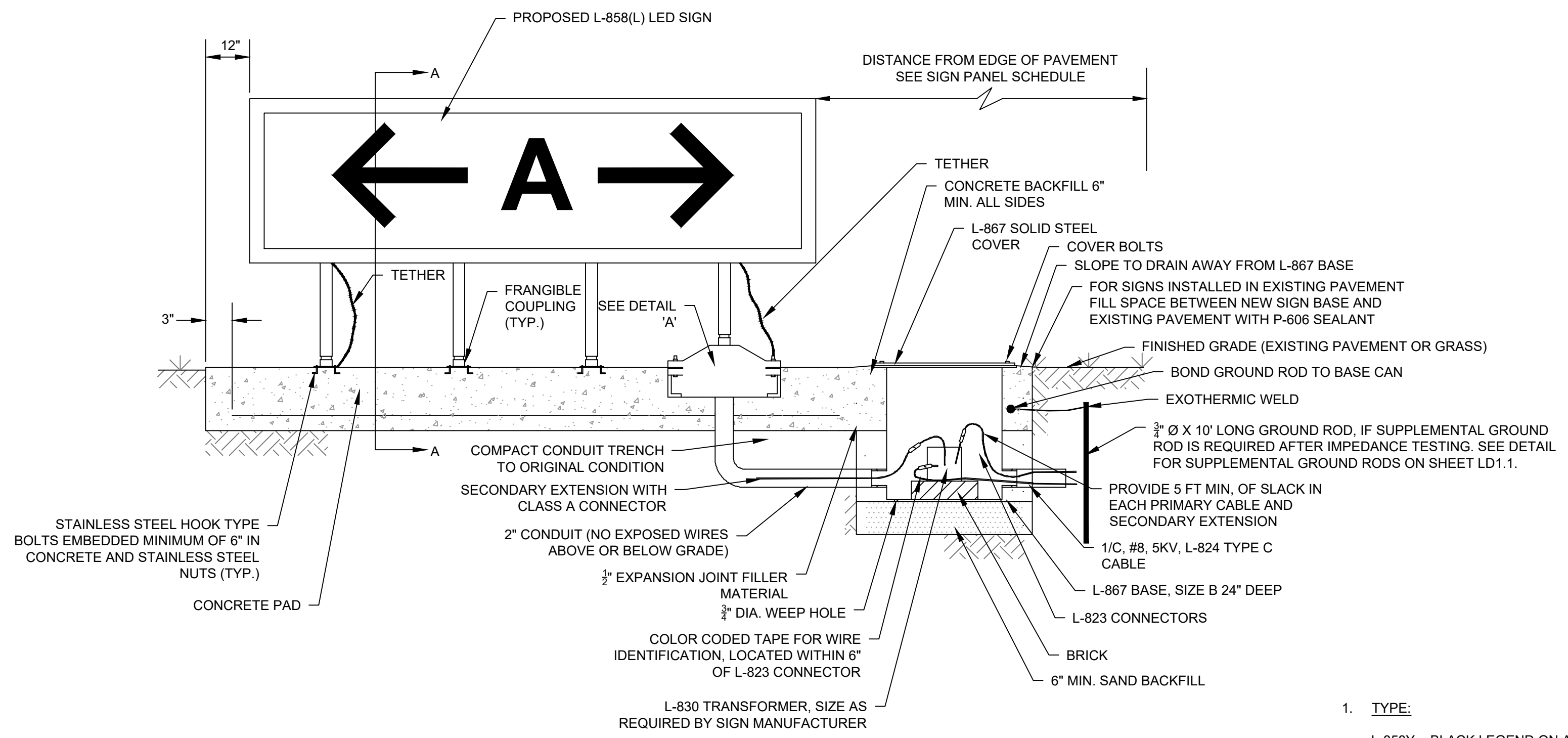
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
LIGHTING DETAILS
(SHEET 1 OF 2)



Date	JUNE 2024
Scale	NTS
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Project No.	2601-2105
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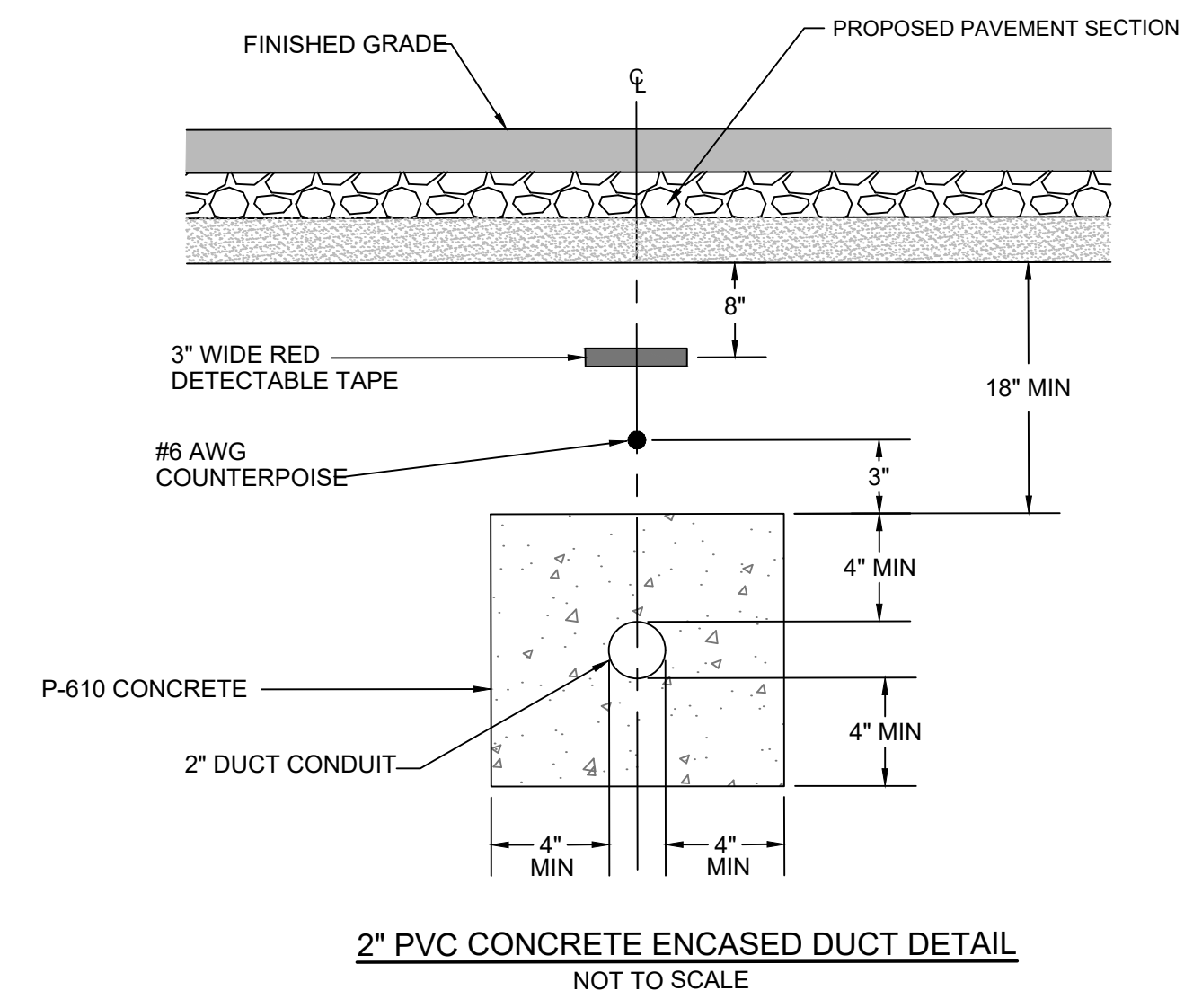


NOTES:
 1. NUMBER AND SPACING OF LEGS AS PER MANUFACTURER'S REQUIREMENTS.

1. **TYPE:**
 L-858Y = BLACK LEGEND ON A YELLOW BACKGROUND
2. **SIZE:**
 2 = 24" FACE, 15" LEGEND

RELOCATED SIGN PANEL SCHEDULE									
SIGN NO.	FACE 1	FACE 2	SIGN TYPE	DISTANCE FROM TWY EDGE AND NEAR SIDE OF SIGN	SIZE	STYLE/CLASS	MODE	NO. OF MODULES	REMARK
S-11	← 23 5 · 11 →	—————	L-858Y	25'	2	-	-	4	NEW REPLACEMENT SIGN PANEL
S-12	← A →	—————	L-858Y	25'	2	2/2	2	2	NEW SIGN ON NEW BASE

NEW L-858(L) LED SIGN INSTALLATION ON NEW BASE
 NOT TO SCALE



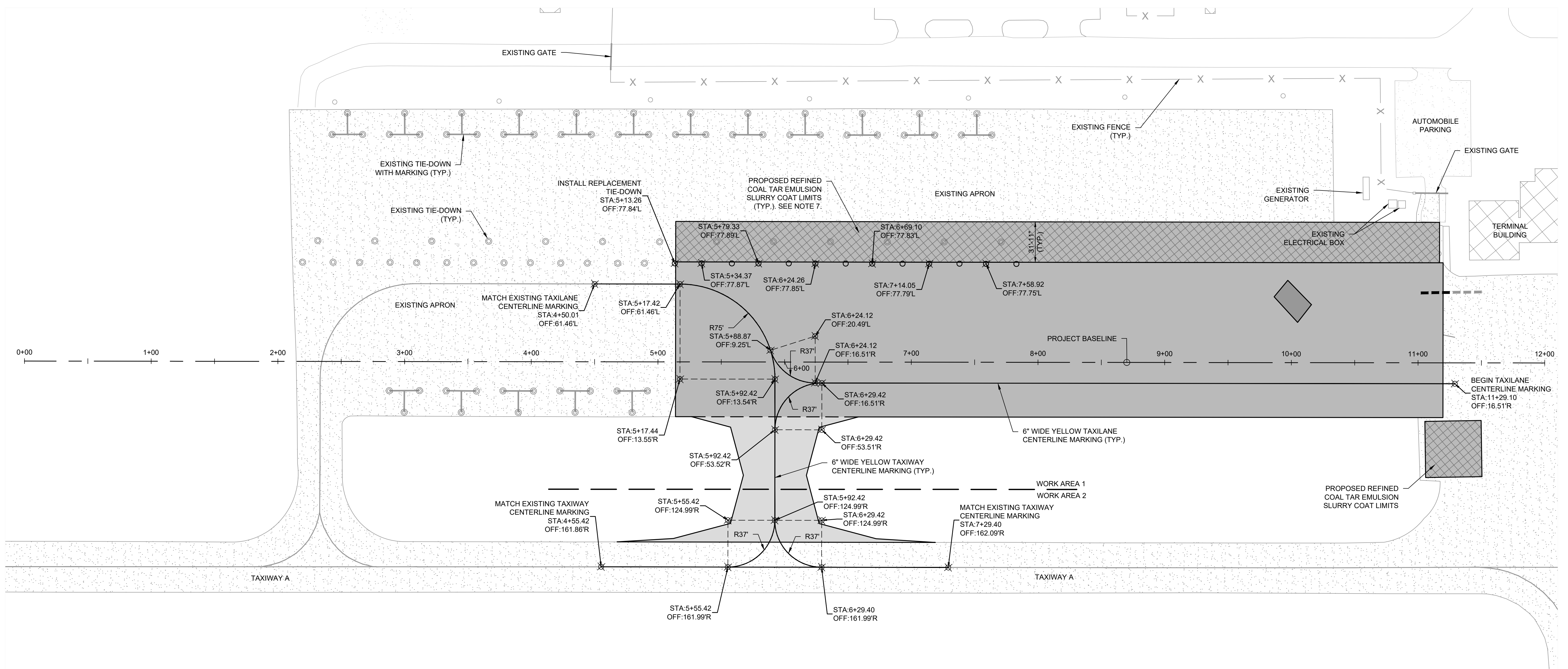
2" PVC CONCRETE ENCASED DUCT DETAIL
 NOT TO SCALE

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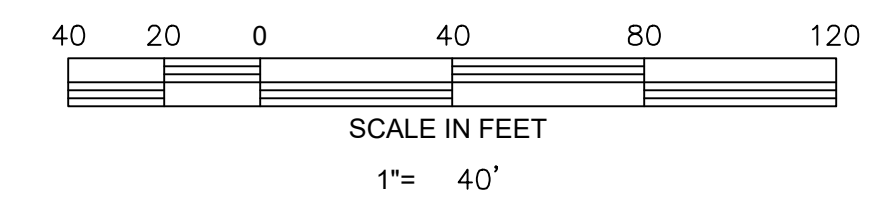
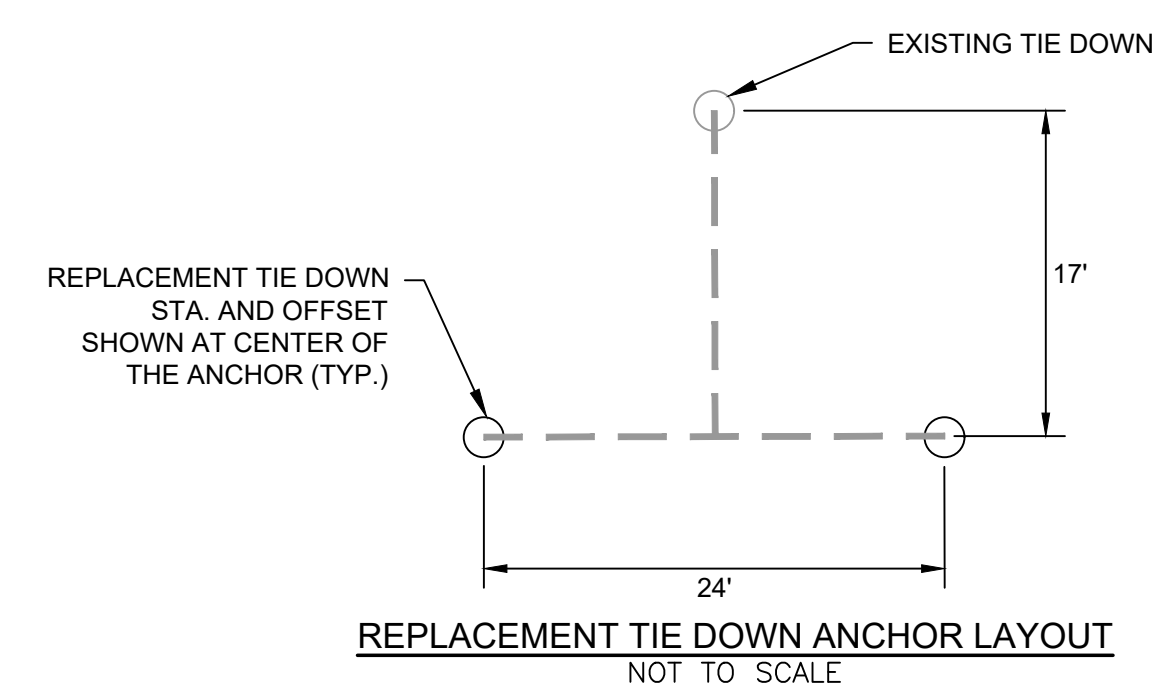
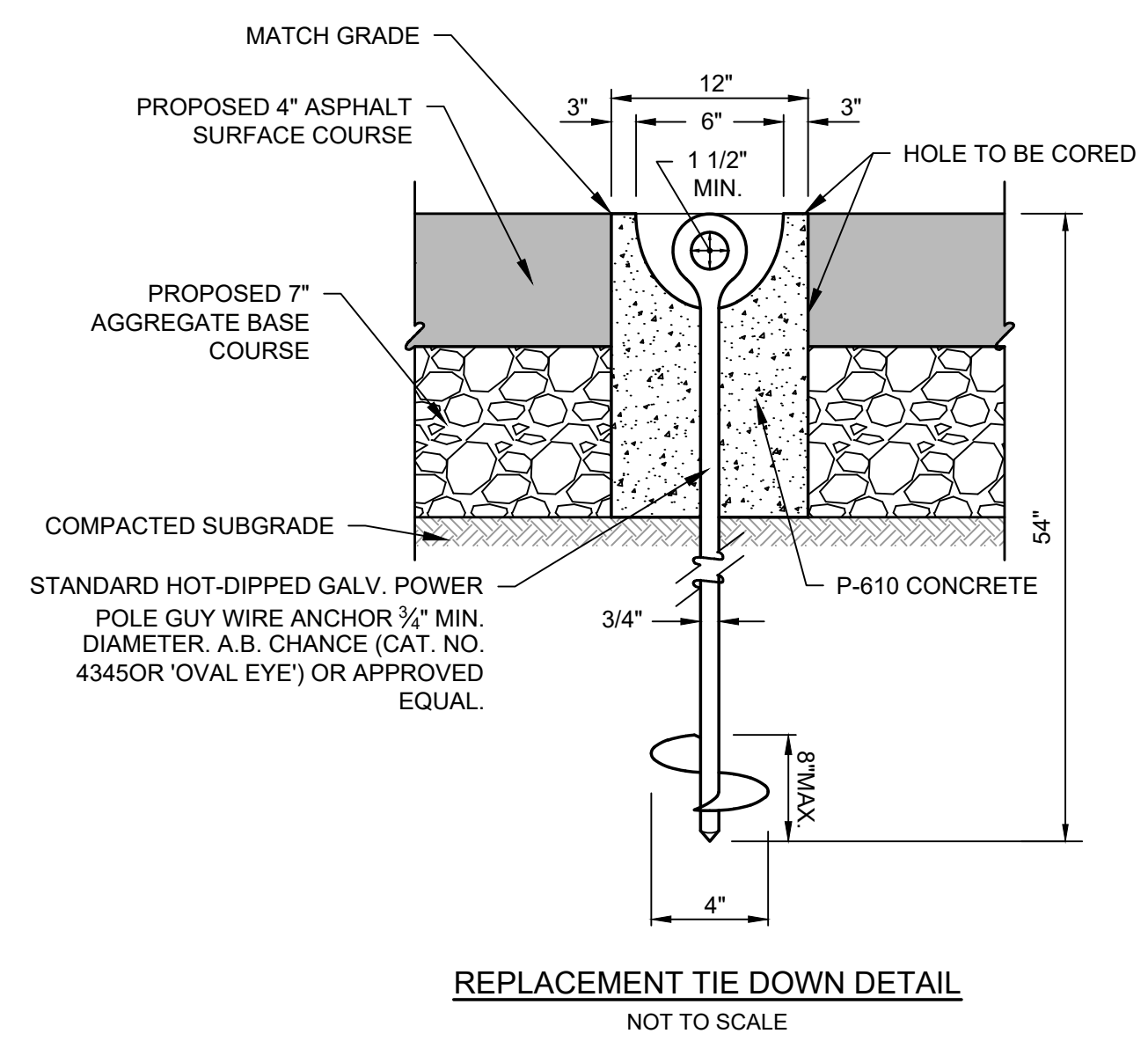


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 Scale: NTS
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 Checked: AMS
 Project No.: 2601-2105
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- GENERAL NOTES:**
- TAXIWAY AND TAXILANE CENTERLINE MARKINGS SHALL BE YELLOW. MARKINGS REQUIRING REFLECTIVE BEADS SHALL HAVE BEADS APPLIED DURING SECOND APPLICATION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT OF MARKINGS. THE RESIDENT PROJECT REPRESENTATIVE SHALL REVIEW LAYOUT PRIOR TO CONTRACTOR BEGINNING PAINTING OPERATIONS.
 - REFLECTIVE BEADS SHALL BE APPLIED ON ALL APPLICATIONS OF FINAL MARKINGS (EXCEPT BLACK AND TIE DOWN POSITION MARKINGS).
 - ANY EXISTING MARKINGS TO REMAIN THAT ARE DAMAGED BY CONTRACTOR'S OPERATIONS/TACK/TRAFFIC SHALL BE REPAIRED AT NO COST TO OWNER.
 - WHERE TYING NEW MARKING TO EXISTING MARKING, CONTRACTOR SHALL VERIFY LAYOUT OF EXISTING MARKING.
 - MARKING WILL BE APPLIED IN TWO APPLICATIONS. THE FIRST APPLICATION SHALL BE AT THE RATE OF 230 SF/GAL. AFTER A 30 DAY CURING PERIOD A SECOND APPLICATION AT THE RATE OF 115 SF/GAL OF MARKING WITH REFLECTIVE BEADS (AS REQUIRED) SHALL BE APPLIED.
 - EXISTING CRACKS 1/4" OR LARGER SHALL BE CLEANED AND SEALED PRIOR TO APPLICATION OF SEAL COAT. SEE SECTION P-101 OF THE SPECIFICATIONS.

LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT - 1
	PROPOSED APRON ASPHALT PAVEMENT - 2
	PROPOSED SLURRY COAT LIMITS
	EXISTING TIE DOWN
	REPLACEMENT TIE DOWN

