

GEORGETOWN COUNTY AIRPORT (GGE)
GEORGETOWN, SOUTH CAROLINA



A1.1

THE INTENT OF THIS PLAN IS TO ESTABLISH CERTAIN SAFETY REQUIREMENTS THAT MUST BE ADHERED TO BY THE CONTRACTOR DURING CONSTRUCTION OF THIS PROJECT. RUNWAY 5-23 AND PARTIAL CLOSURE OF TAXIWAY 'A' WILL BE REQUIRED FOR WORK WITHIN RUNWAY 5-23 ROFZ. RUNWAY 5-23 SHALL BE CLOSED AT 9:00 PM AND REOPENED BY 6:00 AM THE FOLLOWING MORNING. RUNWAY 11-29 SHALL REMAIN OPEN AT ALL TIMES. SEE PROJECT SPECIAL PROVISIONS FOR CLOSURE REQUIREMENTS AND DETAILS. THE AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL OTHER TIMES. SEE SEQUENCE OF CONSTRUCTION ON PHASING PLANS.

- CONTACTS ARE:
AIRPORT DIRECTOR MR. JIM TAYLOR 843-545-3638

15. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA OF AN ACTIVE RUNWAY OR TAXIWAY OBJECT FREE AREA OF AN ACTIVE TAXIWAY. ANY WORK WITHIN AN ACTIVE RUNWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE RUNWAY. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN A ACTIVE TAXIWAY OBJECT FREE AREA WILL REQUIRE CLOSURE OF THE EFFECTED SECTION OF TAXIWAY. PULLBACKS FOR MEN AND EQUIPMENT WITHIN THE RUNWAY OBSTACLE FREE ZONE WILL NOT BE ALLOWED (SEE GENERAL NOTE 1).

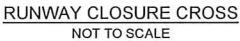


1. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
2. LOW PROFILE LIGHTED BARRICADES SHALL BE MULTI-BARRIER AIRPORT RUNWAY BARRICADE AR10X96 AND SOLAR LIGHTS SHALL BE PROVIDED FOR THE BARRICADES AS MANUFACTURED BY OFF THE WALL PRODUCTS, LLC. OR APPROVED EQUAL. THE CONTRACTOR SHALL PROVIDE WATER NECESSARY TO FILL THE BARRICADES AND ENSURE ADEQUATE WATER HAS BEEN PROVIDED TO ANCHOR THE BARRICADES IN PLACE.
3. CONTRACTOR SHALL CHECK LIGHTS DAILY TO VERIFY THAT THEY ARE IN WORKING CONDITION AND SHALL REPLACE LIGHTS AS REQUIRED.
4. CONTRACTOR SHALL INSTALL BARRICADES AT LOCATIONS SHOWN ON INDIVIDUAL PLAN SHEETS. BARRICADES SHALL BE MOVED AND RELOCATED AS REQUIRED. BARRICADES SHALL BE INSTALLED WITH MAXIMUM 5' SPACE BETWEEN BARRICADE AND CONTRACTOR SHALL LEAVE ONE 15' SPACE FOR EMERGENCY VEHICLES. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST.

10. SEE PROJECT SPECIAL PROVISIONS FOR PROTECTION OF UTILITIES.


NOTAMS (NOTICE TO AIRMEN)

THE AIRPORT MANAGEMENT WILL ISSUE THE NECESSARY NOTAMS TO REFLECT HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS. ACTIVE NOTAMS SHALL BE REVIEWED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS.



RUNWAY CLOSURE CROSS NOTES

1. PLACE RUNWAY CLOSURE CROSSES ON THE RUNWAY NUMERALS ON THE RUNWAY CENTERLINE OR OFF THE END OF THE RUNWAY PAVEMENT WITHIN THE RUNWAY SAFETY AREA BEFORE BEGINNING WORK IN A CLOSED OPERATION AREA.
2. CROSSES SHALL BE AVIATION YELLOW IN COLOR AND CONSTRUCTED OF FABRIC, PLASTIC OR OTHER MATERIAL APPROVED BY THE ENGINEER. CONTRACTOR SHALL PROVIDE A MEANS OF SECURING CROSSES IN PLACE.
3. CONTRACTOR SHALL INSPECT CROSSES FREQUENTLY AND MAKE ANY NECESSARY REPAIRS. ALL LIGHTS SHALL BE INSPECTED FREQUENTLY AND BATTERIES REPLACED AS NECESSARY.
4. RUNWAY CLOSURE SHALL BE REQUIRED WHEN WORKING WITHIN THE RUNWAY OBSTACLE FREE ZONE (ROFZ) AND RUNWAY SAFETY AREA (RSA). CONTRACTOR SHALL INSTALL CLOSURE CROSSES PRIOR TO COMMENCING WORK AND SHALL REMOVE CROSSES AT THE END OF EACH CLOSURE PERIOD.
5. ALL WORK ASSOCIATED WITH SUPPLYING CLOSURE CROSSES AND LIGHTED BARRICADES, INSTALLING AND REMOVING THEM WHEN WORKING WITHIN ROFZ, MAINTAINING THE CLOSURE CROSSES & BARRICADES AND ALL OTHER INCIDENTALS NECESSARY SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR "RUNWAY CLOSURE CROSSES AND LIGHTED BARRICADES".

						GEORGETOWN COUNTY AIRPORT GEORGETOWN, SOUTH CAROLINA					
						HANGAR TAXILANE IMPROVEMENTS AND HANGAR SITEWORK					
						PROJECT LAYOUT AND SAFETY PLAN NOTES AND DETAILS					
Date	FEBRUARY 2023										
Scale	NONE										
Drawn	NT										
Checked	AMS										
Project No.	2601-2101										
Sheet No.	A1.3										

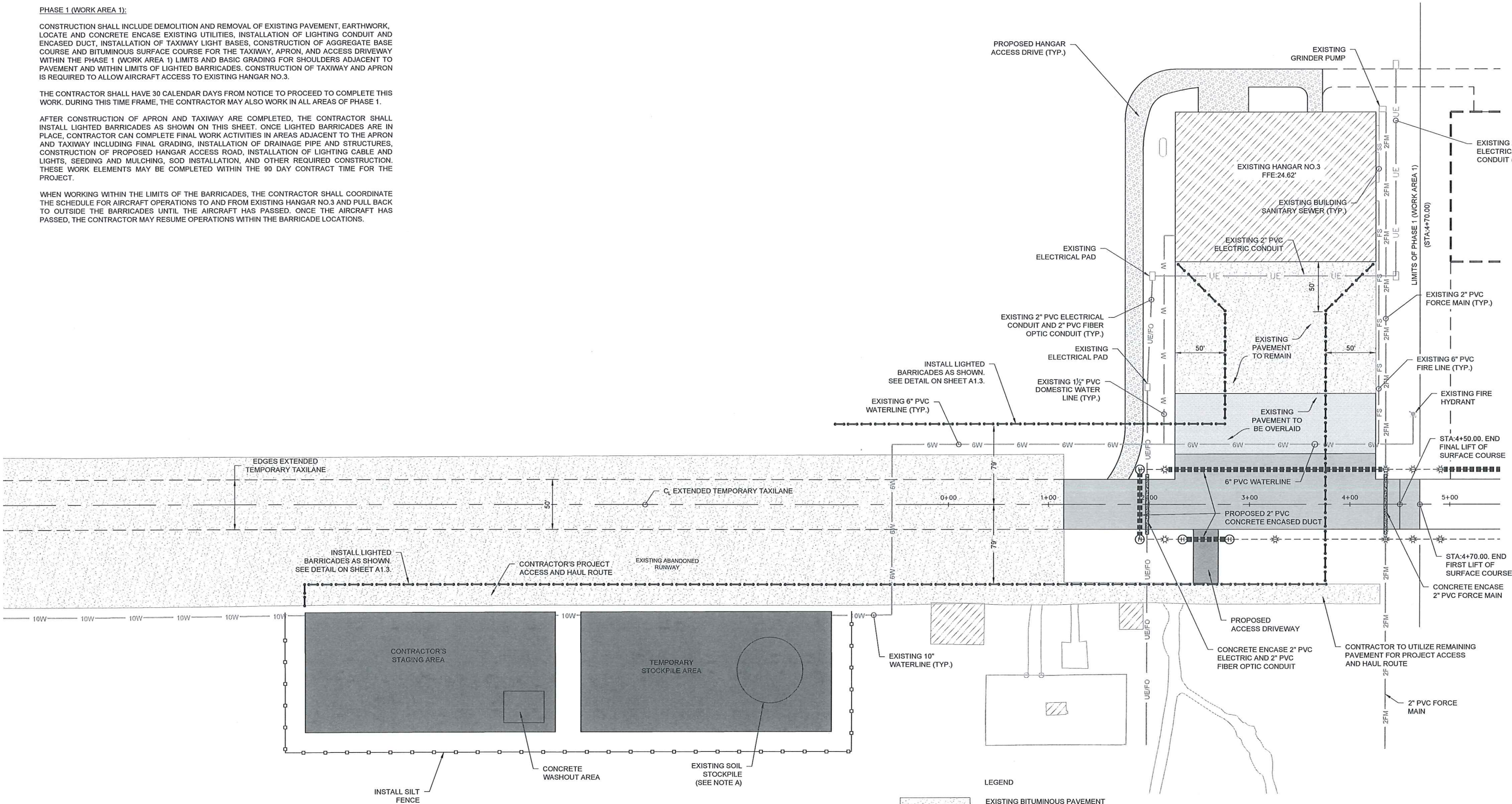
PHASE 1 (WORK AREA 1):

CONSTRUCTION SHALL INCLUDE DEMOLITION AND REMOVAL OF EXISTING PAVEMENT, EARTHWORK, LOCATE AND CONCRETE ENCASE EXISTING UTILITIES, INSTALLATION OF LIGHTING CONDUIT AND ENCASED DUCT, INSTALLATION OF TAXIWAY LIGHT BASES, CONSTRUCTION OF AGGREGATE BASE COURSE AND BITUMINOUS SURFACE COURSE FOR THE TAXIWAY, APRON, AND ACCESS DRIVEWAY WITHIN THE PHASE 1 (WORK AREA 1) LIMITS AND BASIC GRADING FOR SHOULDERS ADJACENT TO PAVEMENT AND WITHIN LIMITS OF LIGHTED BARRICADES. CONSTRUCTION OF TAXIWAY AND APRON IS REQUIRED TO ALLOW AIRCRAFT ACCESS TO EXISTING HANGAR NO.3.

THE CONTRACTOR SHALL HAVE 30 CALENDAR DAYS FROM NOTICE TO PROCEED TO COMPLETE THIS WORK. DURING THIS TIME FRAME, THE CONTRACTOR MAY ALSO WORK IN ALL AREAS OF PHASE 1.

AFTER CONSTRUCTION OF APRON AND TAXIWAY ARE COMPLETED, THE CONTRACTOR SHALL INSTALL LIGHTED BARRICADES AS SHOWN ON THIS SHEET. ONCE LIGHTED BARRICADES ARE IN PLACE, CONTRACTOR CAN COMPLETE FINAL WORK ACTIVITIES IN AREAS ADJACENT TO THE APRON AND TAXIWAY INCLUDING FINAL GRADING, INSTALLATION OF DRAINAGE PIPE AND STRUCTURES, CONSTRUCTION OF PROPOSED HANGAR ACCESS ROAD, INSTALLATION OF LIGHTING CABLE AND LIGHTS, SEEDING AND MULCHING, SOD INSTALLATION, AND OTHER REQUIRED CONSTRUCTION. THESE WORK ELEMENTS MAY BE COMPLETED WITHIN THE 90 DAY CONTRACT TIME FOR THE PROJECT.

WHEN WORKING WITHIN THE LIMITS OF THE BARRICADES, THE CONTRACTOR SHALL COORDINATE THE SCHEDULE FOR AIRCRAFT OPERATIONS TO AND FROM EXISTING HANGAR NO.3 AND PULL BACK TO OUTSIDE THE BARRICADES UNTIL THE AIRCRAFT HAS PASSED. ONCE THE AIRCRAFT HAS PASSED, THE CONTRACTOR MAY RESUME OPERATIONS WITHIN THE BARRICADE LOCATIONS.



NOTES:

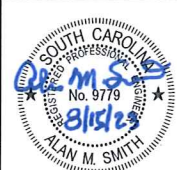
- FOR LOCATIONS OF EXISTING UTILITIES SEE EX1.1. FOR UTILITIES TO BE CONCRETE ENCASED SEE SHEET G1.1 AND FOR DETAILS SEE SHEET TS1.1.
- FOR LOCATIONS OF PROPOSED ELECTRICAL CONDUIT, CONCRETE ENCASED DUCT, AND LIGHT BASES SEE SHEET L1.1 AND SHEET LD1.1 AND LD1.2.
- CONTRACTOR SHALL UTILIZE PROPOSED TAXIWAY PAVEMENT FOR INSTALLATION OF CONTROL STRIP FOR P-401 BITUMINOUS PAVEMENT.

NOTE A - EXISTING SOIL STOCKPILE: THE CONTRACTOR SHALL UTILIZE THE SOIL IN THE EXISTING STOCKPILE FOR REQUIRED FILL OPERATIONS.

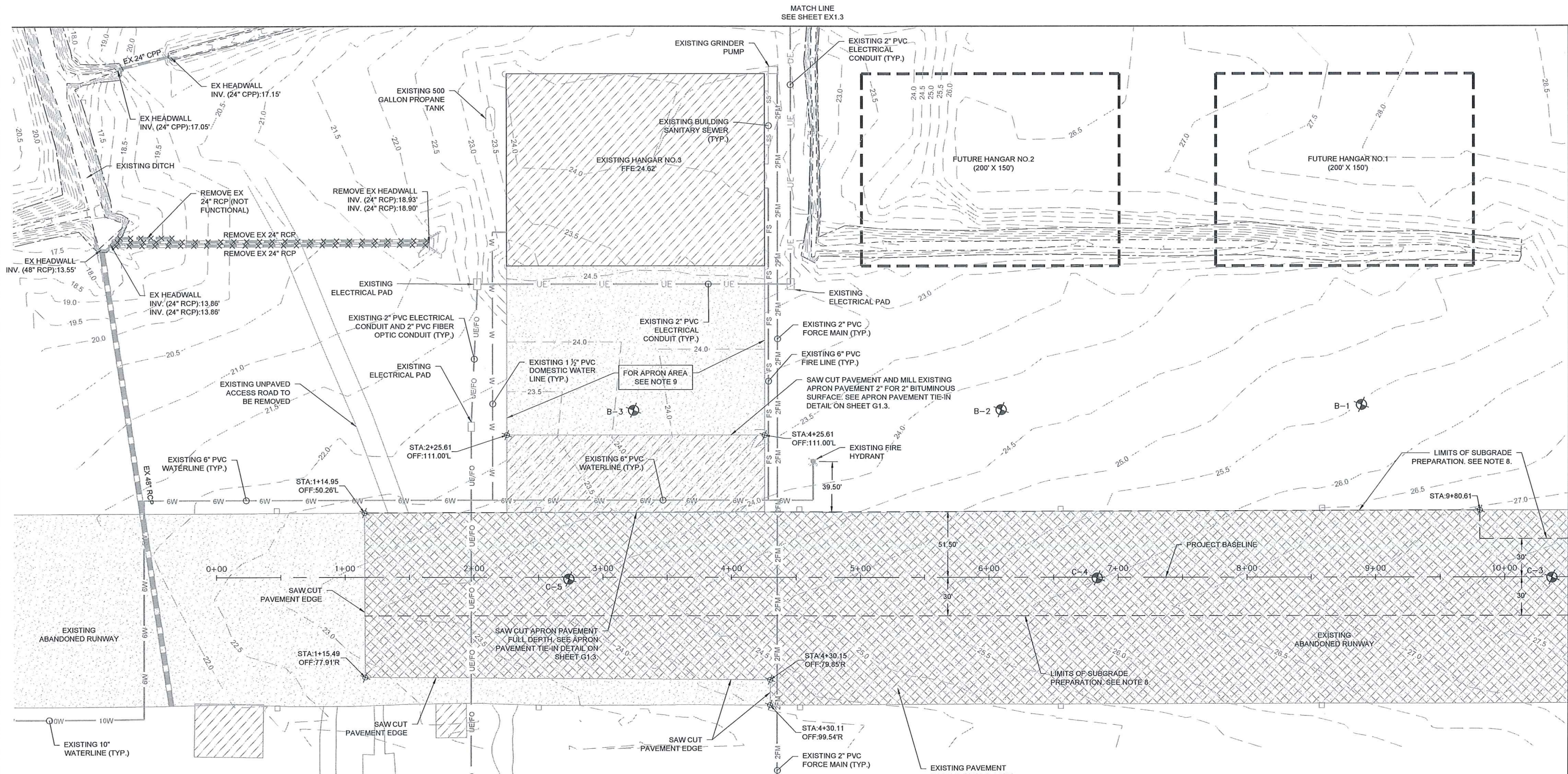
THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
PHASING PLAN - PHASE 1 (WORK AREA 1)

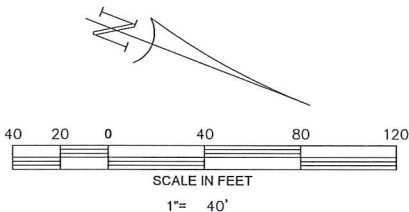


Date: FEBRUARY 2023
Scale: 1" = 50'
Drawn: NT
Checked: AMS
Project No. 2601-2101
Sheet No.



GENERAL NOTES:

- SOIL BORING LOGS AND TEST DATA PERFORMED BY SOIL CONSULTANTS, INC. ARE CONTAINED IN APPENDIX 'F' OF THE PROJECT SPECIFICATIONS AND ARE FOR INFORMATIONAL PURPOSES ONLY. SOIL BORING LOCATIONS SHOWN ARE APPROXIMATE.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON SHEETS SL1.1 AND SL1.2.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED SEPTEMBER, 2022.
- ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- THE CONTRACTOR SHALL STOCKPILE SUFFICIENT MILLINGS FROM REMOVAL OF ABANDONED RUNWAY TO CONSTRUCT PROPOSED HANGAR ACCESS DRIVE. ALL EXCESS MILLINGS SHALL BE DISPOSED OFF AIRPORT PROPERTY AT GEORGETOWN COUNTY SITE LOCATED AT 1324 AVIATION BLVD. CONTRACTOR SHALL DUMP MILLINGS AT SPECIFIED SITE. COUNTY STAFF SHALL BE RESPONSIBLE FOR STOCKPILE MAINTENANCE.
- LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. APPROXIMATE LOCATION OF EXISTING UTILITIES HAS BEEN PROVIDED BY OWNER. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE AND SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
- AFTER EXISTING BITUMINOUS PAVEMENT HAS BEEN REMOVED AND EXCAVATED TO PROPOSED SUBGRADE, THE PROPOSED SUBGRADE SHALL BE BROKEN UP AND ROOT RAKED TO A DEPTH OF TWO (2) FEET BELOW THE EXISTING SURFACE TO APPROXIMATELY FIVE (5) FEET OUTSIDE THE PAVING LIMITS. STUMPS, ROOTS, AND OTHER ORGANIC MATERIALS ARE NOT ACCEPTABLE IN THE SUBGRADE AND SHALL BE REMOVED. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE PROJECT AND DISPOSED OF OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION. CONTRACTOR SHALL LOCATE 2\"/>
- THE EXISTING APRON AREA HAS BEEN INSTALLED BY OTHERS. FOR THE PROPOSED OVERLAY AREA, THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXISTING SURFACE COURSE GRADES AND COMPLETE THE BITUMINOUS LEVELING LIFT PRIOR TO INSTALLATION OF FINAL TWO (2) LIFTS OF BITUMINOUS SURFACE COURSE. SEE G1.1 FOR APPROXIMATE AREA OF LEVELING REQUIRED AND G1.3 FOR APRON PAVEMENT TIE-IN DETAIL.



LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	EXISTING BUILDING
	EXISTING PAVEMENT TO BE REMOVED
	EXISTING PAVEMENT TO BE OVERLAID
	EXISTING CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	APPROXIMATE BORING LOCATION
	FUTURE HANGAR
	LIMITS OF SUBGRADE PREPARATION

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
EXISTING CONDITIONS AND DEMOLITION PLAN
(SHEET 1 OF 4)



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

EX1.1

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION

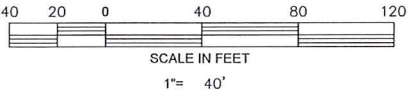
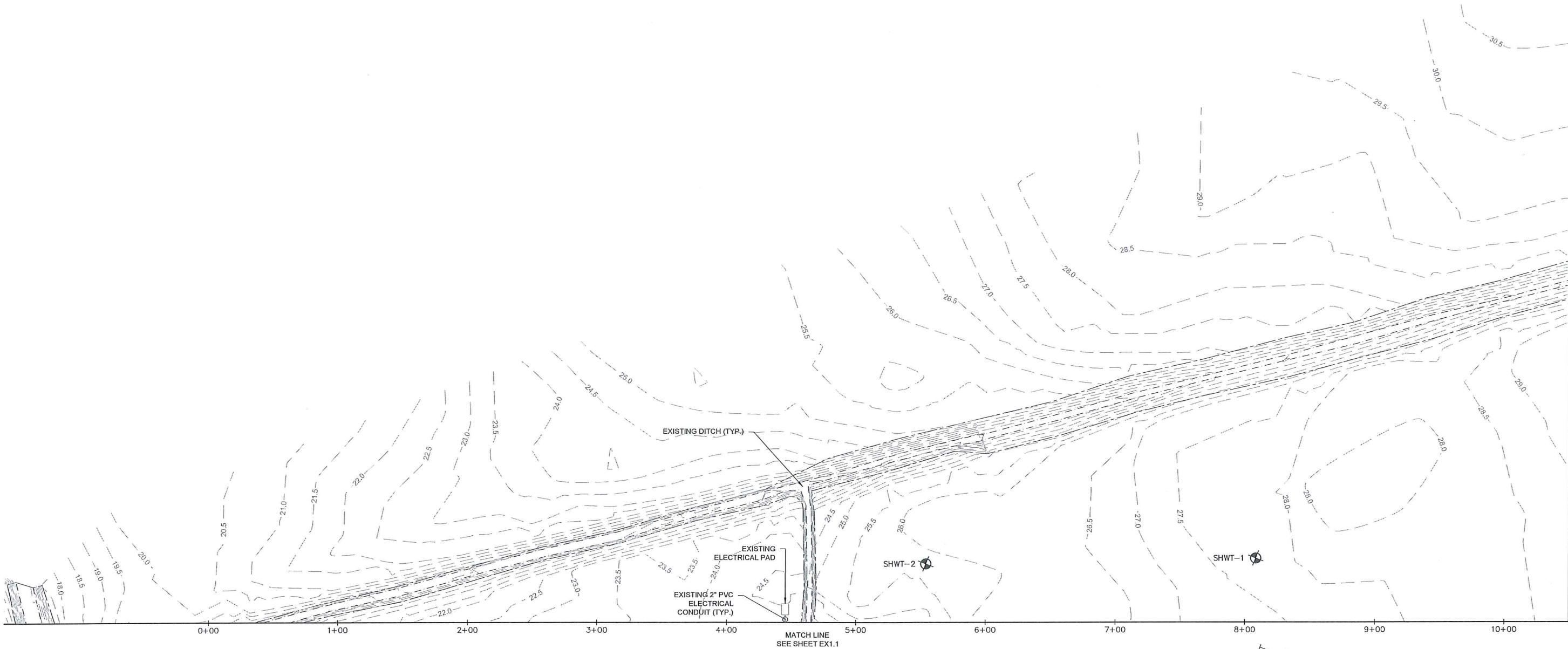
REVISIONS	
REV. NO.	DESCRIPTION

REVISIONS	
REV. NO.	DESCRIPTION
	</

GENERAL NOTES:

- SOIL BORING LOGS AND TEST DATA PERFORMED BY SOIL CONSULTANTS, INC. ARE CONTAINED IN APPENDIX 'F' OF THE PROJECT SPECIFICATIONS AND ARE FOR INFORMATIONAL PURPOSES ONLY. SOIL BORING LOCATIONS SHOWN ARE APPROXIMATE.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON SHEETS SL1.1 AND SL1.2.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED SEPTEMBER, 2022.
- ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
- LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. APPROXIMATE LOCATION OF EXISTING UTILITIES HAS BEEN PROVIDED BY OWNER. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.

LEGEND	
	EXISTING CONTOUR
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	APPROXIMATE BORING LOCATION



MATCH LINE STA: 10+50.00
SEE SHEET EX1.4

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
EXISTING CONDITIONS AND DEMOLITION PLAN
(SHEET 3 OF 4)



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

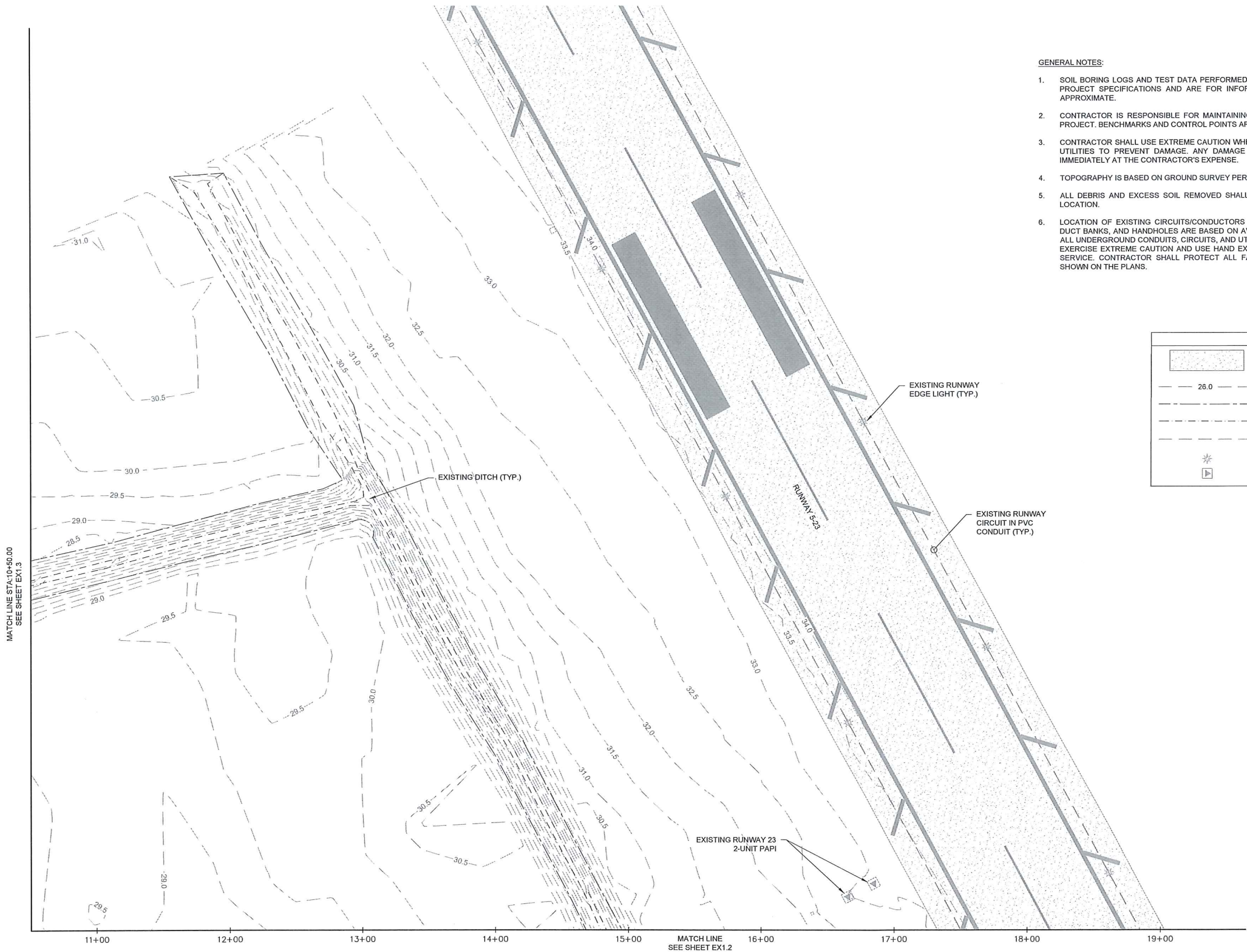
EX1.3

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

REV. NO.	DESCRIPTION	DATE

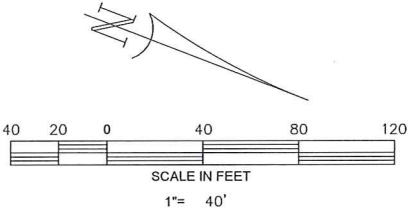
TALBERT & BRIGHT, INC.
© 2023

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILLM@TBLM.COM
SC LICENSE NO. 000366



- GENERAL NOTES:**
1. SOIL BORING LOGS AND TEST DATA PERFORMED BY SOIL CONSULTANTS, INC. ARE CONTAINED IN APPENDIX 'F' OF THE PROJECT SPECIFICATIONS AND ARE FOR INFORMATIONAL PURPOSES ONLY. SOIL BORING LOCATIONS SHOWN ARE APPROXIMATE.
 2. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON SHEETS SL1.1 AND SL1.2.
 3. CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES AND UTILITIES TO PREVENT DAMAGE. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 4. TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED SEPTEMBER, 2022.
 5. ALL DEBRIS AND EXCESS SOIL REMOVED SHALL BE DISPOSED OFF AIRPORT PROPERTY IN A PROPERLY PERMITTED LOCATION.
 6. LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	EXISTING CONTOUR
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	EXISTING RUNWAY CIRCUIT
	EXISTING RUNWAY EDGE LIGHT
	EXISTING PAPI



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILLI@TBILUM.COM
SC LICENSE NO. C03366

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

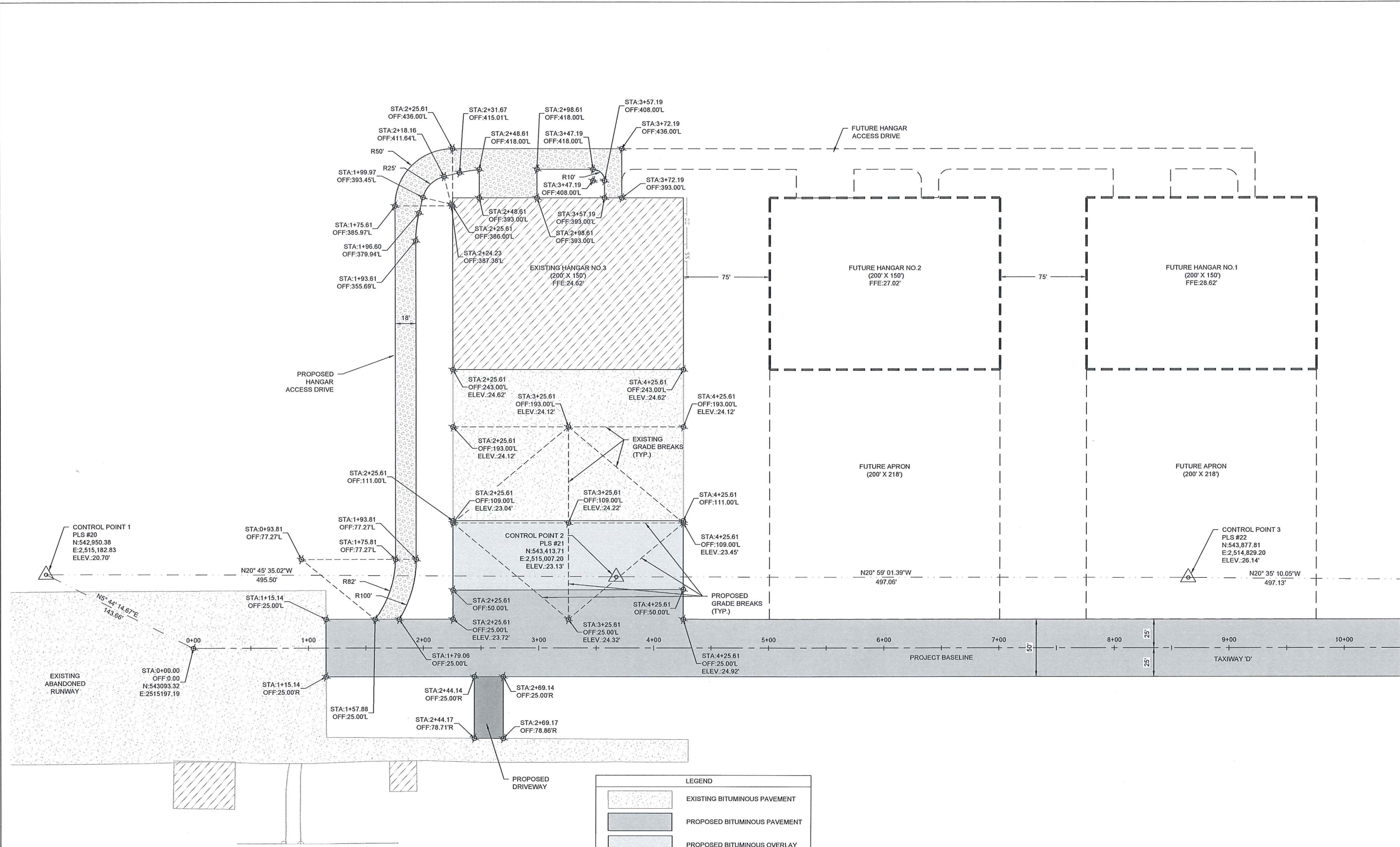
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

**HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK**

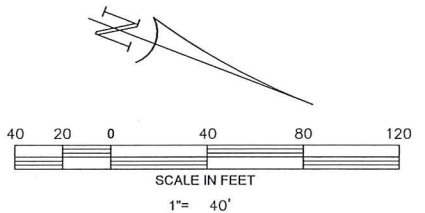
EXISTING CONDITIONS AND DEMOLITION PLAN
(SHEET 4 OF 4)

Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	EX1.4



- GENERAL NOTES:
- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED SEPTEMBER, 2022. ALL STATIONS AND OFFSETS SHOWN ON THE PLANS ARE BASED ON 'PROJECT BASELINE'.
 - ALL BEARINGS AND COORDINATES ARE BASED ON SOUTH CAROLINA STATE PLANE COORDINATE SYSTEM 1983. ELEVATIONS ARE BASED ON NAVD88.
 - SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
 - SEE G SERIES SHEETS FOR GRADING AND DRAINAGE PLAN.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS OVERLAY
	PROPOSED HANGAR ACCESS DRIVE
	PROPOSED DRIVEWAY
	FUTURE HANGAR BY OTHERS
	FUTURE APRON / ACCESS DRIVE



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-5281
EMAIL: TBILL@TBILL.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

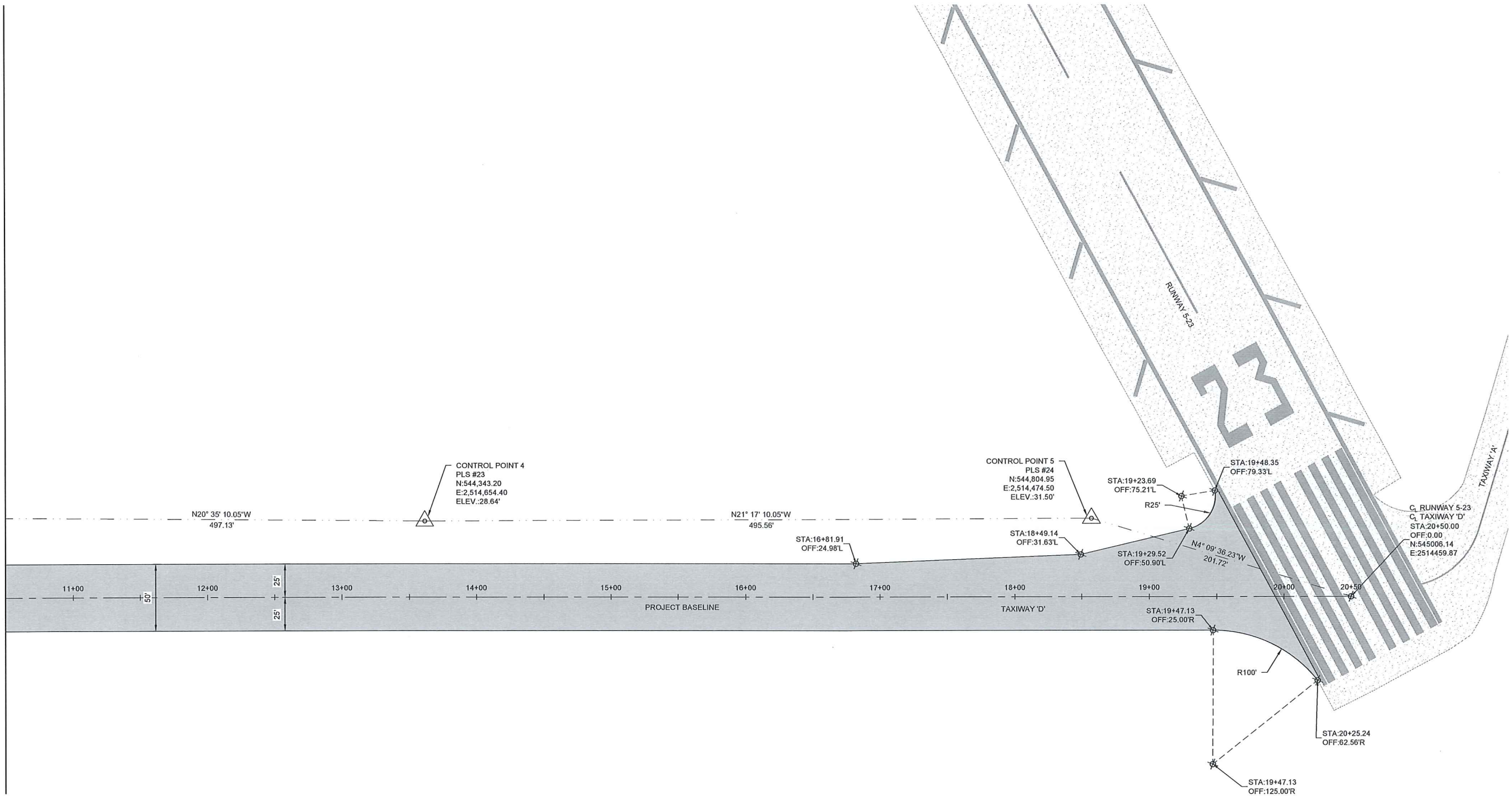
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
SURVEY LAYOUT PLAN
(SHEET 1 OF 2)

ALAN M. SMITH
No. 9779
SOUTH CAROLINA
PROFESSIONAL ENGINEER

Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	SL1.1

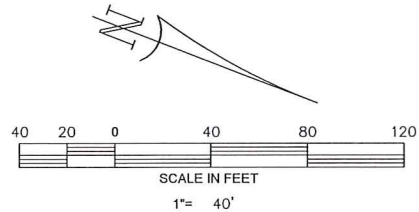
MATCH LINE STA:10+50.00
SEE SHEET SL1.1



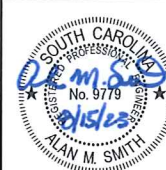
GENERAL NOTES:

- TOPOGRAPHY IS BASED ON GROUND SURVEY PERFORMED BY PARKER LAND SURVEYING, LLC, DATED SEPTEMBER, 2022. ALL STATIONS AND OFFSETS SHOWN ON THE PLANS ARE BASED ON 'PROJECT BASELINE'.
- ALL BEARINGS AND COORDINATES ARE BASED ON SOUTH CAROLINA STATE PLANE COORDINATE SYSTEM 1983. ELEVATIONS ARE BASED ON NAVD88.
- SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
- SEE G SERIES SHEETS FOR GRADING AND DRAINAGE PLAN.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT



GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
SURVEY CONTROL PLAN
(SHEET 2 OF 2)



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

SL1.2

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

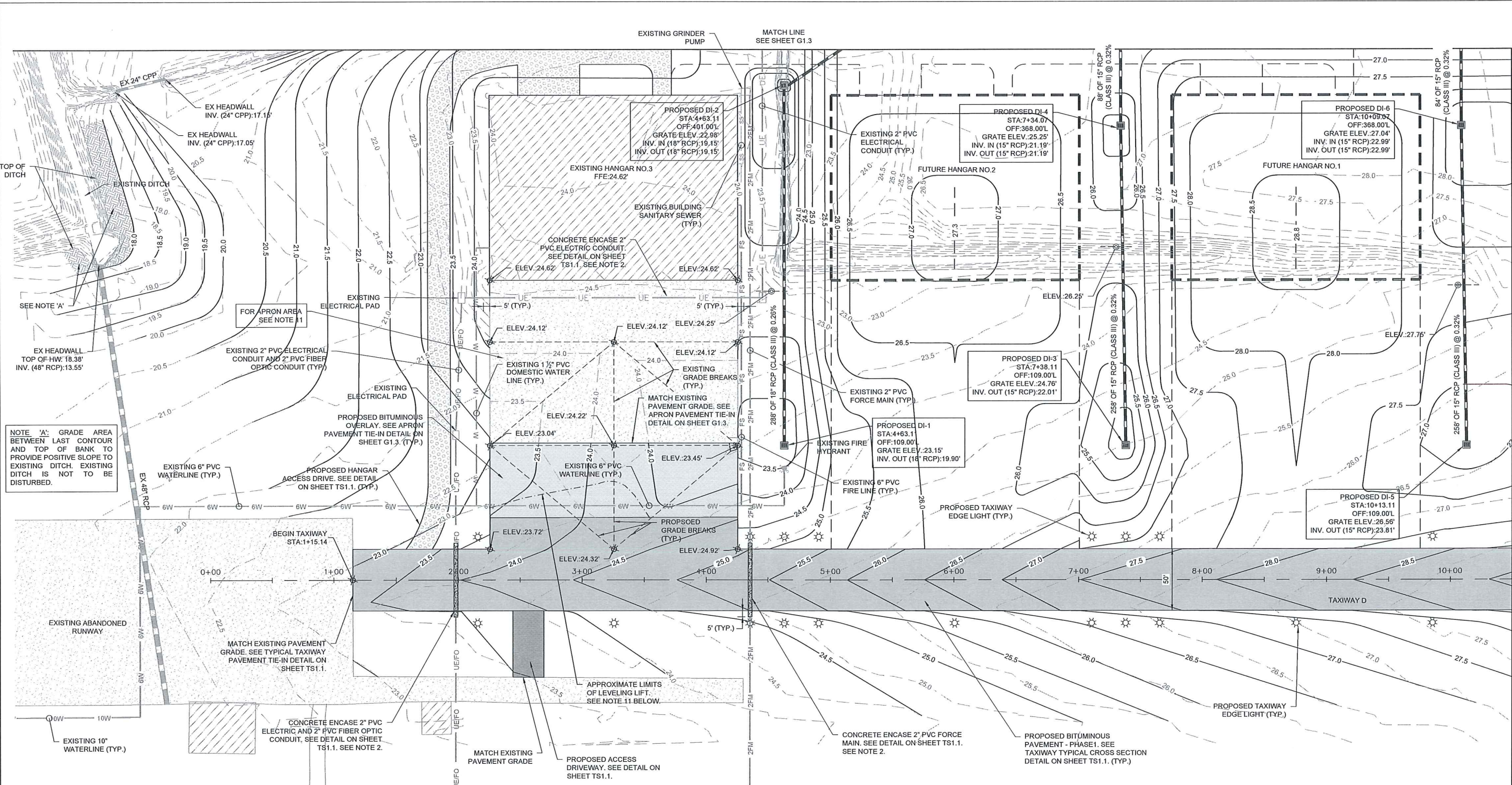
TALBERT & BRIGHT, INC.
© 2023

REV. NO.	DESCRIPTION	DATE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-5350
FAX: 910-762-5281
EMAIL: TBILLM@TBILLM.COM