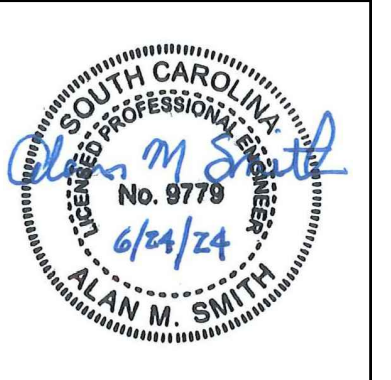


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
4810 SHELLEY DRIVE
SC LICENSE NO. C00296
EMAIL: TBILL@TBILL.COM

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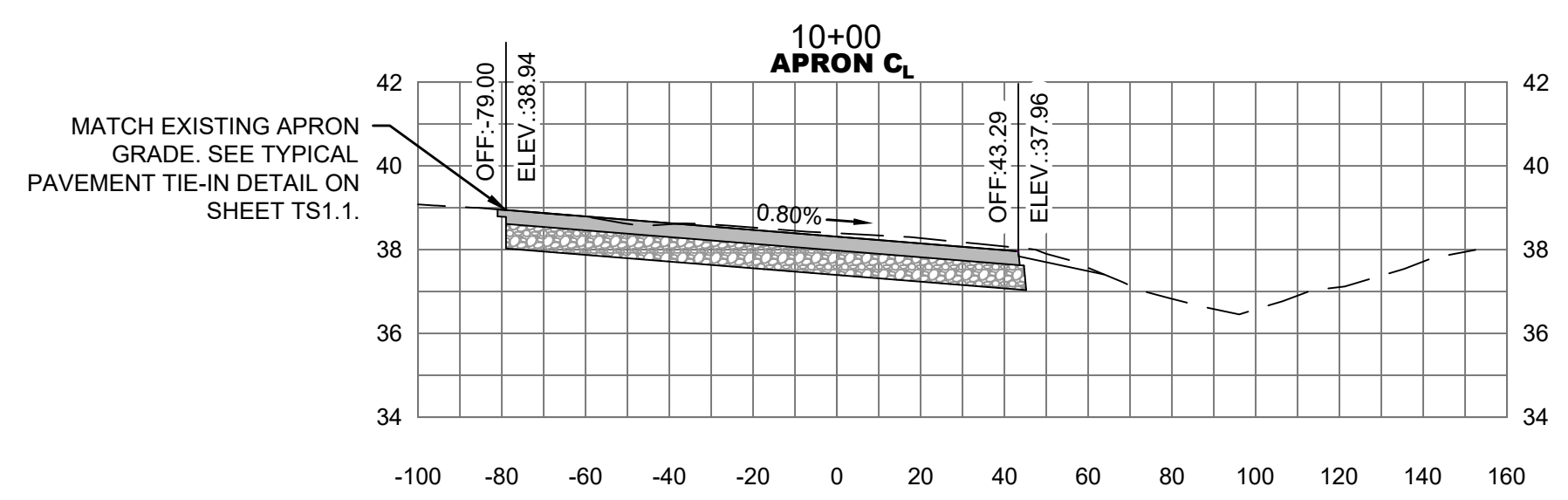
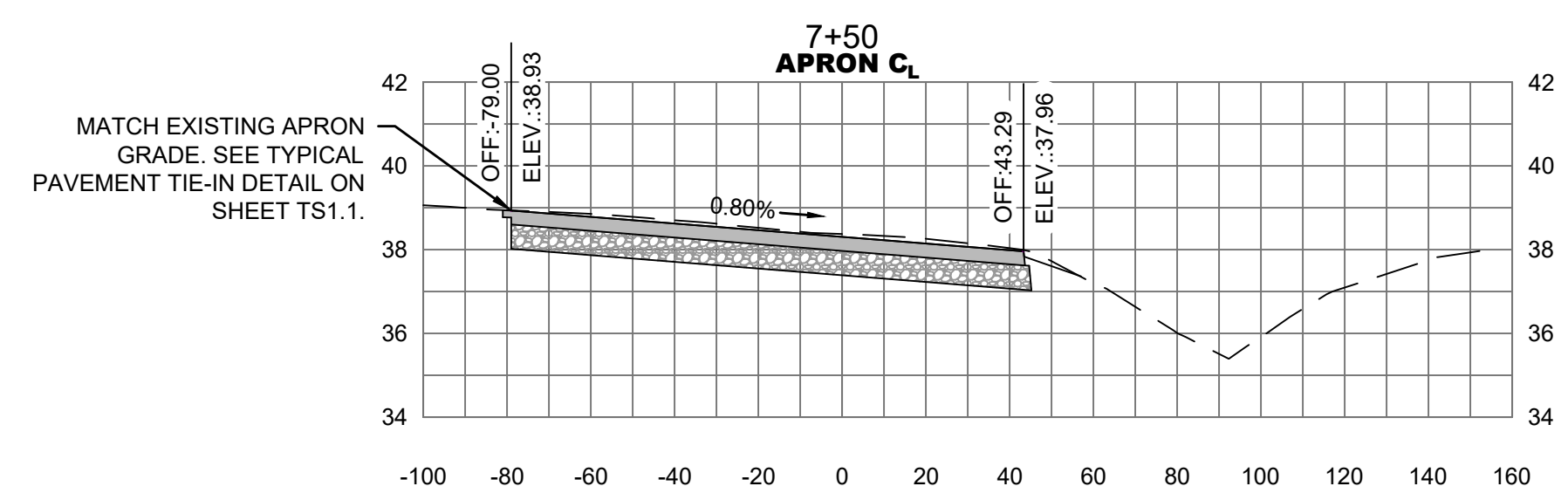
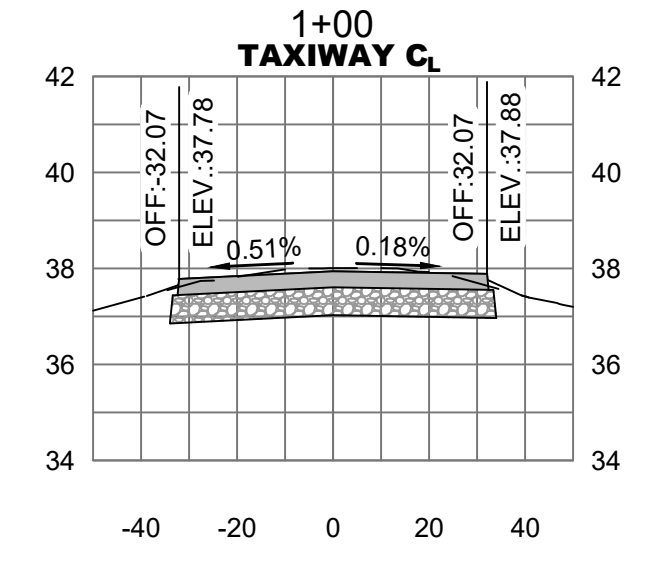
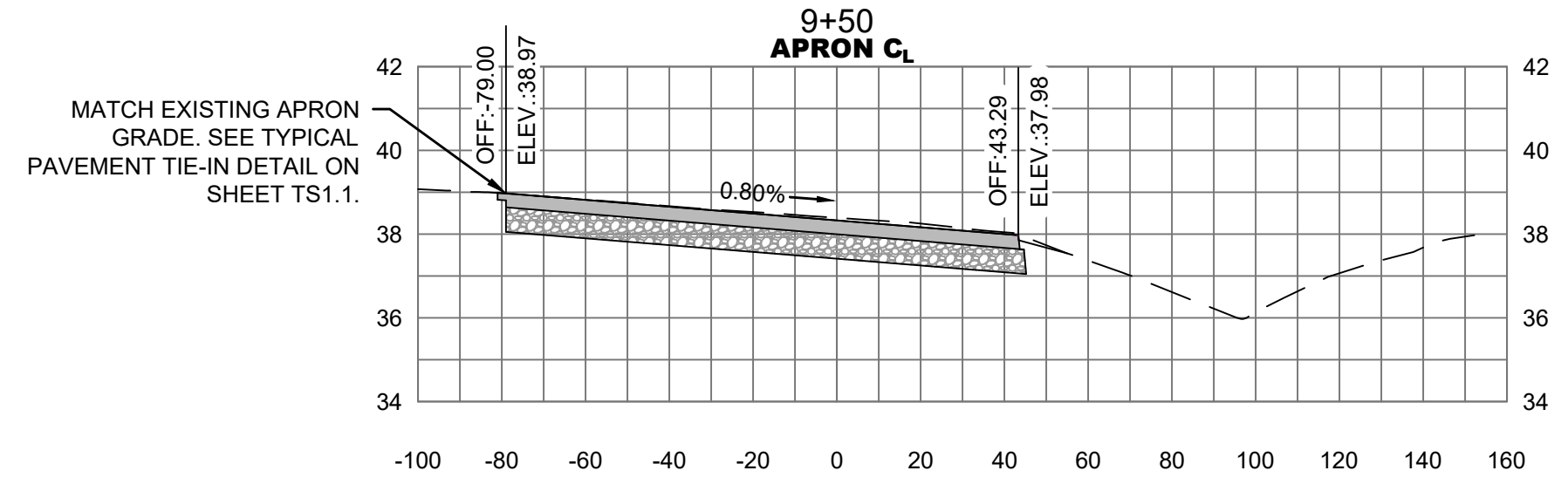
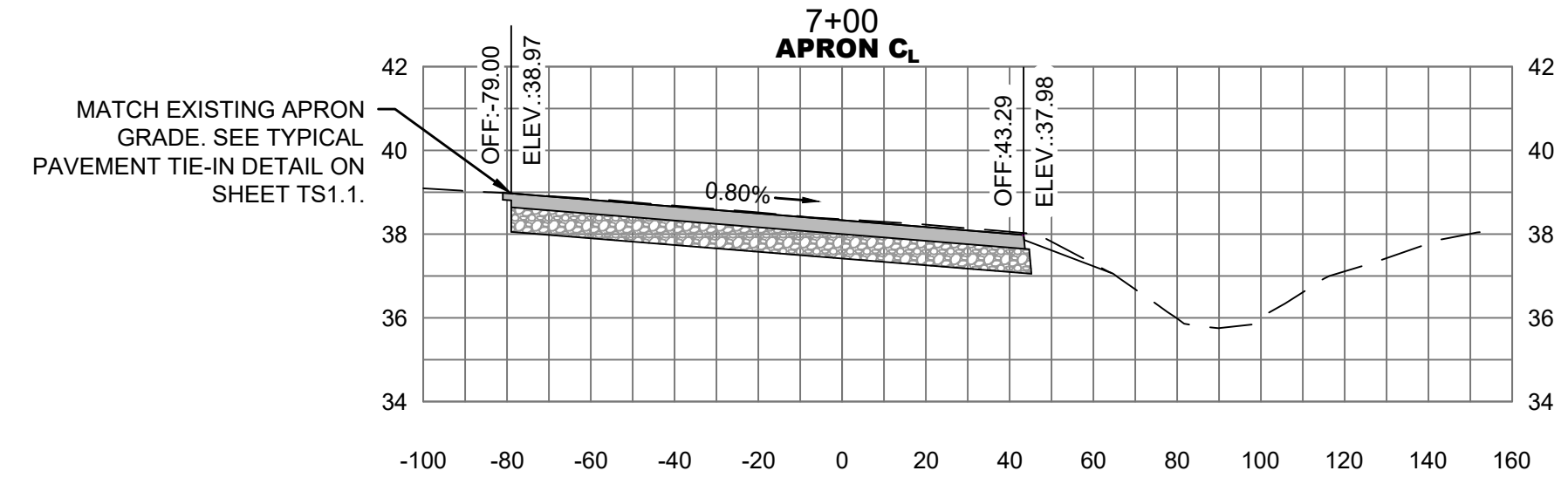
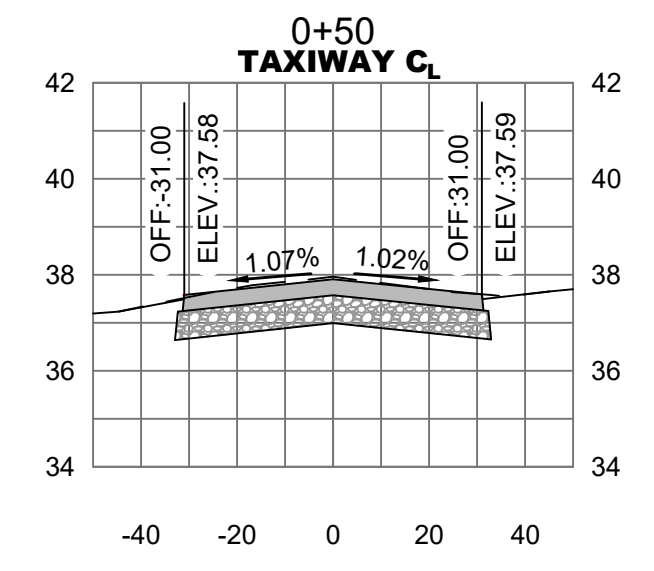
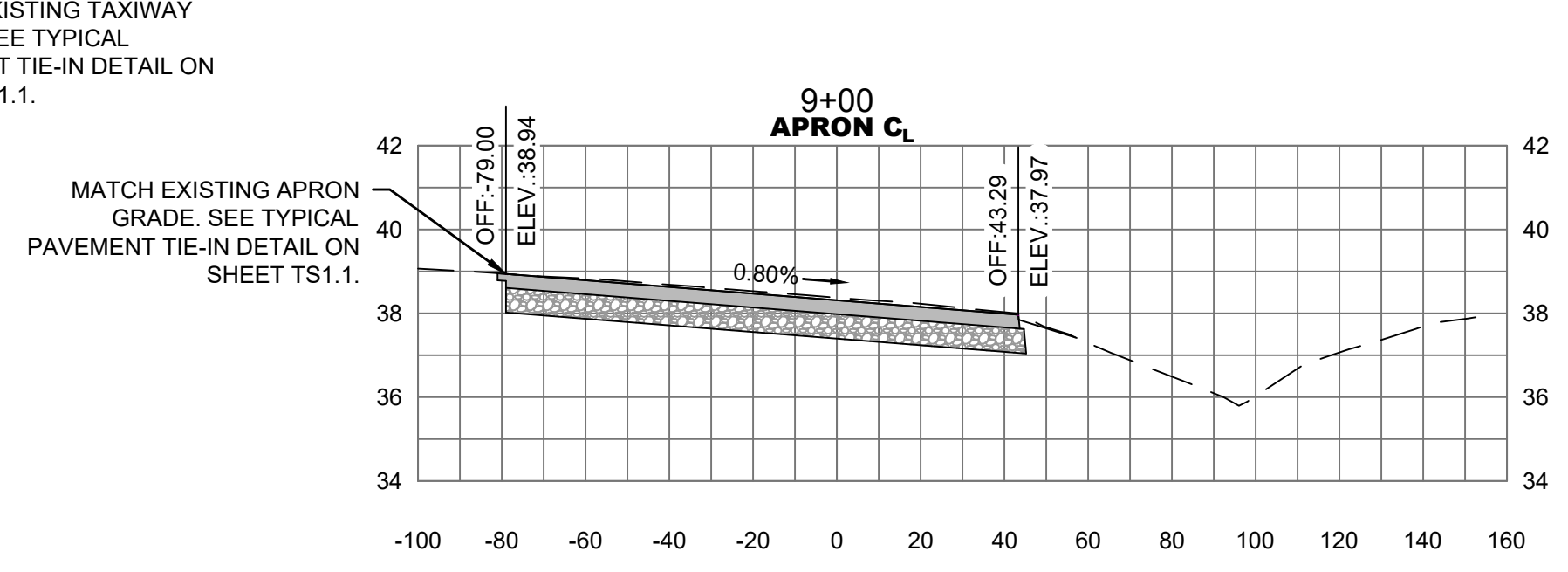
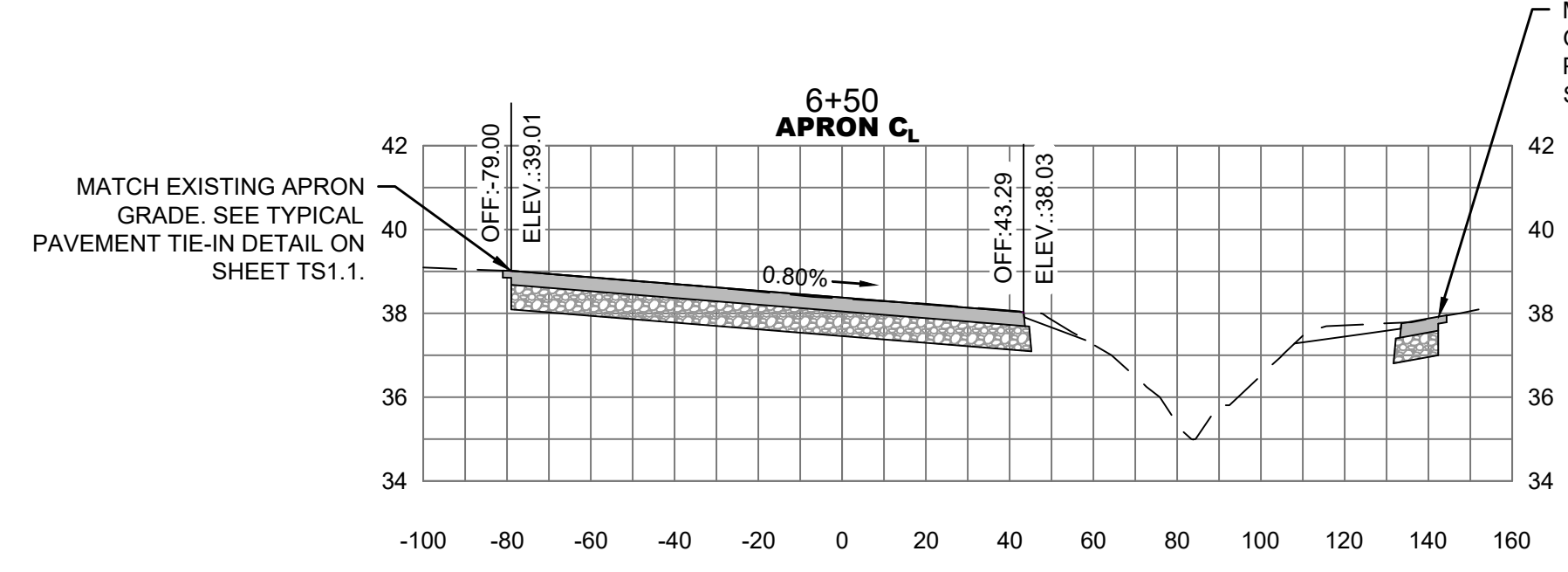
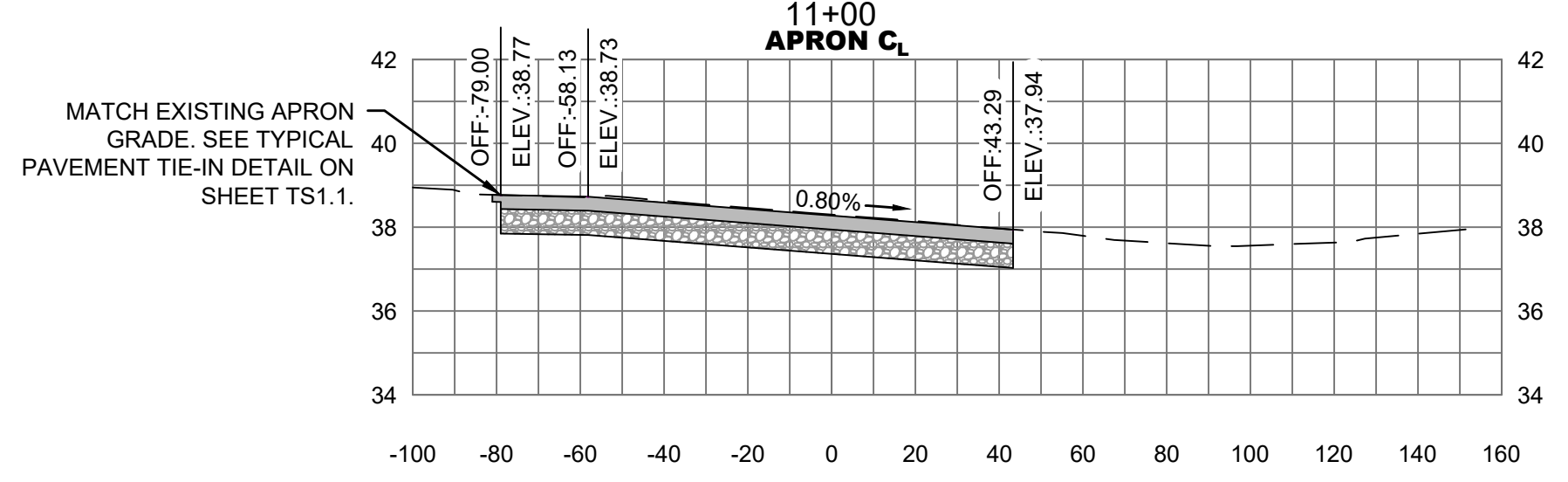
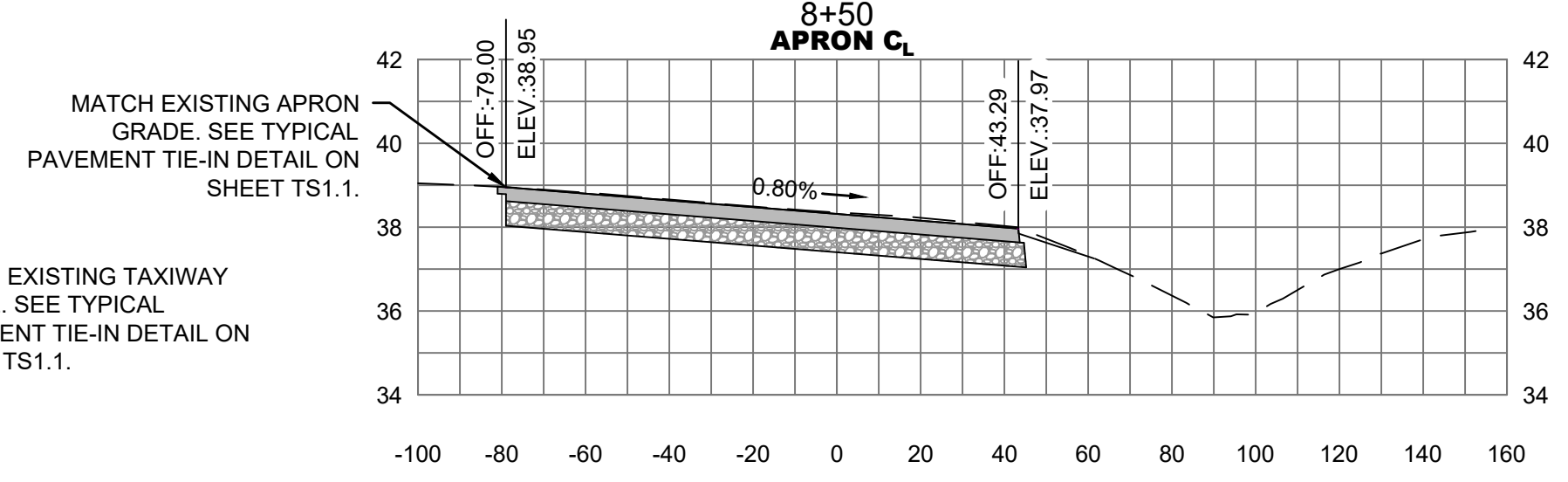
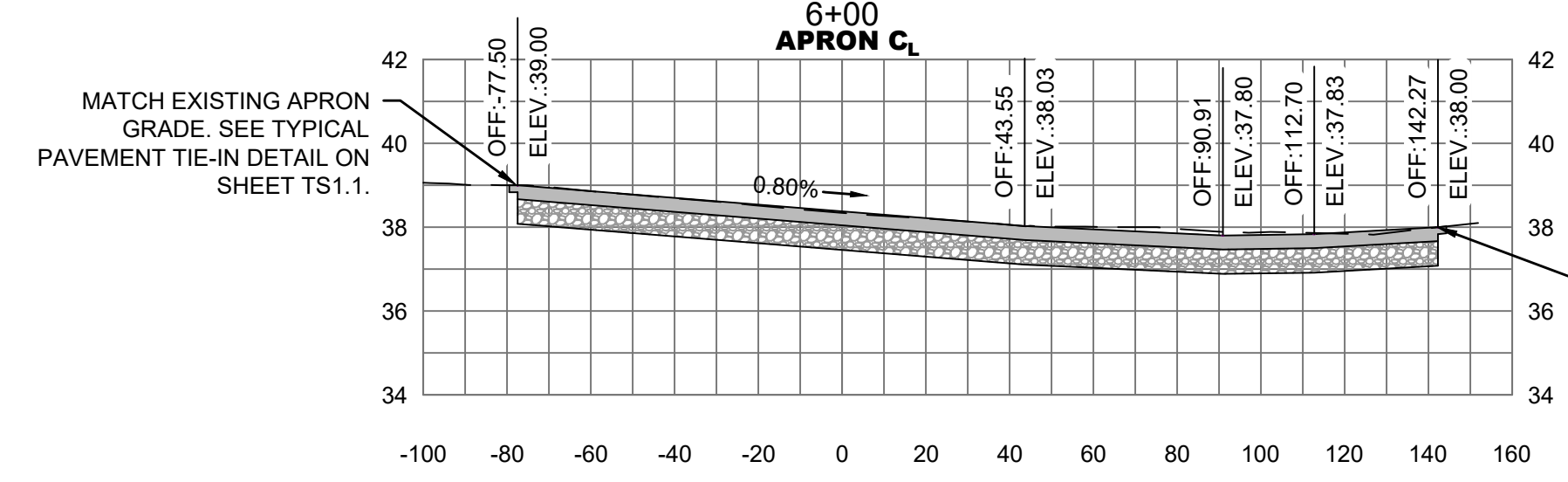
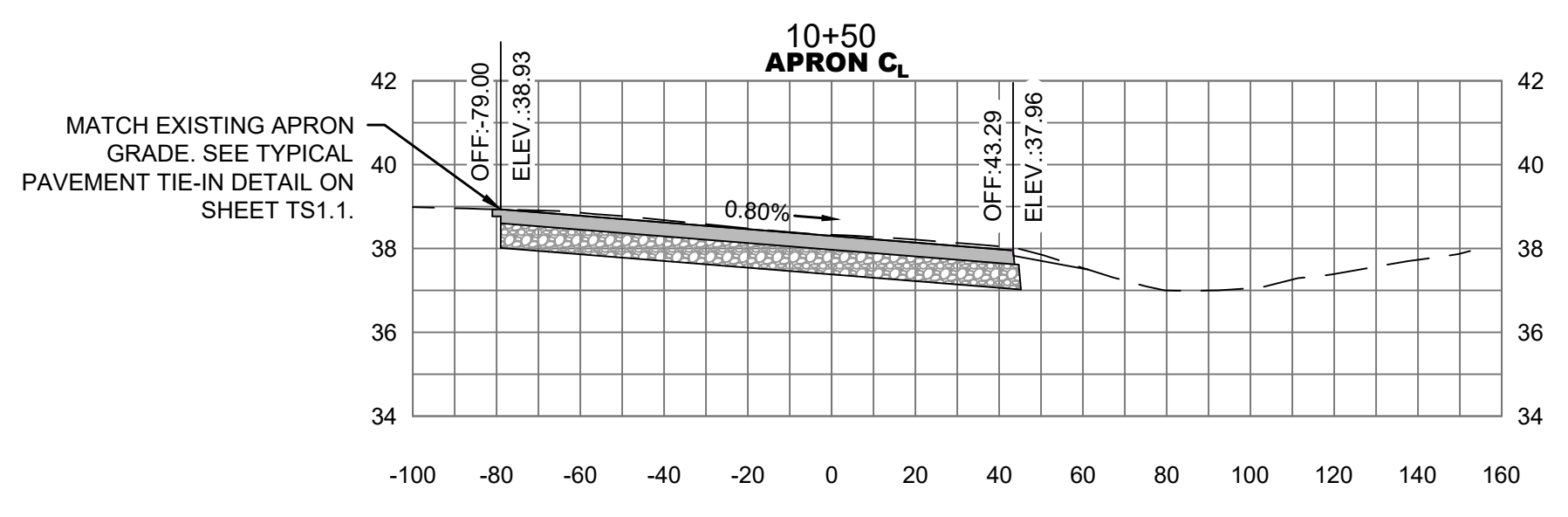
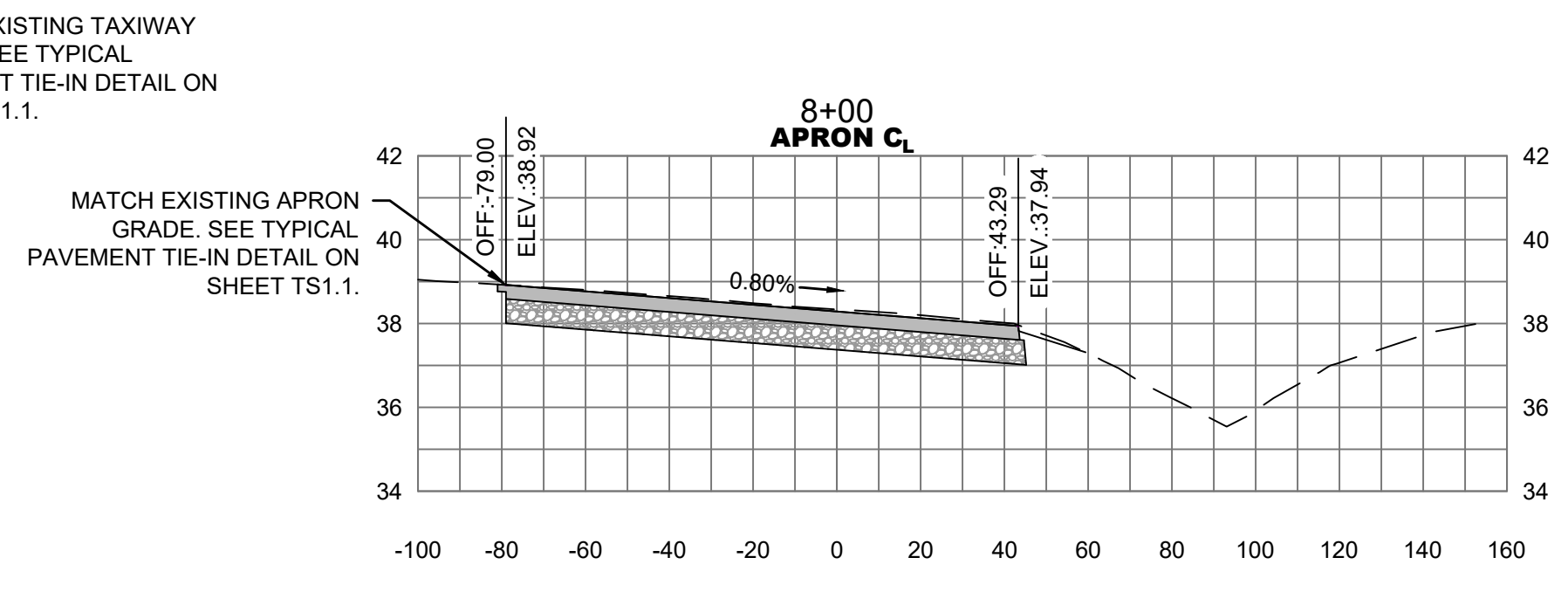
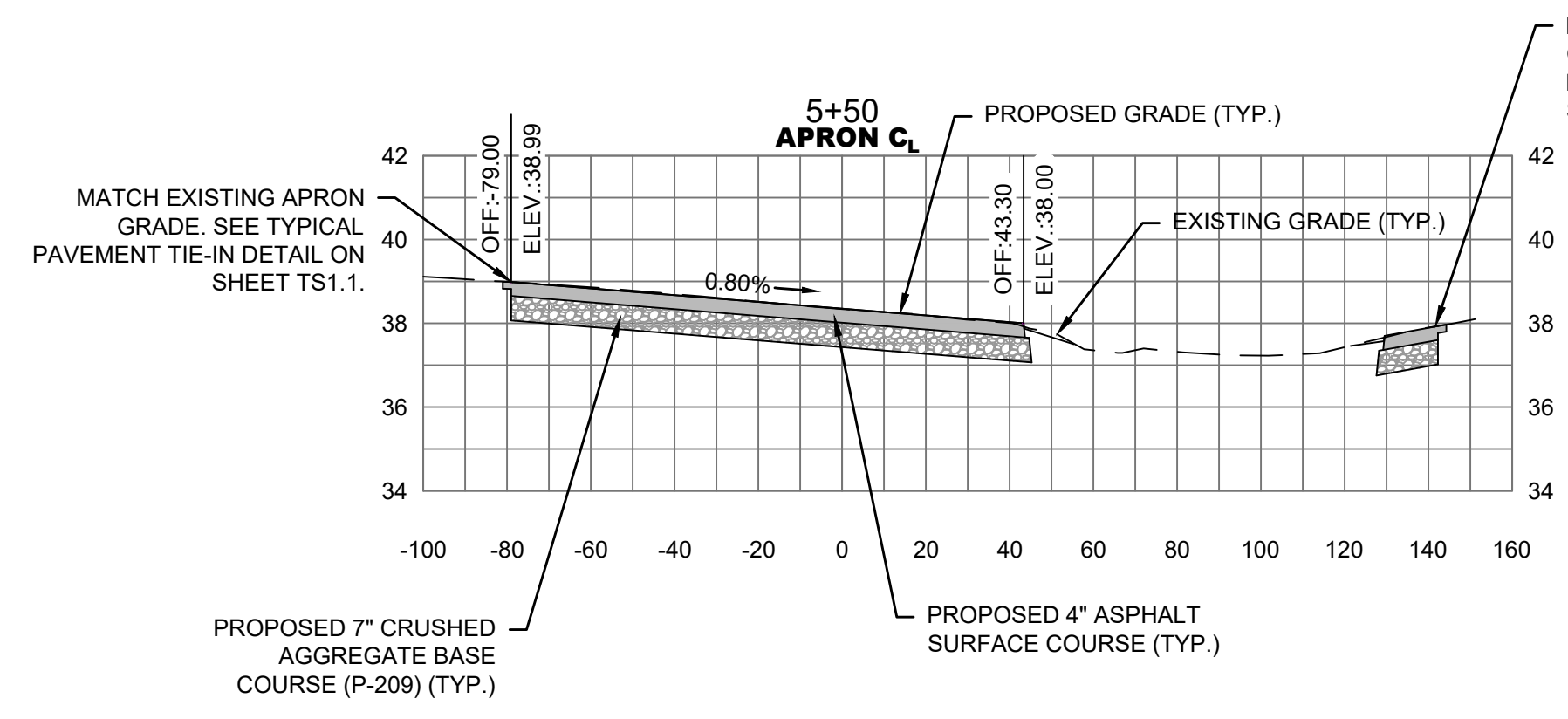
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
PAVEMENT MARKING AND TIE-DOWN PLAN



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Project No.: 2601-2105
Sheet No.:

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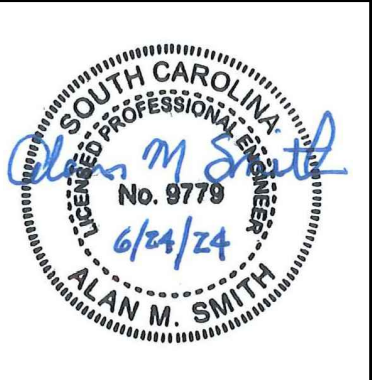
NOTE: FOR BASELINE INFORMATION SEE SHEET SL1.1.

LEGEND	
	PROPOSED 4" ASPHALT SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE

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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE I)
CROSS SECTIONS



Date	JUNE 2024
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Drawn	NT
Checked	AMS
Project No.	2601-2105
Sheet No.	

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CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR, COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

CONTRACTOR'S STAGING AREA. INSTALL SILT FENCE ALONG OUTSIDE EDGE OF STAGING AREA.
CONCRETE WASHOUT AREA. SEE DETAIL ON SHEET EC2.2.

CONTRACTOR'S ACCESS ROUTE ALONG EXISTING ROAD (SEE NOTE B)

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RPR. SEE NOTE 4 OF GENERAL NOTES.

NOTE B - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS 'CONSTRUCTION ENTRANCE/EXIT' TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

WORK AREA 1. SEE SHEET A2.3.

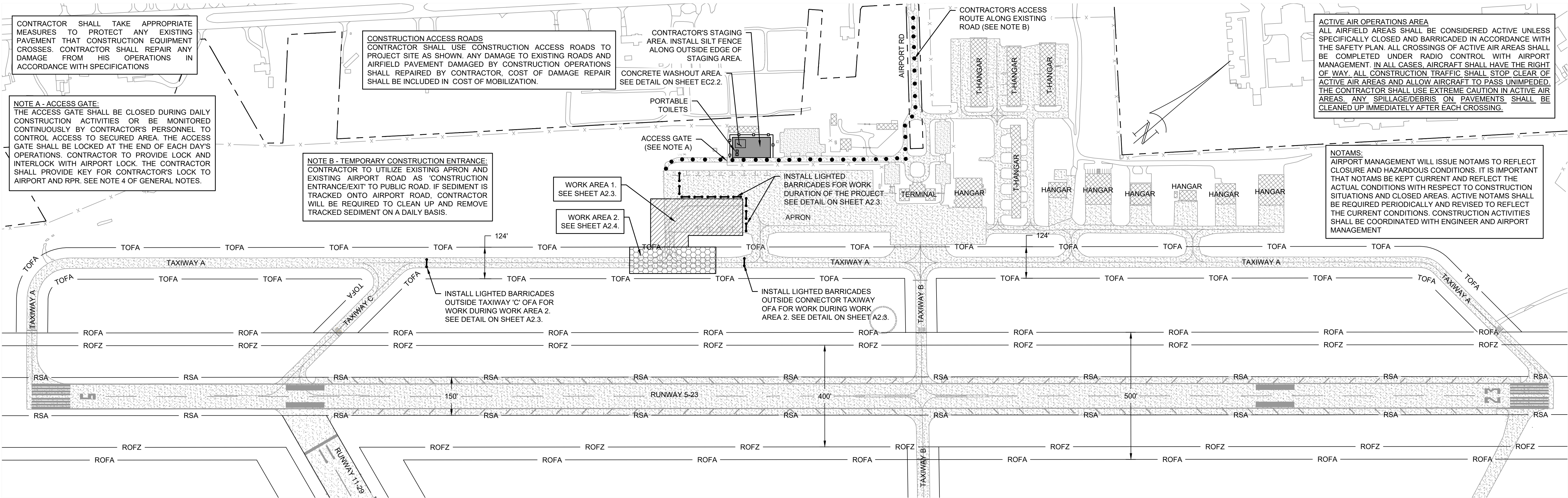
WORK AREA 2. SEE SHEET A2.4.

INSTALL LIGHTED BARRICADES FOR WORK DURATION OF THE PROJECT. SEE DETAIL ON SHEET A2.3.

INSTALL LIGHTED BARRICADES OUTSIDE TAXIWAY 'C' OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A2.3.

INSTALL LIGHTED BARRICADES OUTSIDE CONNECTOR TAXIWAY OFA FOR WORK DURING WORK AREA 2. SEE DETAIL ON SHEET A2.3.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT



SAFETY PLAN REQUIREMENTS

THE INTENT OF THIS PLAN IS TO ESTABLISH CERTAIN SAFETY REQUIREMENTS THAT MUST BE ADHERED TO BY THE CONTRACTOR DURING CONSTRUCTION OF THIS PROJECT. PORTION OF AIRCRAFT PARKING APRON WILL BE CLOSED TO AIR TRAFFIC AT ALL TIMES DURING THIS PROJECT. PARTIAL CLOSURE OF TAXIWAY 'A' WILL BE REQUIRED FOR WORK WITHIN TAXIWAY 'A' OBJECT FREE AREA. SEE PROJECT SPECIAL PROVISIONS FOR CLOSURE REQUIREMENTS AND DETAILS. THE AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. SEE SEQUENCE OF CONSTRUCTION ON PHASING PLANS.

- THE PROJECT AREA IS LOCATED WITHIN AND ADJACENT TO THE AIRCRAFT OPERATIONS AREA (AOA). THIS IS A CLOSELY MAINTAINED SECURITY AREA WITH RESTRICTED ACCESS. THE CONTRACTOR WILL BE REQUIRED TO MEET ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THIS AREA AT ALL TIMES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THE AOA. FURTHER, IT WILL REMAIN THE CONTRACTOR'S RESPONSIBILITY TO KEEP HIMSELF ADVISED OF ANY CHANGES IN REQUIREMENTS, TO ADHERE TO CURRENT REGULATIONS. THE CONTRACTOR SHALL ALSO HAVE PERSONNEL TRAINED TO OPERATE AND MONITOR AIRPORT SECURITY GATES USED DURING THE PROJECT.
- THE CONTRACTOR SHALL NOT BEGIN WORK UNLESS AND UNTIL 72 HOURS PRIOR NOTICE HAS BEEN GIVEN TO THE ENGINEER AND AIRPORT MANAGEMENT. CROSSING OF RUNWAYS OR TAXIWAYS IS ALLOWED ONLY IF THE RUNWAY OR TAXIWAY IS CLOSED AND SHOWN ON THIS PLAN. CONTRACTOR IS PROHIBITED FROM ENTERING THE RUNWAY 5-23 OBSTACLE FREE ZONE AT ANY TIME. SEE GENERAL NOTE 1.
- IN AN EMERGENCY SITUATION, THE CONTRACTOR SHALL CALL 911 AND NOTIFY THE AIRPORT MANAGEMENT IMMEDIATELY. THE AIRPORT CAN BE REACHED BY PHONE AT 843-545-3638.
- SEE GENERAL NOTE 4 FOR AIRPORT ENTRY AND DEPARTURE PROCEDURES AND FOR VEHICLE MARKING REQUIREMENTS.
- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL DESIGNATE A REPRESENTATIVE AND ALTERNATE TO CONTACT ON A 24 HOUR BASIS SHOULD PROBLEMS ARISE. THE CONTRACTOR SHALL PROVIDE A CONTACT LIST FOR ALL SUPERVISORY PERSONNEL AND ALL SUBCONTRACTORS TO THE ENGINEER.
- A DAILY START-UP AND SHUT-DOWN CHECKLIST WILL BE JOINTLY PREPARED BY THE CONTRACTOR AND AIRPORT MANAGEMENT. THE CHECKLIST WILL BE FOLLOWED THROUGHOUT THE PROJECT. THIS CHECKLIST SHALL INCLUDE, BUT NOT BE LIMITED TO BARRICADES, FLAGMEN, HAUL ROUTES, SECURING OF ACCESS GATES, CLEAN UP, ETC. THE CONTRACTOR'S SITE SUPERVISOR AND LABOR CREW SHALL NOT LEAVE THE WORK SITE UNTIL SUCH TIME AS THE AIRPORT HAS INSPECTED THE AREA AND SIGNED OFF ON THE DAILY CHECKLIST.
- UNDERGROUND UTILITIES ARE KNOWN TO BE LOCATED IN THE PROJECT AREAS. EXISTING UNDERGROUND UTILITIES INCLUDING BUT NOT LIMITED TO AIRFIELD LIGHTING, NAVAID POWER AND CONTROL CABLES MAY BE IN THE PATH OF CONSTRUCTION. LOCATIONS OF UTILITIES IF SHOWN ON THE PLANS ARE APPROXIMATE ONLY. ALL UTILITIES AND FACILITIES ARE NOT NECESSARILY INDICATED ON PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES FROM DAMAGE. SEE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGEMENT WHEN WORKING IN AREAS CONTAINING AIRFIELD LIGHTING, NAVAID CABLE, OR OTHER UTILITIES.

CONTACTS ARE:
AIRPORT DIRECTOR MR. JIM TAYLOR 843-545-3638

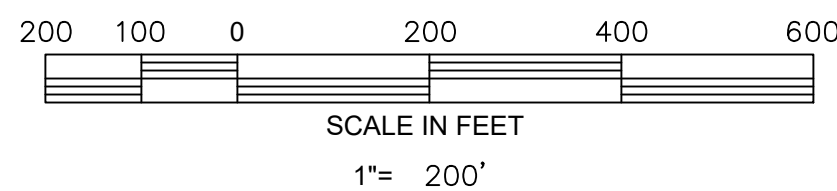
FOR ADDITIONAL REQUIREMENTS RELATED TO PROTECTION OF EXISTING UTILITIES, INCLUDING CABLES, CONTROLS, AND NAVAIDS SEE PROJECT SPECIAL PROVISIONS.

- THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS IN REGARD TO CONSTRUCTION NOISE AND EROSION CONTROL DURING CONSTRUCTION.

- THE CONTRACTOR SHALL CLEAN ALL CONSTRUCTION AREAS OF LITTER, LOOSE PAPERS, DEBRIS, ETC. ON A DAILY BASIS, OR AS DIRECTED BY THE ENGINEER. PRIOR TO THE CLOSE OF DAILY OPERATIONS, CONTRACTOR SHALL INSPECT ALL ACTIVE AIR OPERATIONS AREAS AND CONSTRUCTION AREA FOR FOD AND LITTER. ALL DEBRIS SHALL BE CLEANED UP AND PROPERLY DISPOSED OF PRIOR TO RELEASE OF CREWS FROM EACH SHIFT.
- MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL ARE NOT ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) OF AN OPEN RUNWAY AT ANY TIME UNLESS THE RUNWAY IS CLOSED OR THE TAXIWAY OBJECT FREE AREA (TOFA) UNLESS THE SECTION OF TAXIWAY IS CLOSED (SEE GENERAL NOTE 1). MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL WILL NOT BE PERMITTED CLOSER THAN 200 FEET FROM THE EDGE OF THE RUNWAY OR 50 FEET FROM THE EDGE OF ANY TAXIWAY WITHOUT PRIOR PERMISSION FROM THE AIRPORT MANAGEMENT.
- DURING CONSTRUCTION, ADJACENT TAXIWAYS AND THE RUNWAY WILL BE OPEN TO AIRCRAFT UNLESS OTHERWISE NOTED. AIRCRAFT HAVE THE RIGHT OF WAY AT ALL TIMES. THE CONTRACTOR SHALL BE AWARE OF THE AIRCRAFT MOVEMENTS AND THE JETBLAST AND/OR PROP-WASH ASSOCIATED WITH THESE AIRCRAFT. THE CONTRACTOR SHALL SECURE LOOSE ITEMS AT ALL TIMES AND SHALL LOCATE STOCKPILES OF MATERIALS OR EQUIPMENT AWAY FROM AIRCRAFT OPERATION AREAS.
- INSPECTION - FREQUENT INSPECTIONS WILL BE MADE BY AIRPORT MANAGEMENT DURING CRITICAL PHASES OF THE WORK TO ENSURE THAT THE CONTRACTOR IS FOLLOWING THE RECOMMENDED AIRFIELD SAFETY PROCEDURES.
- EXCAVATIONS - CONTRACTOR MUST PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE AIRPORT, AND BACKFILL OR LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN 250 FEET OF THE RUNWAY CENTERLINE WHILE THE RUNWAY IS OPEN. IF THE RUNWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, COVER OR BACKFILL THE EXCAVATIONS APPROPRIATELY. COVERINGS FOR OPEN TRENCHES OR EXCAVATIONS MUST BE OF SUFFICIENT STRENGTH TO SUPPORT THE WEIGHT OF THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY.
- ALL AIRFIELD LIGHTING AND LIGHTED SIGNS OUTSIDE AREAS CLOSED FOR CONSTRUCTION SHALL BE KEPT OPERATIONAL THROUGHOUT THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL DISABLE TAXIWAY LIGHTING IN CLOSED SECTIONS OF TAXIWAYS BY INSTALLING TEMPORARY JUMPER CABLE OR COVERING LIGHTS. ALL GUIDANCE SIGNS WITHIN CLOSED AREAS AND LEADING TO CLOSED AREAS SHALL BE COVERED.
- NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA OF AN ACTIVE RUNWAY OR TAXIWAY OBJECT FREE AREA OF AN ACTIVE TAXIWAY. ANY WORK WITHIN AN ACTIVE RUNWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE RUNWAY. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN A TAXIWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE EFFECTED SECTION OF TAXIWAY. PULLBACKS FOR MEN AND EQUIPMENT WITHIN THE RUNWAY OBSTACLE FREE ZONE WILL NOT BE ALLOWED (SEE GENERAL NOTE 1).

NOTAMS (NOTICE TO AIRMEN)

THE AIRPORT MANAGEMENT WILL ISSUE THE NECESSARY NOTAMS TO REFLECT HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS. ACTIVE NOTAMS SHALL BE REVIEWED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS.



GENERAL NOTES:

- IT IS THE INTENT OF THE OWNER THAT THE GEORGETOWN COUNTY AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. RUNWAY 5-23 AND RUNWAY 11-29 SHALL NOT BE CLOSED. PARTIAL CLOSURE OF TAXIWAY 'A' WILL BE REQUIRED FOR THE CONSTRUCTION OF PROJECT. THE CONTRACTOR SHALL PROVIDE A MINIMUM 72 HOURS NOTICE TO AIRPORT MANAGEMENT AND ENGINEER PRIOR TO ANY PROPOSED CLOSURE DATES. PRIOR TO REOPENING ANY PAVEMENT, THE CONTRACTOR MUST REMOVE BARRICADES AND PERFORM A WALK THROUGH OF THE CONSTRUCTION AREA WITH AIRPORT MANAGEMENT, THE RESIDENT PROJECT REPRESENTATIVE, OR ENGINEER TO CONFIRM THAT THE PAVEMENT AND SAFETY AREAS ARE FREE OF FOD OR OTHER HAZARDS.
- PRIOR TO LEAVING WORK EACH DAY, CONTRACTOR SHALL RETURN HIS EQUIPMENT AND MATERIALS TO THE STAGING AREA IDENTIFIED ON THE PLANS.
- ALL CONTRACTOR PERSONNEL, INCLUDING BUT NOT LIMITED TO, GENERAL LABORERS, SUBCONTRACTORS, DRIVERS, AND JOURNEYMEN WORKING WITHIN ACTIVE AIR OPERATIONS AREAS MUST AT ALL TIMES REMAIN WITHIN VISUAL AND VOICE RANGE OF CONTRACTOR SUPERVISORY PERSONNEL. FOR THE PURPOSES OF THIS PROJECT, THE AIR OPERATIONS AREA (AOA) REFERS TO ALL AREAS WITHIN THE AIRPORT SECURITY FENCE.
- PRIOR TO ENTERING THE SECURED AOA OF THE AIRPORT EACH DAY, THE CONTRACTOR SHALL CHECK IN WITH THE AIRPORT. CLOSE COORDINATION FOR ACCESS TO WORK AREAS AND SCHEDULES BETWEEN THE CONTRACTOR, AND AIRPORT WILL BE REQUIRED THROUGHOUT THE PROJECT.

THE CONTRACTOR SHALL COORDINATE INGRESS-EGRESS REQUIREMENTS WITH THE AIRPORT MANAGEMENT AND RESIDENT PROJECT REPRESENTATIVE (RPR). ALL OPEN GATES TO SECURED AIRPORT AREAS SHALL BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA OR SHALL BE CLOSED AND LOCKED. CONTRACTOR PERSONNEL SHALL NOT ALLOW ANY UNAUTHORIZED PERSONNEL TO ENTER THROUGH THE CONSTRUCTION GATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING AND LOCKING ALL GATES WHEN NOT IN USE AND AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR SHALL PROVIDE INTERLOCK AT PADLOCKED GATES. CONTRACTOR SHALL PROVIDE A COPY OF ALL GATE KEYS TO THE AIRPORT AND RPR. CONTRACTOR SHALL PROVIDE A LIST OF ALL KEY HOLDERS WHICH SHALL BE KEPT UPDATED THROUGHOUT THE PROJECT.

ACCESS BY PRIVATE VEHICLES TO THE APRON SHALL BE COORDINATED THROUGH AND ESCORTED BY REPRESENTATIVES OF THE FBO. ACCESS TO THE APRON SHALL BE THROUGH GATE LOCATED ADJACENT TO TERMINAL BUILDING.

ALL CONSTRUCTION VEHICLES MUST BE CLEARED FOR ACCESS BY THE AIRPORT MANAGEMENT AND RESIDENT PROJECT REPRESENTATIVE. PERSONAL CARS SHALL BE PARKED IN STAGING AREA. ALL VEHICLES OPERATING IN THE AOA SHALL BE LIGHTED OR FLAGGED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G. COPIES OF THE ADVISORY CIRCULAR WILL BE MADE AVAILABLE UPON REQUEST.

- THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN SUCH A MANNER AS TO ASSURE THAT SUCH OPERATIONS DO NOT IMPEDE ACCESS TO ANY AREA OF THE AIRFIELD AT ANY TIME FOR THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) VEHICLES AND OTHER EMERGENCY VEHICLES. EMERGENCY VEHICLE ACCESS SHALL BE A STANDING AGENDA ITEM FOR ALL PROGRESS MEETINGS. THE CONTRACTOR SHALL COOPERATE FULLY AND IMMEDIATELY WITH ANY DIRECTIVES ISSUED BY AIRPORT MANAGEMENT RELATIVE TO EMERGENCY ACCESS.

- ACCESS ROADS TO BE USED UNDER THIS CONTRACT SHALL BE THOSE DESIGNATED AND APPROVED BY THE ENGINEER. IN GENERAL, THE CONTRACTOR SHALL CONFINE HIS EQUIPMENT AND HAULING WHERE PRACTICAL TO EXISTING ROADS ON THE AIRPORT. IF EXISTING PAVEMENT OR ROAD SURFACE IS DAMAGED BY THE CONTRACTOR'S HAULING OPERATIONS, IT SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. HAUL ROADS ACROSS TURFED AREAS SHALL BE REPAIRED, SCARIFIED, SEEDED, MULCHED, AND FERTILIZED AT THE CONTRACTOR'S EXPENSE. METAL TRACK VEHICLES WILL NOT BE PERMITTED TO OPERATE ON OR ACROSS EXISTING PAVEMENT WITHOUT PROTECTIVE MATTING TO PREVENT MARRING OF THE PAVEMENT SURFACE. ACCESS ROADS SHALL BE CONSTRUCTED BY CONTRACTOR AS REQUIRED. ALL COSTS ASSOCIATED WITH SUPPLYING, CONSTRUCTING, MAINTAINING AND RESTORING TEMPORARY HAUL AND ACCESS ROADS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR "TEMPORARY CONSTRUCTION ITEMS"

- ALL EXISTING FACILITIES WILL BE CAREFULLY PROTECTED BY THE CONTRACTOR. ANY FACILITIES DAMAGED BY THE CONTRACTOR WILL BE REPAIRED IMMEDIATELY AND RESTORED TO ORIGINAL CONDITION AT CONTRACTOR'S COST.
- CONTRACTOR WILL, BY WATERING, CHEMICALS, VEGETATION, OR OTHER MEANS, PREVENT THE OCCURRENCE OF DUST WHICH WILL BE OBJECTIONABLE TO THE RESIDENTS OF THE AREA, VIOLATE EXISTING LAWS OR REGULATION, OR CAUSE HAZARDS TO AIR TRAFFIC.
- CONTRACTOR MAY ENCOUNTER WET CONDITIONS DURING CONSTRUCTION. ALL COST FOR DEWATERING IS CONSIDERED INCIDENTAL TO COST OF ITEMS OF WORK BID UPON.
- SEE PROJECT SPECIAL PROVISIONS FOR PROTECTION OF UTILITIES.

CONSTRUCTION CONTRACTOR'S RESPONSIBILITIES

- CONTRACTOR SHALL HAVE AVAILABLE A COPY OF THE CONSTRUCTION SAFETY AND PHASING PLAN (GSP) AND PROJECT AIRPORT SAFETY PLAN ON SITE AT ALL TIMES. SEE APPENDIX 'G' OF THE SPECIFICATIONS.
- CONTRACTOR SHALL COMPLY WITH THE AIRPORT SAFETY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT.
- CONTRACTOR SHALL PROVIDE A POINT OF CONTACT WHO WILL COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION-RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT.
- CONTRACTOR SHALL PROVIDE A SAFETY/CONSTRUCTION INSPECTOR FAMILIAR WITH AIRPORT SAFETY TO MONITOR CONSTRUCTION ACTIVITIES AT ALL TIMES DURING CONSTRUCTION.
- CONTRACTOR SHALL RESTRICT MOVEMENT OF CONSTRUCTION VEHICLES TO CONSTRUCTION AREAS BY FLAGGING AND BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING FLAGMEN OR ESCORTS AS APPROPRIATE.
- CONTRACTOR SHALL ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE ACTIVE AIR OPERATIONS AREAS (AOA) FROM THE CONSTRUCTION SITE UNLESS AUTHORIZED.

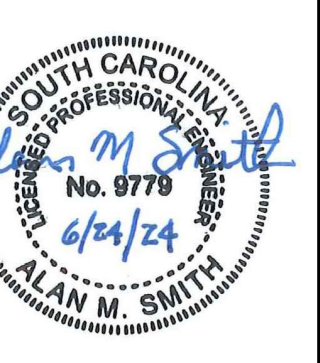
LEGEND	
---	APPROXIMATE AIRPORT PROPERTY LINE
— RSA —	RUNWAY SAFETY AREA
— ROFA —	RUNWAY OBJECT FREE AREA
— ROFZ —	RUNWAY OBSTACLE FREE ZONE
— TOFA —	TAXIWAY OBJECT AREA
[Hatched Box]	WORK AREA 1
[Dotted Box]	WORK AREA 2
[Dashed Line]	CONTRACTOR'S ACCESS ROUTE
[Solid Grey Box]	CONTRACTOR'S STAGING/STOCKPILE AREA
[Dashed Line]	LIGHTED BARRICADES

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350

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REV. NO.	DESCRIPTION	DATE
	REVISIONS	

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
PROJECT LAYOUT AND SAFETY PLAN



Date	JUNE 2024
Scale	1" = 200'
Drawn	NT
Checked	AMS
Project No.	2601-2105
Sheet No.	

A2.2

CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR, COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

CONTRACTOR'S STAGING AREA. INSTALL SILT FENCE ALONG OUTSIDE EDGE OF STAGING AREA.
CONCRETE WASHOUT AREA. SEE DETAIL ON SHEET EC2.2.

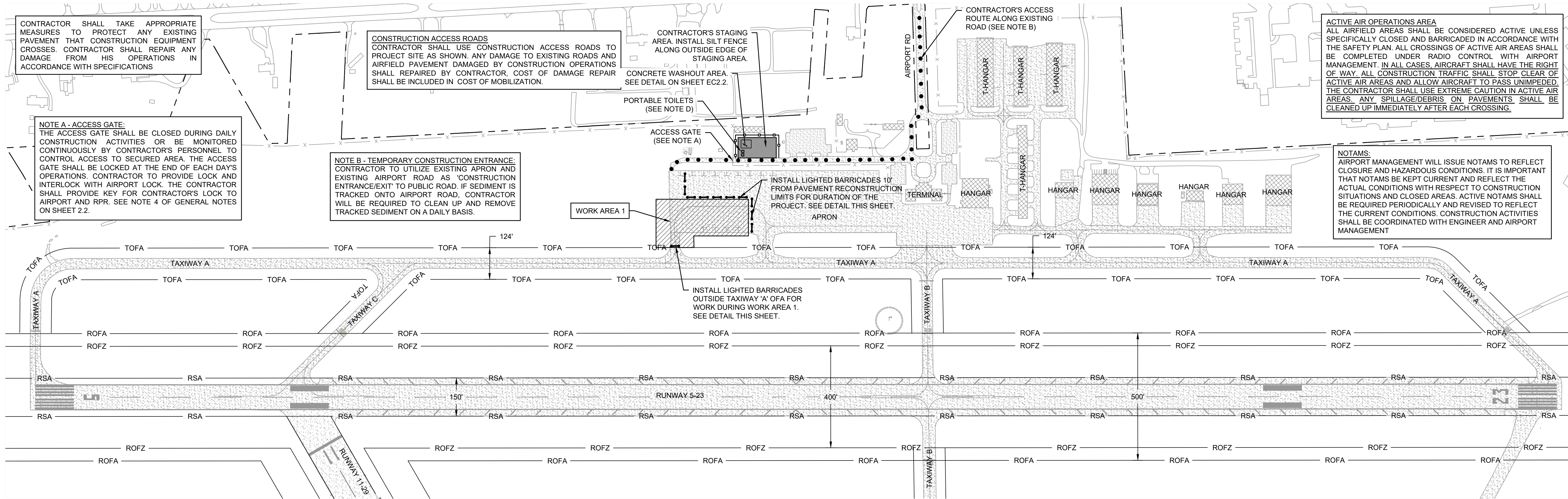
CONTRACTOR'S ACCESS ROUTE ALONG EXISTING ROAD (SEE NOTE B)

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RPR. SEE NOTE 4 OF GENERAL NOTES ON SHEET 2.2.

NOTE B - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS 'CONSTRUCTION ENTRANCE/EXIT' TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT



THE SEQUENCE OF CONSTRUCTION FOR THIS PROJECT WILL FOLLOW TYPICAL PATTERN FOR PROJECTS OF THIS TYPE. INCLUDING ESTABLISHMENT OF STAGING AREA AND STOCKPILE AREA. INSTALLATION OF APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES. PAVEMENT REMOVAL, GRADING, PAVING, MARKING, LIGHTING, AND SEEDING AND MULCHING.

THE FOLLOW SEQUENCE OF CONSTRUCTION HAS BEEN DEVELOPED TO HELP THE CONTRACTOR UNDERSTAND THE OPERATIONAL NEEDS OF THE AIRPORT AND HELP ENSURE MINIMAL CLOSURE TIME. IN ACCORDANCE WITH THE SPECIFICATIONS, THE CONTRACTOR SHALL PROVIDE A DETAILED SCHEDULE OF CONSTRUCTION TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.

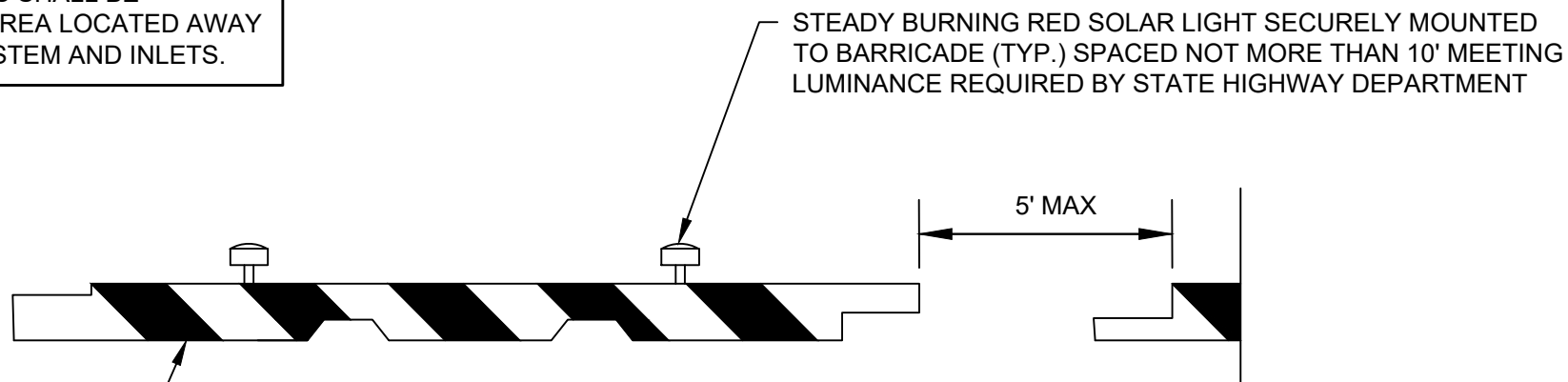
PRIOR TO BEGINNING PROJECT AND CONTRACT START TIME

1. DEVELOP DETAILED SCHEDULE TO ENSURE CONSTRUCTION CAN BE COMPLETED FOR ALL WORK WITHIN CONSTRUCTION TIME ALLOTTED FOR THE PROJECT. SCHEDULE I SHALL BE COMPLETED PRIOR TO THE START OF SCHEDULE II.
2. MOBILIZE EQUIPMENT AND SET UP STAGING AND STOCKPILE AREA FOR THE PROJECT. LOCATION OF STAGING AND STOCKPILE AREA IS SHOWN ON THE SAFETY AND PHASING PLANS SHALL BE COORDINATED WITH AIRPORT MANAGEMENT. INSTALL TEMPORARY SILT FENCE IN STAGING AREA AS SHOWN.
3. CONTRACTOR MAY COMPLETE REQUIRED SURVEY WORK ON PROJECT AREA PRIOR TO CONSTRUCTION START TIME. SCHEDULING FOR THE SURVEY WORK WILL BE REQUIRED TO BE APPROVED BY AIRPORT MANAGEMENT. SEE PROJECT SPECIAL PROVISIONS.

WORK AREA 1 - CONSTRUCTION SHALL INCLUDE SEDIMENTATION AND EROSION CONTROL; DEMOLITION AND REMOVAL OF EXISTING PAVEMENT; LIGHTING DEMOLITION; EARTHWORK; AGGREGATE BASE COURSE; P-401 AND P-404 ASPHALT SURFACE COURSE; MARKING; LIGHTING INSTALLATION; SEEDING; SODDING; AND MULCHING. ALL WORK IN WORK AREA 1 SHALL BE COMPLETED WITHIN 60 CALENDAR DAYS.

1. INSTALL LIGHTED BARRICADES AT LOCATIONS SHOWN ON THIS SHEET. CONTRACTOR WILL BE WORKING ADJACENT TO ACTIVE TAXIWAY AND SHALL UTILIZE CAUTION AS REQUIRED BY THE SAFETY PLAN.
2. INSTALL TEMPORARY SEDIMENT BASIN AND OTHER SEDIMENTATION AND EROSION CONTROL MEASURES AS SHOWN ON SHEET EC2.1.
3. COMPLETE AIRCRAFT PARKING TIE-DOWN REMOVAL, PAVEMENT REMOVAL, AND LIGHTING REMOVAL OPERATIONS. MILLINGS SHALL BE REMOVED FROM AIRPORT AND DISPOSED OF AT DESIGNATED OFF SITE LOCATION.
4. COMPLETE GRADING OPERATIONS FOR PROJECT SITE, FINE GRADE APRON AND CONNECTOR TAXIWAY SUBGRADE AND COMPACT SUBGRADE AS REQUIRED.
5. COMPLETE PLACEMENT, GRADING AND COMPACTION OF AGGREGATE BASE COURSE FOR APRON AND CONNECTOR TAXIWAY.
6. APPLY BITUMINOUS PRIME COAT TO AGGREGATE BASE IN ACCORDANCE WITH SPECIFICATIONS.
7. COMPLETE PAVING OPERATIONS ON APRON AND CONNECTOR TAXIWAY. PAVEMENT SHALL BE PLACED IN TWO 2" LIFTS OF P-401 ASPHALT SURFACE COURSE ON CONNECTOR TAXIWAY AND ONE 2" LIFT OF P-401 ASPHALT SURFACE COURSE AND ONE 2" LIFT OF P-404 FUEL RESISTANT ASPHALT SURFACE COURSE ON THE APRON.
8. COMPLETE INSTALLATION OF NEW TAXIWAY LIGHTING SYSTEM MODIFICATIONS AS SHOWN ON THE PLANS.
9. COMPLETE PLACEMENT OF SHOULDER FILL, COMPACT AND FINE GRADE SHOULDERS IN ACCORDANCE WITH DETAILS SHOWN ON THE PLANS.
10. COMPLETE FIRST APPLICATION OF PAVEMENT MARKING. MARKING SHALL BE INSTALLED WITHOUT BEADS.
11. COMPLETE INSTALLATION OF SOD ALONG THE EDGE OF PAVEMENT AND SEEDING AND MULCHING OPERATIONS FOR SITE. RESTORE ALL DISTURBED AREAS AND SEED AND MULCH ALL AREAS AS REQUIRED.
12. REMOVE ALL EQUIPMENT, MATERIALS AND DEBRIS.
13. REMOVE LIGHTED BARRICADES AND OPEN APRON AND CONNECTOR TAXIWAY TO AIR OPERATIONS.
14. AFTER 30 DAY CURING PERIOD, CONTRACTOR SHALL APPLY FINAL APPLICATION OF MARKING WITH BEADS FOR ALL MARKING.
15. AFTER GRASS IS ESTABLISHED AND SITE IS STABILIZED OR AS DIRECTED BY THE ENGINEER, REMOVE ALL TEMPORARY EROSION MEASURES.

NOTE D - PORTABLE TOILET FACILITIES:
PORTABLE TOILET FACILITIES SHALL BE LOCATED WITHIN STAGING AREA LOCATED AWAY FROM STORM DRAINAGE SYSTEM AND INLETS.



4" WIDE REFLECTIVE ORANGE TAPE AT 12" O.C. SET DIAGONAL TO BARRICADE TO CREATE ALTERNATING ORANGE/WHITE STRIPES (TYP.)

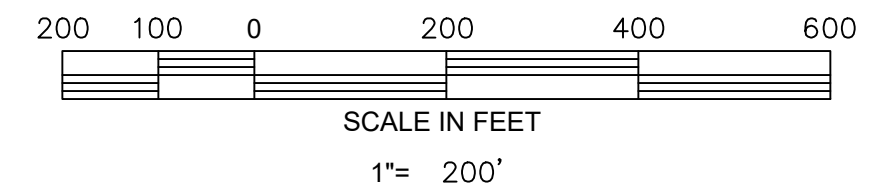
LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

AVIATION BARRICADE NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
2. LOW PROFILE LIGHTED BARRICADES SHALL BE MULTI-BARRIER AIRPORT RUNWAY BARRICADE AR10X96 AND SOLAR LIGHTS SHALL BE PROVIDED FOR THE BARRICADES AS MANUFACTURED BY OFF THE WALL PRODUCTS, LLC. OR APPROVED EQUAL. THE CONTRACTOR SHALL PROVIDE WATER NECESSARY TO FILL THE BARRICADES AND ENSURE ADEQUATE WATER HAS BEEN PROVIDED TO ANCHOR THE BARRICADES IN PLACE.
3. CONTRACTOR SHALL CHECK LIGHTS DAILY TO VERIFY THAT THEY ARE IN WORKING CONDITION AND SHALL REPLACE LIGHTS AS REQUIRED.
4. CONTRACTOR SHALL INSTALL BARRICADES AT LOCATIONS SHOWN ON INDIVIDUAL PLAN SHEETS. BARRICADES SHALL BE MOVED AND RELOCATED AS REQUIRED. BARRICADES SHALL BE INSTALLED WITH MAXIMUM 5' SPACE BETWEEN BARRICADE AND CONTRACTOR SHALL LEAVE ONE 15' SPACE FOR EMERGENCY VEHICLES. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST.

LEGEND

---	APPROXIMATE AIRPORT PROPERTY LINE
— RSA —	RUNWAY SAFETY AREA
— ROFA —	RUNWAY OBJECT FREE AREA
— ROFZ —	RUNWAY OBSTACLE FREE ZONE
— TOFA —	TAXIWAY OBJECT AREA
[Hatched Box]	WORK AREA 1
[Dotted Line]	CONTRACTOR'S ACCESS ROUTE
[Solid Grey Box]	CONTRACTOR'S STAGING/STOCKPILE AREA
[Dashed Line]	LIGHTED BARRICADES

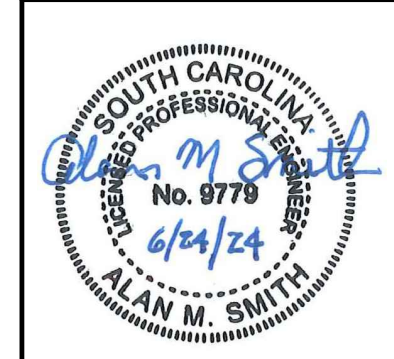


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
4810 SHELLEY DRIVE
FAX: 910-762-6281
SC LICENSE NO. C00296
EMAIL: TBILL@TBILL.COM

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REV. NO.	DESCRIPTION	DATE
1 <td>ADDED NOTE D <td>02/29/24 </td></td>	ADDED NOTE D <td>02/29/24 </td>	02/29/24
REVISIONS		

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
PROJECT PHASING PLAN - WORK AREA 1



Date	JUNE 2024
Scale	1" = 200'
Drawn	NT
Checked	AMS
Project No.	2601-2105
Sheet No.	