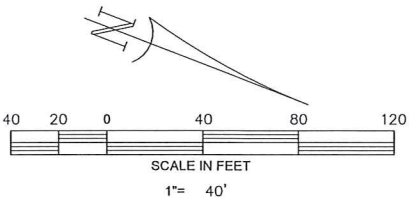
SC LICENSE NO. 000366



GENERAL NOTES:

- APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN PROVIDED BY OWNER. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN PLACE AND SERVICE. ANY EXISTING UTILITIES DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL LOCATE EXISTING CONDUITS AND FORCE MAINS TO BE CONCRETE ENCASED BY HAND DIGGING PRIOR TO BEGINNING EXCAVATION OPERATIONS. CONTRACTOR SHALL USE EXTREME CAUTION DURING EXCAVATION OPERATIONS FOR EXISTING CONDUITS AND FORCE MAINS TO BE CONCRETE ENCASED. IF CONDUITS OR FORCE MAINS ARE DAMAGED, CONTRACTOR SHALL COMPLETE NECESSARY REPAIRS AT NO ADDITIONAL COSTS TO THE OWNER. COST OF LOCATING CONDUITS, EXCAVATION, CONCRETE ENCASEMENT, AND BACKFILL SHALL BE PAID FOR UNDER ITEMS "CONCRETE ENCASEMENT - EXISTING 2\"/>
- SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
- SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
- SEE SHEET P1.1 FOR TAXIWAY PROFILES.
- FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
- SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.6.

- SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.
- SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
- CONTRACTOR SHALL PROVIDE "AS-CONSTRUCTED SURVEY" DRAWINGS FOR THE PROJECT PREPARED BY AND CERTIFIED BY A LICENSE SOUTH CAROLINA PUBLIC LAND SURVEYOR. THE "AS-CONSTRUCTED SURVEY" SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT SPECIAL PROVISIONS, SECTION PSP-16 OF THE PROJECT SPECIFICATIONS.
- THE EXISTING APRON AREA HAS BEEN INSTALLED BY OTHERS. FOR THE PROPOSED OVERLAY AREA, THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXISTING SURFACE COURSE GRADES AND COMPLETE THE BITUMINOUS LEVELING LIFT PRIOR TO INSTALLATION OF FINAL TWO 2\"/>



LEGEND			
	EXISTING BITUMINOUS PAVEMENT		EXISTING CONTOUR
	EXISTING BUILDING		PROPOSED CONTOUR
	PROPOSED BITUMINOUS PAVEMENT		EXISTING STORM DRAIN PIPE
	PROPOSED BITUMINOUS OVERLAY		PROPOSED STORM DRAIN PIPE
	PROPOSED ACCESS DRIVEWAY		EXISTING TOP OF DITCH
	PROPOSED HANGAR ACCESS DRIVE		EXISTING BOTTOM OF DITCH
			FUTURE HANGAR
			FUTURE APRON / ACCESS ROAD

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-5281
FAX: 910-762-5350
EMAIL: TB@TALBRT.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

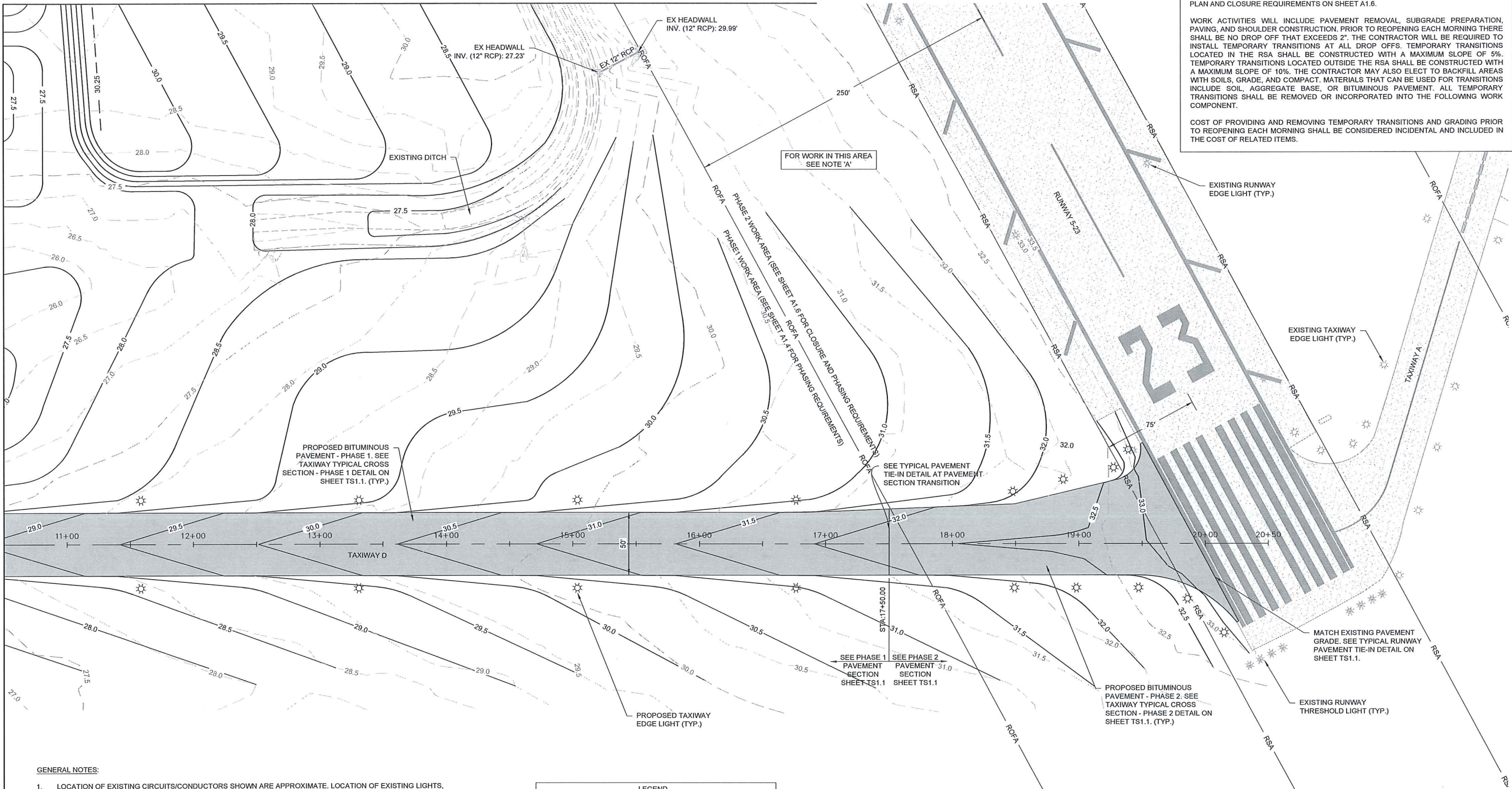
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEMWORK
GRADING AND DRAINAGE PLAN
(SHEET 1 OF 4)

Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

G1.1

MATCH LINE STA:10+50.00
SEE SHEET G1.1

MATCH LINE
SEE SHEET G1.4



NOTE 'A': WORK WITHIN THE PHASE 2 WORK AREA WILL REQUIRE CLOSURE OF RUNWAY 5-23 AND SHALL BE COMPLETED AT NIGHT BETWEEN 9:00 PM AND 6:00 AM. SEE PHASING PLAN AND CLOSURE REQUIREMENTS ON SHEET A1.6.

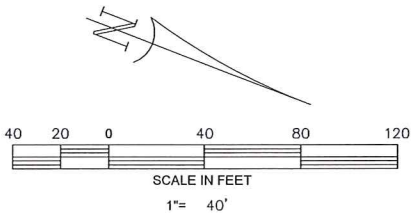
WORK ACTIVITIES WILL INCLUDE PAVEMENT REMOVAL, SUBGRADE PREPARATION, PAVING, AND SHOULDER CONSTRUCTION. PRIOR TO REOPENING EACH MORNING THERE SHALL BE NO DROP OFF THAT EXCEEDS 2". THE CONTRACTOR WILL BE REQUIRED TO INSTALL TEMPORARY TRANSITIONS AT ALL DROP OFFS. TEMPORARY TRANSITIONS LOCATED IN THE RSA SHALL BE CONSTRUCTED WITH A MAXIMUM SLOPE OF 5%. TEMPORARY TRANSITIONS LOCATED OUTSIDE THE RSA SHALL BE CONSTRUCTED WITH A MAXIMUM SLOPE OF 10%. THE CONTRACTOR MAY ALSO ELECT TO BACKFILL AREAS WITH SOILS, GRADE, AND COMPACT. MATERIALS THAT CAN BE USED FOR TRANSITIONS INCLUDE SOIL, AGGREGATE BASE, OR BITUMINOUS PAVEMENT. ALL TEMPORARY TRANSITIONS SHALL BE REMOVED OR INCORPORATED INTO THE FOLLOWING WORK COMPONENT.

COST OF PROVIDING AND REMOVING TEMPORARY TRANSITIONS AND GRADING PRIOR TO REOPENING EACH MORNING SHALL BE CONSIDERED INCIDENTAL AND INCLUDED IN THE COST OF RELATED ITEMS.

GENERAL NOTES:

1. LOCATION OF EXISTING CIRCUITS/CONDUCTORS SHOWN ARE APPROXIMATE. LOCATION OF EXISTING LIGHTS, SIGNS, DUCT BANKS, AND HANDHOLES ARE BASED ON AVAILABLE INFORMATION. CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. CONTRACTOR SHALL PROTECT ALL FACILITIES AND OTHER IMPROVEMENTS WHETHER OR NOT THEY ARE SHOWN ON THE PLANS.
2. SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
3. SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
4. SEE SHEET P1.1 FOR TAXIWAY PROFILES.
5. FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
6. SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.6.
7. SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.
8. SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
9. CONTRACTOR SHALL PROVIDE "AS-CONSTRUCTED SURVEY" DRAWINGS FOR THE PROJECT PREPARED BY AND CERTIFIED BY A LICENSE SOUTH CAROLINA PUBLIC LAND SURVEYOR. THE "AS-CONSTRUCTED SURVEY" SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT SPECIAL PROVISIONS, SECTION PSP-16 OF THE PROJECT SPECIFICATIONS.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
WILMINGTON, NC 28405
4810 SHELLEY DRIVE
PHONE: 910-762-5350
FAX: 910-762-5350
EMAIL: TBILIM@TBILIM.COM

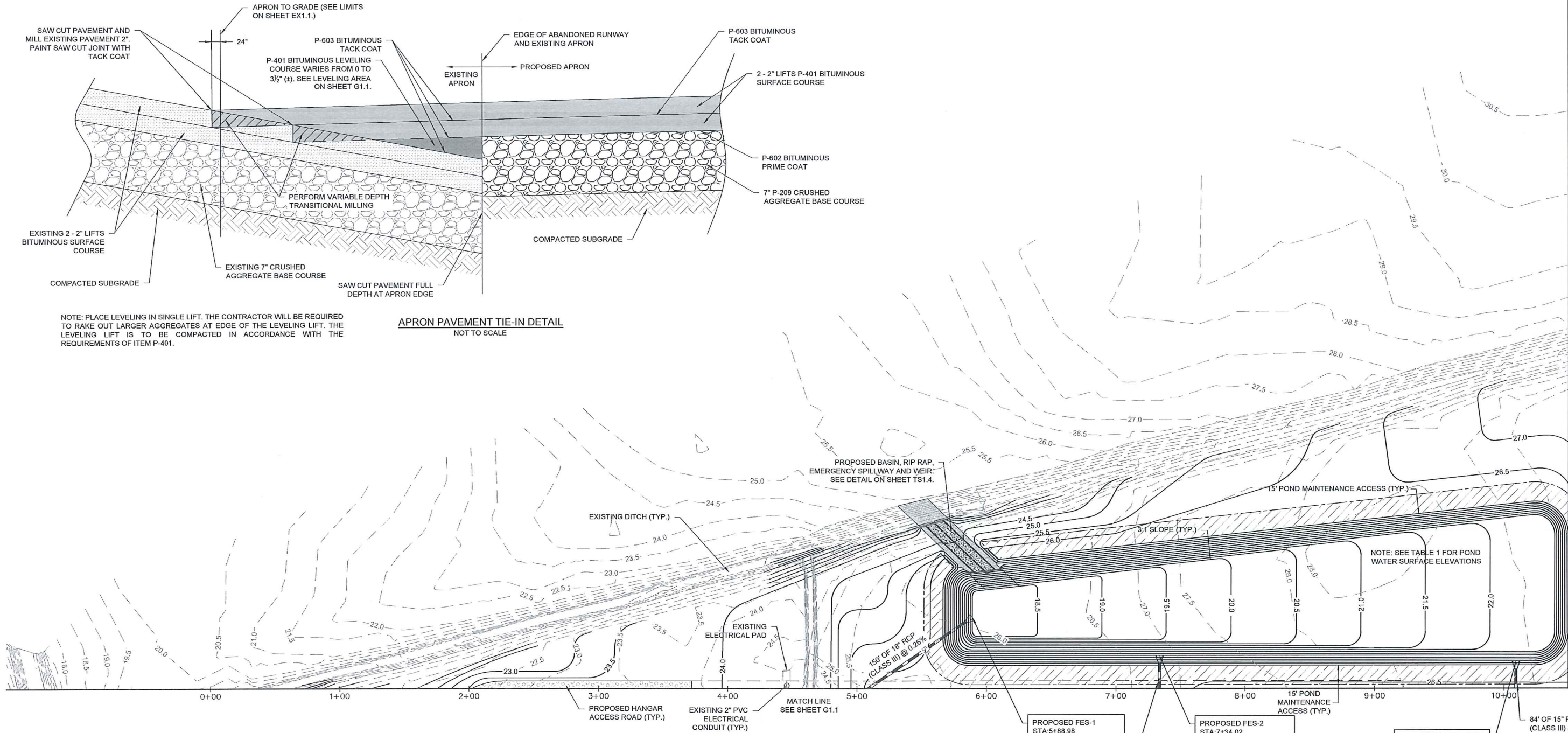
THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY, OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.
TALBERT & BRIGHT, INC.
© 2023

REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
GRADING AND DRAINAGE PLAN
(SHEET 2 OF 4)

Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

G1.2

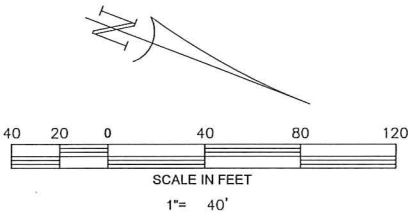


- GENERAL NOTES:**
- APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN PROVIDED BY OWNER. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. ANY EXISTING UTILITIES DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
 - SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
 - SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
 - SEE SHEET P1.1 FOR TAXIWAY PROFILES.
 - FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
 - SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.6.
 - SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.
 - SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
 - CONTRACTOR SHALL PROVIDE "AS-CONSTRUCTED SURVEY" DRAWINGS FOR THE PROJECT PREPARED BY AND CERTIFIED BY A LICENSE SOUTH CAROLINA PUBLIC LAND SURVEYOR. THE "AS-CONSTRUCTED SURVEY" SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT SPECIAL PROVISIONS, SECTION PSP-16 OF THE PROJECT SPECIFICATIONS.

LEGEND	
	PROPOSED HANGAR ACCESS ROAD
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED STORM DRAIN PIPE
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	FUTURE ACCESS ROAD
	POND MAINTENANCE ACCESS

TABLE 1: POND WATER SURFACE ELEVATIONS

	WATER SURFACE ELEVATION (FT.)
BOTTOM	18.50'
NWL	22.50'
2-YEAR, 24-HOUR	23.91'
10-YEAR, 24-HOUR	24.41'
25-YEAR, 24-HOUR	24.65'
100-YEAR, 24-HOUR	24.98'
EMERGENCY SPILLWAY	24.80'
TOP OF BANK	26.00'



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
SC LICENSE NO. C03366
EMAIL: TBILM@TBILM.COM

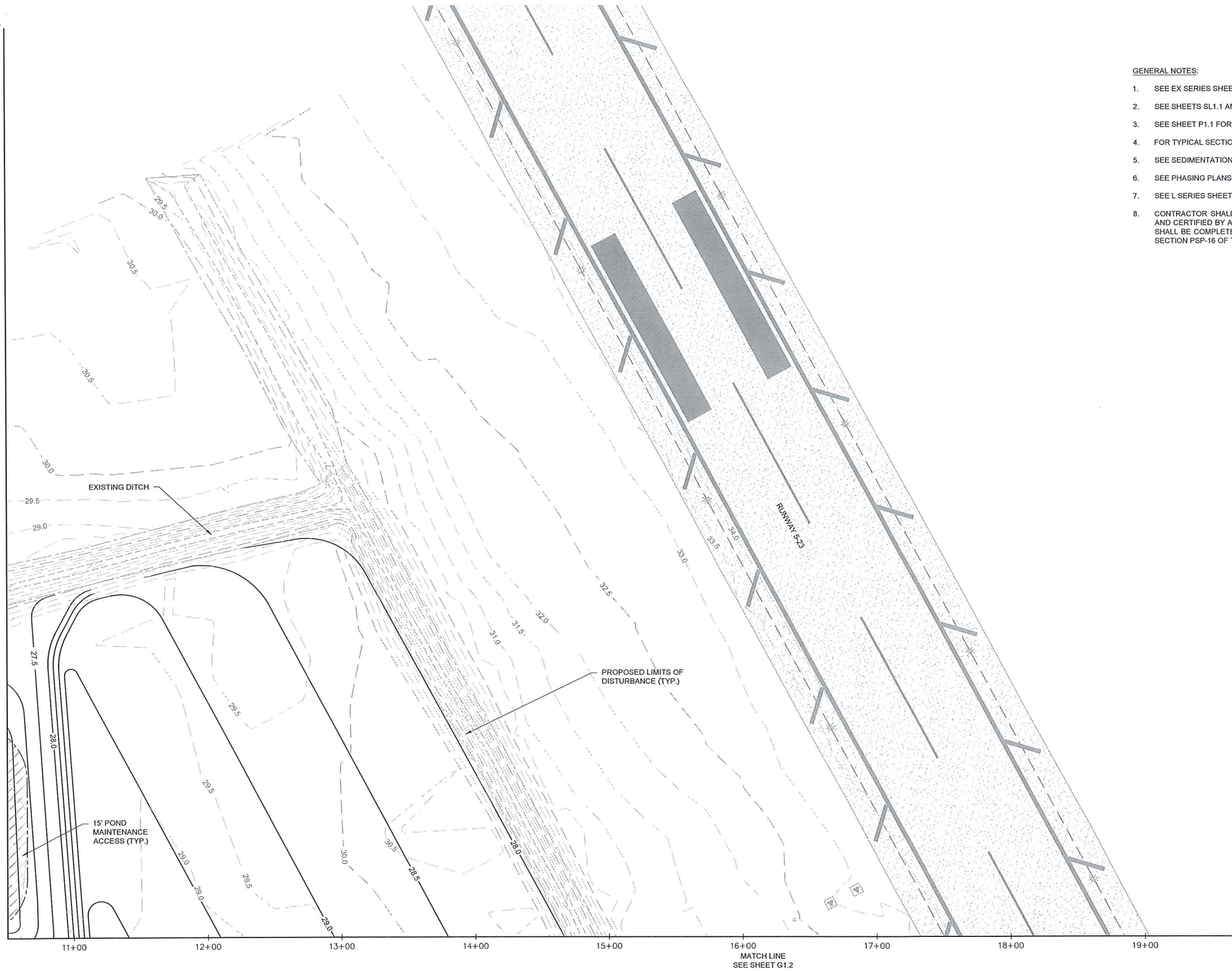
THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.
TALBERT & BRIGHT, INC.
© 2023

REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXIWAY IMPROVEMENTS
AND HANGAR SITEWORK
GRADING AND DRAINAGE PLAN
(SHEET 3 OF 4)

Date: FEBRUARY 2023
Scale: 1" = 40'
Drawn: NT
Checked: AMS
Project No.: 2601-2101
Sheet No.: G1.3

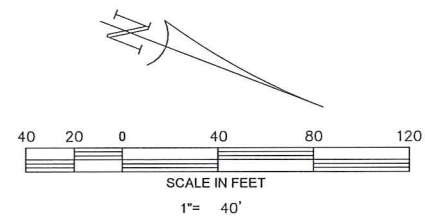
MATCH LINE STA:10+50.00
SEE SHEET G1.3



GENERAL NOTES:

1. SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
2. SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
3. SEE SHEET P1.1 FOR TAXIWAY PROFILES.
4. FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
5. SEE SEDIMENTATION AND EROSION CONTROL PLAN, NOTES, AND DETAILS ON SHEETS EC1.1 THRU EC1.6.
6. SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.
7. SEE L SERIES SHEETS FOR LIGHTING LAYOUT AND DETAILS.
8. CONTRACTOR SHALL PROVIDE "AS-CONSTRUCTED SURVEY" DRAWINGS FOR THE PROJECT PREPARED BY AND CERTIFIED BY A LICENSE SOUTH CAROLINA PUBLIC LAND SURVEYOR. THE "AS-CONSTRUCTED SURVEY" SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT SPECIAL PROVISIONS, SECTION PSP-16 OF THE PROJECT SPECIFICATIONS.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	POND MAINTENANCE ACCESS



GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEMWORK
GRADING AND DRAINAGE PLAN
(SHEET 4 OF 4)



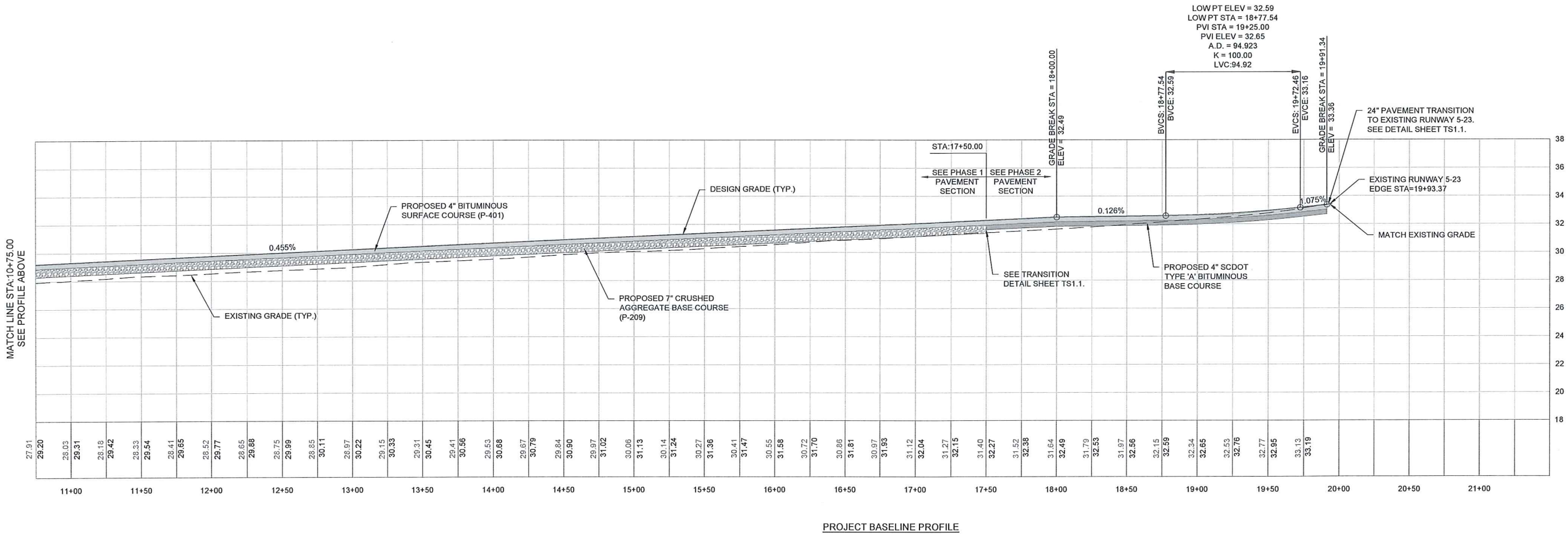
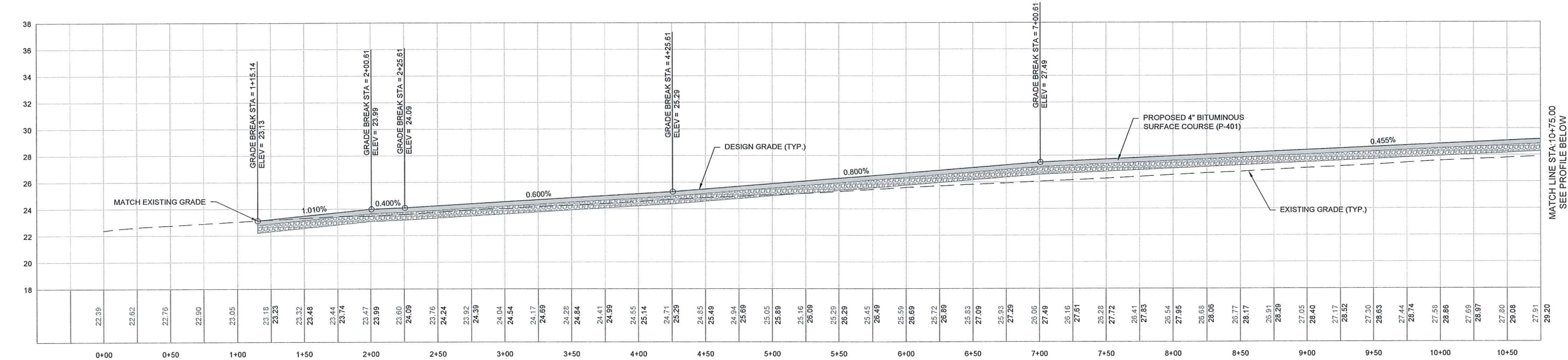
Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

G1.4

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

REVISIONS		DATE
REV. NO.	DESCRIPTION	

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILLM@TBILLM.COM
SC LICENSE NO. C00396



TALBERT & BRIGHT

ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-5350
FAX: 910-762-4281
EMAIL: TBILL@TBILL.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023
SC LICENSE NO. 003596

REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK

TAXIWAY PROFILE

Seal of South Carolina

Professional Engineer

No. 9719

ALAN M. SMITH

Date

FEBRUARY 2023

Scale

V: 1" = 4'
H: 1" = 40'

Drawn

NT

Checked

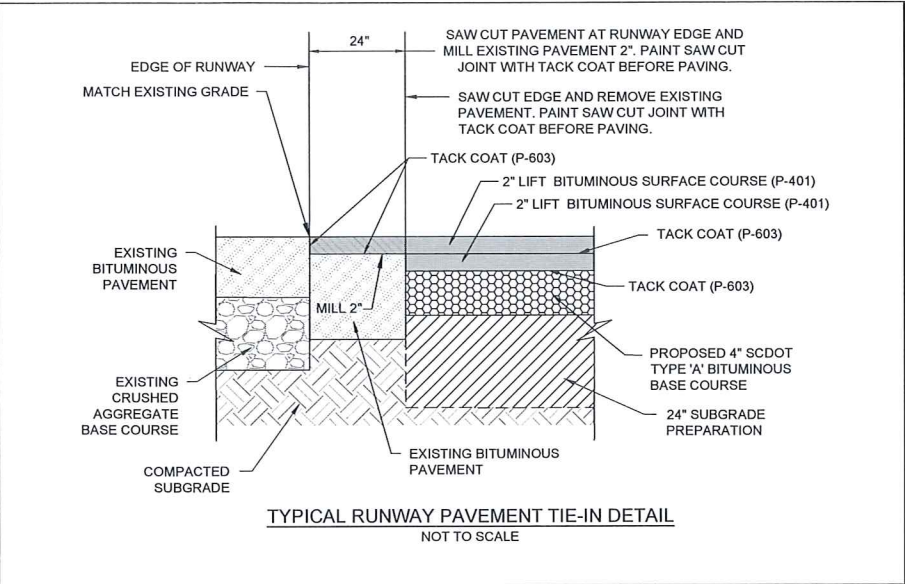
AMS

Project No.

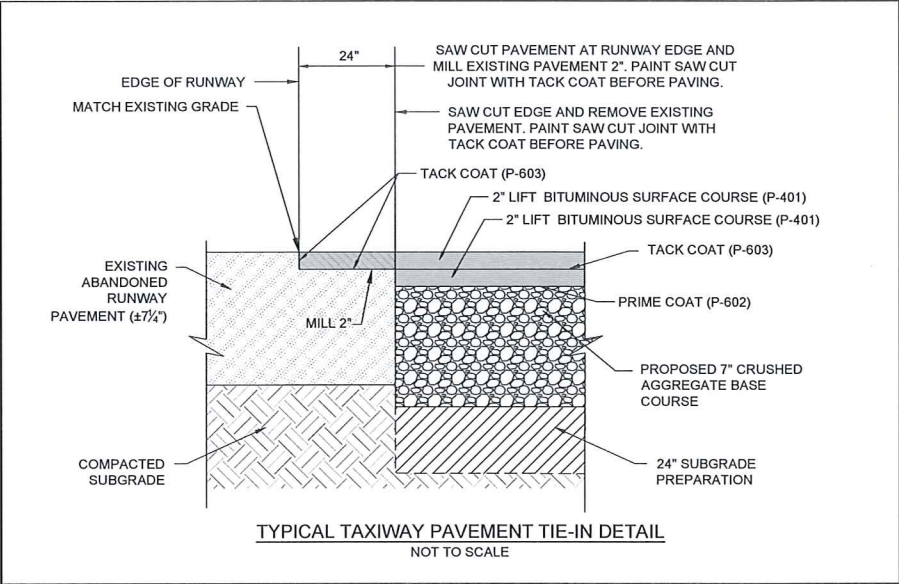
2601-2101

Sheet No.

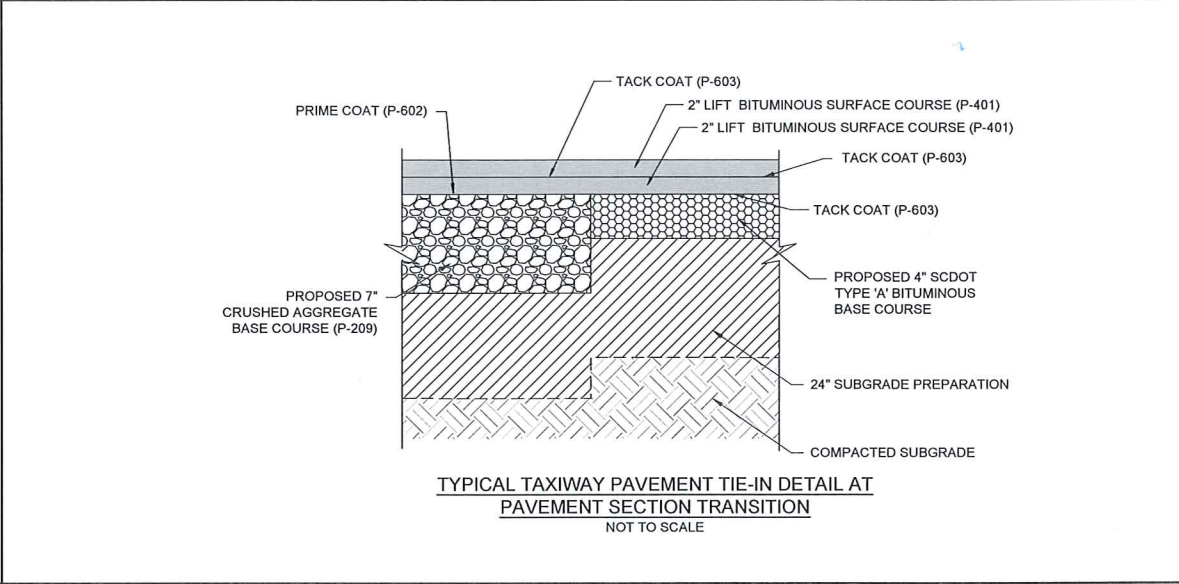
P1.1



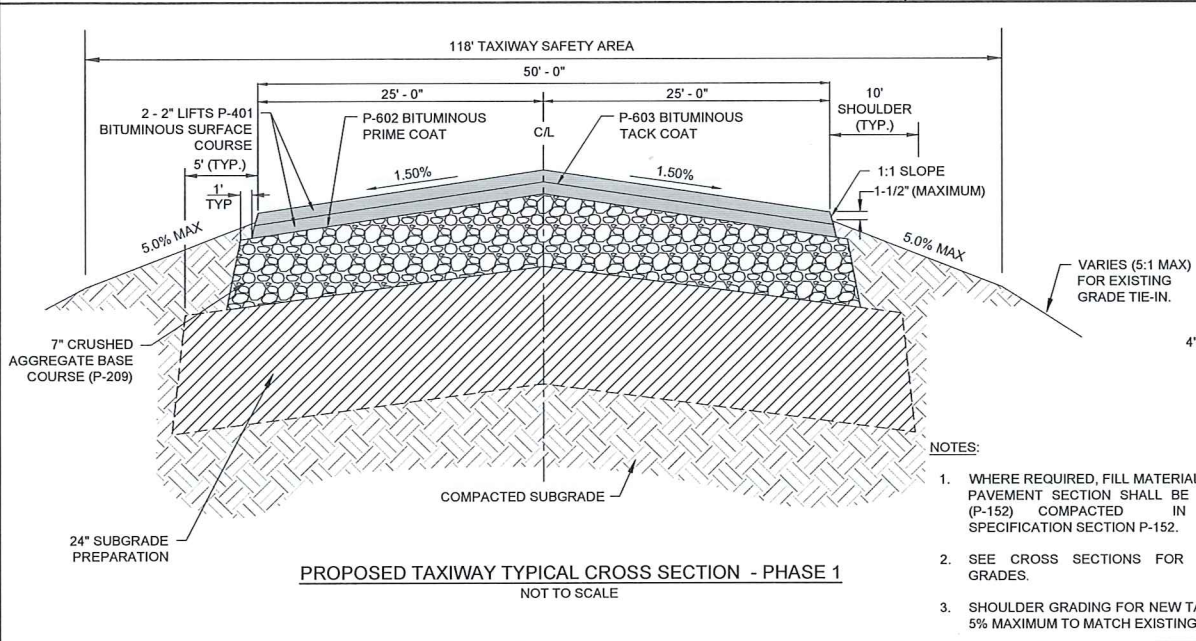
TYPICAL RUNWAY PAVEMENT TIE-IN DETAIL
NOT TO SCALE



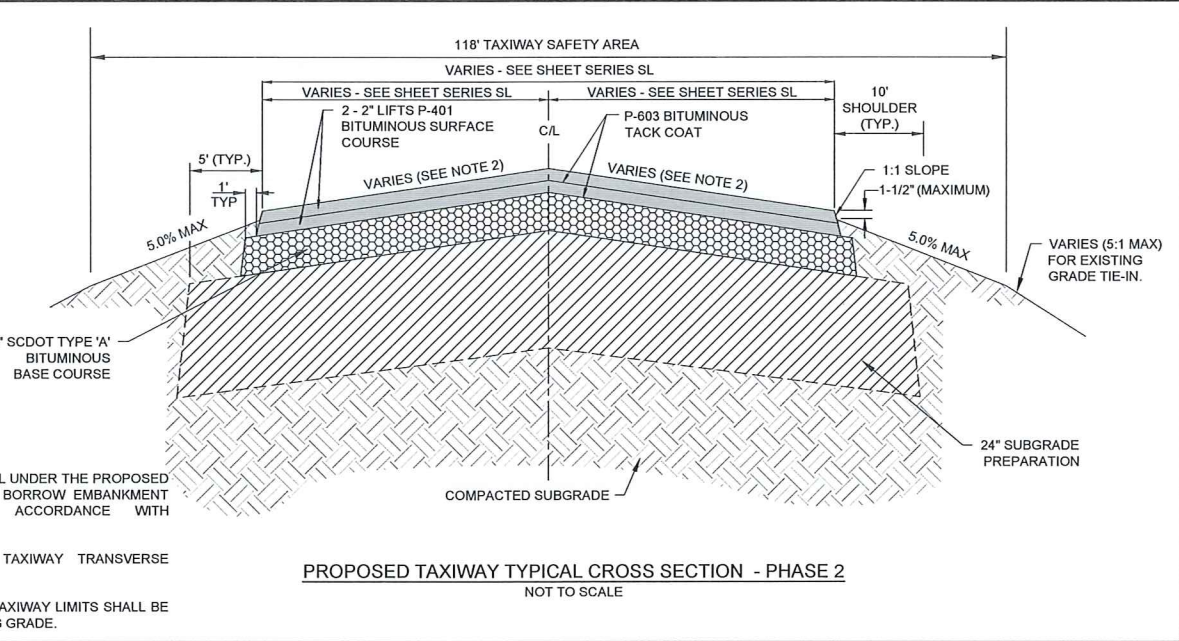
TYPICAL TAXIWAY PAVEMENT TIE-IN DETAIL
NOT TO SCALE



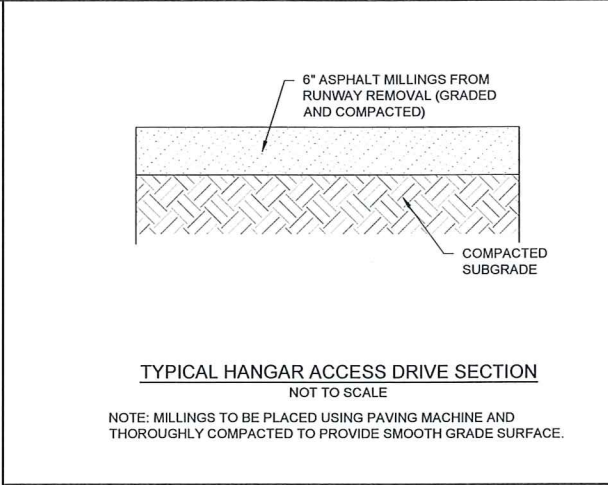
TYPICAL TAXIWAY PAVEMENT TIE-IN DETAIL AT
PAVEMENT SECTION TRANSITION
NOT TO SCALE