A2.3

CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO PROTECT ANY EXISTING PAVEMENT THAT CONSTRUCTION EQUIPMENT CROSSES. CONTRACTOR SHALL REPAIR ANY DAMAGE FROM HIS OPERATIONS IN ACCORDANCE WITH SPECIFICATIONS

CONSTRUCTION ACCESS ROADS
CONTRACTOR SHALL USE CONSTRUCTION ACCESS ROADS TO PROJECT SITE AS SHOWN. ANY DAMAGE TO EXISTING ROADS AND AIRFIELD PAVEMENT DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED BY CONTRACTOR, COST OF DAMAGE REPAIR SHALL BE INCLUDED IN COST OF MOBILIZATION.

CONTRACTOR'S STAGING AREA. INSTALL SILT FENCE ALONG OUTSIDE EDGE OF STAGING AREA.
CONCRETE WASHOUT AREA. SEE DETAIL ON SHEET EC2.2.

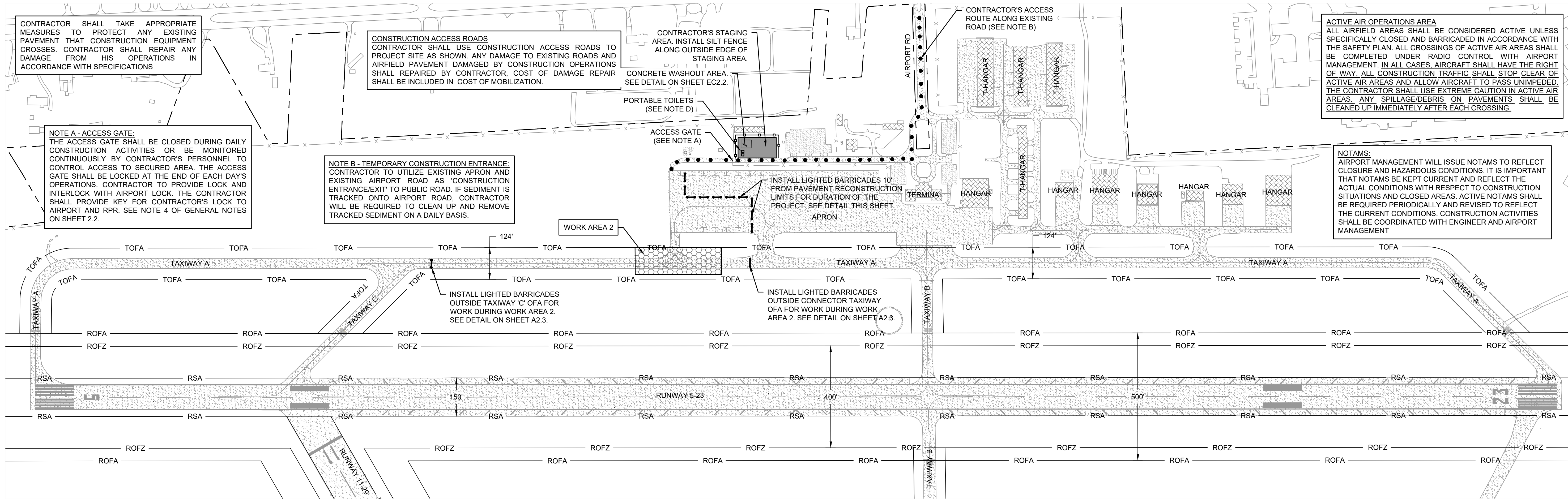
CONTRACTOR'S ACCESS ROUTE ALONG EXISTING ROAD (SEE NOTE B)

ACTIVE AIR OPERATIONS AREA
ALL AIRFIELD AREAS SHALL BE CONSIDERED ACTIVE UNLESS SPECIFICALLY CLOSED AND BARRICADED IN ACCORDANCE WITH THE SAFETY PLAN. ALL CROSSINGS OF ACTIVE AIR AREAS SHALL BE COMPLETED UNDER RADIO CONTROL WITH AIRPORT MANAGEMENT. IN ALL CASES, AIRCRAFT SHALL HAVE THE RIGHT OF WAY. ALL CONSTRUCTION TRAFFIC SHALL STOP CLEAR OF ACTIVE AIR AREAS AND ALLOW AIRCRAFT TO PASS UNIMPEDED. THE CONTRACTOR SHALL USE EXTREME CAUTION IN ACTIVE AIR AREAS. ANY SPILLAGE/DEBRIS ON PAVEMENTS SHALL BE CLEANED UP IMMEDIATELY AFTER EACH CROSSING.

NOTE A - ACCESS GATE:
THE ACCESS GATE SHALL BE CLOSED DURING DAILY CONSTRUCTION ACTIVITIES OR BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA. THE ACCESS GATE SHALL BE LOCKED AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR TO PROVIDE LOCK AND INTERLOCK WITH AIRPORT LOCK. THE CONTRACTOR SHALL PROVIDE KEY FOR CONTRACTOR'S LOCK TO AIRPORT AND RPR. SEE NOTE 4 OF GENERAL NOTES ON SHEET 2.2.

NOTE B - TEMPORARY CONSTRUCTION ENTRANCE:
CONTRACTOR TO UTILIZE EXISTING APRON AND EXISTING AIRPORT ROAD AS 'CONSTRUCTION ENTRANCE/EXIT' TO PUBLIC ROAD. IF SEDIMENT IS TRACKED ONTO AIRPORT ROAD, CONTRACTOR WILL BE REQUIRED TO CLEAN UP AND REMOVE TRACKED SEDIMENT ON A DAILY BASIS.

NOTAMS:
AIRPORT MANAGEMENT WILL ISSUE NOTAMS TO REFLECT CLOSURE AND HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS AND CLOSED AREAS. ACTIVE NOTAMS SHALL BE REQUIRED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS. CONSTRUCTION ACTIVITIES SHALL BE COORDINATED WITH ENGINEER AND AIRPORT MANAGEMENT

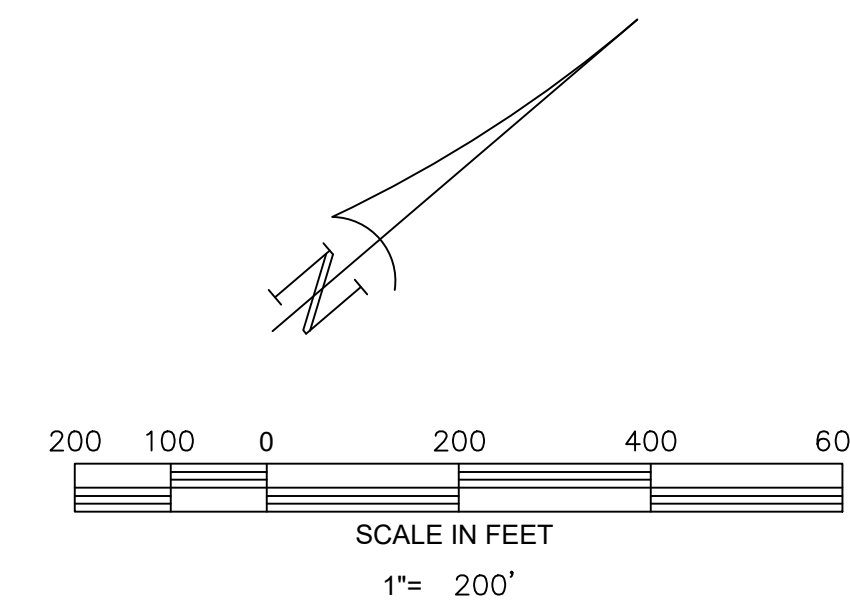


WORK AREA 2 SEQUENCE OF CONSTRUCTION - CONSTRUCTION IN WORK AREA 2 SHALL REQUIRE PARTIAL CLOSURE OF TAXIWAY 'A' AS SHOWN ON THIS PLAN. WORK ELEMENTS SHALL INCLUDE SEDIMENTATION AND EROSION CONTROL; DEMOLITION AND REMOVAL OF EXISTING PAVEMENT; LIGHTING DEMOLITION; EARTHWORK; AGGREGATE BASE COURSE; P-401 ASPHALT SURFACE COURSE; MARKING; LIGHTING INSTALLATION; SEEDING; SODDING; AND MULCHING. ALL WORK IN WORK AREA 2 SHALL BE COMPLETED IN 12 CALENDAR DAYS.

- INSTALL LIGHTED BARRICADES AT LOCATIONS SHOWN ON THIS SHEET. CONTRACTOR WILL BE WORKING ADJACENT TO ACTIVE TAXIWAY AND SHALL UTILIZE CAUTION AS REQUIRED BY THE SAFETY PLAN. DE-ENERGIZE THE TAXIWAY LIGHTS WITHIN THE CLOSED PORTION OF TAXIWAY USING TEMPORARY JUMPER CABLE OR COVERING OF LIGHTS.
- INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS SHOWN ON SHEET EC2.1.
- COMPLETE PAVEMENT REMOVAL AND LIGHTING REMOVAL OPERATIONS. MILLINGS SHALL BE REMOVED FROM AIRPORT AND DISPOSED OF AT DESIGNATED OFF SITE LOCATION.
- COMPLETE GRADING OPERATIONS FOR PROJECT SITE, FINE GRADE CONNECTOR TAXIWAY SUBGRADE AND COMPACT SUBGRADE AS REQUIRED.
- COMPLETE PLACEMENT, GRADING AND COMPACTION OF AGGREGATE BASE COURSE FOR CONNECTOR TAXIWAY.
- APPLY BITUMINOUS PRIME COAT TO AGGREGATE BASE IN ACCORDANCE WITH SPECIFICATIONS.
- COMPLETE PAVING OPERATIONS ON CONNECTOR TAXIWAY. PAVEMENT SHALL BE PLACED IN TWO 2" LIFTS OF P-401 ASPHALT SURFACE COURSE.
- COMPLETE INSTALLATION OF NEW TAXIWAY LIGHTING SYSTEM MODIFICATIONS AS SHOWN ON THE PLANS.
- COMPLETE PLACEMENT OF SHOULDER FILL, COMPACT AND FINE GRADE SHOULDERS IN ACCORDANCE WITH DETAILS SHOWN ON THE PLANS.
- COMPLETE FIRST APPLICATION OF PAVEMENT MARKING. MARKING SHALL BE INSTALLED WITHOUT BEADS.
- COMPLETE INSTALLATION OF SOD ALONG THE EDGE OF PAVEMENT AND SEEDING AND MULCHING OPERATIONS FOR SITE. RESTORE ALL DISTURBED AREAS AND SEED AND MULCH ALL AREAS AS REQUIRED.
- REMOVE ALL EQUIPMENT, MATERIALS AND DEBRIS.
- REMOVE LIGHTED BARRICADES AND REOPEN TAXIWAY 'A' TO AIR OPERATIONS.
- AFTER 30 DAY CURING PERIOD, CONTRACTOR SHALL APPLY FINAL APPLICATION OF MARKING WITH BEADS FOR ALL MARKING.
- AFTER GRASS IS ESTABLISHED AND SITE IS STABILIZED OR AS DIRECTED BY THE ENGINEER, REMOVE ALL TEMPORARY EROSION MEASURES.

NOTE D - PORTABLE TOILET FACILITIES:
PORTABLE TOILET FACILITIES SHALL BE LOCATED WITHIN STAGING AREA LOCATED AWAY FROM STORM DRAINAGE SYSTEM AND INLETS.

- LEGEND**
- APPROXIMATE AIRPORT PROPERTY LINE
 - RSA — RUNWAY SAFETY AREA
 - ROFA — RUNWAY OBJECT FREE AREA
 - ROFZ — RUNWAY OBSTACLE FREE ZONE
 - TOFA — TAXIWAY OBJECT AREA
 - [Pattern] WORK AREA 2
 - [Dotted] CONTRACTOR'S ACCESS ROUTE
 - [Shaded] CONTRACTOR'S STAGING/STOCKPILE AREA
 - [Dashed] LIGHTED BARRICADES

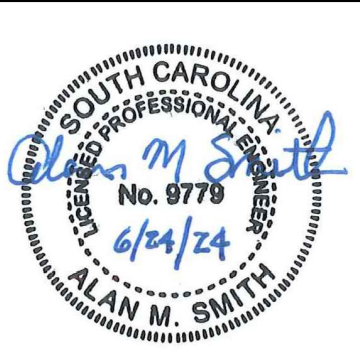


TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
4810 SHELLEY DRIVE
SC LICENSE NO. C003296
EMAIL: TB@T&B.COM

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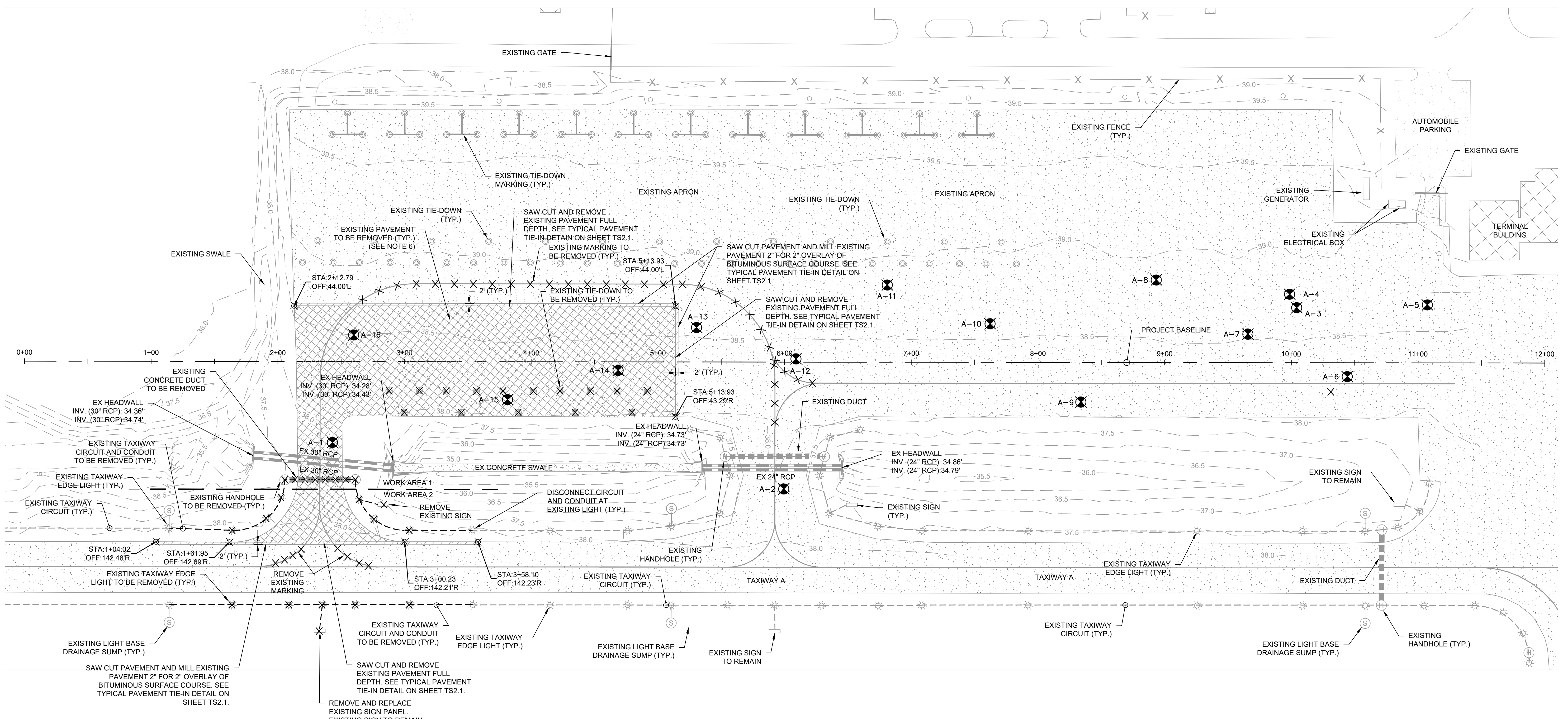
REV. NO.	DESCRIPTION	DATE
1	ADDED NOTE D	02/29/24

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
PROJECT PHASING PLAN - WORK AREA 2



Date	JUNE 2024
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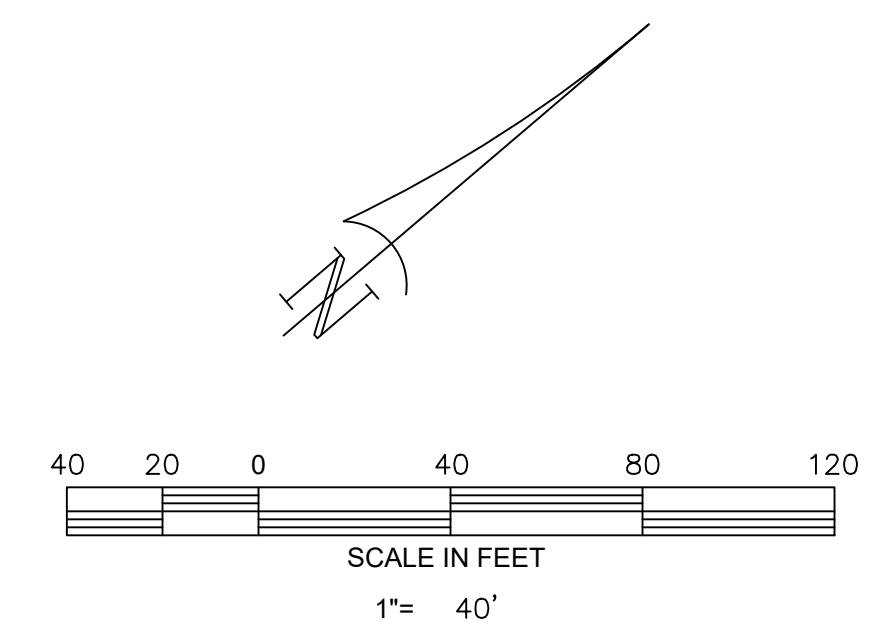
A2.4



GENERAL NOTES:

- SOIL BORING LOGS AND TEST DATA PERFORMED BY GEOTECHNOLOGIES, INC. ARE CONTAINED IN APPENDIX 'F' OF THE PROJECT SPECIFICATIONS AND ARE FOR INFORMATIONAL PURPOSES ONLY. SOIL BORING LOCATIONS SHOWN ARE APPROXIMATE.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON SHEET SL2.1.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES, AND TAXIWAY LIGHTS TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022.
- ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- ALL MILLINGS SHALL BE DISPOSED OFF AIRPORT PROPERTY AT GEORGETOWN COUNTY SITE LOCATED AT 1324 AVIATION BLVD. CONTRACTOR SHALL DUMP MILLINGS AT SPECIFIED SITE. COUNTY STAFF SHALL BE RESPONSIBLE FOR STOCKPILE MAINTENANCE.
- LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE OR SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
- AFTER EXISTING BITUMINOUS PAVEMENT HAS BEEN REMOVED AND EXCAVATED TO PROPOSED SUBGRADE, THE PROPOSED SUBGRADE SHALL BE PROOFROLLED TO LOCATE STUMPS AND LOOSE SOILS. STUMPS, ROOTS, AND OTHER ORGANIC MATERIAL ARE NOT ACCEPTABLE IN THE SUBGRADE AND SHALL BE REMOVED. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE PROJECT AND DISPOSED OF OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- TAXIWAY LIGHTS AND TRANSFORMERS REMOVED FROM PROJECT SHALL REMAIN PROPERTY OF THE AIRPORT. CONTRACTOR SHALL TAKE CARE DURING REMOVAL OPERATIONS TO NOT DAMAGE THE LIGHTING EQUIPMENT. THE CONTRACTOR SHALL STOCKPILE SALVAGED LIGHTS AND TRANSFORMERS AT LOCATION ON AIRFIELD DESIGNATED BY AIRPORT MANAGEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY AT A PROPERLY PERMITTED LOCATION.
- SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC2.1 THRU EC2.4. CONTRACTOR SHALL INSTALL SEDIMENTATION AND EROSION CONTROL MEASURES AS REQUIRED PRIOR TO BEGINNING OF REMOVAL OF DEMOLITION ITEMS.

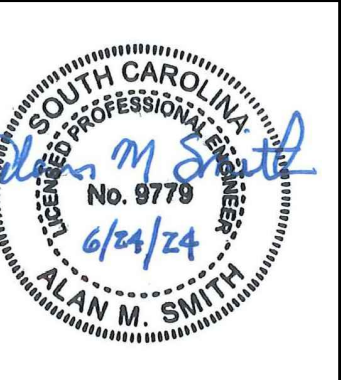
LEGEND	
	EXISTING ASPHALT PAVEMENT
	EXISTING PAVEMENT TO BE REMOVED
	EXISTING CONTOUR
	EXISTING TAXIWAY CIRCUIT AND CONDUIT TO BE REMOVED
	EXISTING DUCT
	EXISTING TAXIWAY CIRCUIT
	EXISTING STORM DRAIN PIPE
	EXISTING TAXIWAY EDGE LIGHT
	EXISTING HANDHOLE
	EXISTING LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	APPROXIMATE BORING LOCATION
	EXISTING TIE DOWN



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REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
EXISTING CONDITIONS AND DEMOLITION PLAN

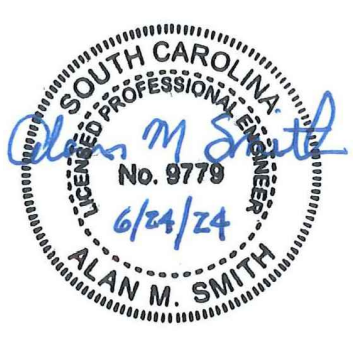


Date	JUNE 2024
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Checked	AMS
Project No.	2601-2105
Sheet No.	

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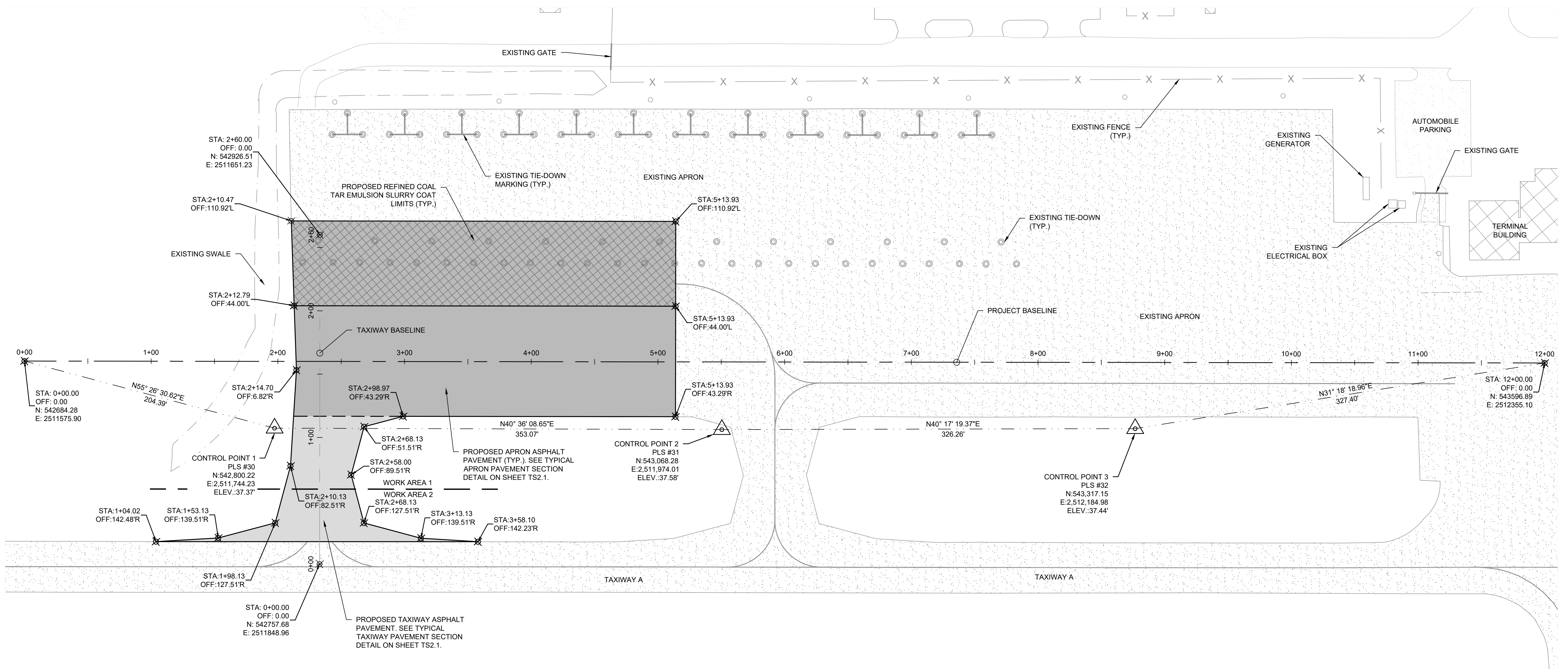
REV. NO.	DESCRIPTION	DATE
	REVISIONS	

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
SURVEY LAYOUT PLAN



Date	JUNE 2024
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Project No.	2601-2105
Sheet No.	

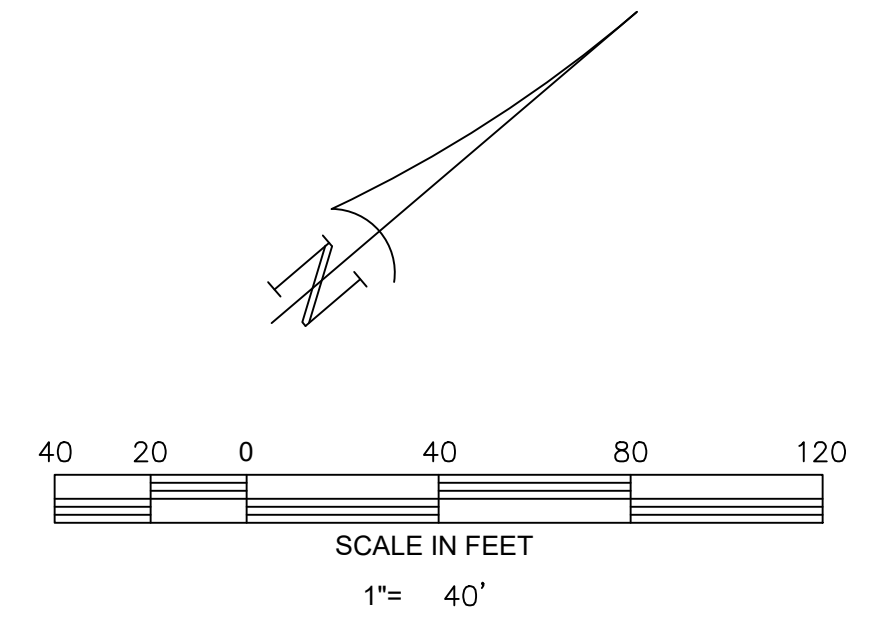
SL2.1

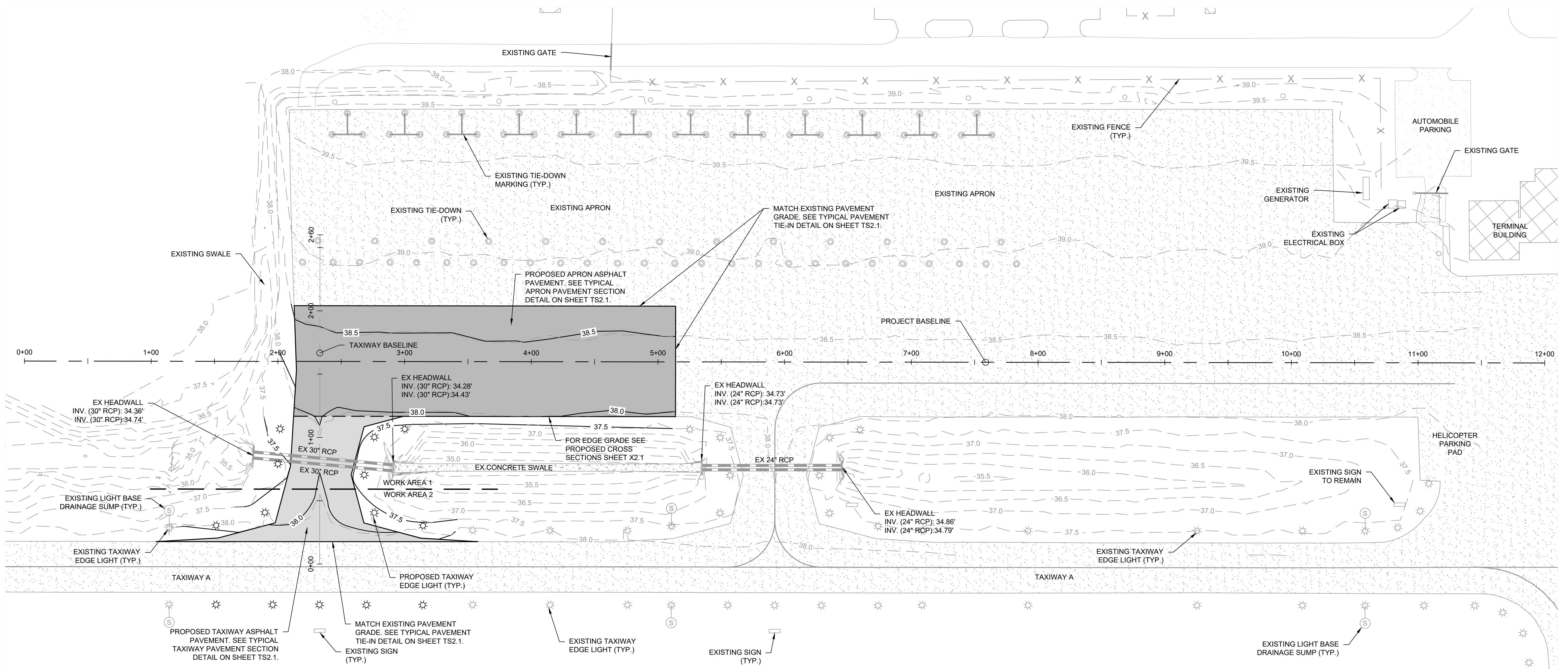


GENERAL NOTES:

1. TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022. ALL STATIONS AND OFFSETS SHOWN ON THE PLANS ARE BASED ON 'PROJECT BASELINE'.
2. ALL BEARINGS AND COORDINATES ARE BASED ON SOUTH CAROLINA STATE PLANE COORDINATE SYSTEM 1983. ELEVATIONS ARE BASED ON NAVD88.
3. SEE SHEET EX2.1 FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
4. SEE SHEET G2.1 FOR GRADING AND DRAINAGE PLAN.
5. SEE SHEET TS2.1 FOR TYPICAL PAVEMENT SECTIONS AND MISCELLANEOUS DETAILS.