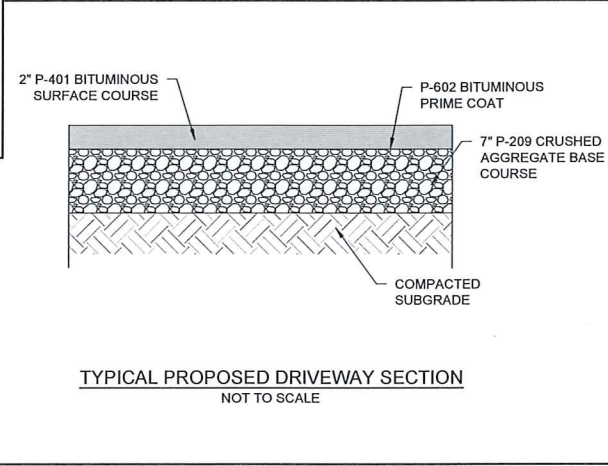
PROPOSED TAXIWAY TYPICAL CROSS SECTION - PHASE 1
NOT TO SCALE



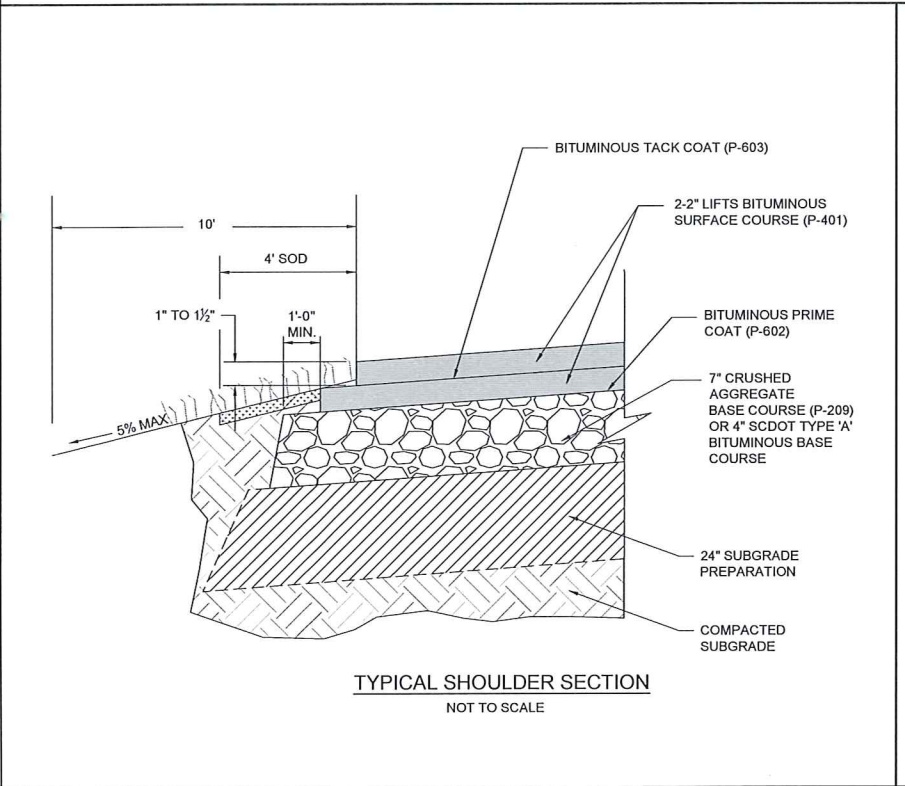
PROPOSED TAXIWAY TYPICAL CROSS SECTION - PHASE 2
NOT TO SCALE



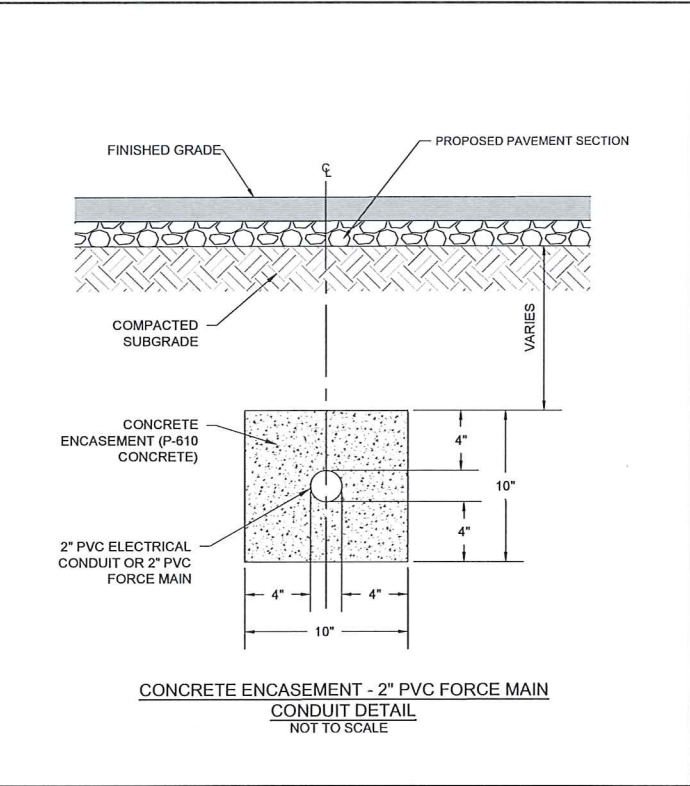
TYPICAL HANGAR ACCESS DRIVE SECTION
NOT TO SCALE



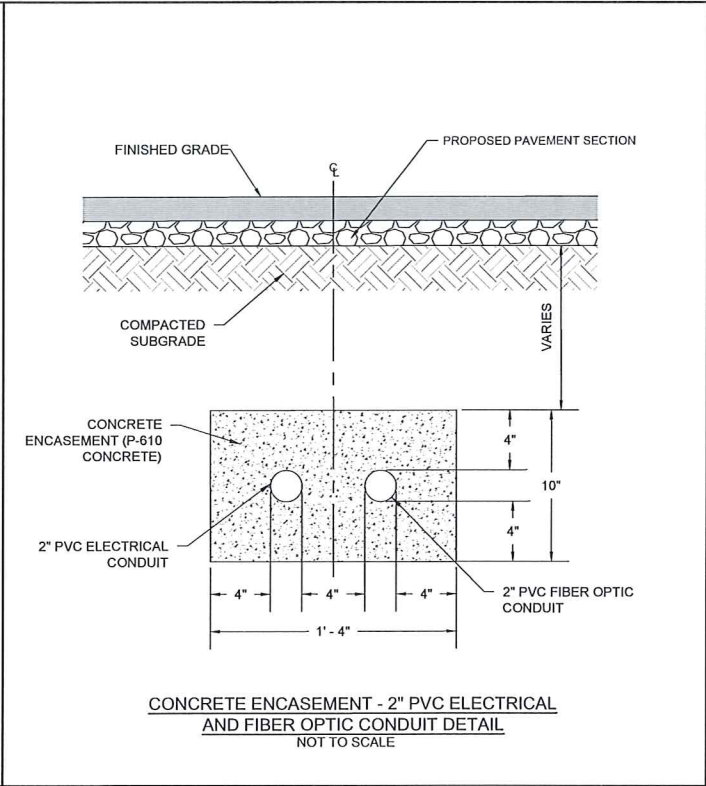
TYPICAL PROPOSED DRIVEWAY SECTION
NOT TO SCALE



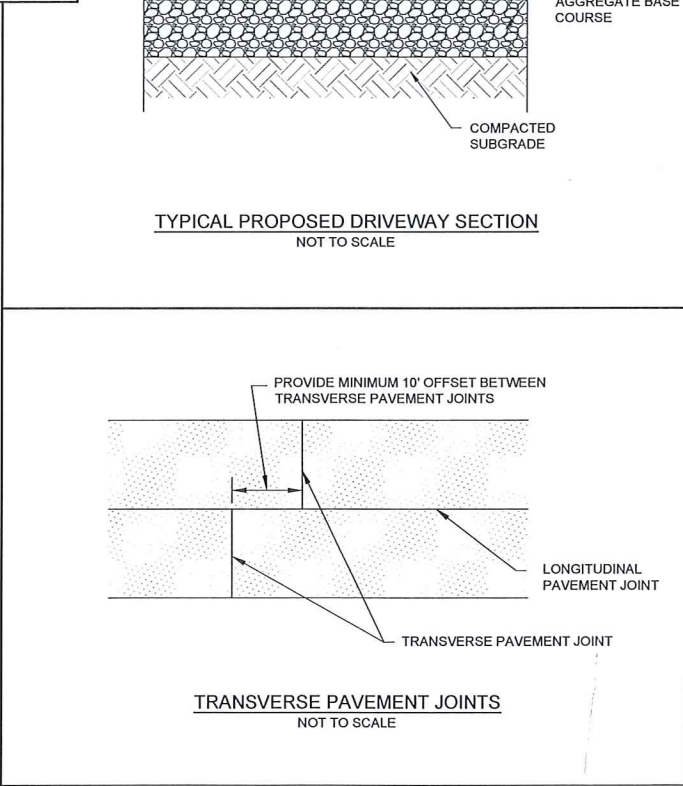
TYPICAL SHOULDER SECTION
NOT TO SCALE



CONCRETE ENCASEMENT - 2\"/>

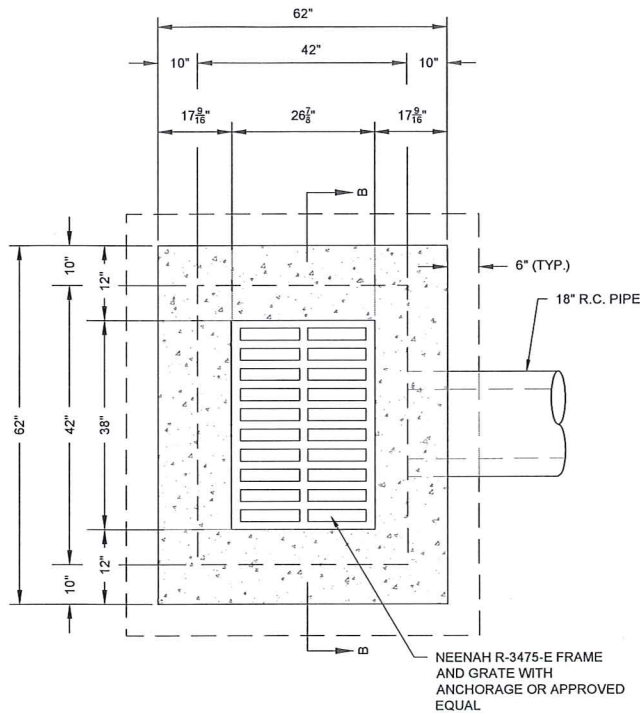


CONCRETE ENCASEMENT - 2\"/>

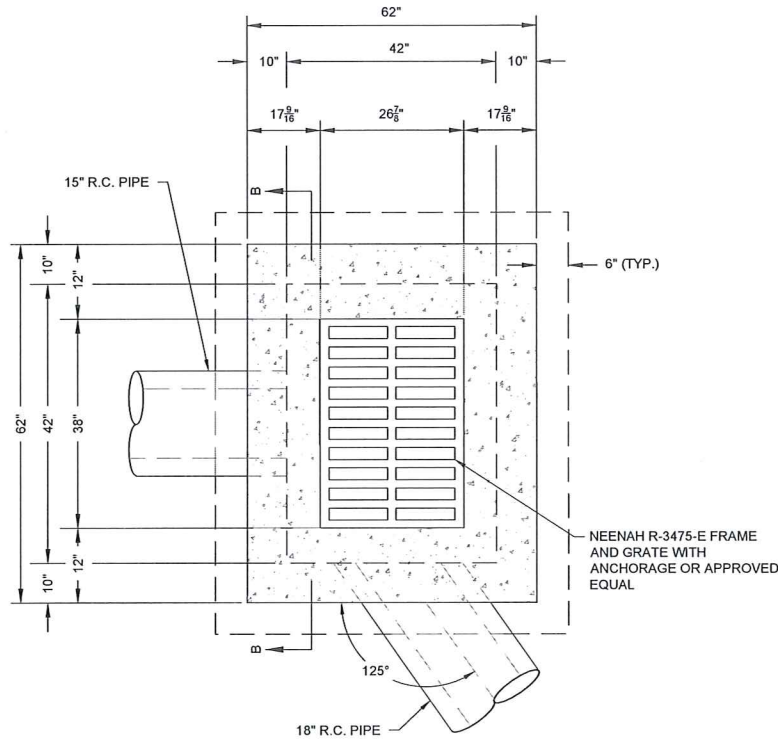


TRANSVERSE PAVEMENT JOINTS
NOT TO SCALE

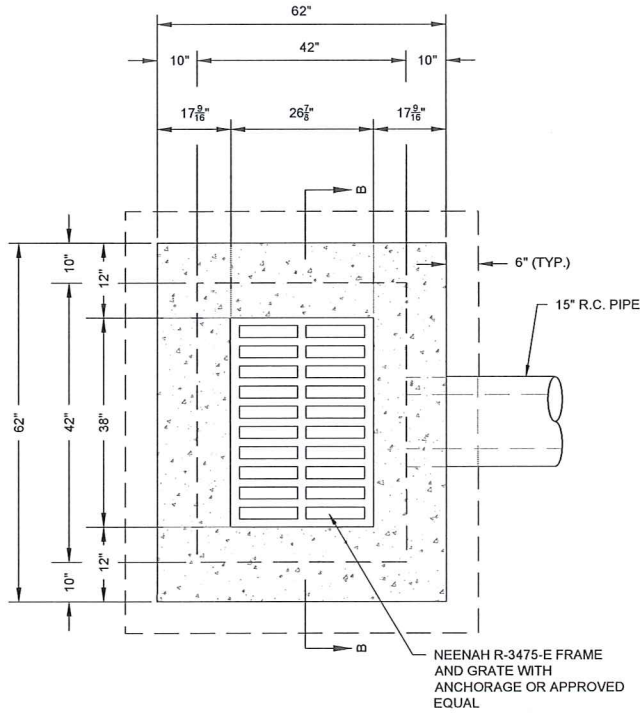
REV.	NO.	DESCRIPTION	DATE



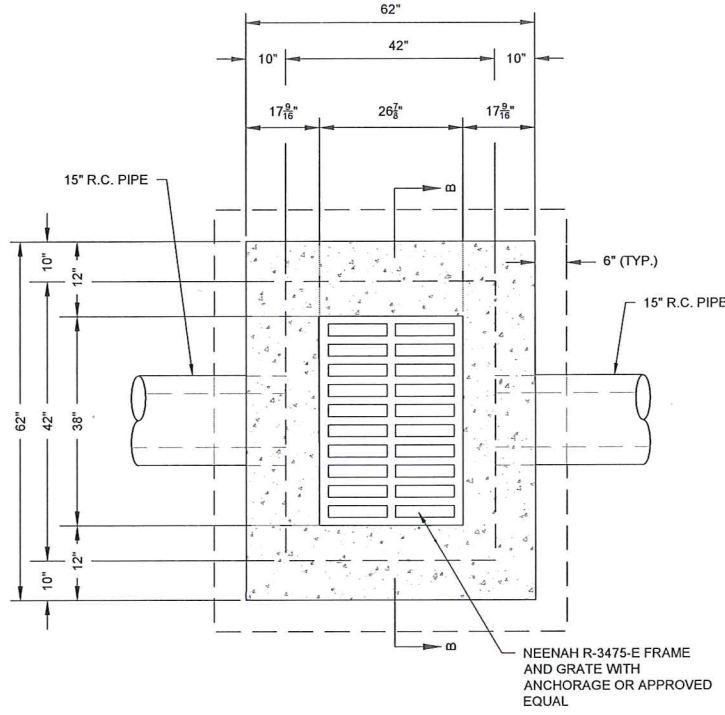
PLAN VIEW - STRUCTURES DI-1



PLAN VIEW - STRUCTURE DI-2



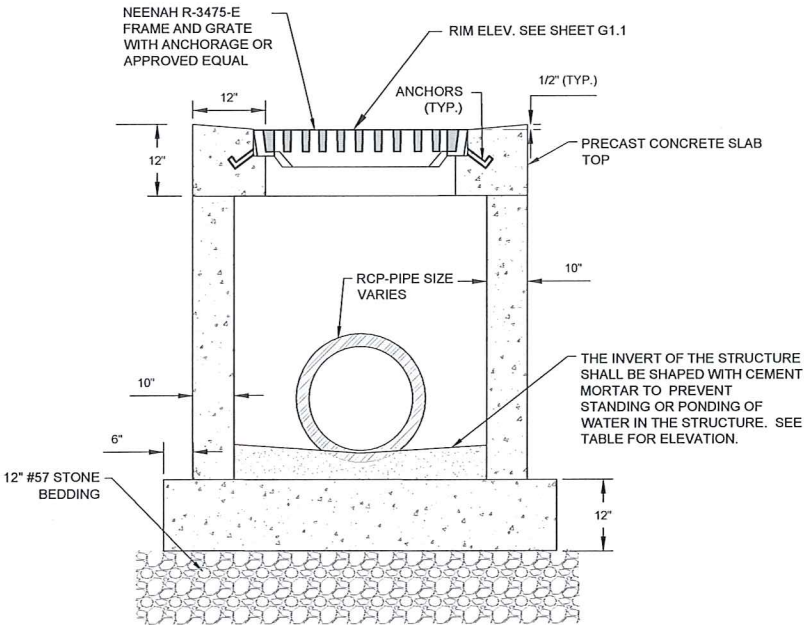
PLAN VIEW - STRUCTURES DI-3 & DI-5



PLAN VIEW - STRUCTURE DI-4 & DI-6

PRECAST CONCRETE DROP INLETS
NOT TO SCALE

NOTE: SEE SHEET G1.1 FOR STORM DRAINAGE STRUCTURE SCHEDULE

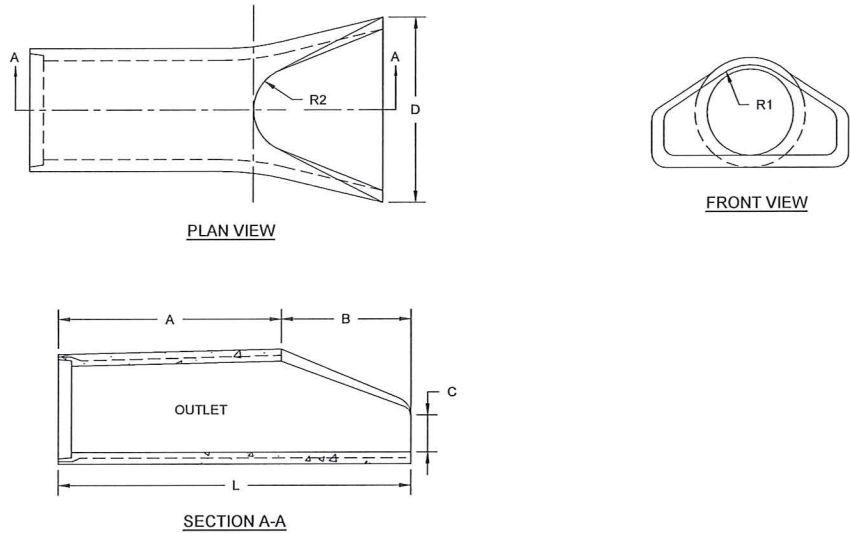


SECTION B-B

NOTE: BACKFILL ALL DRAINAGE STRUCTURES EXCAVATIONS WITH SAND FILL AND COMPACT (COST INCLUDED IN UNIT PRICE PER EACH STRUCTURE).

GENERAL NOTES FOR PRECAST CONCRETE DROP INLET

1. THE DETAIL SHOWN FOR 'PRECAST CONCRETE DROP INLET' IS TO SHOW NOMINAL SIZE AND DIMENSIONS OF PROPOSED STRUCTURES ONLY AND MAY VARY BASED ON DESIGN. FOR ALL PRECAST CONCRETE STRUCTURES, THE CONTRACTOR WILL BE REQUIRED TO SUBMIT DESIGN CALCULATIONS, DETAILED SHOP DRAWINGS, AND DESIGN PREPARED AND SEALED BY A SOUTH CAROLINA PROFESSIONAL ENGINEER. PRECAST CONCRETE STRUCTURES SHALL BE DESIGNED TO CARRY ALL APPLICABLE LOADINGS, INCLUDING VERTICAL AND LATERAL EARTH PRESSURES, DEAD LOADS, LIFTING LOADS AND AIRCRAFT LIVE LOADS. AIRCRAFT LIVE LOADS SHALL BE TREATED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5320-5D, CHAPTER 7, USING A 30,000 POUND DUAL GEAR AIRCRAFT LOAD FOR DIRECT LOADING AND LOADING ON BURIED STRUCTURES.
2. ALL MATERIALS, DESIGN, MANUFACTURE, TESTING, AND PRODUCT PERFORMANCE FOR THE PRECAST CONCRETE COMPONENTS AND ACCESSORIES SHALL BE IN ACCORDANCE WITH ASTM C913.
3. BASE SECTIONS SHALL HAVE A BOTTOM POURED MONOLITHICALLY WITH THE WALLS OR AN APPROVED WATER-STOP CAST INTO THE BOTTOM FOR THE JOINT TO THE WALLS.
4. JOINTS SHALL BE TONGUE AND GROOVE. JOINT SEALANT SHALL BE BUTYL RUBBER AND SHALL MEET THE REQUIREMENTS OF AASHTO M 198, TYPE B. SIZE AND AMOUNT OF SEALANT SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
5. CONCRETE SHALL BE 4000 PSI COMPRESSIVE STRENGTH MEETING THE REQUIREMENTS OF SECTION P-610 OF THE PROJECT SPECIFICATIONS.
6. REINFORCING STEEL SHALL BE ASTM A-706, LOW-ALLOY STEEL DEFORMED AND PLAIN BARS FOR CONCRETE REINFORCEMENT, GRADE 60. WIRE AND WIRE MESH SHALL CONFORM TO AASHTO M 55 AND M 221.
7. FLOW LINE OF BASE TO BE GROUTED TO OUTLET PIPE FLOW LINE TO MAINTAIN A CONTINUOUS FLOW. GROUT SHALL BE TYPE M MORTAR MATERIAL.
8. IF STRUCTURE DEPTH EXCEEDS 4'-6", STEPS ARE TO BE PLACED ON WALL. SEE SCDOT STANDARD DRAWING FOR STEP. STEPS SHALL BE ALIGNED IN ALL SECTIONS TO FORM A CONTINUOUS LADDER. STEPS SHALL BE ALIGNED WITH OPENING IN TOP OR FLAT SLAB ADAPTER SO AS TO PROVIDE REASONABLE ACCESS. STEP SPACING SHALL NOT EXCEED 1'-0".
9. LIFT HOLES AND/OR DEVICES MAY BE PLACED AS NECESSARY. ALL LIFT HOLES SHALL BE GROUTED CLOSED PRIOR TO COMPLETION OF THE INSTALLATION. ALL LIFTING METHODS MUST MEET OSHA REGULATIONS.
10. AFTER PIPE IS SET INTO THE DRAINAGE STRUCTURE, THE REMAINING OPENING AROUND THE PIPE MUST BE SEALED WITH BRICK AND MORTAR OR CONCRETE FOR THE FULL WALL THICKNESS OF THE STRUCTURE.
11. THE CONTRACT UNIT PRICE PRECAST DROP INLETS SHALL INCLUDE THE COST OF FURNISHING ALL MATERIALS AND WORK INCIDENTAL TO THE CONSTRUCTION OF THE STRUCTURE COMPLETE IN PLACE AS SHOWN.