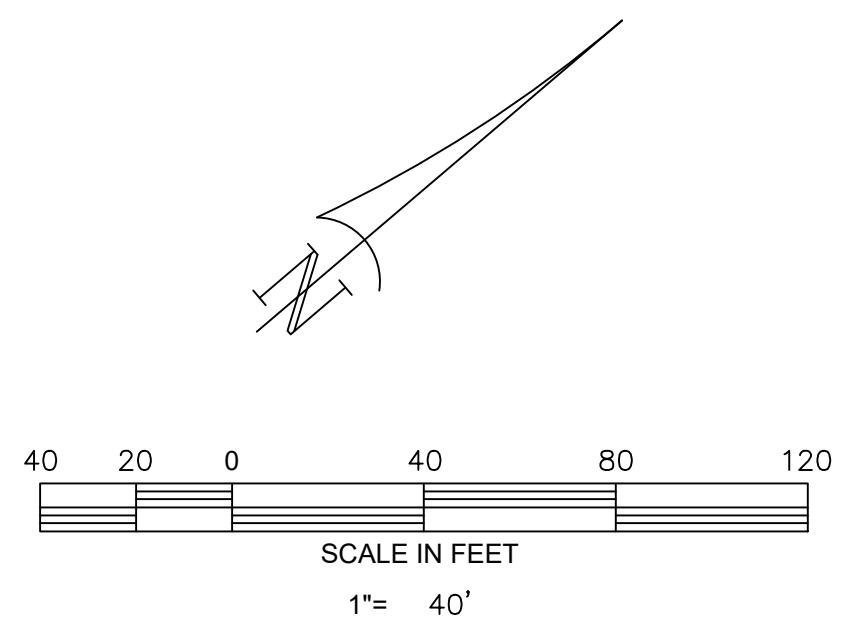
LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT
	PROPOSED SLURRY COAT LIMITS
	EXISTING SWALE
	EXISTING TIE DOWN





- GENERAL NOTES:**
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES, AND TAXIWAY LIGHTS TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED NOVEMBER, 2022.
 - ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
 - LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE OR SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
 - SEE SHEET EX2.1 FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
 - SEE SHEET SL2.1 FOR SURVEY CONTROL AND LAYOUT.
 - FOR TYPICAL SECTIONS AND MISCELLANEOUS DETAILS SEE SHEET TS2.1.
 - SEE TAXIWAY BASELINE PROFILE ON SHEET TS2.1.
 - SEE PHASING PLANS SHEET A2.3 AND A2.4 FOR SEQUENCE OF CONSTRUCTION.
 - SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
 - SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEET EC2.1 THRU EC2.4. CONTRACTOR SHALL INSTALL SEDIMENTATION AND EROSION CONTROL MEASURES AS ITEMS ARE COMPLETED.

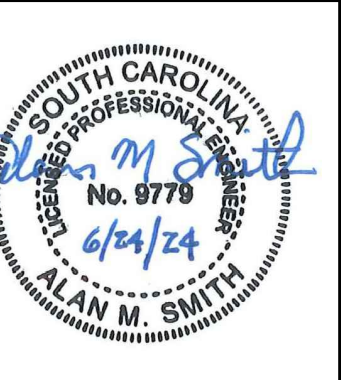
LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING SWALE
	EXISTING TAXIWAY EDGE LIGHT
	PROPOSED TAXIWAY EDGE LIGHT
	EXISTING LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	EXISTING TIE DOWN



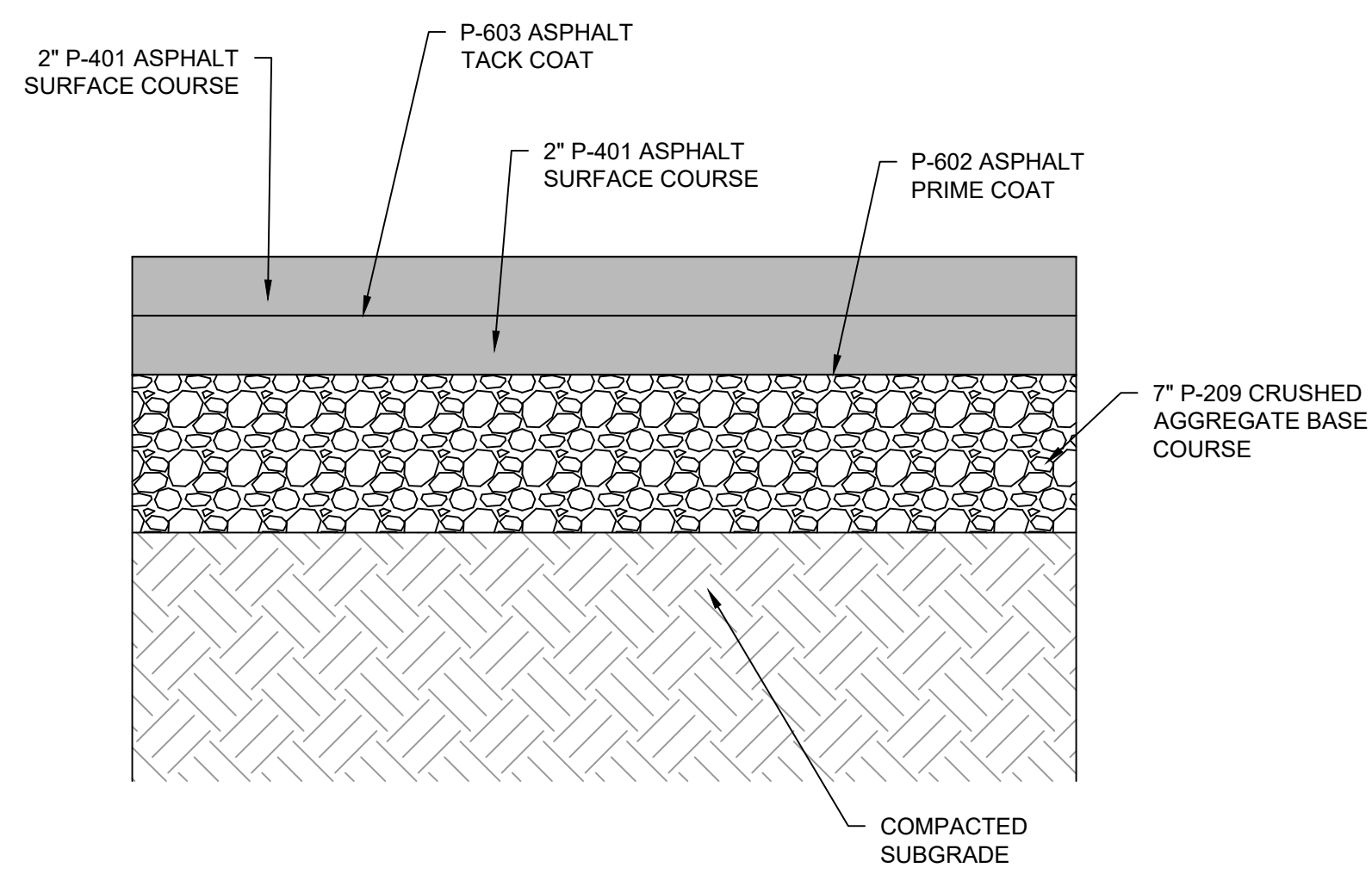
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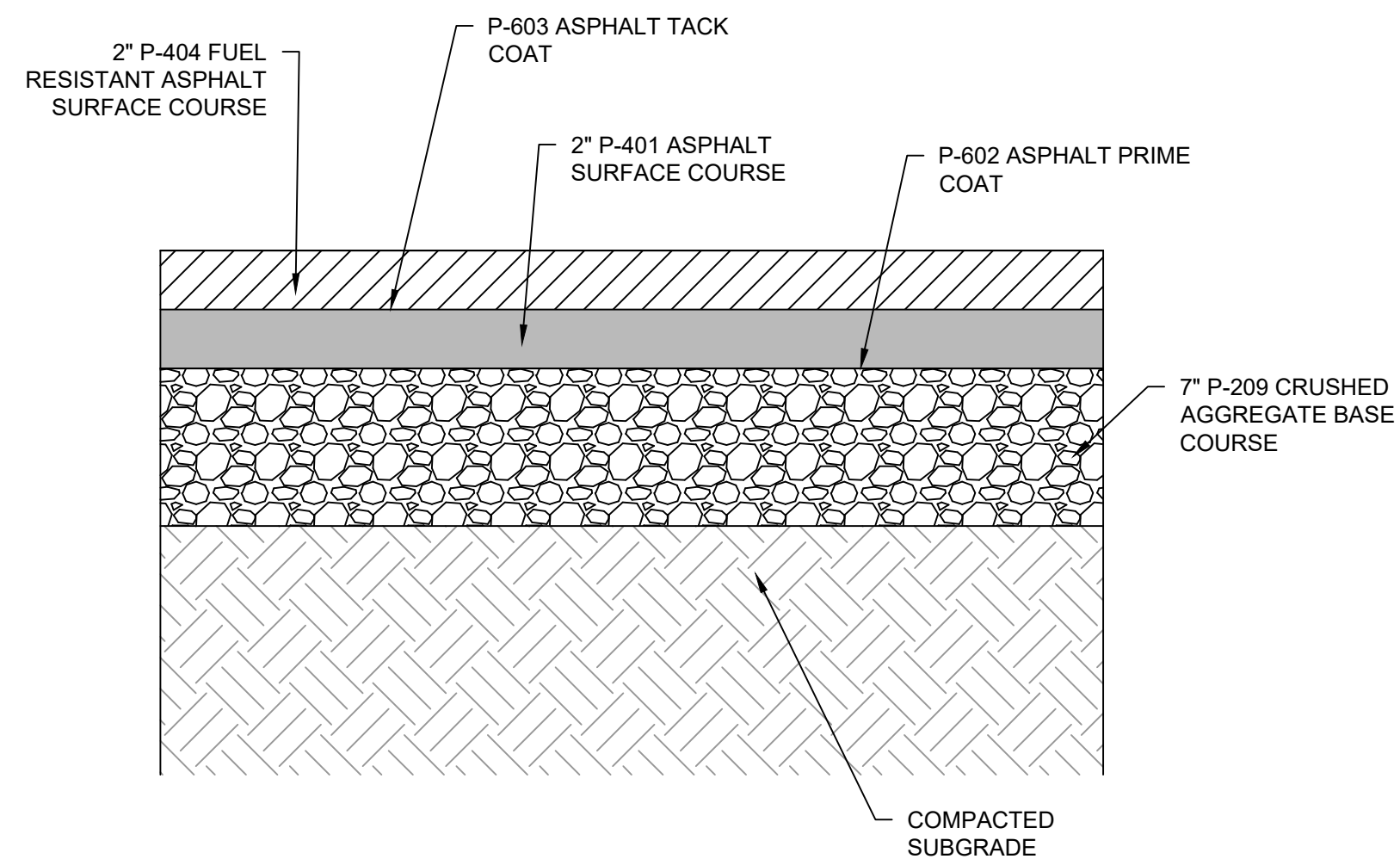
GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
GRADING AND PAVING PLAN



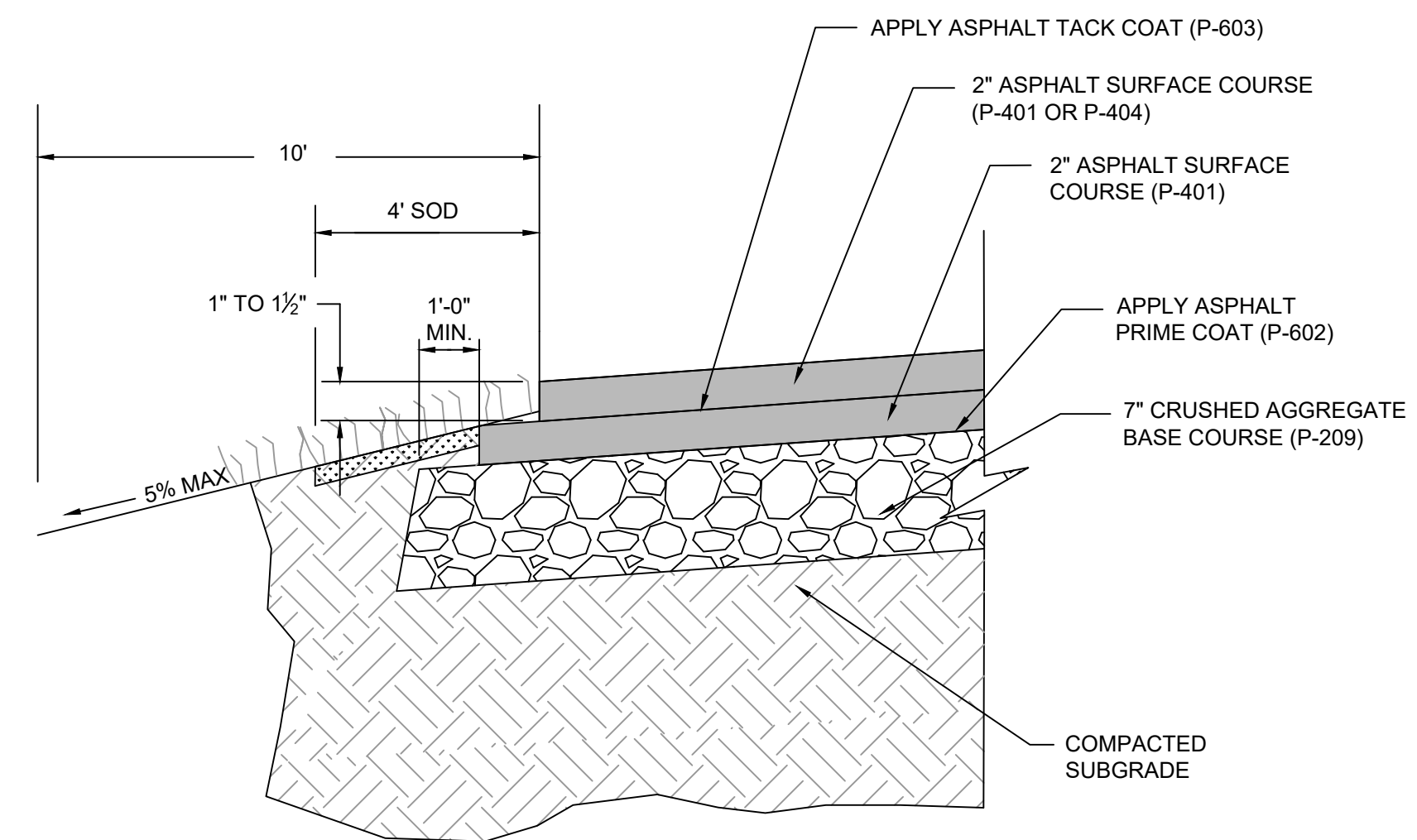
Date	JUNE 2024
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Project No.	2601-2105
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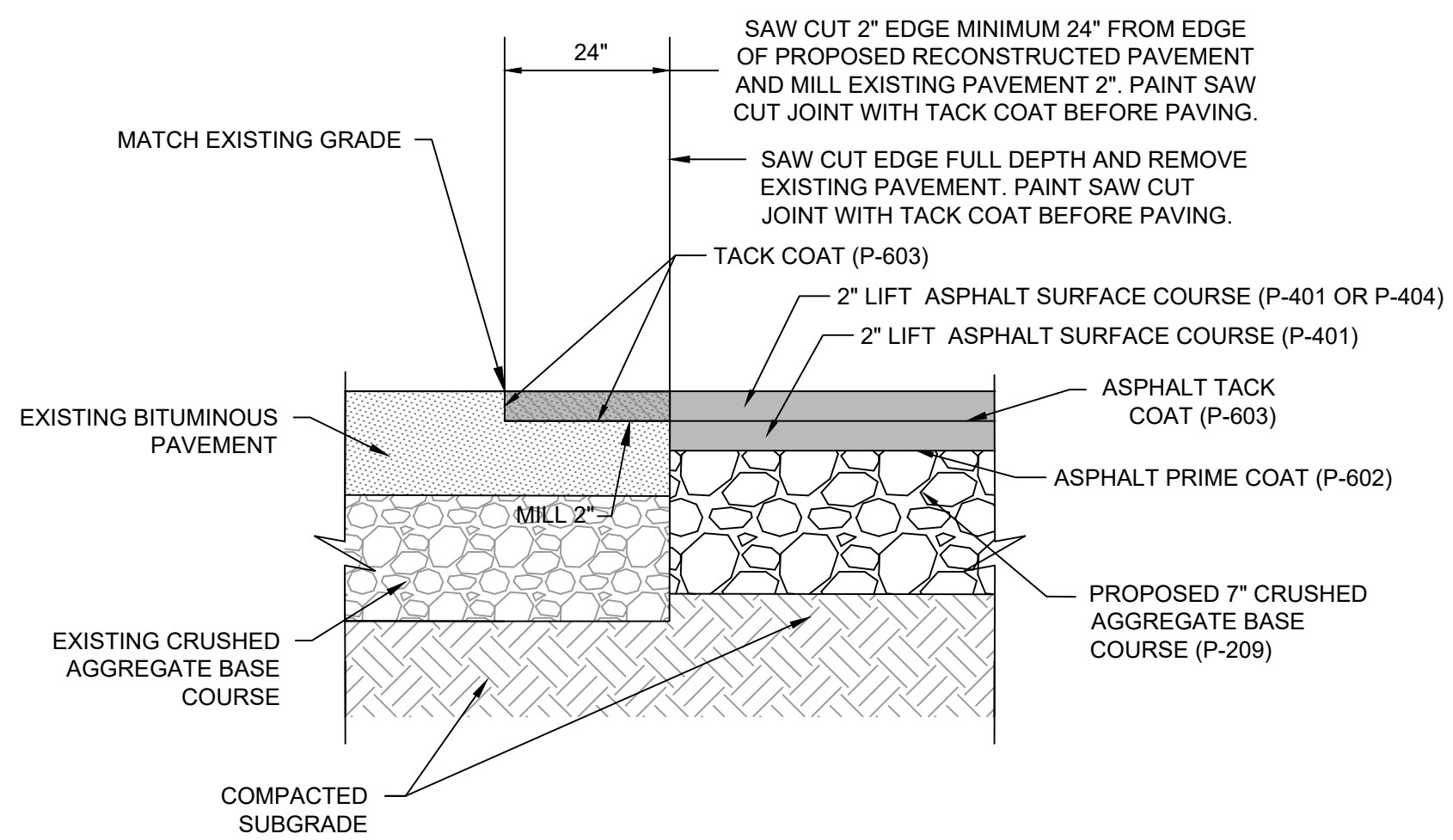
TYPICAL TAXIWAY PAVEMENT SECTION
NOT TO SCALE



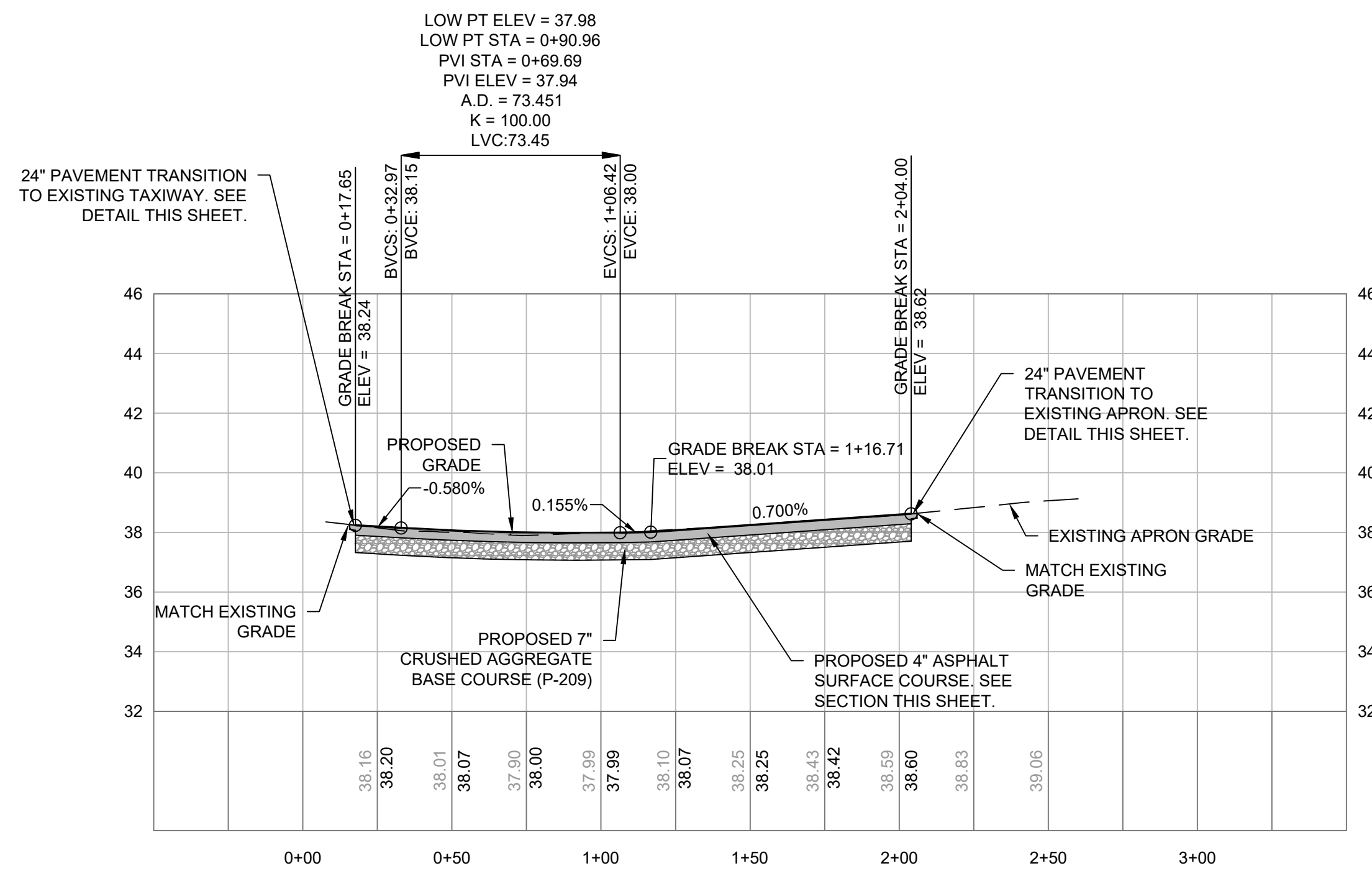
TYPICAL APRON PAVEMENT SECTION
NOT TO SCALE



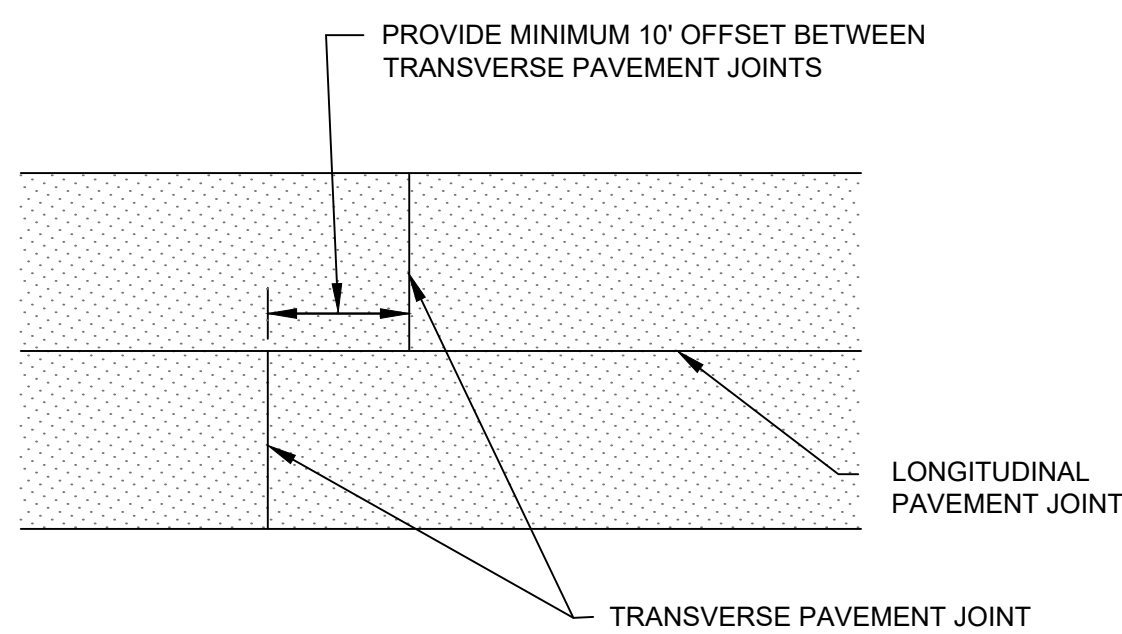
TYPICAL SHOULDER DETAIL
NOT TO SCALE



TYPICAL PAVEMENT TIE-IN DETAIL
NOT TO SCALE



TAXIWAY BASELINE PROFILE
SCALE - H: 1" = 40', V: 1" = 4'



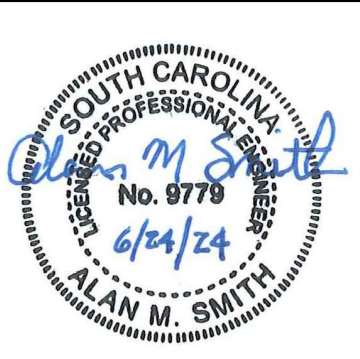
TRANSVERSE PAVEMENT JOINTS
NOT TO SCALE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
EMAIL: TBILLM@TBILLM.COM

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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
TYPICAL PAVEMENT SECTIONS AND MISCELLANEOUS DETAILS



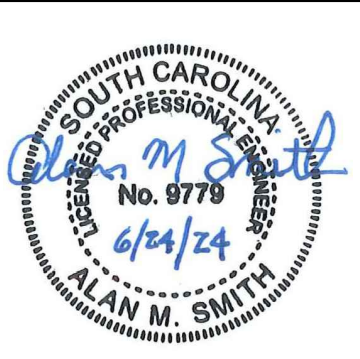
Date	JUNE 2024
Scale	NTS
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Checked	AMS
Project No.	2601-2105
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TS2.1

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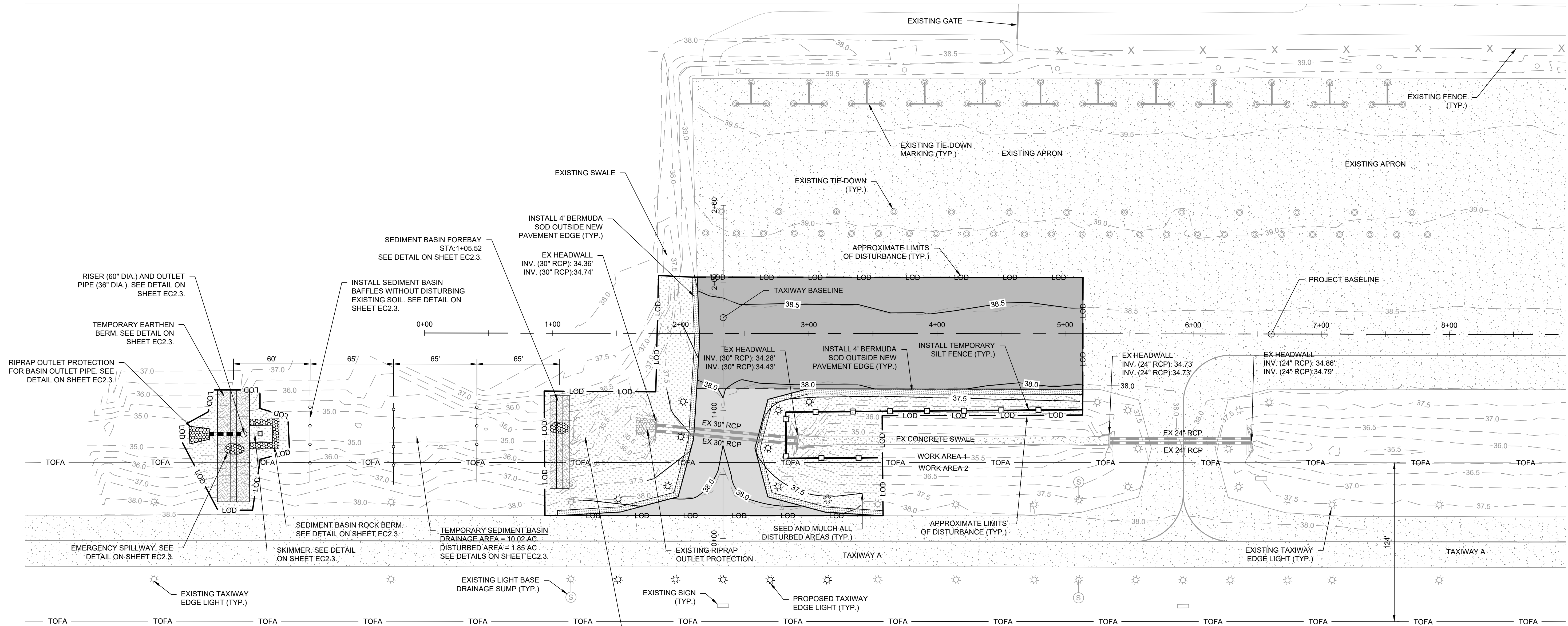
REV. NO.	DESCRIPTION	DATE
1	ADDED NOTES	02/29/24
	REVISIONS	

GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
SEDIMENTATION AND EROSION CONTROL PLAN



Date	JUNE 2024
Scale	1" = 40'
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Project No.	2601-2105
Sheet No.	

EC2.1



NOTE: FOR COMPLETION OF CONSTRUCTION OF EARTHEN DAM, ROCK BERM, AND BAFFLES WITHIN THE TOFA, THE CONTRACTOR SHALL WORK ON A PULL BACK BASIS. CONTRACTOR SHALL CONTINUOUSLY MONITOR AIR TRAFFIC ON TAXIWAY 'A' AND SHALL PULL BACK OUTSIDE LIMITS OF TOFA FOR ALL APPROACHING TRAFFIC. AIRCRAFT MOVING ALONG TAXIWAY 'A' HAVE THE RIGHT OF WAY AT ALL TIMES.

NOTE: CONTRACTOR TO USE EXISTING SWALE ALIGNMENT AND ELEVATIONS FOR TEMPORARY SEDIMENT BASIN. INSTALL SEDIMENT BASIN AS SHOWN WITHIN EXISTING SWALE. ALL DISTURBED AREAS WITHIN SWALE SHALL BE SEEDED AND MULCHED.

GENERAL NOTES:

- SEE SHEET EX2.1 FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
- SEE SHEET SL2.1 FOR SURVEY CONTROL AND LAYOUT.
- FOR TYPICAL SECTIONS AND MISCELLANEOUS DETAILS SEE SHEET TS2.1.
- SEE PHASING PLANS SHEET A2.3 AND A2.4 FOR SEQUENCE OF CONSTRUCTION.
- SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
- SEE SEDIMENTATION AND EROSION CONTROL NOTES AND DETAILS ON SHEETS EC2.2, EC2.3 AND EC2.4. CONTRACTOR SHALL INSTALL PORTABLE TOILETS AND CONCRETE WASHOUT AREA IN STAGING AREA AS SHOWN ON SHEETS A2.2, A2.3, AND A2.4.
- ALL DISTURBED AREAS AND AREA WITHIN LIMITS OF DISTURBANCE OR AS DIRECTED BY THE ENGINEER SHALL BE SEEDED AND MULCHED. SEE PROJECT SPECIFICATIONS.
- PORTABLE TOILET FACILITIES SHALL BE LOCATED IN THE PROJECT STAGING AREA. SEE SHEETS A2.3 AND A2.4 FOR LOCATION.