SECTION A-A

PIPE DIAMETER	OVERALL LAYING LENGTH (L)	BARREL LENGTH (A)	FLARE LENGTH (B)	C	D	R1	R2	WALL THICKNESS
15"	6' -1"	3' - 10"	2' - 3"	6"	30"	12 1/2"	11"	2 1/4"
18"	6' -1"	3' - 10"	2' - 3"	9"	36"	15 1/2"	12"	2 1/2"

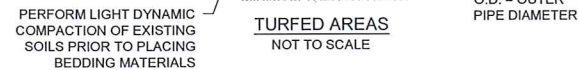
NOTE: SEE SHEET G1.3 FOR OUTLET INVERTS

CONCRETE FLARED END SECTION DETAIL
NOT TO SCALE

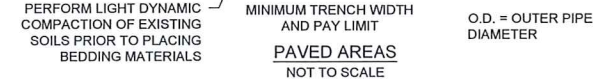
REV.	NO.	DESCRIPTION	DATE



Date	FEBRUARY 2023
Scale	NTS
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	



PROPERTY	ASTM STANDARD	MINIMUM AVERAGE ROLL VALUE
GRAB TENSILE STRENGTH	D4632	200 LBS.
GRAB TENSILE ELONGATION	D4632	10%
TRAPEZOID TEAR STRENGTH	D4533	75 LBS.
CBR PUNCTURE STRENGTH	D6241	700 LBS.
APPARENT OPENING SIZE (AOS)	D4751	40 U.S. SIEVE
PERMITTIVITY	D4491	0.05 SEC-1
UV RESISTANCE (AT 500 HRS.)	D4355	70% STRENGTH RETAINED



(A) PIPE FOUNDATION MATERIAL:
SCDOT STANDARD SPECIFICATIONS SECTION 1005, #67 OR #78M STONE AGGREGATE.
PLACE STONE ON WOVEN GEOTEXTILE MATERIAL CONFORMING TO TABLE 1. STONE
SHALL BE COMPACTED USING HAND OPERATED ROLLING EQUIPMENT.

(B) SELECT MATERIAL:
MATERIAL TAKEN FROM THE EXCAVATION (AS AVAILABLE) OR FROM OFF-SITE
SOURCES AND CONFORMING TO THE REQUIREMENTS FOR USCS CLASSIFICATIONS
SW, SM, SP, SC OR BETTER WITH NO MORE THAN 20% PASSING THE #200 SIEVE.
COMPACT TO 95% OF ASTM D 1557 MODIFIED PROCTOR DENSITY (EXCEPT MIDDLE
BEDDING).

(C) GENERAL TRENCH BACKFILL MATERIAL:
MATERIAL TAKEN FROM THE EXCAVATION (AS AVAILABLE) OR FROM OFF-SITE
SOURCES AND CONFORMING TO THE REQUIREMENTS FOR USCS CLASSIFICATIONS
SW, SM, SP, SC, ML, CL OR BETTER. ML OR CL SOILS SHALL BE PROPERLY MOISTURE
CONDITIONED TO ACHIEVE SPECIFIED DENSITY. CLASSIFICATIONS OL, MH, CH, OH AND
PT SHALL BE EXCLUDED. COMPACT TO 95% OF ASTM D 1557 MODIFIED PROCTOR
DENSITY.

NOT TO SCALE

1. SEE SPECIFICATION ITEM D-701 FOR PIPE INSTALLATION.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MANIPULATE WET SOILS FOR DRYING OR TO ADD WATER AS NECESSARY TO ACHIEVE THE SPECIFIED DENSITY. MOISTURE CONDITION SOILS TO WITHIN +/- 3% OF STANDARD PROCTOR OPTIMUM FOR SOILS CONTAINING SIGNIFICANT FINES (SC, SM, ML, CL, ETC.). MOISTURE CONDITION SOILS TO WITHIN +/- 5% OF STANDARD PROCTOR OPTIMUM FOR SOILS WITH MINIMAL FINES (SP, ETC.)
3. ALL MATERIALS SHALL BE COMPACTED IN 6" MAXIMUM LOOSE LIFTS.
4. CONTRACTOR SHOULD EXPECT TO ENCOUNTER WET CONDITIONS AND GROUND WATER DURING PIPE INSTALLATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DESIGN OF EXCAVATION SUPPORT AND MEANS AND METHODS OF DEWATERING INCLUDING WELL-POINTING, SUMPS, PUMPS OR OTHER MEANS. ALL COSTS FOR EXCAVATION SUPPORT AND DEWATERING WILL BE CONSIDERED INCIDENTAL TO THE PRICE BID FOR PIPE INSTALLATION. CONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED DEWATERING PLAN TO THE ENGINEER PRIOR TO THE START OF CONSTRUCTION.
5. PIPE JOINTS SHALL BE CONSTRUCTED WITH RUBBER GASKETS AND BE WRAPPED IN GEOTEXTILE FABRIC. FABRIC SHALL EXTEND 2 FEET ON EITHER SIDE OF THE JOINT AND SHALL BE TIED DOWN WITH WIRE TO HOLD IT IN PLACE UNTIL BACKFILL IS ABOVE THE TOP OF THE PIPE.

1. ALL EXCAVATION AND BACKFILL FOR EXISTING PIPE AND STRUCTURE REMOVAL (WHETHER FROM THE EXCAVATION OR FROM OFF-SITE SOURCES), DEWATERING, EXCAVATION SUPPORT, INCIDENTALS AND LABOR SHALL BE INCLUDED IN THE UNIT PRICE PER STRUCTURE REMOVAL, OR LINEAR FOOT PER PIPE REMOVAL.
2. DEWATERING TRENCH EXCAVATIONS AND EXISTING PIPES MAY BE REQUIRED DURING PIPE AND STRUCTURE REMOVAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MEANS AND METHODS OF DEWATERING INCLUDING WELL POINTING, SUMPS, PUMPS, PIPE PLUGS, OR OTHER MEANS AND METHODS. ALL COSTS FOR DEWATERING WILL BE CONSIDERED INCIDENTAL TO THE PRICE BID FOR PIPE REMOVAL OR STRUCTURE REMOVAL.
3. ALL BACKFILL FOR EXISTING PIPE AND STRUCTURE REMOVAL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE REQUIREMENTS OF P-152.

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK

DRAINAGE DETAILS
(SHEET 2 OF 2)

Date	FEBRUARY 2
Scale	NTS
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

TS1.3

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

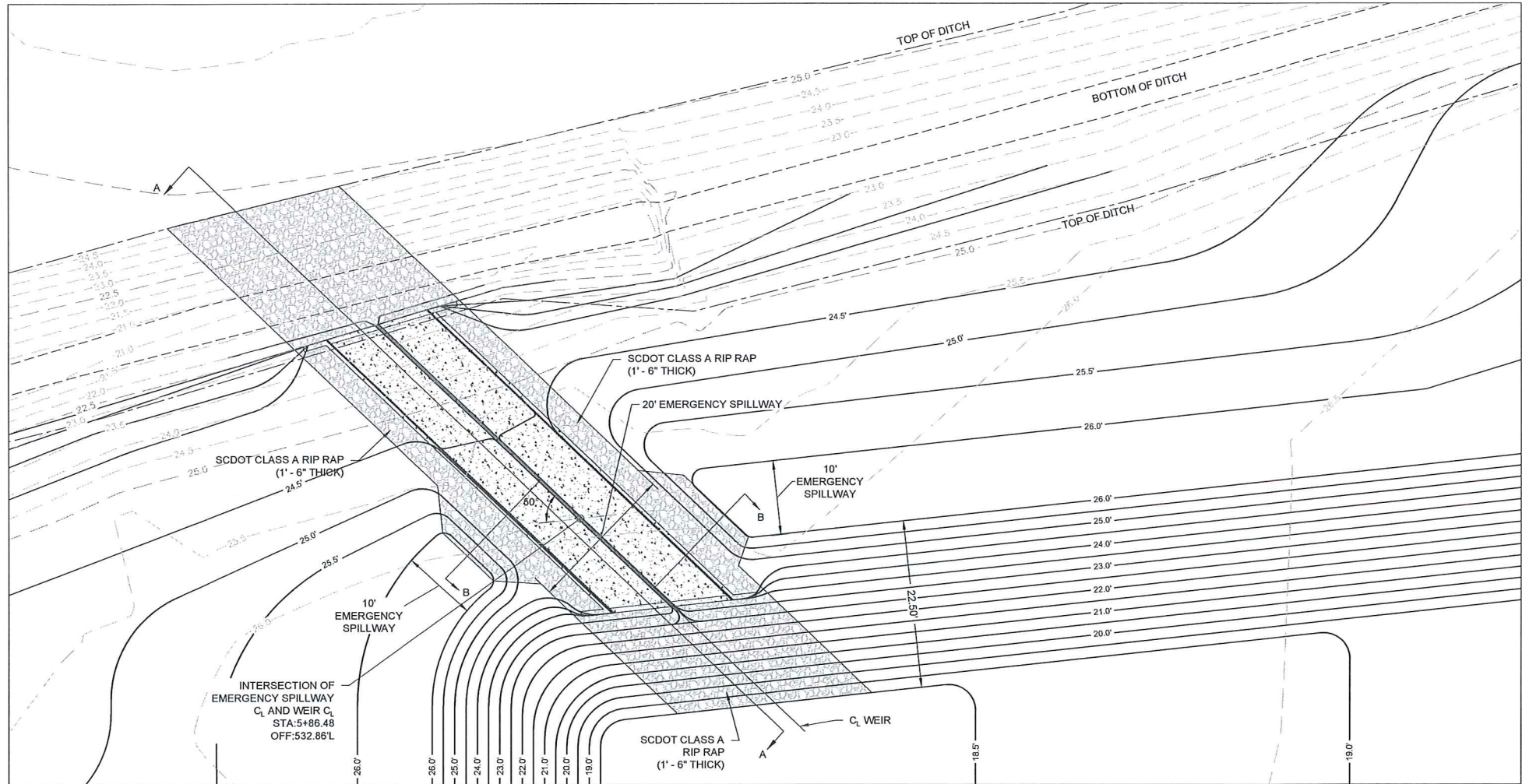
[illegible]

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

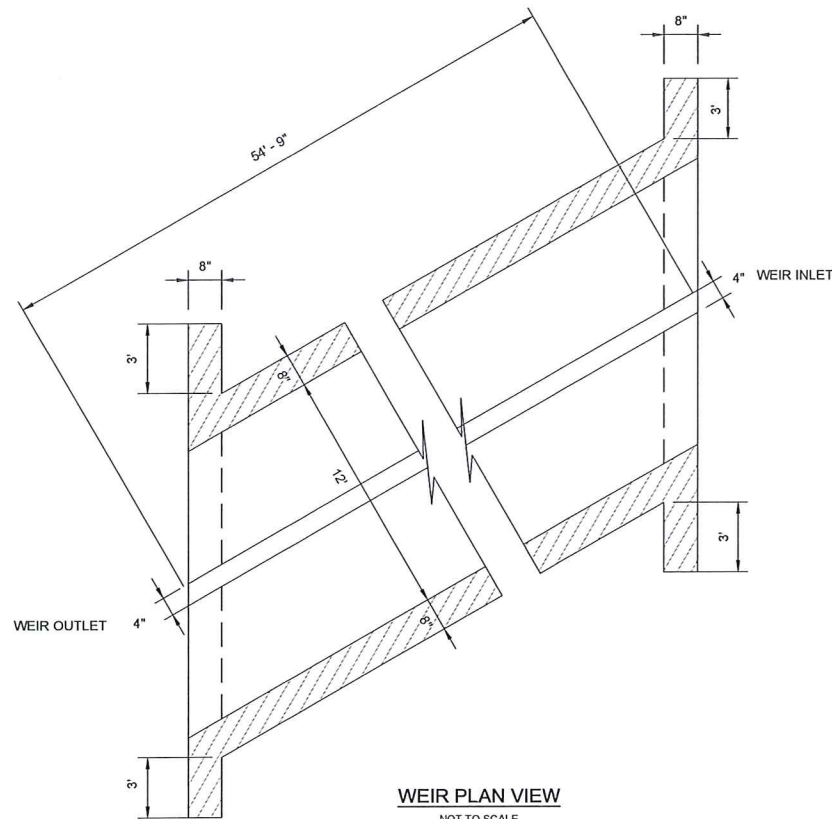
4810 SHELLEY DRIVE
FAX: 910-762-6281

WILMINGTON, NC 28405
PHONE: 910-763-5350

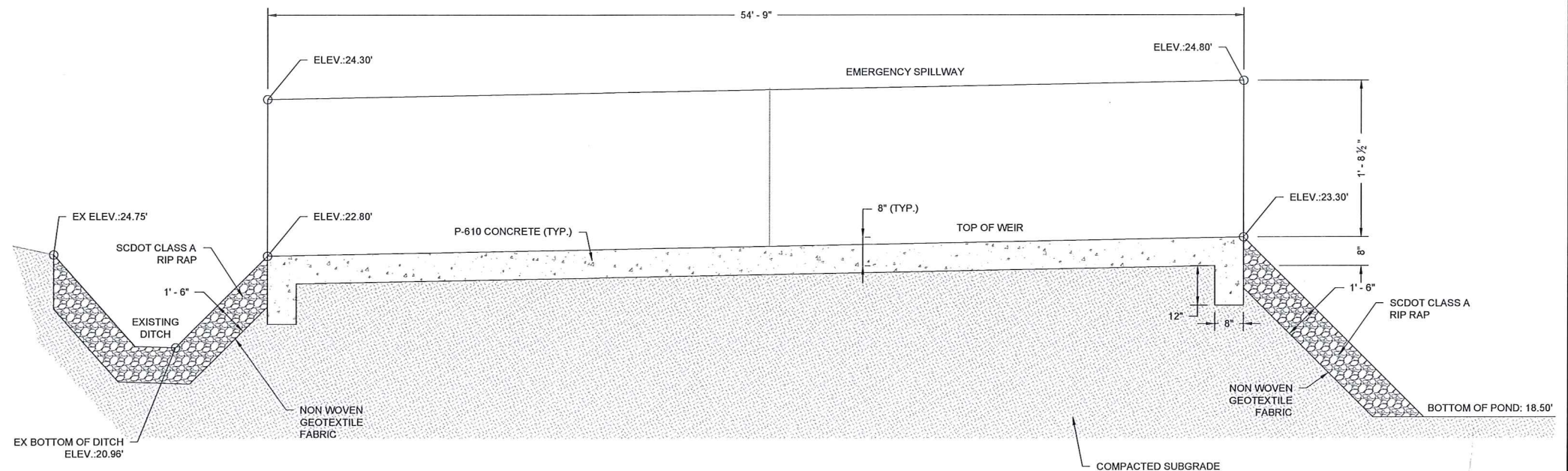
SC LICENSE NO. C00386



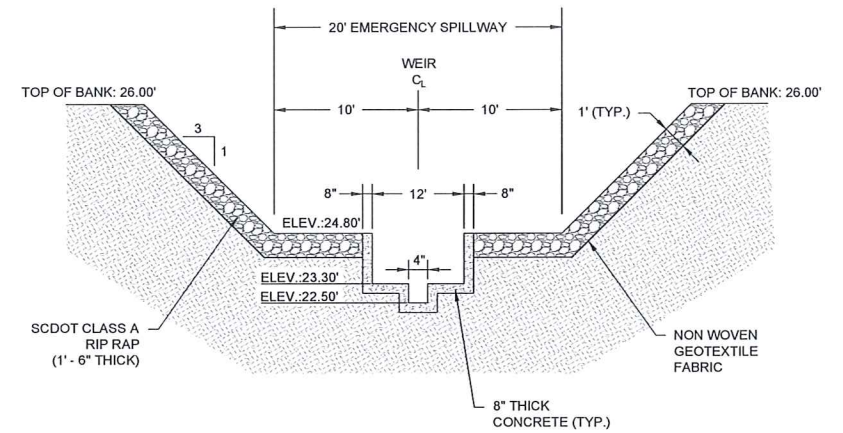
EMERGENCY SPILLWAY AND WEIR OUTLET - PLAN VIEW
SCALE: 1"=10'



WEIR PLAN VIEW
NOT TO SCALE

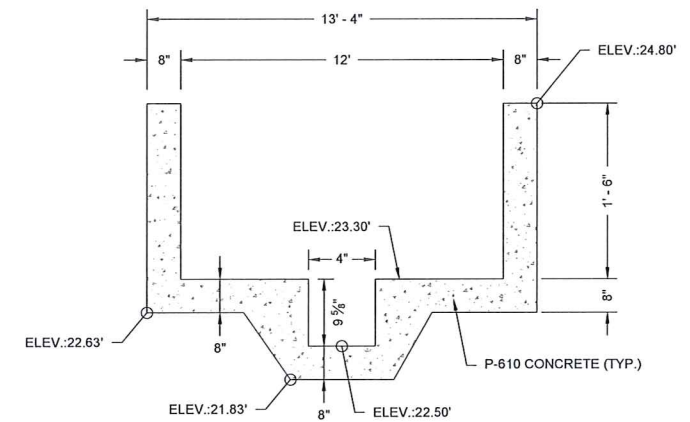


SECTION A-A - WEIR SECTION
NOT TO SCALE



NOTE: ELEVATIONS SHOWN ARE ELEVATIONS AT THE WEIR INLET

SECTION B-B
NOT TO SCALE



NOTES:

1. FOR STEEL REINFORCEMENT SEE DETAILS ON SHEET TS1.5.
2. ELEVATIONS SHOWN ARE ELEVATIONS AT THE WEIR INLET. SEE WEIR FRONT ELEVATION DETAIL FOR WEIR OUTLET ELEVATIONS.

WEIR SECTION DETAIL
NOT TO SCALE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-6281
FAX: 910-763-5350
EMAIL: TBILLMOTBILM.COM
SC LICENSE NO. 000366

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.
TALBERT & BRIGHT, INC.
© 2023

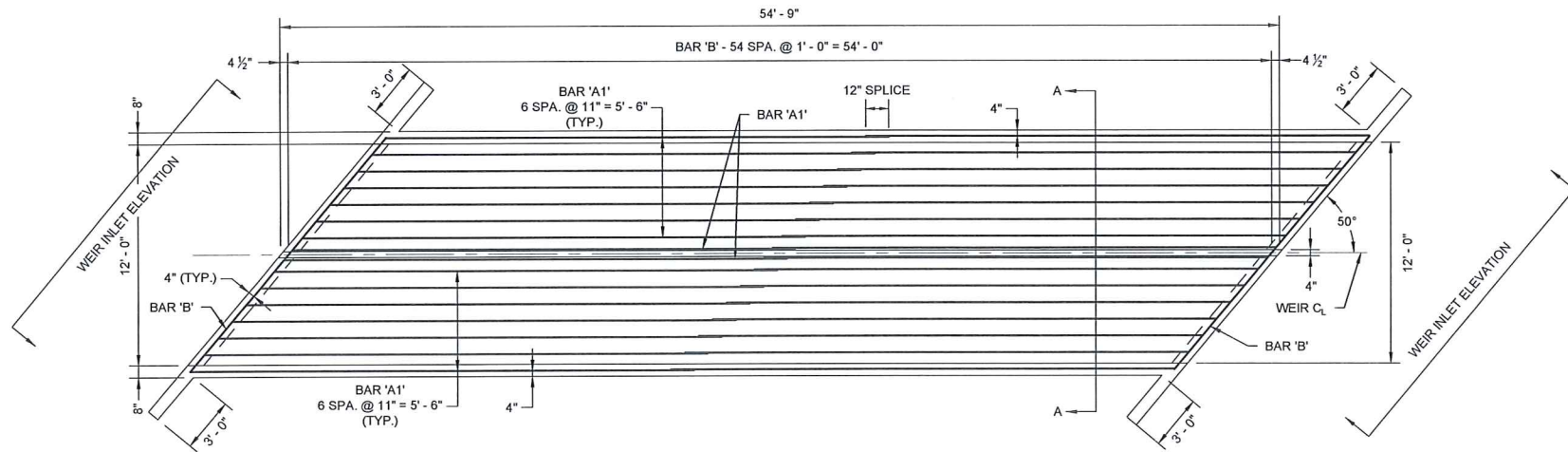
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
EMERGENCY SPILLWAY AND WEIR LAYOUT
AND DETAILS



Date	FEBRUARY 2023
Scale	AS SHOWN
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

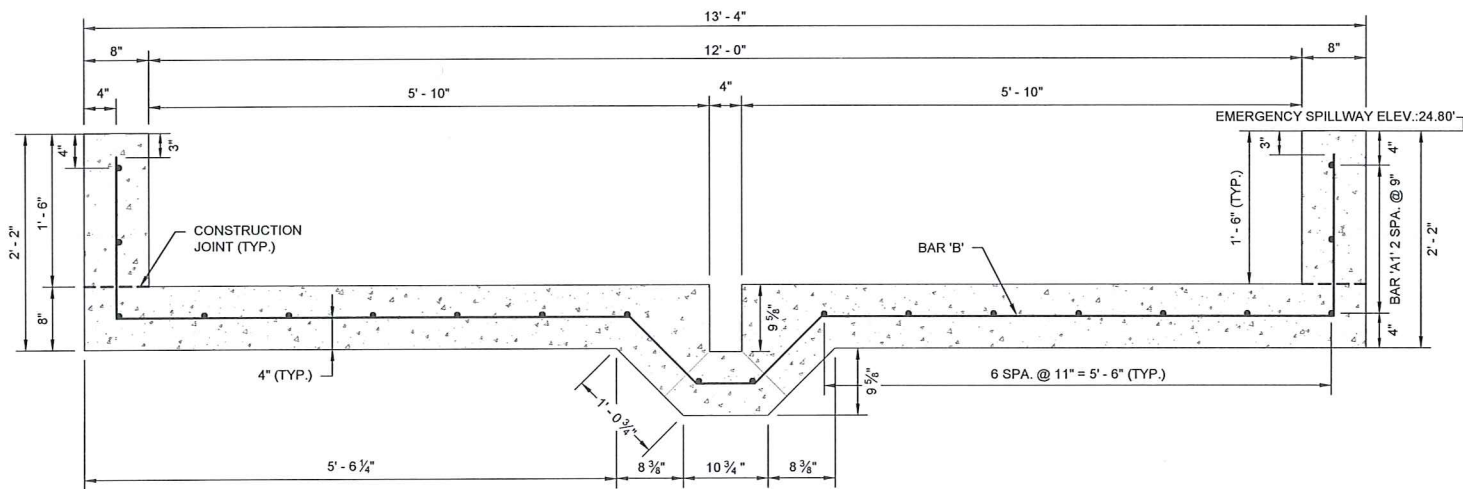
TS1.4



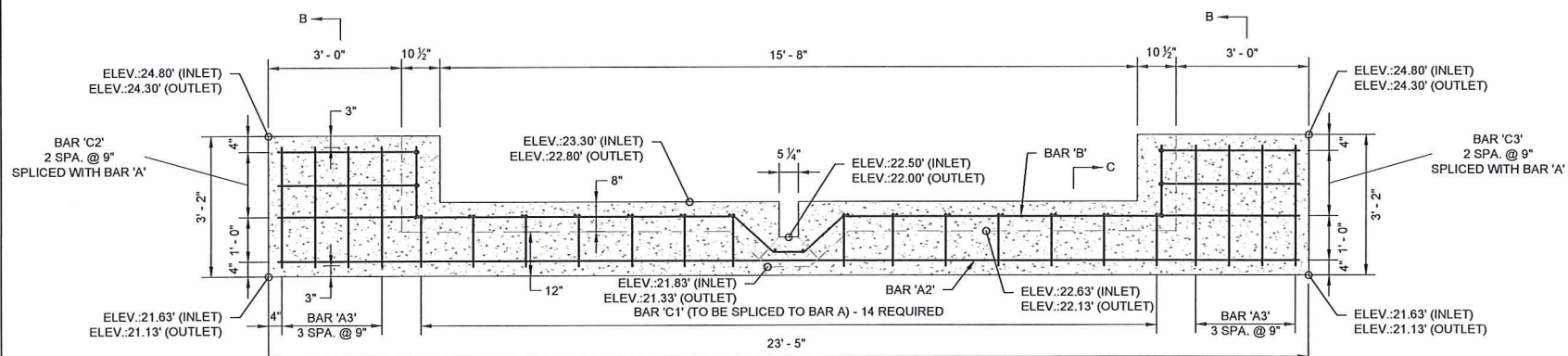
WEIR PLAN VIEW
SCALE:1\"/>

GENERAL NOTES:

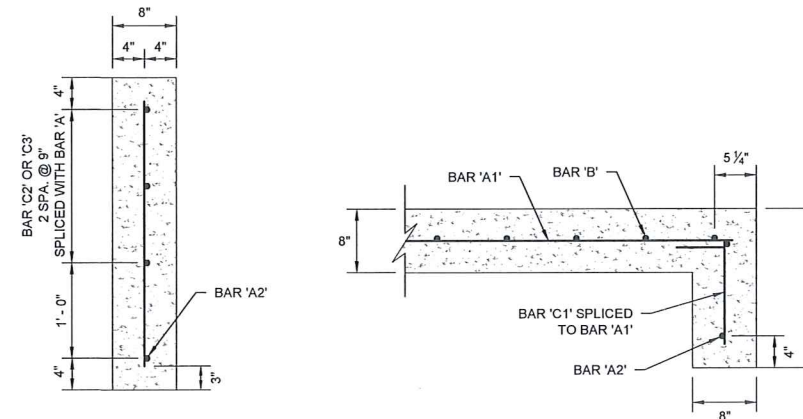
1. REINFORCING STEEL: REINFORCING STEEL SHALL BE ASTM A615 - GRADE 60.
2. CONCRETE: $f'_c = 4,000$ psi
3. CHAMFER ALL EXPOSE CORNERS 1\"/>



WEIR SECTION VIEW (SECTION A-A)
SCALE:1\"/>

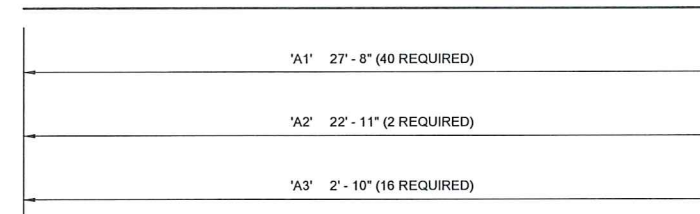


INLET AND OUTLET
WEIR FRONT ELEVATION
SCALE:1\"/>

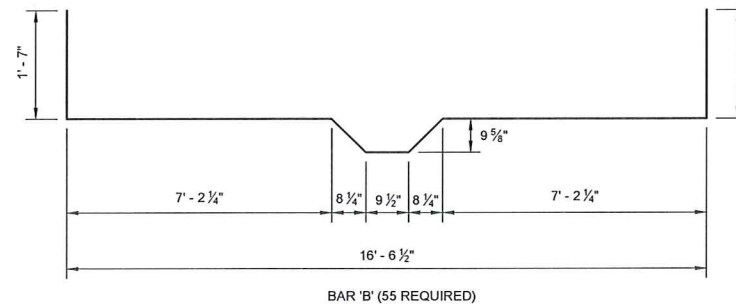


SECTION B-B
SCALE:1\"/>

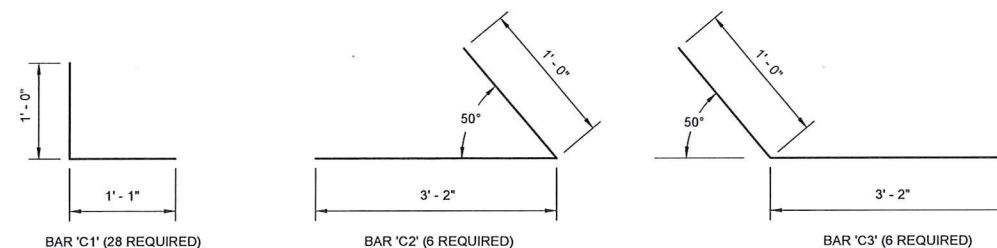
SECTION C-C
SCALE:1\"/>



'A' BARS



'B' BARS



'C' BARS

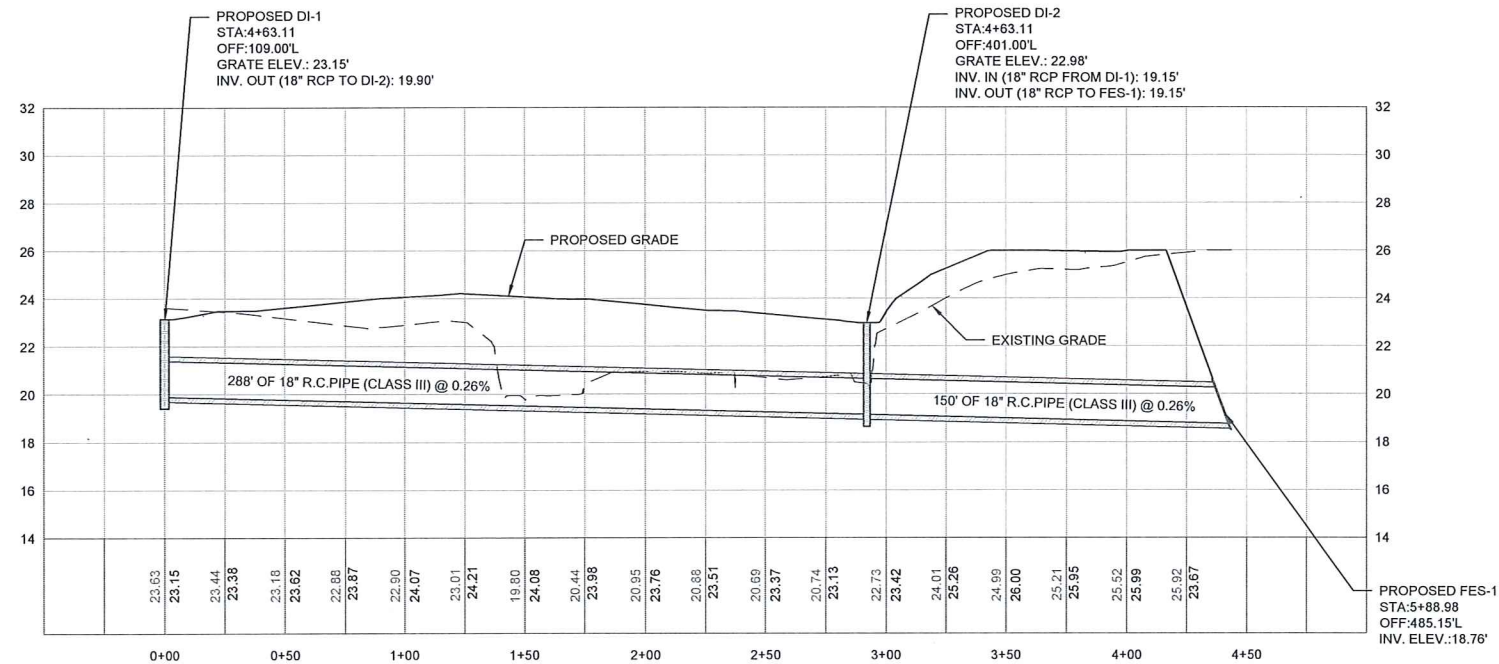
NOTE: ALL REINFORCING STEEL IS #4 BAR. ALL REINFORCING STEEL SHALL BE ASTM 615 GRADE 60.

REINFORCING STEEL DETAILS
NOT TO SCALE

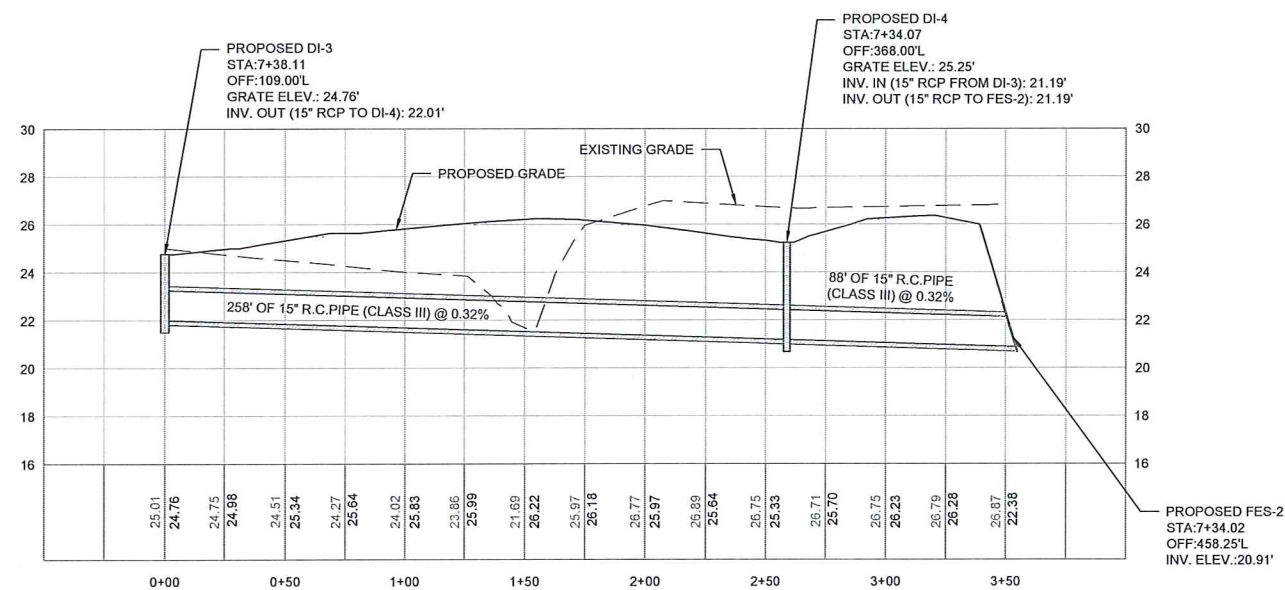
REV. NO.	DESCRIPTION	DATE



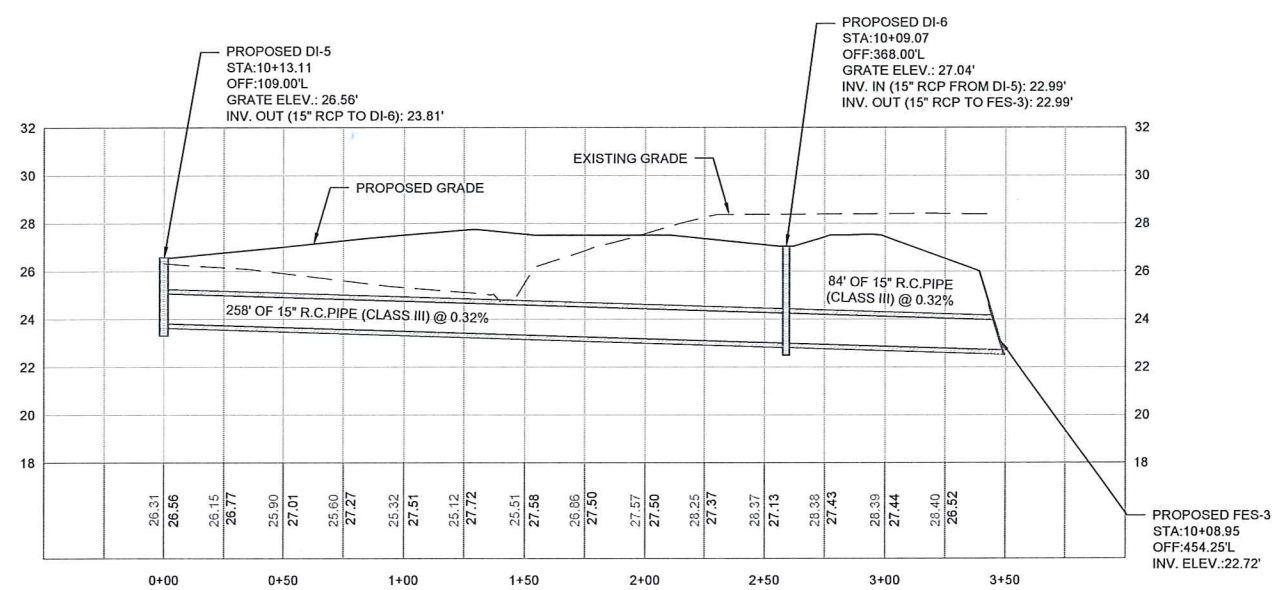
Date	FEBRUARY 2023
Scale	AS SHOWN
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	



STORMWATER INFRASTRUCTURE PROFILE - PROPOSED DI-1, DI-2 AND FES-1



STORMWATER INFRASTRUCTURE PROFILE - PROPOSED DI-3, DI-4 AND FES-2



STORMWATER INFRASTRUCTURE PROFILE - PROPOSED DI-5, DI-6 AND FES-3

REV.	NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
PIPE PROFILES



Date	FEBRUARY 2023
Scale	V: 1" = 4' H: 1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

MATCH LINE STA:10+50.00
SEE SHEET EC1.1

MATCH LINE
SEE SHEET EC1.4

INSTALL TEMPORARY
SILT FENCE (TYP.)

FOR WORK IN THIS AREA
SEE GENERAL NOTE 7

PHASE 2 WORK AREA (SEE SHEET A1.6 FOR CLOSURE AND PHASING REQUIREMENTS)

PHASE 1 WORK AREA (SEE SHEET A1.4 FOR PHASING REQUIREMENTS)

INSTALL 4' BERMUDA SOD
OUTSIDE NEW PAVEMENT
EDGE (TYP.).

PROPOSED
BITUMINOUS
PAVEMENT (TYP.)

INSTALL 4' BERMUDA SOD
OUTSIDE NEW PAVEMENT
EDGE (TYP.).

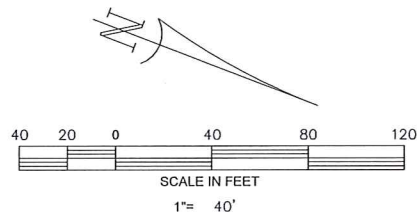
MAXIMUM TOTAL LIMITS OF
DISTURBANCE = 29.0 ACRES

PROPOSED LIMITS OF
DISTURBANCE (TYP.).

GENERAL NOTES:

- SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
- SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
- SEE G SERIES SHEETS FOR GRADING AND STORMWATER INFRASTRUCTURE INFORMATION.
- FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
- SEE SEDIMENTATION AND EROSION CONTROL NOTES AND DETAILS ON SHEETS EC1.5 AND EC1.6.
- SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.
- WORK WITHIN PHASE 2 WORK AREA WILL REQUIRE CLOSURE OF RUNWAY 5-23 AND SHALL BE COMPLETED AT NIGHT BETWEEN 9:00 PM AND 6:00 AM. SEE PHASING PLAN AND CLOSURE REQUIREMENTS ON SHEET A1.6. WORK ACTIVITIES WILL INCLUDE SOD INSTALLATION AND SEEDING AND MULCHING. PRIOR TO REOPENING THERE SHALL BE NO HOLES OR TRENCHES LEFT OPEN.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT
	PROPOSED SOD
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM DRAIN PIPE
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	TEMPORARY SILT FENCE
	LIMITS OF DISTURBANCE



GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
SEDIMENTATION AND EROSION CONTROL PLAN
(SHEET 2 OF 4)



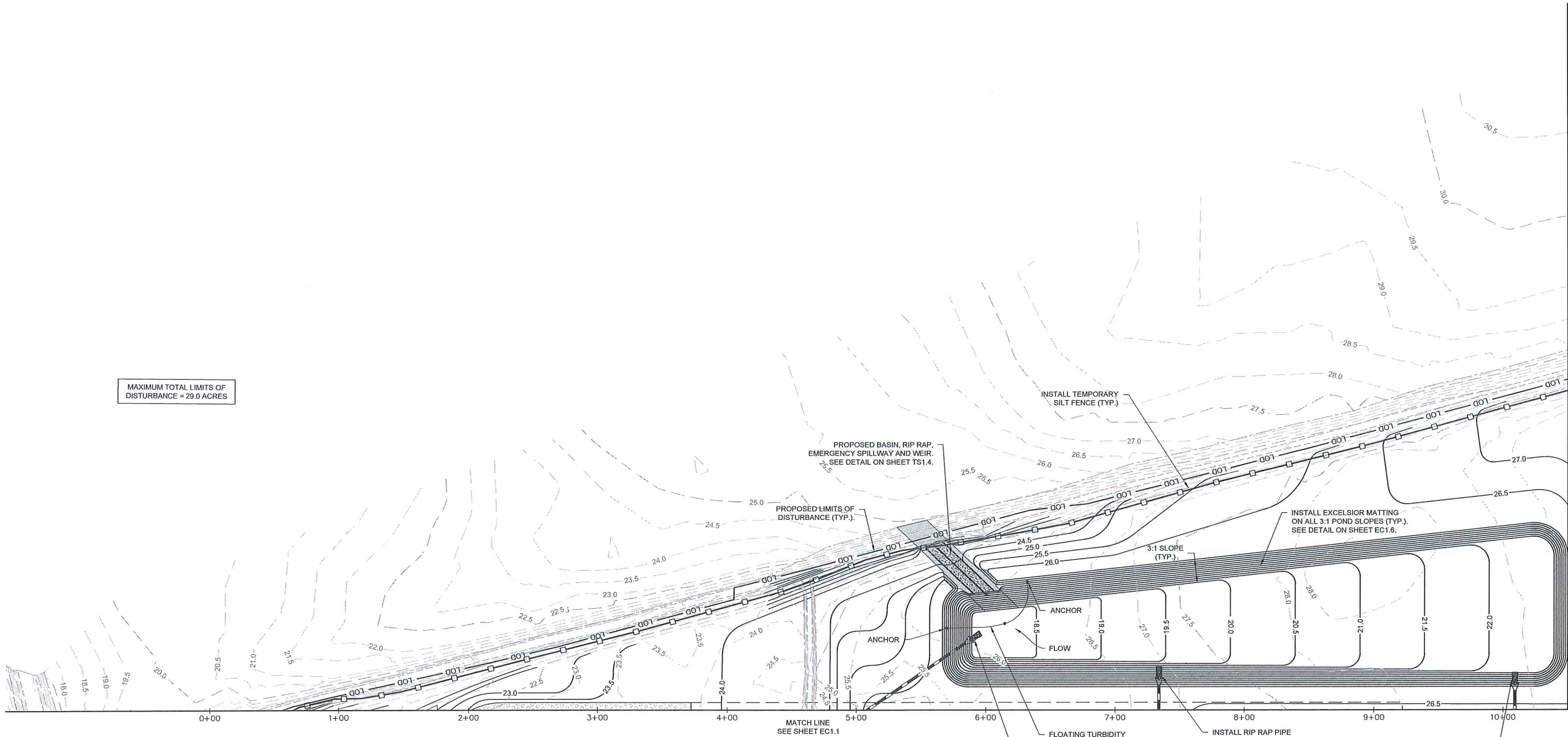
Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

EC1.2

THIS DRAWING AND THE DESIGN
SHOWN IS THE PROPERTY OF
TALBERT & BRIGHT, INC. THE
REPRODUCTION, COPY OR USE
OF THIS DRAWING WITHOUT
THEIR WRITTEN CONSENT IS
PROHIBITED.