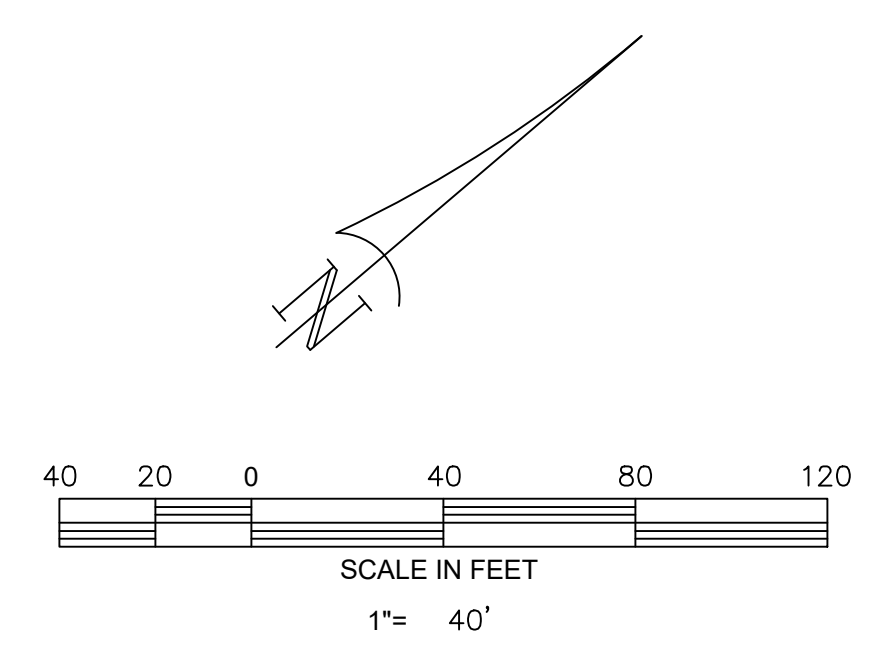
LIMITS OF DISTURBANCE:

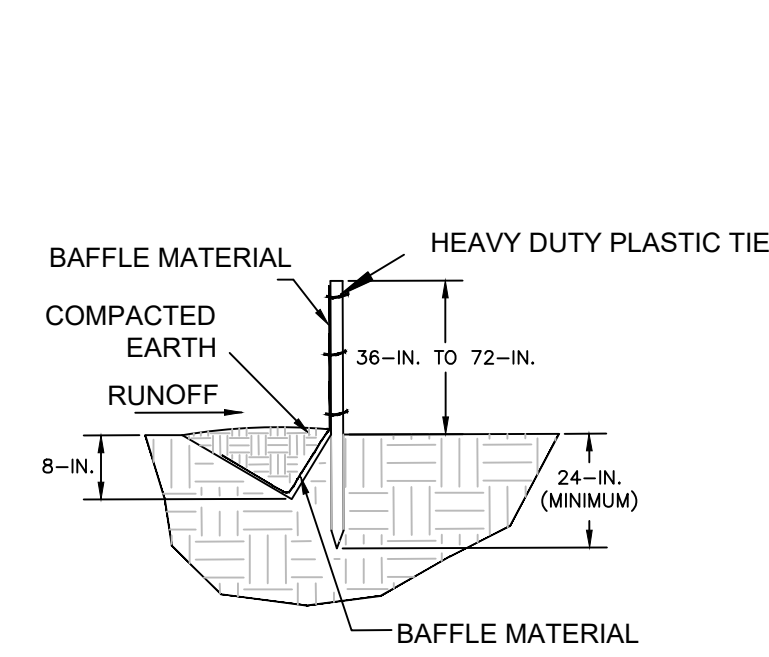
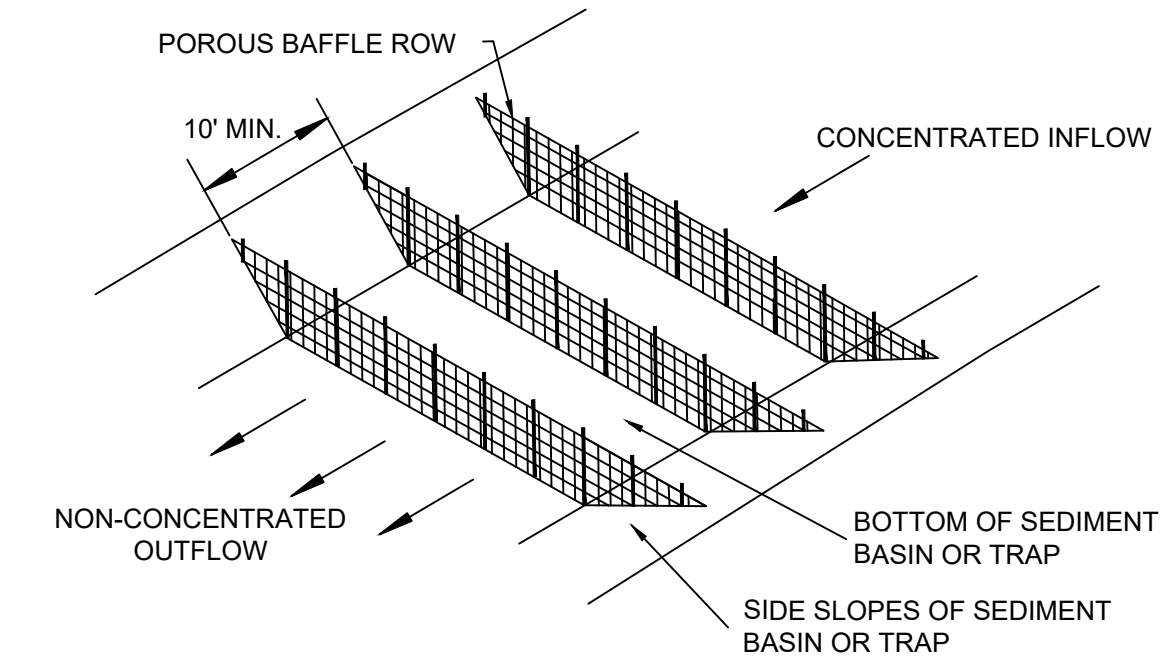
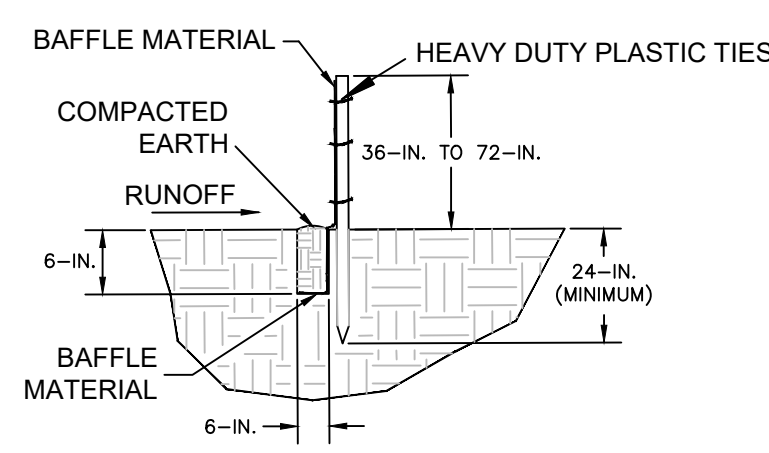
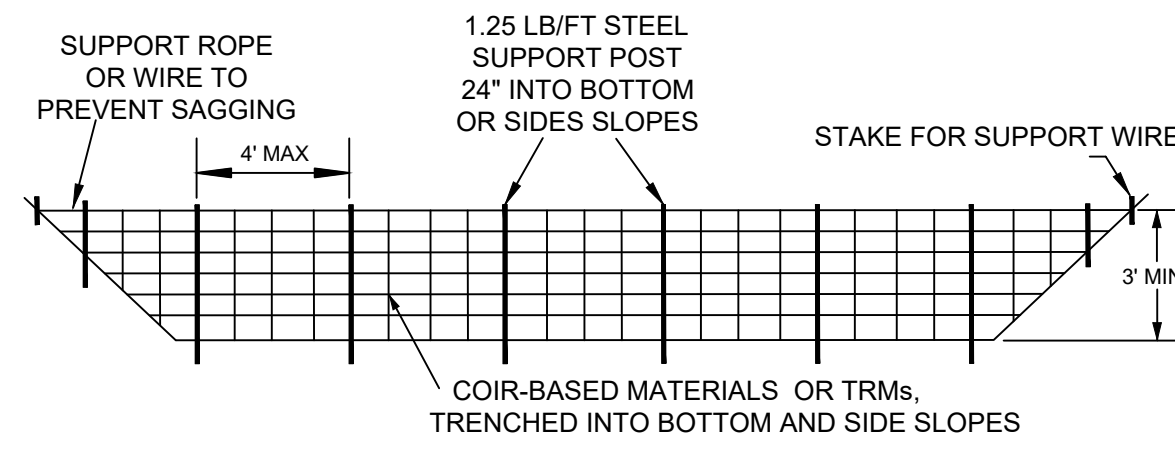
MAXIMUM TOTAL LIMITS OF DISTURBANCE = 3.20 ACRES (SCHEDULE I)
MAXIMUM TOTAL LIMITS OF DISTURBANCE = 1.85 ACRES (SCHEDULE II)
TOTAL MAXIMUM LIMITS OF DISTURBANCE = 5.05 ACRES (SCHEDULE I AND II)

NOTE: DUE TO FAA FUNDING LIMITATIONS, SCHEDULE I WILL BE CONSTRUCTED AS A STAND ALONE PROJECT. IT IS ANTICIPATED SCHEDULE II WILL BE CONSTRUCTED THE FOLLOWING CONSTRUCTION SEASON.

LEGEND

	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT
	PROPOSED SOD
	PERMANENT SEEDED AND MULCHING AREA
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING SWALE
	TEMPORARY SILT FENCE
	LIMITS OF DISTURBANCE
	EXISTING TAXIWAY EDGE LIGHT
	PROPOSED TAXIWAY EDGE LIGHT
	EXISTING LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	EXISTING TIE DOWN





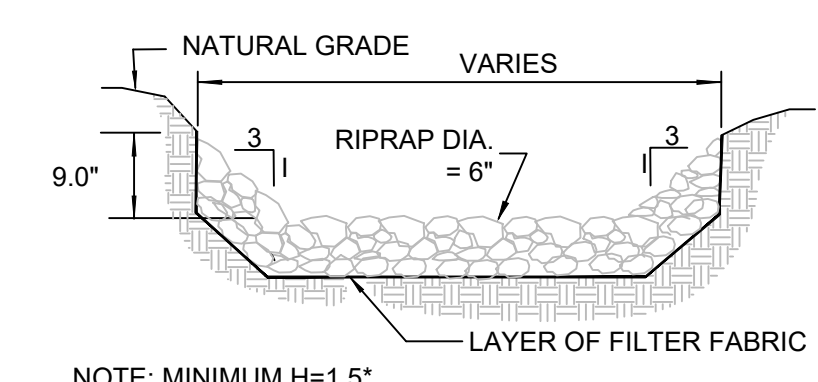
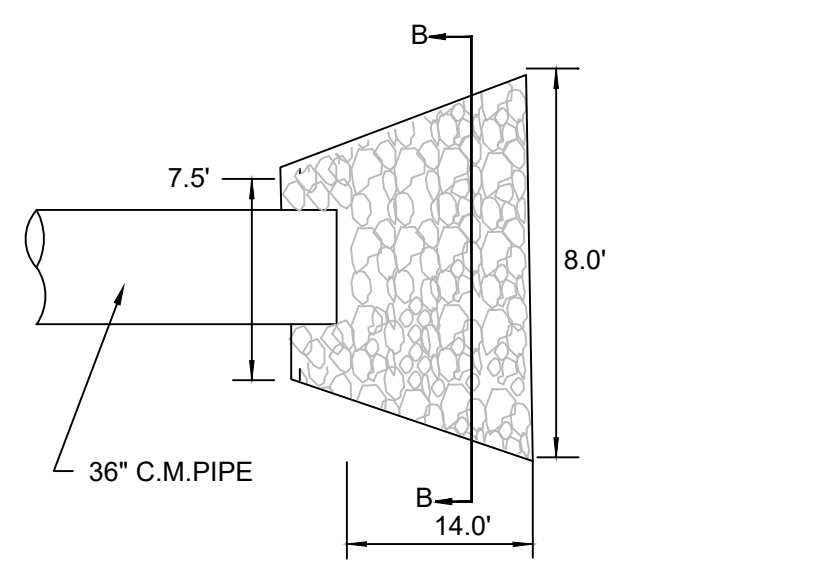
- BAFFLES - POST REQUIREMENTS**
- POROUS BAFFLE POSTS MUST BE 60-INCH TO 96-INCH LONG STEEL POSTS THAT MEET, AT A MINIMUM, THE FOLLOWING PHYSICAL CHARACTERISTICS.
 - COMPOSED OF A HIGH STRENGTH STEEL WITH A MINIMUM YIELD STRENGTH OF 50,000 PSI.
 - INCLUDE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND A NOMINAL "T" LENGTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (± 8%)
 - POSTS SHALL BE EQUIPPED WITH PROJECTIONS TO AID IN FASTENING OF BAFFLE MATERIAL.
 - INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. A MINIMUM HEIGHT OF 1- TO 2- INCHES ABOVE THE FABRIC SHALL BE MAINTAINED, AND A MAXIMUM HEIGHT OF 3 FEET SHALL BE MAINTAINED ABOVE THE GROUND.
 - POST SPACING SHALL BE AT A MAXIMUM OF 4- FEET ON CENTER.

- BAFFLES - INSPECTION & MAINTENANCE**
- THE KEY TO FUNCTIONAL POROUS BAFFLES IS WEEKLY INSPECTION, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
 - REGULAR INSPECTIONS OF POROUS BAFFLES SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVENT THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
 - ATTENTION TO SEDIMENT ACCUMULATIONS ALONG EACH ROW OF BAFFLES IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
 - REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE BAFFLE ROW OR WHEN IT REACHES THE CLEAN-OUT HEIGHT OF THE SEDIMENT BASIN OR TRAP, WHICHEVER IS REACHED FIRST.
 - REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
 - CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH EACH ROW OF BAFFLES, OR WHERE THE BAFFLE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE BAFFLE.
 - CHECK FOR TEARS/RIPS WITHIN THE BAFFLES, AREAS WHERE THE BAFFLE HAS BEGUN TO DECOMPOSE, AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE BAFFLE INEFFECTIVE. REMOVED DAMAGED BAFFLES AND REINSTALL NEW BAFFLES IMMEDIATELY.
 - POROUS BAFFLES SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

- BAFFLES - MATERIAL REQUIREMENTS**
- BAFFLE MATERIAL MUST BE COMPOSED OF COIR-BASED MATERIALS OR TURF REINFORCEMENT MATTING (TRM) THAT CONSISTS OF THE FOLLOWING REQUIREMENTS:
 - HAVE A LIGHT PENETRATION (% OPENINGS) BETWEEN 10-35%;
 - FREE OF LOOSE STRAW MATERIAL;
 - HAVE A MINIMUM TENSILE STRENGTH OF 145 LB/FT. AND,
 - HAVE A MINIMUM WIDTH OF 48-INCHES.
 - 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED OR BAFFLE MATERIAL MAY BE STAPLED INTO GROUND BY USING 12-INCH STAPLES WITH A MAXIMUM SPACING OF 12-INCHES.
 - BAFFLE MATERIAL SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE WIDTH OF THE SEDIMENT BASIN OR TRAP TO AVOID JOINTS.

- BAFFLES - GENERAL NOTES**
- ATTACH BAFFLE TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED ALONG THE ABOVE GROUND PORTION OF EACH POST.
 - INSTALL THE BAFFLE ROWS PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE EACH BAFFLE THE PROPER DISTANCE FROM INLET AND OUTLETS TO ALLOW ACCESS FOR MAINTENANCE AND CLEAN-OUT.

POROUS BAFFLES
NOT TO SCALE

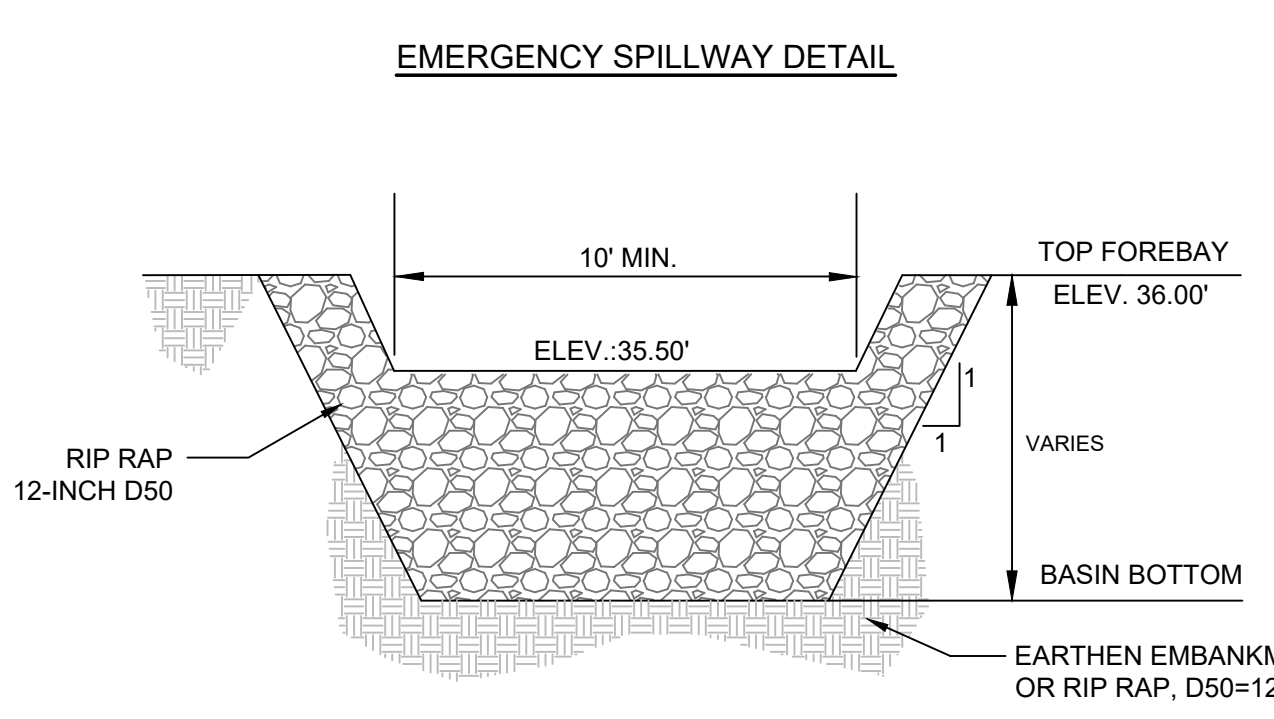
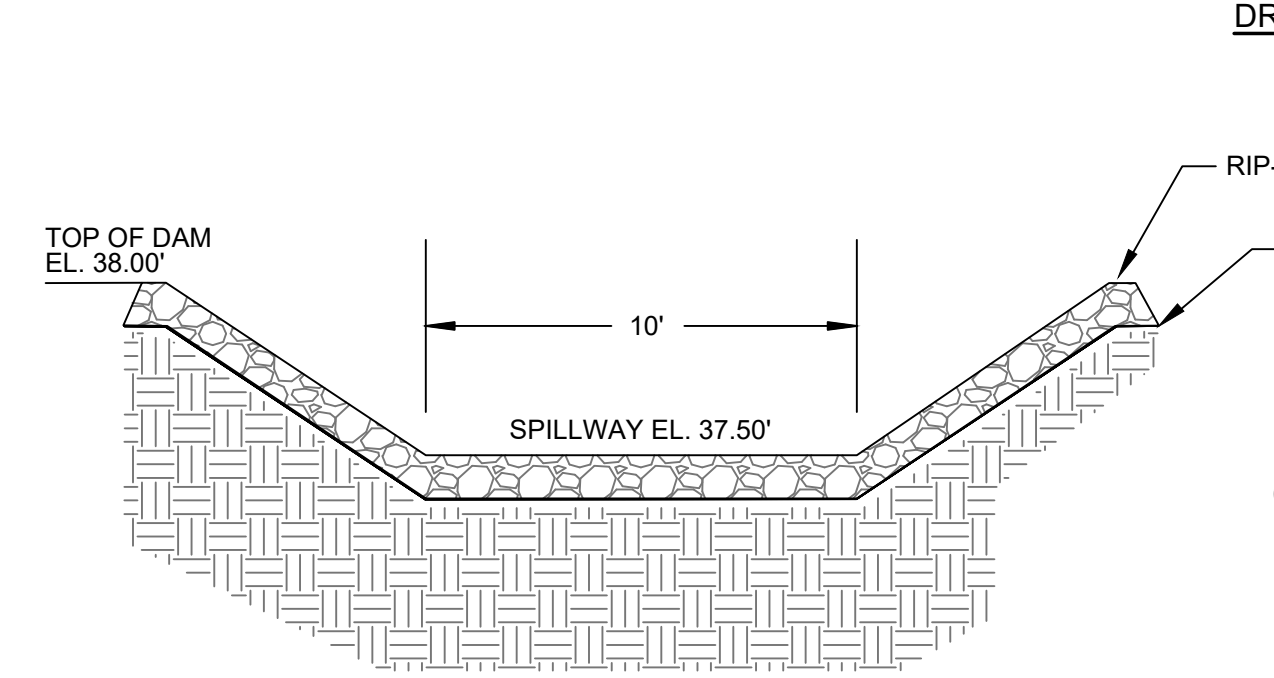
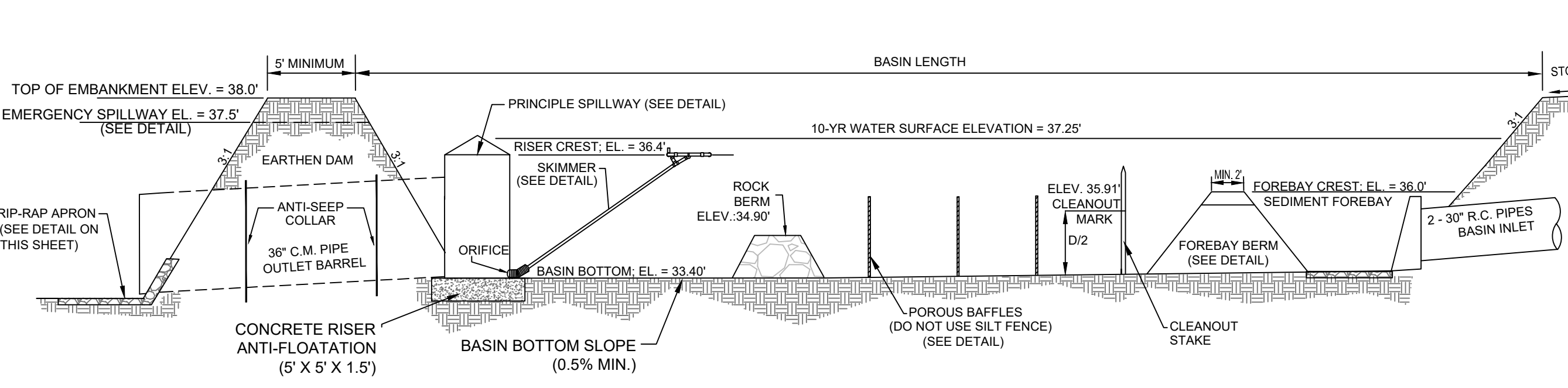


NOTE: MINIMUM H=1.5' ROCK DIAMETER

RIPRAP APRON OUTLET PROTECTION FOR BASIN OUTLET PIPE
NOT TO SCALE

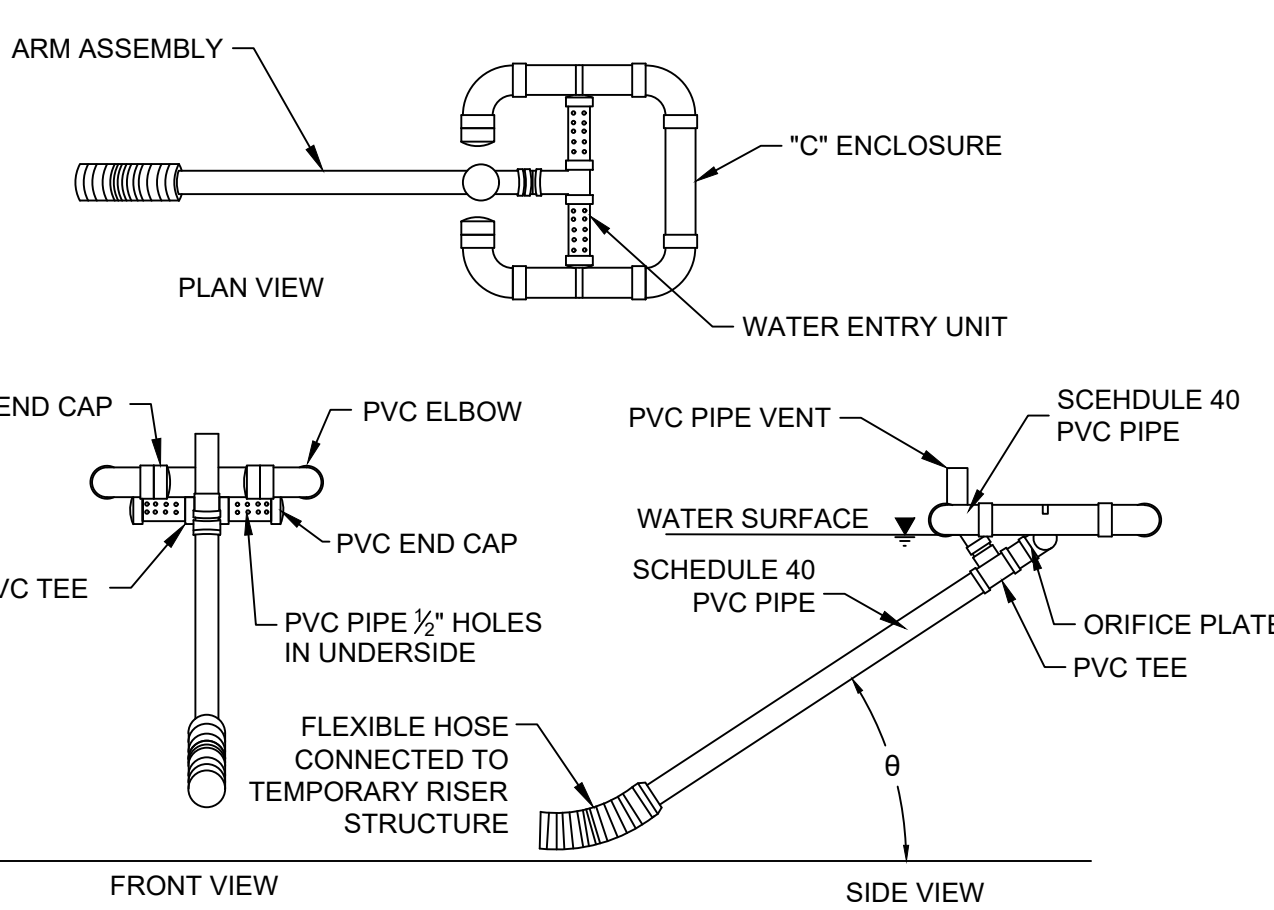
NOTES:

- RIPRAP SHOULD EXTEND UP BOTH SIDES OF THE APRON AND AROUND THE END OF THE PIPE OR CULVERT AT THE DISCHARGE OUTLET AT A MAXIMUM SLOPE OF 3:1 AND A HEIGHT NOT LESS THAN ONE HALF THE ROCK DIAMETER.
- THERE SHALL BE NO OVERFLOW FROM THE END OF THE APRON TO THE SURFACE OF THE RECEIVING CHANNEL. THE AREA TO BE PAVED OR RIPRAPPED SHALL BE UNDERCUT SO THAT THE INVERT OF THE APRON SHALL BE AT THE SAME GRADE (FLUSH) WITH THE SURFACE OF THE RECEIVING CHANNEL. THE APRON SHALL HAVE A CUTOFF OR TOE WALL AT THE DOWNSTREAM END.
- THE WIDTH OF THE END OF THE APRON SHALL BE EQUAL TO THE BOTTOM WIDTH OF THE RECEIVING CHANNEL. MAXIMUM TAPER TO RECEIVING CHANNEL 5:1
- ALL SUBGRADE FOR STRUCTURE TO BE COMPACTED TO 95% OR GREATER.
- THE PLACING OF FILL, EITHER LOOSE OR COMPACTED IN THE RECEIVING CHANNEL SHALL NOT BE ALLOWED.
- NO BENDS OR CURVES IN THE HORIZONTAL ALIGNMENT OF THE APRON WILL BE PERMITTED.

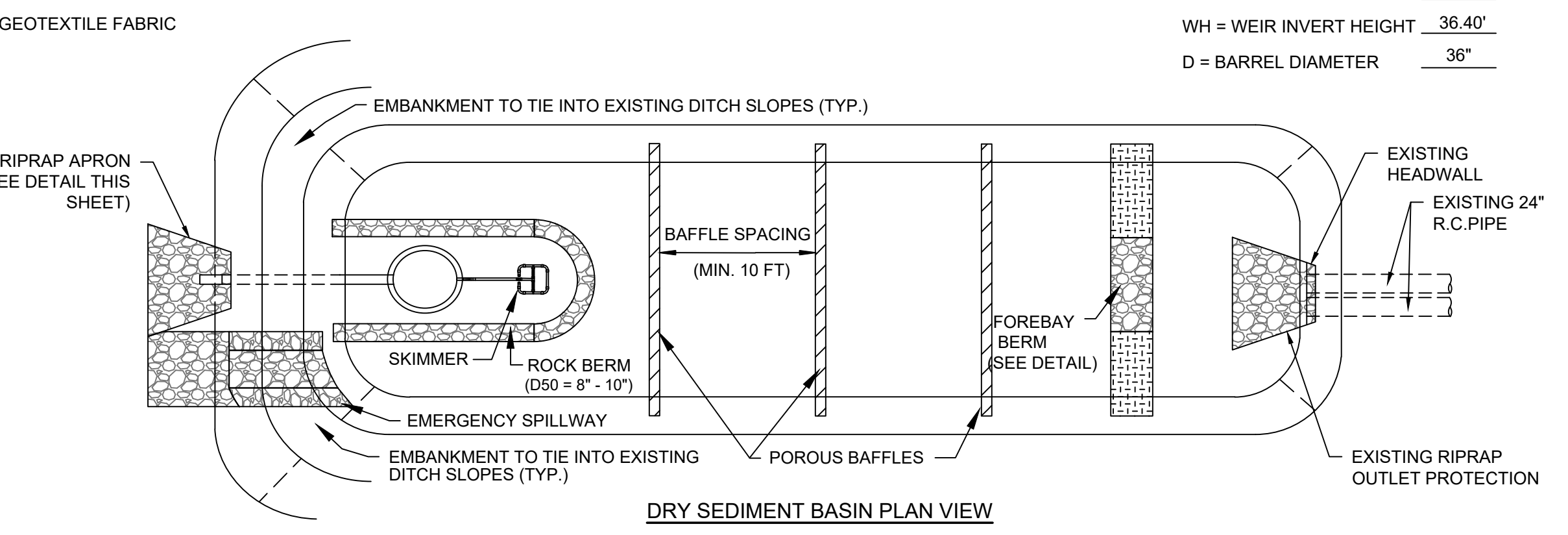


NOTE: THE "TEMPORARY SKIMMER BASIN (COMPLETE AND IN PLACE) WILL BE PAID FOR ON A PER EACH BASIS. THE PRICE SHALL INCLUDE ALL COMPONENTS AS SHOWN ABOVE FOR A COMPLETE INSTALLATION. THE PRICE SHALL ALSO INCLUDE REMOVAL OF ALL ITEMS INSTALLED FOR THE BASIN AND RESTORATION OF ALL DISTURBED AREAS.

	SKIMMER SIZE	ORIFICE SIZE
SEDIMENT BASIN - 1	4.00 INCHES	3.75 INCHES



NOTE: ORIFICE SIZES ARE FOR THE FAIRCLOTH SKIMMER. IF ANOTHER TYPE OF SKIMMER IS USED THE DEVICE MUST BE ABLE TO DRAIN THE BASIN WITHIN 48 HOURS.



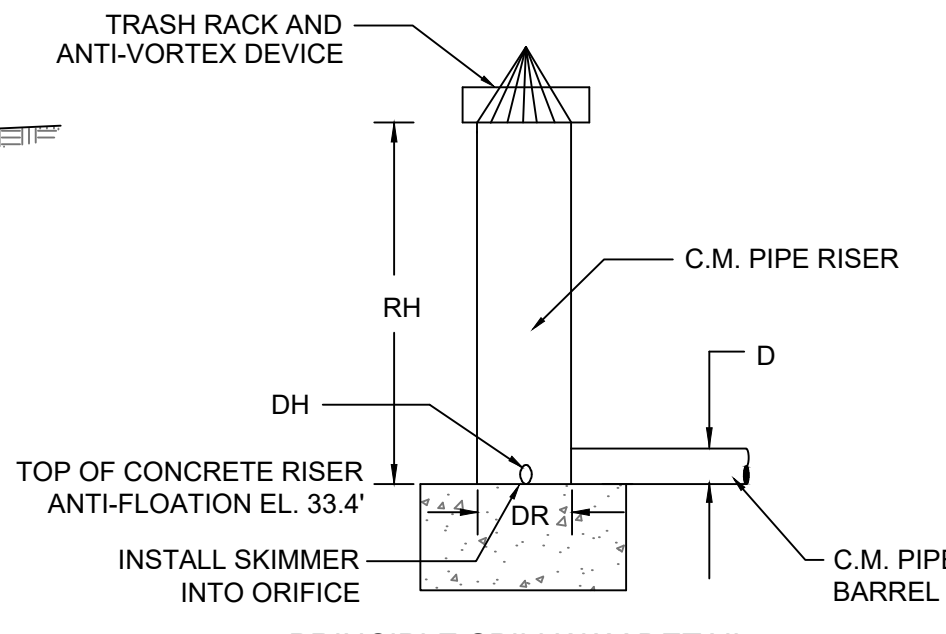
DRY SEDIMENT BASIN - GENERAL NOTES

- SEDIMENT BASIN'S SIDE SLOPES SHALL BE SEEDED AND, WHEN NECESSARY, STABILIZED WITH VEGETATIVE OR SYNTHETIC MATTING TO PREVENT THE FORMATION OF RILLS AND GULLIES.
- INSTALL THREE (3) ROWS OF POROUS BAFFLES WITH A MINIMUM SPACING OF 10 FEET. BAFFLES SHOULD ULTIMATELY BE PLACED TO MAXIMIZE THE SPACE BETWEEN EACH ROW OF BAFFLES AND THE BASIN'S INLETS/OUTLETS. ONLY TWO (2) ROWS OF BAFFLES ARE NECESSARY FOR BASINS THAT ARE LESS THAN 50 FEET IN LENGTH.
- POROUS BAFFLES SHOULD BE COMPOSED OF COIR-BASED MATERIALS OR TRMS WITH A LIGHT PENETRATION (OPEN SPACES) BETWEEN 10-35%. THESE MATERIALS SHOULD NOT HAVE LOOSE STRAW. SILT FENCE MAY NOT BE USED AS POROUS BAFFLES.
- EACH POROUS BAFFLE SHALL BE INSTALLED ACROSS THE ENTIRE WIDTH OF THE BASIN AND ALONG THE BASIN'S SIDE SLOPE UNTIL THE HEIGHT OF THE BAFFLE INTERSECTS THE SLOPE.
- INSTALL SKIMMER AND COUPLING (AS NECESSARY) TO RISER STRUCTURE AT ORIFICE ALONG BOTTOM OF THE PRINCIPLE SPILLWAY'S RISER STRUCTURE. (REFER TO SKIMMER MANUFACTURER FOR INSTALLATION PROCEDURES AND SKIMMER SPECIFICATIONS.)
- SKIMMER SHOULD BE EQUIPPED WITH A MECHANISM, SUCH AS A ROPE, TO ALLOW EASY ACCESS TO SKIMMER TO UNCLOGGED ORIFICE OR PERFORM OTHER NECESSARY MAINTENANCE.
- STORMWATER RUNOFF ENTERING THE BASIN MUST BE DIRECTED INTO PROPER BMPS TO PREVENT EROSION ALONG SIDE SLOPES AND TO PREVENT SCOUR AT THE BASIN'S INLETS.
- THE FOREBAY BERM SHOULD CONSIST OF RIPRAP, GABION, OR AN EARTHEN BERM WITH A ROCK FILLED OUTLET THAT IS CONSTRUCTED ACROSS THE BOTTOM OF THE BASIN'S WIDTH.
- AN ADDITIONAL CLEANOUT STAKE FOR THE FOREBAY AREA IS RECOMMENDED AND SHOULD BE MARKED FOR CLEANOUT AT 50% OF PROVIDED SEDIMENT STORAGE.
- THE ELEVATION OF THE EMERGENCY SPILLWAY SHOULD BE AT LEAST 1 FOOT BELOW THE TOP OF THE EMBANKMENT. THE EMERGENCY SPILLWAY SHOULD NOT BE LOCATED ON FILL MATERIAL, WHEN POSSIBLE. RIPRAP AND GEOTEXTILE LINER SHOULD BE PLACED ON ALL SPILLWAYS THAT MUST BE LOCATED ON FILL MATERIAL.

DRY SEDIMENT BASIN - INSPECTION AND MAINTENANCE

- THE KEY TO A FUNCTIONAL SEDIMENT BASIN IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- ATTENTION TO SEDIMENT ACCUMULATIONS WITHIN THE BASIN IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT DEPOSITION SHOULD BE CONTINUALLY CHECKED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 50% OF THE DESIGN SEDIMENT STORAGE VOLUME OR 1/2 THE HEIGHT OF THE RISER STRUCTURE, WHICHEVER IS REACHED FIRST.
- REMOVED SEDIMENT FROM THE BASIN SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS THE DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- INSPECTIONS OF SEDIMENT BASINS SHOULD BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS OF EACH RAINFALL EVENT THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ALL TEMPORARY SEDIMENT BASINS, WHICH ARE NOT TO BE CONVERTED TO A DETENTION BASIN POST-CONSTRUCTION, SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED.
- DISTURBED AREAS RESULTING FROM THE REMOVAL OF THE SEDIMENT BASIN SHOULD BE PERMANENTLY STABILIZED AND ADDITIONAL BMPS, SUCH AS SILT FENCE, SHOULD BE UTILIZED TO ACCEPT STORMWATER RUNOFF FROM THIS DISTURBED AREA UNTIL FINAL STABILIZATION IS REACHED.

TEMPORARY SEDIMENT BASIN DETAIL
NOT TO SCALE



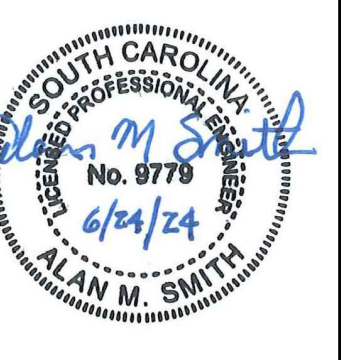
- RH = RISER HEIGHT 3.0'
- DR = RISER DIAMETER 60"
- DH = ORIFICE DIAMETER 4"
- WH = WEIR INVERT HEIGHT 36.40'
- D = BARREL DIAMETER 36"

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
4810 SHELLY DRIVE
SC LICENSE NO. C003296
EMAIL: TBILL@TBILL.COM

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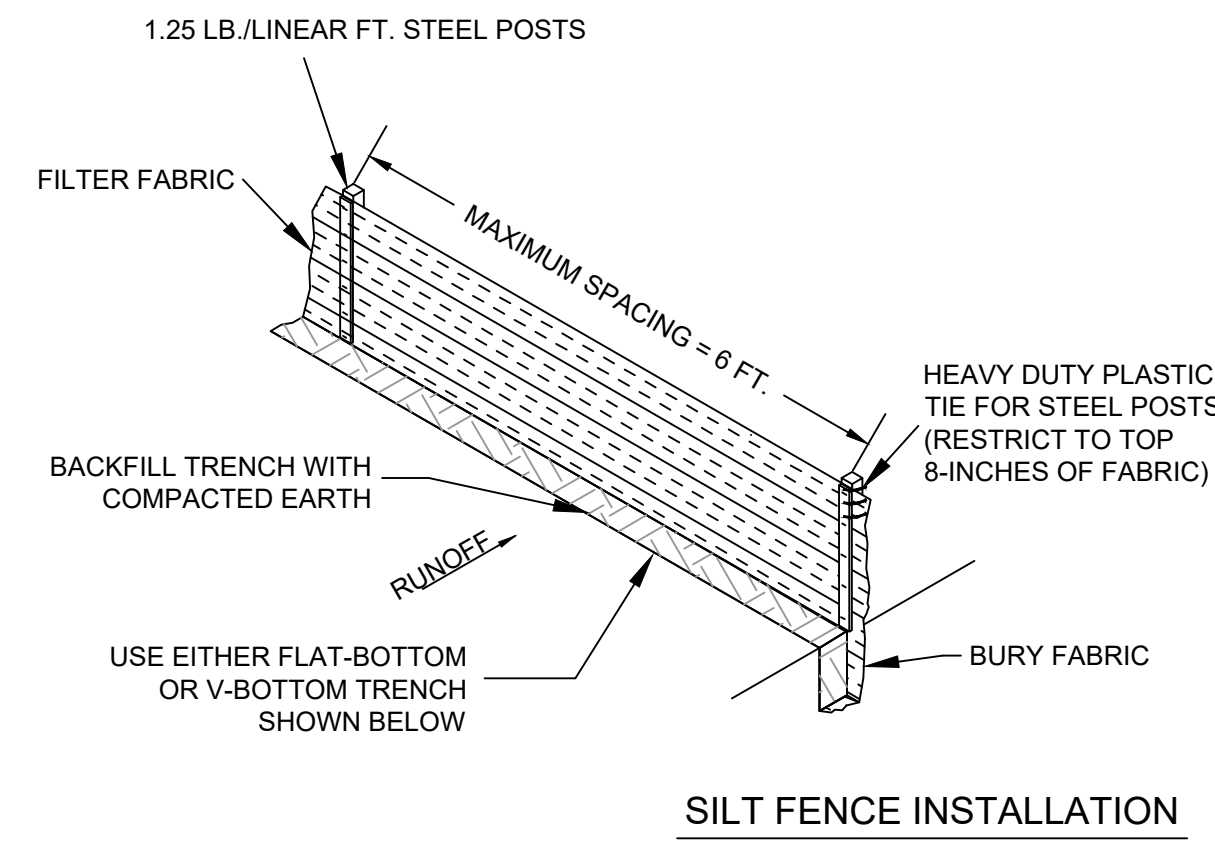
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE 1)
SEDIMENTATION AND EROSION CONTROL DETAILS
(SHEET 2 OF 3)

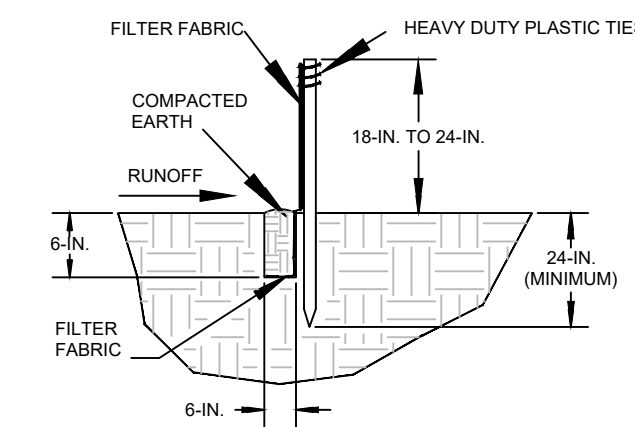


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Project No.	2601-2105
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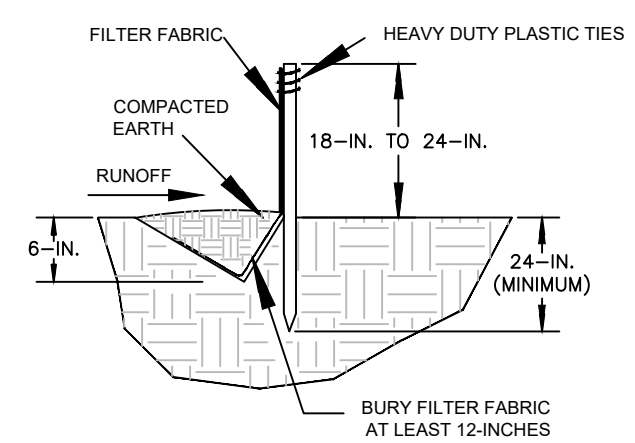
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FLAT-BOTTOM TRENCH DETAIL



V-SHAPED TRENCH DETAIL



SILT FENCE - GENERAL NOTES

- DO NOT PLACE SILT FENCE ACROSS CHANNELS OR IN OTHER AREAS SUBJECT TO CONCENTRATED FLOWS. SILT FENCE SHOULD NOT BE USED AS A VELOCITY CONTROL BMP. CONCENTRATED FLOWS ARE ANY FLOWS GREATER THAN 0.5 CFS.
- MAXIMUM SHEET OR OVERLAND FLOW PATH LENGTH TO THE SILT FENCE SHALL BE 100-FEET.
- MAXIMUM SLOPE STEEPNESS (NORMAL [PERPENDICULAR] TO THE FENCE LINE) SHALL BE 2:1.
- SILT FENCE JOINTS, WHEN NECESSARY, SHALL BE COMPLETED BY ONE OF THE FOLLOWING OPTIONS:
 - WRAP EACH FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 1-FOOT MINIMUM OVERLAP;
 - OVERLAP SILT FENCE BY INSTALLING 3-FEET PASSED THE SUPPORT POST TO WHICH THE NEW SILT FENCE ROLL IS ATTACHED. ATTACH OLD ROLL TO NEW ROLL WITH HEAVY-DUTY PLASTIC TIES; OR,
 - OVERLAP ENTIRE WIDTH OF EACH SILT FENCE ROLL FROM ONE SUPPORT POST TO THE NEXT SUPPORT POST.
- ATTACH FILTER FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED WITHIN THE TOP 8-INCHES OF THE FABRIC.
- INSTALL THE SILT FENCE PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE THE SILT FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.
- INSTALL SILT FENCE CHECKS (TIE-BACKS) EVERY 50-100 FEET, DEPENDENT ON SLOPE, ALONG SILT FENCE THAT IS INSTALLED WITH SLOPE AND WHERE CONCENTRATED FLOWS ARE EXPECTED OR ARE DOCUMENTED ALONG THE PROPOSED/INSTALLED SILT FENCE.

MATERIALS - STEEL POSTS

- USE 48-INCH LONG STEEL POSTS THAT MEET THE FOLLOWING MINIMUM PHYSICAL REQUIREMENTS:
- COMPOSED OF HIGH STRENGTH STEEL WITH MINIMUM YIELD STRENGTH OF 50,000 PSI.
 - HAVE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND NOMINAL "T" LENGTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (± 8%).
 - HAVE A SOIL STABILIZATION PLATE WITH A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES ATTACHED TO THE STEEL POSTS.
 - PAINTED WITH A WATER BASED BAKED ENAMEL PAINT.

- USE STEEL POSTS WITH A MINIMUM LENGTH OF 48 INCHES, WEIGHING 1.25 POUNDS PER LINEAR FOOT (± 8%) WITH PROJECTIONS TO AID IN FASTENING THE FABRIC. WHEN HEAVY CLAY SOILS ARE PRESENT ON SITE, STEEL POSTS WILL HAVE A METAL SOIL STABILIZATION PLATE WELDED NEAR THE BOTTOM SUCH THAT WHEN THE POST IS DRIVEN TO THE PROPER DEPTH, THE PLATE WILL BE BELOW THE GROUND LEVEL FOR ADDED STABILITY. THE SOIL PLATES SHOULD HAVE THE FOLLOWING CHARACTERISTICS:
- BE COMPOSED OF MINIMUM 15 GAUGE STEEL.
 - HAVE A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES.

GEOTEXTILE FILTER FABRIC

- SILT FENCE MUST BE COMPOSED OF WOVEN GEOTEXTILE FILTER FABRIC THAT CONSISTS OF THE FOLLOWING REQUIREMENTS:
 - COMPOSED OF FIBERS CONSISTING OF LONG CHAIN SYNTHETIC POLYMERS OF AT LEAST 85% BY WEIGHT OF POLYESTERS, POLYESTERS, OR POLYAMIDES THAT ARE FORMED INTO A NETWORK SUCH THAT THE FILAMENTS OR YARNS RETAIN DIMENSIONAL STABILITY RELATIVE TO EACH OTHER;
 - FREE OF ANY TREATMENT OR COATING WHICH MIGHT ADVERSELY ALTER ITS PHYSICAL PROPERTIES AFTER INSTALLATION; FREE OF ANY DEFECTS OR FLAWS THAT SIGNIFICANTLY AFFECT ITS PHYSICAL AND/OR FILTERING PROPERTIES; AND HAVE A MINIMUM WIDTH OF 36-INCHES.
- USE ONLY FABRIC APPEARING ON SC DOT'S QUALIFIED PRODUCTS LISTING (QPL), APPROVAL SHEET #34, MEETING THE REQUIREMENTS OF THE MOST CURRENT EDITION OF THE SC DOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
- 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED.
- FILTER FABRIC SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS.
- FILTER FABRIC SHALL BE INSTALLED AT A MINIMUM OF 24-INCHES ABOVE THE GROUND.

INSTALLATION

- EXCAVATE A TRENCH APPROXIMATELY 6-INCHES WIDE AND 6-INCHES DEEP WHEN PLACING FABRIC BY HAND. PLACE 12-INCHES OF GEOTEXTILE FABRIC INTO THE 6-INCH DEEP TRENCH, EXTENDING THE REMAINING 6-INCHES TOWARDS THE UPSLOPE SIDE OF THE TRENCH. BACKFILL THE TRENCH WITH SOIL OR GRAVEL AND COMPACT.
- BURY 12-INCHES OF FABRIC INTO THE GROUND WHEN PNEUMATICALLY INSTALLING SILT FENCE WITH A SLICING METHOD.
- PURCHASE FABRIC IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS. WHEN JOINTS ARE NECESSARY, WRAPPED THE FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 6-INCH MINIMUM OVERLAP.
- INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. INSTALL POSTS A MINIMUM OF 1 TO 2 INCHES ABOVE THE FABRIC, WITH NO MORE THAN 3-FEET OF THE POST ABOVE THE GROUND. SPACE POSTS TO MAXIMUM 6-FEET CENTERS.
- ATTACH FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED AND PLACED IN A MANNER TO PREVENT SAGGING OR TEARING OF THE FABRIC. IN ALL CASES, TIES SHOULD BE AFFIXED IN NO LESS THAN 4 PLACES.
- INSTALL THE FABRIC A MINIMUM OF 24-INCHES ABOVE THE GROUND. WHEN NECESSARY, THE HEIGHT OF THE FENCE ABOVE GROUND MAY BE GREATER THAN 24-INCHES. IN TIDAL AREAS, EXTRA SILT FENCE HEIGHT MAY BE REQUIRED.
- THE POST HEIGHT WILL BE TWICE THE EXPOSED POST HEIGHT. POST SPACING WILL REMAIN THE SAME AND EXTRA HEIGHT FABRIC WILL BE 4-, 5-, OR 6-FEET TALL.
- LOCATE SILT FENCE CHECKS EVERY 100 FEET MAXIMUM AND AT LOW POINTS.
- INSTALL THE FENCE PERPENDICULAR TO THE DIRECTION OF FLOW AND PLACE THE FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.

INSPECTION AND MAINTENANCE

- THE KEY TO FUNCTIONAL SILT FENCE IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- REGULAR INSPECTIONS OF SILT FENCE SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVEN THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ATTENTION TO SEDIMENT ACCUMULATIONS ALONG THE SILT FENCE IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE SILT FENCE.
- REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH THE SILT FENCE, OR WHERE THE FENCE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE SILT FENCE. INSTALL CHECKS/TIE-BACKS AND/OR REINSTALL SILT FENCE, AS NECESSARY.
- CHECK FOR TEARS WITHIN THE SILT FENCE. AREAS WHERE SILT FENCE HAS BEGUN TO DECOMPOSE, AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE SILT FENCE INEFFECTIVE. REMOVED DAMAGED SILT FENCE AND REINSTALL NEW SILT FENCE IMMEDIATELY.
- SILT FENCE SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

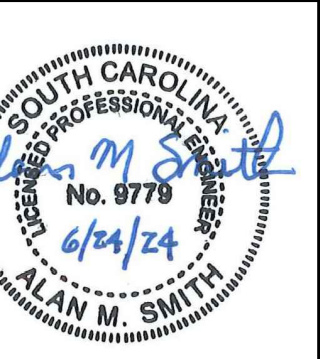
TEMPORARY SILT FENCE

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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
SEDIMENTATION AND EROSION CONTROL DETAILS
(SHEET 3 OF 3)



Date	JUNE 2024
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Project No.	2601-2105
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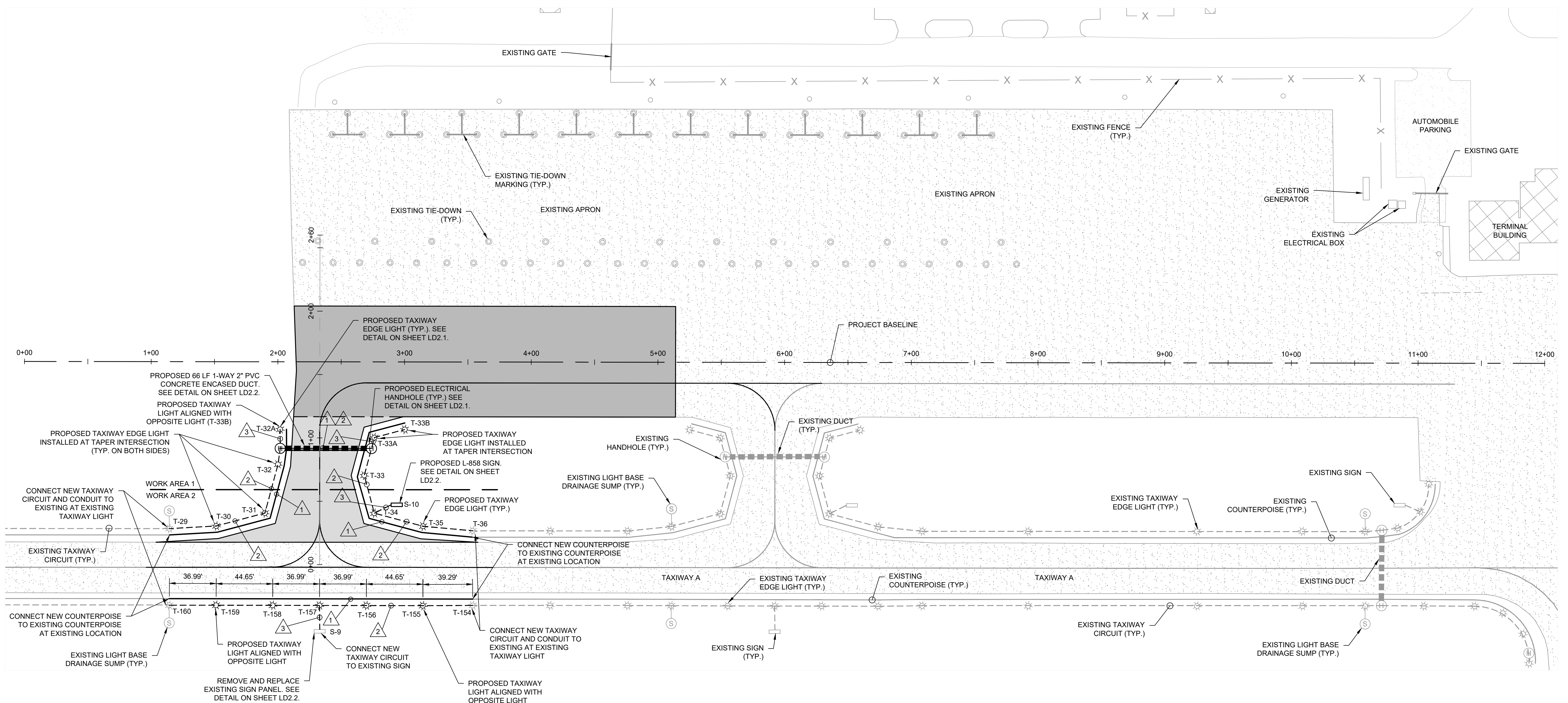
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GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
LIGHTING LAYOUT AND CIRCUIT PLAN



Date	JUNE 2024
Scale	1" = 40'
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Project No.	2601-2105
Sheet No.	

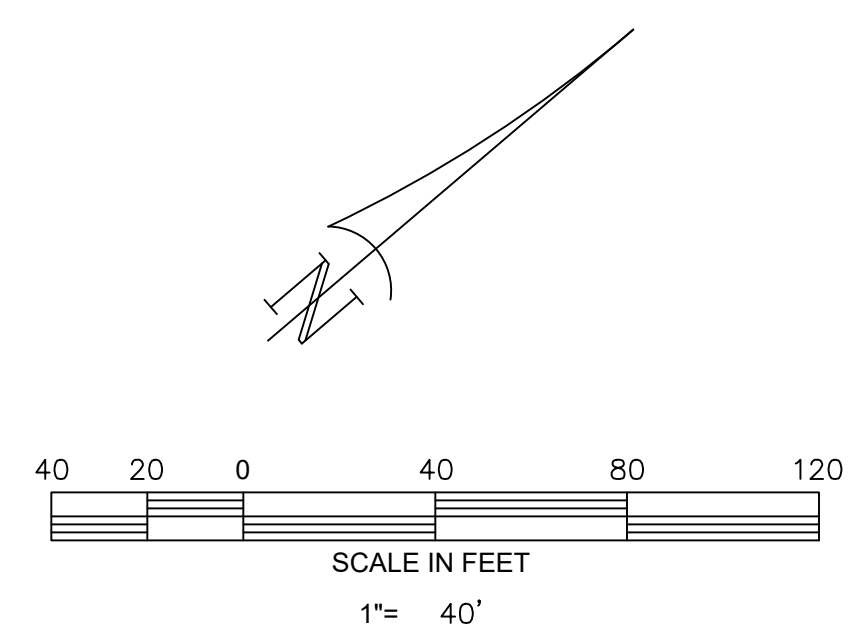
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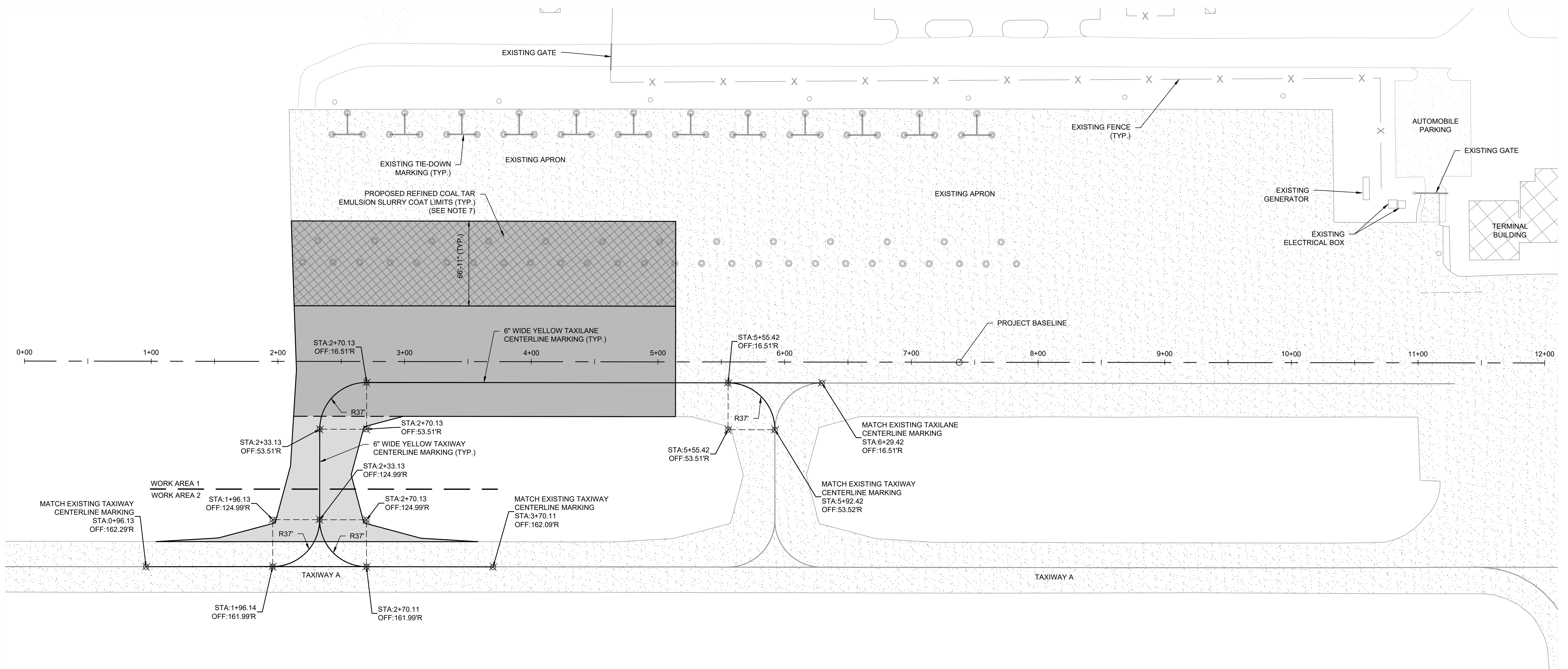


- GENERAL NOTES**
- LOCATIONS OF EXISTING CIRCUITS/CONDUCTORS ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, HANDHOLES, AND DUCT BANKS ARE BASED ON AVAILABLE INFORMATION. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. ANY EXISTING LIGHTING FACILITIES DAMAGED BY THE CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
 - THE EXISTING TAXIWAY LIGHTING SYSTEMS IN ACTIVE AIR OPERATIONS AREAS SHALL BE OPERATIONAL AT THE END OF EACH DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING ANY DAMAGE TO EXISTING LIGHTING SYSTEMS/CIRCUITS TO RESTORE LIGHTING OPERATIONS.
 - NEW INSTALLATIONS WHICH CROSS EXISTING INSTALLATIONS SHALL BE SEPARATED BY A MINIMUM OF 2 INCHES.
 - CONCRETE FOR BASE CAN INSTALLATION AND DUCT ENCASEMENT SHALL BE IN ACCORDANCE WITH SECTION P-610, STRUCTURAL PORTLAND CEMENT CONCRETE.
 - DUCT MARKERS SHALL BE INSTALLED AT THE ENDS OF ALL NEW DUCTS, AND WHERE THE NEW CABLE HOME RUN CHANGES DIRECTION AND EVERY 200' ALONG HOMERUN.
 - COPPER COUNTERPOISE GROUND WIRE SHALL BE CONNECTED TO 3/4" x 10' COPPER CLAD GROUND RODS SPACED AT MINIMUM 500' ALONG COUNTERPOISE TRENCH. WHERE DUCT RUNS CONTINUE BENEATH PAVEMENT, INSTALL THE COUNTERPOISE A MINIMUM OF 4" ABOVE DUCTS ALONG ENTIRE RUN. INSTALL GROUND RODS AT ENDS OF EACH DUCT. DO NOT INSTALL COUNTERPOISE THROUGH DUCT.
 - CONTRACTOR SHALL INSTALL NEW TAXIWAY LIGHTS AND CIRCUITS AS SHOWN. PROPOSED TAXIWAY LIGHTS SHALL BE INSTALLED 10' FROM PAVEMENT EDGE.
 - SEE SHEETS LD2.1 AND LD2.2 FOR LIGHTING INSTALLATION DETAILS.
 - CONTRACTOR TO INSTALL #8 AWG TEMPORARY JUMPER CABLE TO MAINTAIN EXISTING TAXIWAY LIGHTING CIRCUIT IN OPERATION AT ALL TIMES.

LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT
	EXISTING TAXIWAY CIRCUIT
	PROPOSED TAXIWAY CIRCUIT
	EXISTING COUNTERPOISE
	PROPOSED COUNTERPOISE
	EXISTING DUCT
	PROPOSED DUCT
	EXISTING TAXIWAY EDGE LIGHT
	PROPOSED TAXIWAY EDGE LIGHT
	EXISTING LIGHT BASE DRAINAGE SUMP
	EXISTING SIGN
	PROPOSED SIGN
	EXISTING HANDHOLE
	PROPOSED HANDHOLE

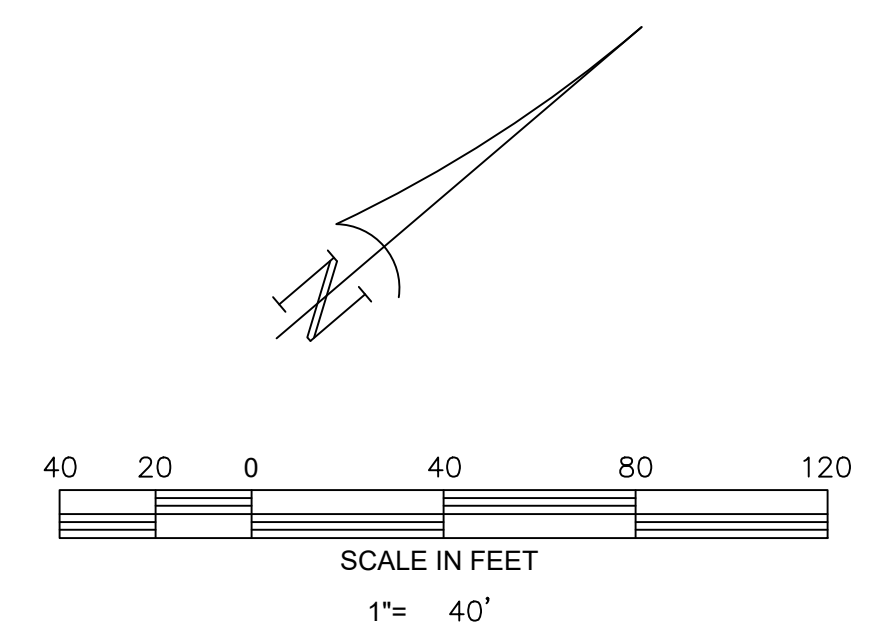
- CIRCUIT LEGEND**
- PROPOSED COUNTERPOISE (1 - #6 AWG BARE COPPER COUNTERPOISE)
 - PROPOSED TAXIWAY CIRCUIT (1-1/C, #8 AWG, 5KV, L-824C INSTALLED IN NEW 2" CONDUIT OR DUCT)
 - PROPOSED TAXIWAY CIRCUIT (2-1/C, #8 AWG, 5KV, L-824C INSTALLED IN NEW 2" CONDUIT OR DUCT)





- GENERAL NOTES:**
- TAXIWAY AND TAXILANE CENTERLINE MARKINGS SHALL BE YELLOW. MARKINGS REQUIRING REFLECTIVE BEADS SHALL HAVE BEADS APPLIED DURING SECOND APPLICATION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT OF MARKINGS. THE RESIDENT PROJECT REPRESENTATIVE SHALL REVIEW LAYOUT PRIOR TO CONTRACTOR BEGINNING PAINTING OPERATIONS.
 - REFLECTIVE BEADS SHALL BE APPLIED ON ALL APPLICATIONS OF FINAL MARKINGS (EXCEPT BLACK AND TIE DOWN POSITION MARKINGS).
 - ANY EXISTING MARKINGS TO REMAIN THAT ARE DAMAGED BY CONTRACTOR'S OPERATIONS/TACK/TRAFFIC SHALL BE REPAIRED AT NO COST TO OWNER.
 - WHERE TYING NEW MARKING TO EXISTING MARKING, CONTRACTOR SHALL VERIFY LAYOUT OF EXISTING MARKING.
 - MARKING WILL BE APPLIED IN TWO APPLICATIONS. THE FIRST APPLICATION SHALL BE AT THE RATE OF 230 SF/GAL. AFTER A 30 DAY CURING PERIOD A SECOND APPLICATION AT THE RATE OF 115 SF/GAL OF MARKING WITH REFLECTIVE BEADS (AS REQUIRED) SHALL BE APPLIED.
 - EXISTING CRACK 1/4" OR GREATER SHALL BE CLEANED AND SEALED PRIOR TO APPLICATION OF SEAL COAT. EXISTING CENTERLINE MARKING TO BE REMOVED. SEE SECTION P-101 OF SPECIFICATIONS.

LEGEND	
	EXISTING ASPHALT PAVEMENT
	PROPOSED TAXIWAY ASPHALT PAVEMENT
	PROPOSED APRON ASPHALT PAVEMENT
	PROPOSED SLURRY COAT LIMITS
	EXISTING TIE DOWN

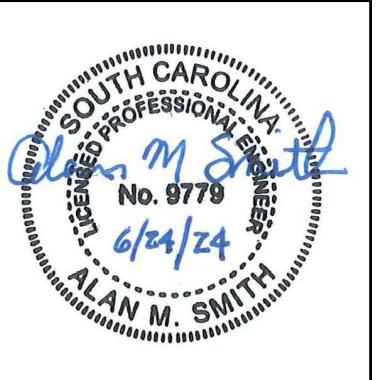


TALBERT & BRIGHT
 ENGINEERING & PLANNING CONSULTANTS
 4810 SHELLEY DRIVE
 WILMINGTON, NC 28405
 PHONE: 910-763-5350
 FAX: 910-762-6281
 EMAIL: TBILLM@TBILLM.COM
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GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
PAVEMENT MARKING AND TIE-DOWN PLAN



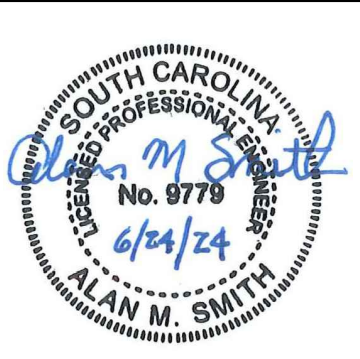
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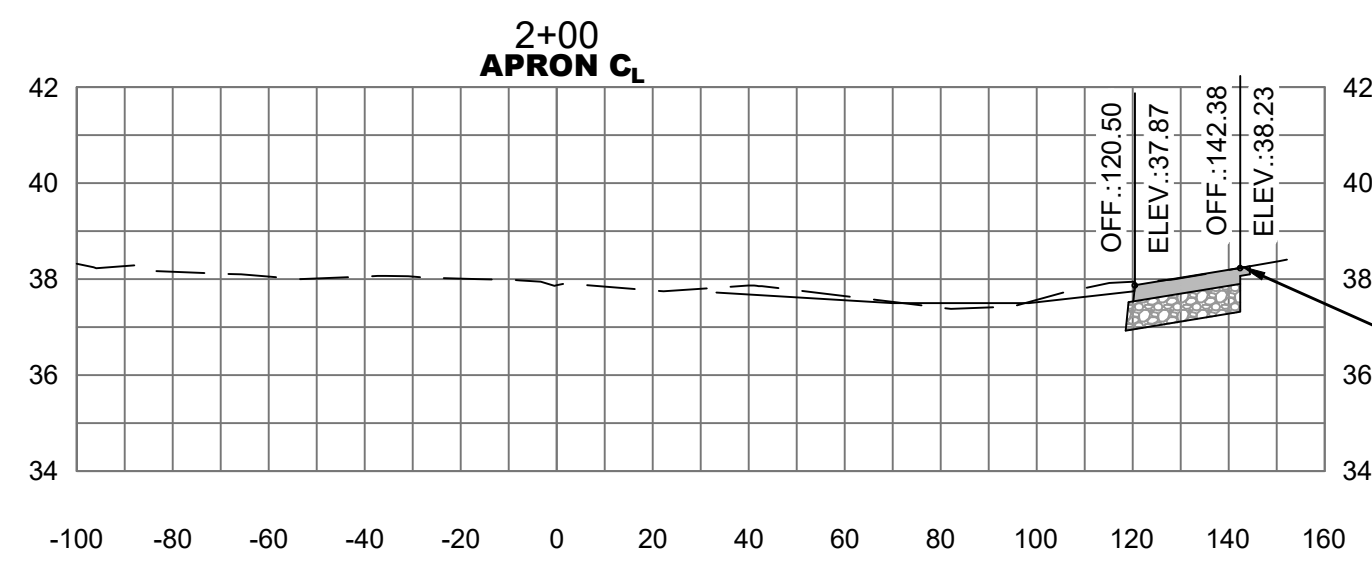
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GEORGETOWN COUNTY AIRPORT
 GEORGETOWN, SOUTH CAROLINA
APRON RECONSTRUCTION (SCHEDULE II)
CROSS SECTIONS

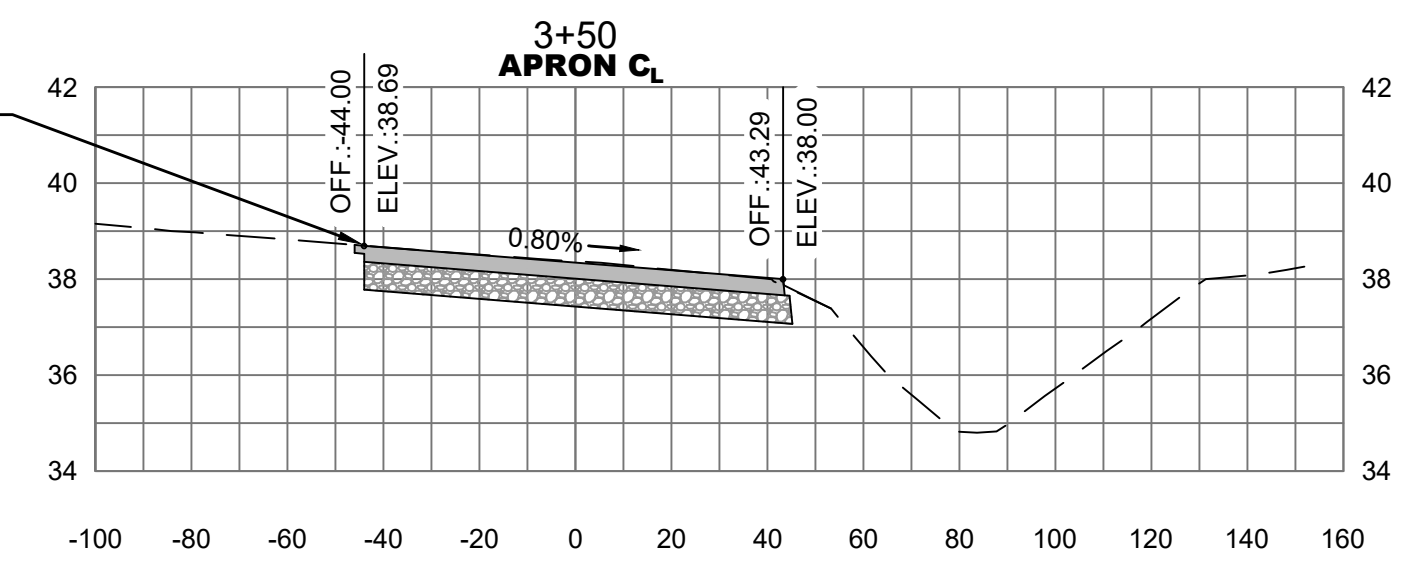


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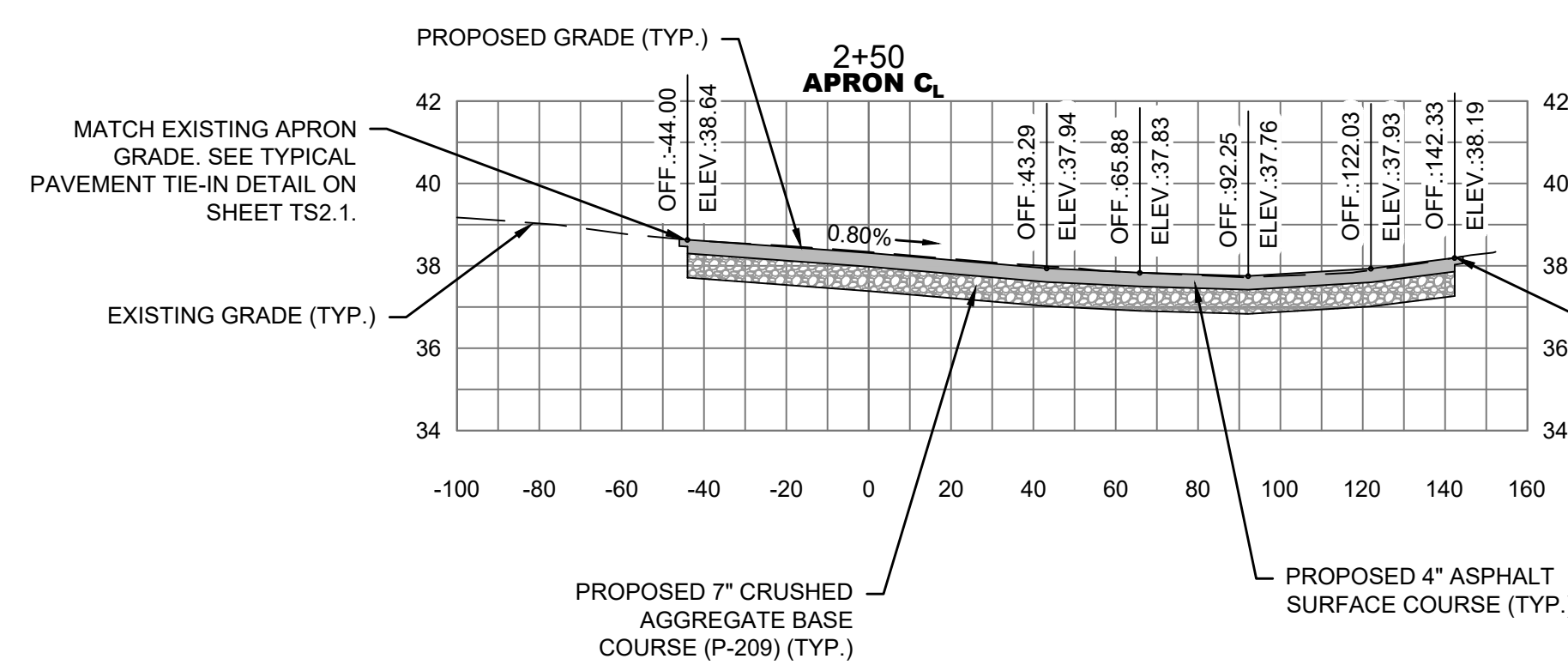
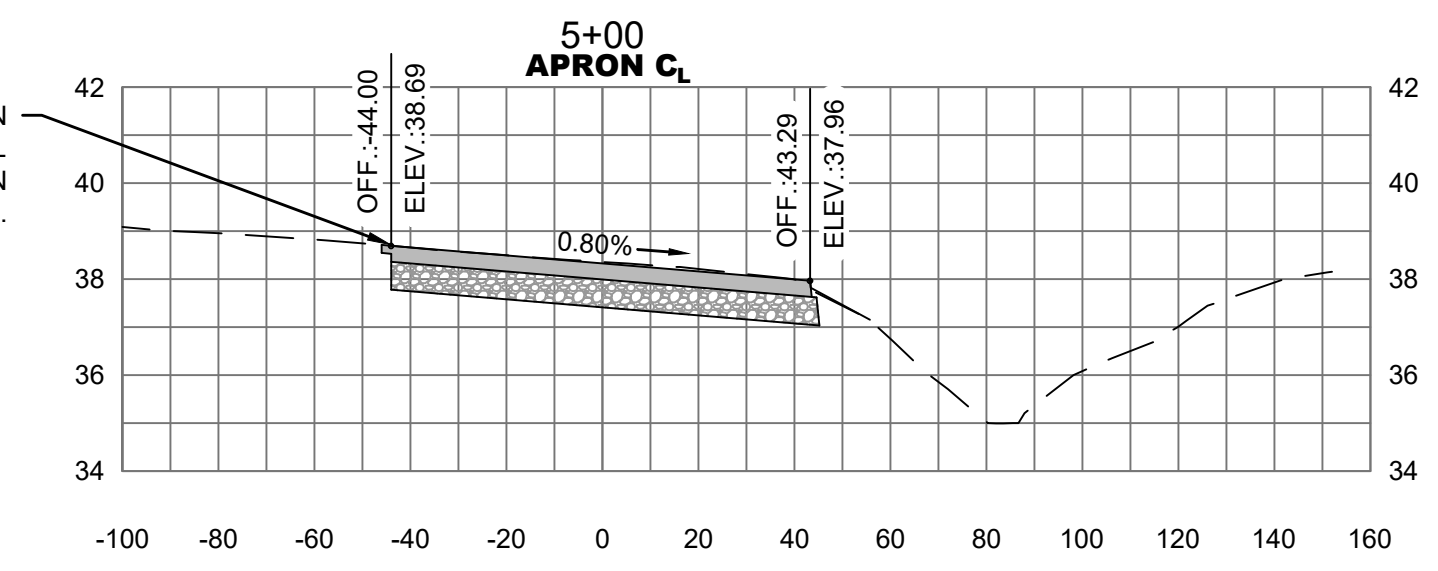
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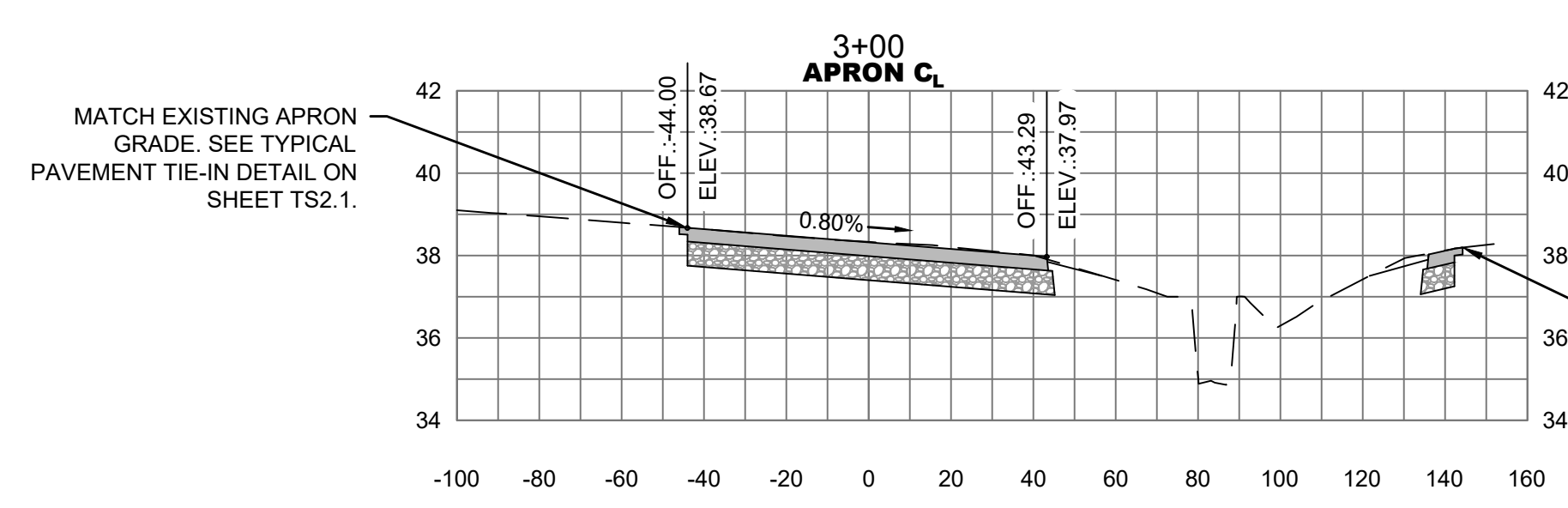
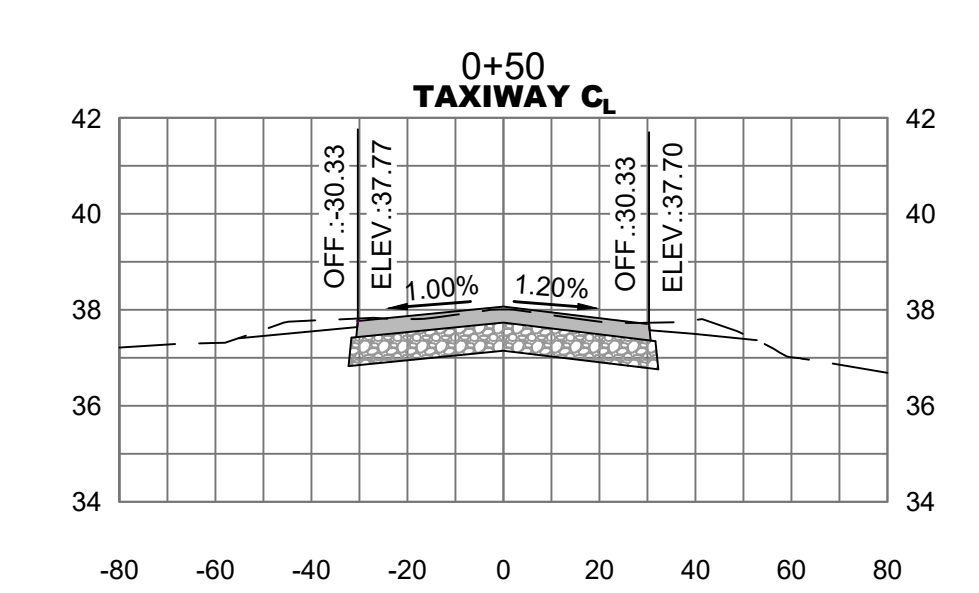
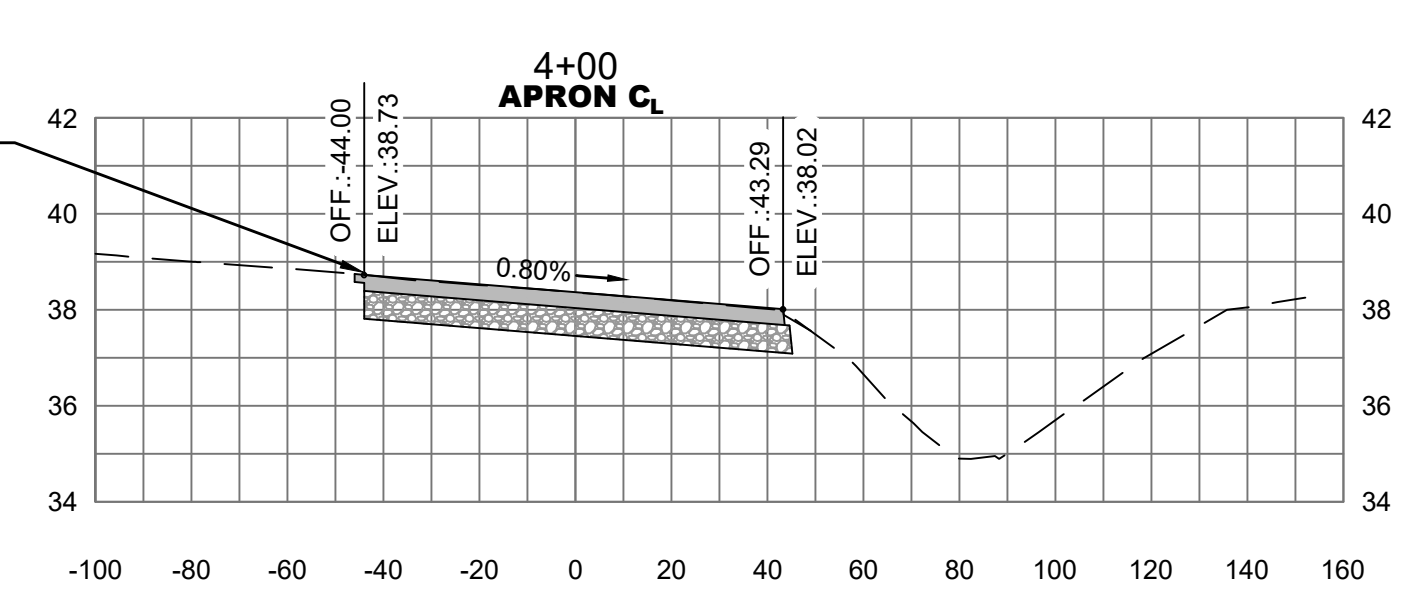
MATCH EXISTING APRON GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.
 MATCH EXISTING TAXIWAY GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.



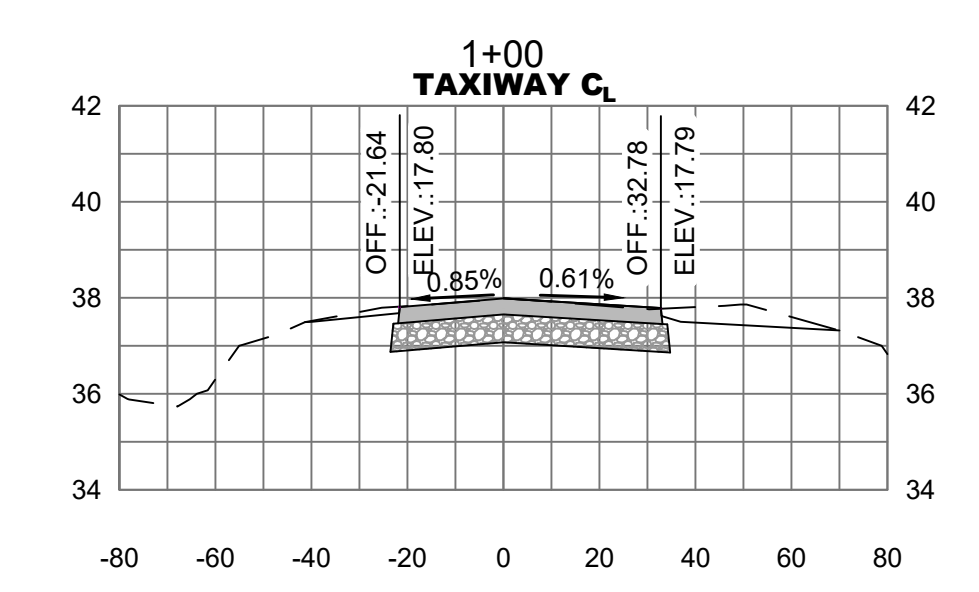
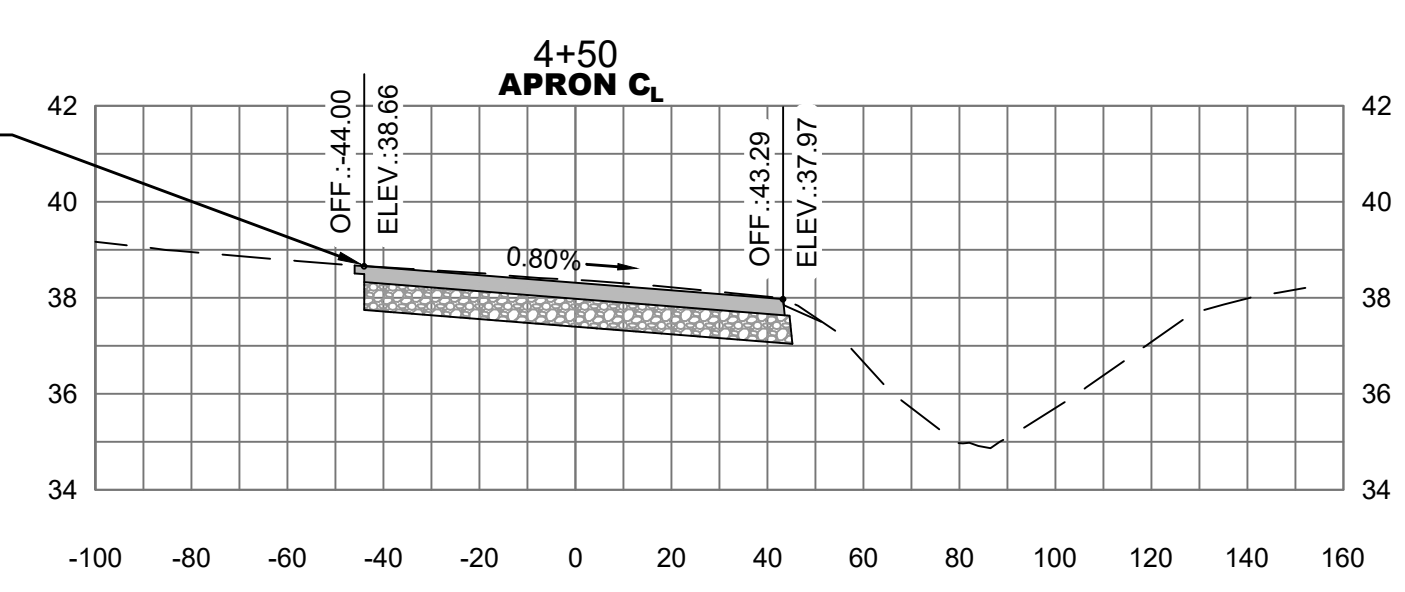
MATCH EXISTING APRON GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.



MATCH EXISTING APRON GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.
 MATCH EXISTING TAXIWAY GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.



MATCH EXISTING APRON GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.
 MATCH EXISTING TAXIWAY GRADE. SEE TYPICAL PAVEMENT TIE-IN DETAIL ON SHEET TS2.1.



LEGEND

	PROPOSED 4" ASPHALT SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE

NOTE: FOR BASELINE INFORMATION SEE SHEET SL2.1.