REV. NO.	DESCRIPTION	DATE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILL@TBILK.COM
SC LICENSE NO. 000396

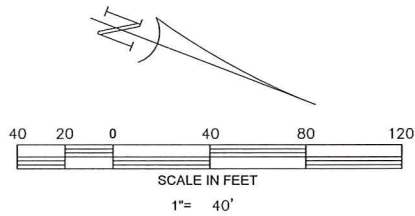


MAXIMUM TOTAL LIMITS OF
DISTURBANCE = 29.0 ACRES

GENERAL NOTES:

1. SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
2. SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
3. SEE G SERIES SHEETS FOR GRADING AND STORMWATER INFRASTRUCTURE INFORMATION.
4. FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
5. SEE SEDIMENTATION AND EROSION CONTROL NOTES AND DETAILS ON SHEETS EC1.5 AND EC1.6.
6. SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.

LEGEND	
	RIPRAP PIPE OUTLET PROTECTION
	PROPOSED HANGAR ACCESS DRIVE
	PROPOSED EXCELSIOR MATTING
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED STORM DRAIN PIPE
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	TEMPORARY SILT FENCE
	FUTURE ACCESS ROAD
	LOD LIMITS OF DISTURBANCE



MATCH LINE STA 10+50.00
SEE SHEET EC1.4

THIS DRAWING AND THE DESIGN
SHOWN IS THE PROPERTY OF
TALBERT & BRIGHT, INC. THE
REPRODUCTION, COPY, OR USE
HEREOF WITHOUT THE WRITTEN
CONSENT OF TALBERT & BRIGHT, INC.
IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
SEDIMENTATION AND EROSION CONTROL PLAN
(SHEET 3 OF 4)

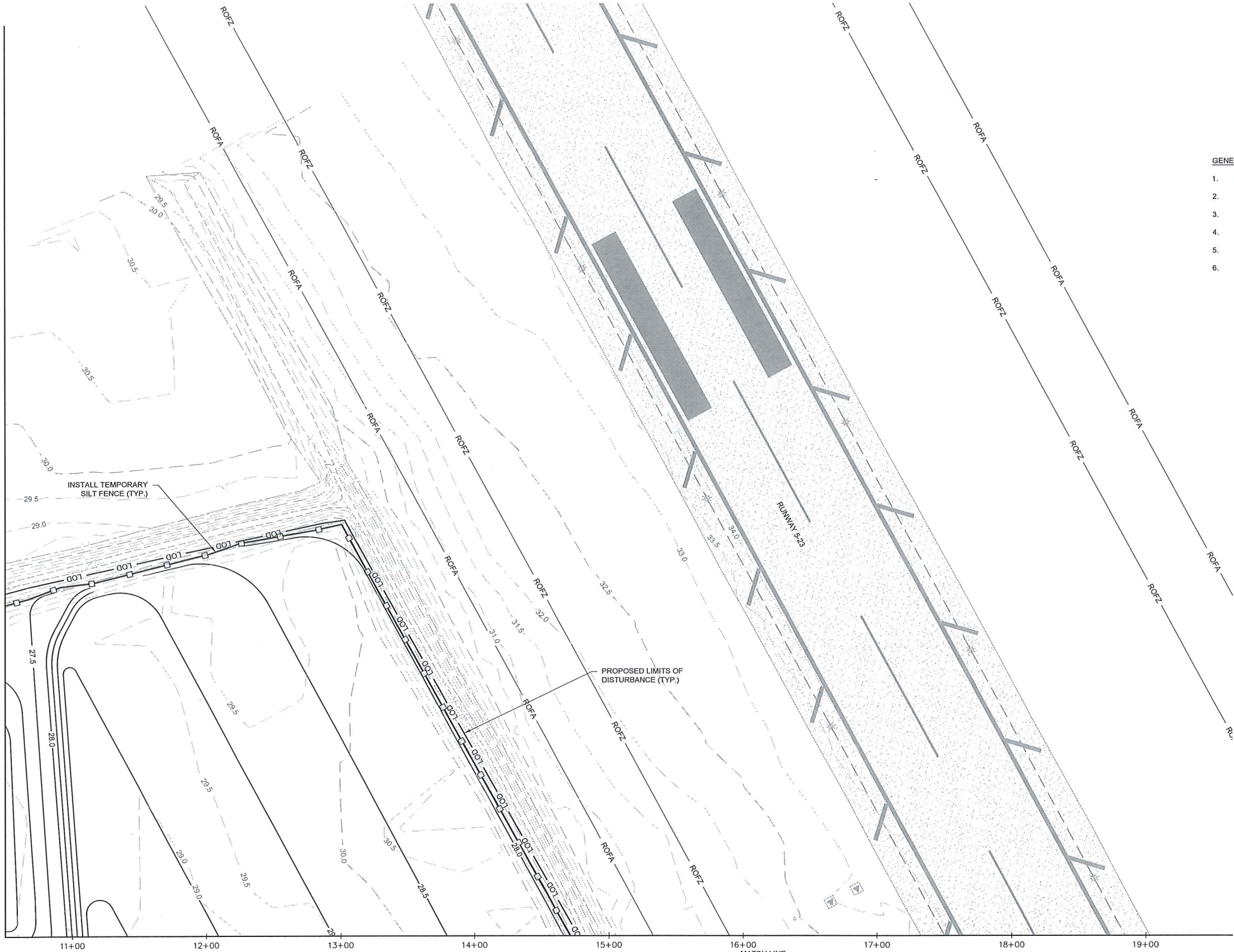


Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

EC1.3

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-6281
FAX: 910-763-5350
EMAIL: TBILLMOTBILM.COM
SC LICENSE NO. 000366

MATCH LINE STA:10+50.00
SEE SHEET EC1.3

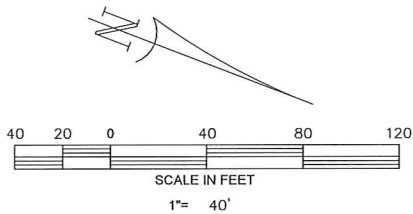


MAXIMUM TOTAL LIMITS OF
DISTURBANCE = 29.0 ACRES

GENERAL NOTES:

1. SEE EX SERIES SHEETS FOR EXISTING CONDITIONS AND DEMOLITION ITEMS.
2. SEE SHEETS SL1.1 AND SL1.2 FOR PAVING AND GEOMETRY LAYOUT.
3. SEE G SERIES SHEETS FOR GRADING AND STORMWATER INFRASTRUCTURE INFORMATION.
4. FOR TYPICAL SECTIONS AND DRAINAGE DETAILS SEE TS SERIES SHEETS.
5. SEE SEDIMENTATION AND EROSION CONTROL NOTES AND DETAILS ON SHEETS EC1.5 AND EC1.6.
6. SEE PHASING PLANS SHEETS A1.4 AND A1.6 FOR SEQUENCE OF CONSTRUCTION.

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING TOP OF DITCH
	EXISTING BOTTOM OF DITCH
	TEMPORARY SILT FENCE
	LIMITS OF DISTURBANCE



GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
SEDIMENTATION AND EROSION CONTROL PLAN
(SHEET 4 OF 4)



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

EC1.4

THIS DRAWING AND THE DESIGN
SHOWN IS THE PROPERTY OF
TALBERT & BRIGHT, INC. THE
REPRODUCTION, COPY OR USE
OF THIS DRAWING WITHOUT
THEIR WRITTEN CONSENT IS
PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

REV.	NO.	DESCRIPTION	DATE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-5350
FAX: 910-762-5350
EMAIL: TBILL@TBILM.COM
SC LICENSE NO. 000356

SILT FENCE - GENERAL NOTES

- DO NOT PLACE SILT FENCE ACROSS CHANNELS OR IN OTHER AREAS SUBJECT TO CONCENTRATED FLOWS. SILT FENCE SHOULD NOT BE USED AS A VELOCITY CONTROL BMP. CONCENTRATED FLOWS ARE ANY FLOWS GREATER THAN 0.5 CFS.
- MAXIMUM SHEET OR OVERLAND FLOW PATH LENGTH TO THE SILT FENCE SHALL BE 100- FEET.
- MAXIMUM SLOPE STEEPNESS (NORMAL [PERPENDICULAR] TO THE FENCE LINE) SHALL BE 2:1.
- SILT FENCE JOINTS, WHEN NECESSARY, SHALL BE COMPLETED BY ONE OF THE FOLLOWING OPTIONS:
 - WRAP EACH FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 1-FOOT MINIMUM OVERLAP;
 - OVERLAP SILT FENCE BY INSTALLING 3- FEET PASSED THE SUPPORT POST TO WHICH THE NEW SILT FENCE ROLL IS ATTACHED. ATTACH OLD ROLL TO NEW ROLL WITH HEAVY-DUTY PLASTIC TIES; OR,
 - OVERLAP ENTIRE WIDTH OF EACH SILT FENCE ROLL FROM ONE SUPPORT POST TO THE NEXT SUPPORT POST.
- ATTACH FILTER FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED WITHIN THE TOP 8-INCHES OF THE FABRIC.
- INSTALL THE SILT FENCE PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE THE SILT FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.
- INSTALL SILT FENCE CHECKS (TIE-BACKS) EVERY 50-100 FEET, DEPENDENT ON SLOPE, ALONG SILT FENCE THAT IS INSTALLED WITH SLOPE AND WHERE CONCENTRATED FLOWS ARE EXPECTED OR ARE DOCUMENTED ALONG THE PROPOSED/INSTALLED SILT FENCE.

MATERIALS - STEEL POSTS

- USE 48-INCH LONG STEEL POSTS THAT MEET THE FOLLOWING MINIMUM PHYSICAL REQUIREMENTS:
- COMPOSED OF HIGH STRENGTH STEEL WITH MINIMUM YIELD STRENGTH OF 50,000 PSI.
 - HAVE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND NOMINAL "T" LENGTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (±8%).
 - HAVE A SOIL STABILIZATION PLATE WITH A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES ATTACHED TO THE STEEL POSTS.
 - PAINTED WITH A WATER BASED BAKED ENAMEL PAINT.

- USE STEEL POSTS WITH A MINIMUM LENGTH OF 48 INCHES, WEIGHING 1.25 POUNDS PER LINEAR FOOT (± 8%) WITH PROJECTIONS TO AID IN FASTENING THE FABRIC. WHEN HEAVY CLAY SOILS ARE PRESENT ON SITE, STEEL POSTS WILL HAVE A METAL SOIL STABILIZATION PLATE WELDED NEAR THE BOTTOM SUCH THAT WHEN THE POST IS DRIVEN TO THE PROPER DEPTH, THE PLATE WILL BE BELOW THE GROUND LEVEL FOR ADDED STABILITY. THE SOIL PLATES SHOULD HAVE THE FOLLOWING CHARACTERISTICS:
- BE COMPOSED OF MINIMUM 15 GAUGE STEEL.
 - HAVE A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES.

GEOTEXTILE FILTER FABRIC

- SILT FENCE MUST BE COMPOSED OF WOVEN GEOTEXTILE FILTER FABRIC THAT CONSISTS OF THE FOLLOWING REQUIREMENTS:
 - COMPOSED OF FIBERS CONSISTING OF LONG CHAIN SYNTHETIC POLYMERS OF AT LEAST 85% BY WEIGHT OF POLYOLEFINS, POLYESTERS, OR POLYAMIDES THAT ARE FORMED INTO A NETWORK SUCH THAT THE FILAMENTS OR YARNS RETAIN DIMENSIONAL STABILITY RELATIVE TO EACH OTHER;
 - FREE OF ANY TREATMENT OR COATING WHICH MIGHT ADVERSELY ALTER ITS PHYSICAL PROPERTIES AFTER INSTALLATION; FREE OF ANY DEFECTS OR FLAWS THAT SIGNIFICANTLY AFFECT ITS PHYSICAL AND/OR FILTERING PROPERTIES; AND
 - HAVE A MINIMUM WIDTH OF 36-INCHES.
- USE ONLY FABRIC APPEARING ON SC DOT'S QUALIFIED PRODUCTS LISTING (QPL), APPROVAL SHEET #34, MEETING THE REQUIREMENTS OF THE MOST CURRENT EDITION OF THE SC DOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
- 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED.
- FILTER FABRIC SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS.
- FILTER FABRIC SHALL BE INSTALLED AT A MINIMUM OF 24-INCHES ABOVE THE GROUND.

TEMPORARY SILT FENCE

NOT TO SCALE

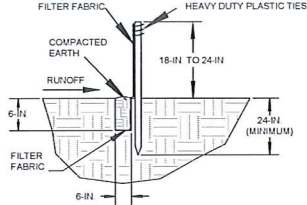
INSTALLATION

- EXCAVATE A TRENCH APPROXIMATELY 6-INCHES WIDE AND 6-INCHES DEEP WHEN PLACING FABRIC BY HAND. PLACE 12-INCHES OF GEOTEXTILE FABRIC INTO THE 6-INCH DEEP TRENCH, EXTENDING THE REMAINING 6-INCHES TOWARDS THE UPSLOPE SIDE OF THE TRENCH. BACKFILL THE TRENCH WITH SOIL OR GRAVEL AND COMPACT.
- BURY 12-INCHES OF FABRIC INTO THE GROUND WHEN PNEUMATICALLY INSTALLING SILT FENCE WITH A SLICING METHOD.
- PURCHASE FABRIC IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS. WHEN JOINTS ARE NECESSARY, WRAPPED THE FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 6-INCH MINIMUM OVERLAP.
- INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. INSTALL POSTS A MINIMUM OF 1 TO 2 INCHES ABOVE THE FABRIC, WITH NO MORE THAN 3- FEET OF THE POST ABOVE THE GROUND. SPACE POSTS TO MAXIMUM 6- FEET CENTERS.
- ATTACH FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED AND PLACED IN A MANNER TO PREVENT SAGGING OR TEARING OF THE FABRIC. IN ALL CASES, TIES SHOULD BE AFFIXED IN NO LESS THAN 4 PLACES.
- INSTALL THE FABRIC A MINIMUM OF 24-INCHES ABOVE THE GROUND. WHEN NECESSARY, THE HEIGHT OF THE FENCE ABOVE GROUND MAY BE GREATER THAN 24-INCHES. IN TIDAL AREAS, EXTRA SILT FENCE HEIGHT MAY BE REQUIRED.
- THE POST HEIGHT WILL BE TWICE THE EXPOSED POST HEIGHT. POST SPACING WILL REMAIN THE SAME AND EXTRA HEIGHT FABRIC WILL BE 4-, 5-, OR 6- FEET TALL.
- LOCATE SILT FENCE CHECKS EVERY 100 FEET MAXIMUM AND AT LOW POINTS.
- INSTALL THE FENCE PERPENDICULAR TO THE DIRECTION OF FLOW AND PLACE THE FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.

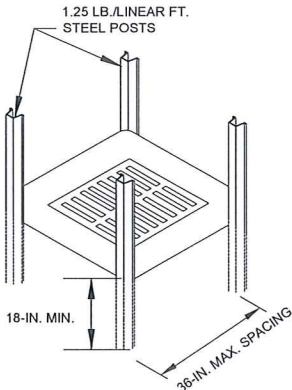
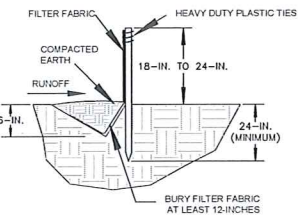
INSPECTION AND MAINTENANCE

- THE KEY TO FUNCTIONAL SILT FENCE IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- REGULAR INSPECTIONS OF SILT FENCE SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVEN THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ATTENTION TO SEDIMENT ACCUMULATIONS ALONG THE SILT FENCE IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE SILT FENCE.
- REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH THE SILT FENCE, OR WHERE THE FENCE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE SILT FENCE. INSTALL CHECKS/TIE-BACKS AND/OR REINSTALL SILT FENCE, AS NECESSARY.
- CHECK FOR TEARS WITHIN THE SILT FENCE, AREAS WHERE SILT FENCE HAS BEGUN TO DECOMPOSE, AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE SILT FENCE INEFFECTIVE. REMOVED DAMAGED SILT FENCE AND REINSTALL NEW SILT FENCE IMMEDIATELY.
- SILT FENCE SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

FLAT-BOTTOM TRENCH DETAIL



V-SHAPED TRENCH DETAIL



POST INSTALLATION DETAIL

INLET PROTECTION GENERAL NOTES

- USE HARDWARE FABRIC OR COMPARABLE WIRE MESH WITH MAXIMUM OPENINGS OF 0.5-INCHES X 0.5-INCHES AS THE SUPPORTING MATERIAL.
- USE STEEL POSTS THAT MEET THE FOLLOWING PHYSICAL REQUIREMENTS:
 - BE COMPOSED OF HIGH STRENGTH STEEL WITH A MINIMUM YIELD OF 50,000 PSI.
 - HAVE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38 INCHES AND A NOMINAL "T" WIDTH OF 1.48-INCHES.
 - WEIGH 1.25 POUNDS PER FOOT (±8%)
- USE HEAVY-DUTY WIRE TIES TO ATTACH THE WIRE MESH MATERIAL TO THE STEEL POSTS.
- SPACE THE STEEL POSTS A MAXIMUM OF 3- FEET APART AROUND THE PERIMETER OF THE INLET AND DRIVE THEM INTO THE GROUND A MINIMUM OF 18-INCHES.
- EXCAVATE A TRENCH 6-INCHES DEEP AROUND THE OUTSIDE PERIMETER OF THE INLET TO INSTALL WIRE MESH. BACKFILL THE TRENCH WITH SOIL OR CRUSHED STONE AND COMPACT OVER THE WIRE MESH.
- PLACE AGGREGATE NO. 5 WASHED STONE (OR 1-INCH D50 STONE) TO A MINIMUM HEIGHT OF 12-INCHES, AND A MAXIMUM OF 24-INCHES AGAINST THE WIRE MESH ON ALL SIDES.

ATTACH 1/2 X 1/2 IN. MAX. OPENING WIRE MESH TO POSTS WITH WIRE TIES SPACED A MAX. OF 6-IN. APART

FOLD WIRE MESH TO OVERLAP ENDS AND SECURE TO POSTS WITH WIRE TIES

BURY WIRE MESH (SEE DETAIL)

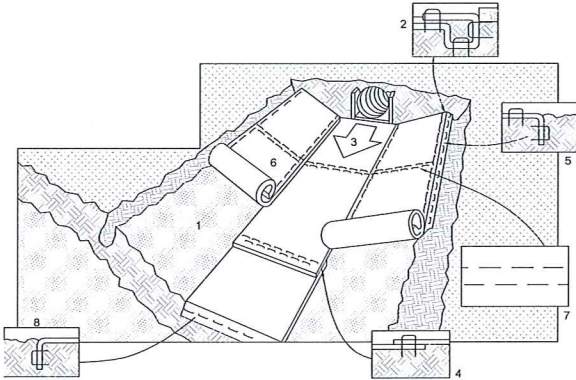
WIRE MESH INSTALLATION DETAIL

INSPECTION AND MAINTENANCE

- THE KEY TO FUNCTIONAL INLET PROTECTION IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- REGULAR INSPECTIONS OF WIRE MESH AND STONE INLET PROTECTION SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVEN THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION.
- ATTENTION TO SEDIMENT ACCUMULATIONS IN FRONT OF THE INLET PROTECTION IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN THE SEDIMENT REACHES 1/3 HEIGHT OF THE STONE FILL OR WHEN STONE BECOMES CLOGGED. WHEN A SUMP IS INSTALLED IN FRONT OF INLET PROTECTION, SEDIMENT SHOULD BE REMOVED WHEN IT FILLS APPROXIMATELY 1/3 THE DEPTH OF THE SUMP.
- LARGE DEBRIS, TRASH, AND LEAVES SHOULD BE REMOVED FROM IN FRONT OF THE INLET PROTECTION WHEN FOUND.
- AFTER ACCUMULATED SEDIMENT IS REMOVED, PULL STONES FROM AROUND WIRE MESH TO WASH OR REPLACE WITH FRESH STONES AS NECESSARY.
- INLET PROTECTION STRUCTURES SHOULD BE REMOVED AFTER THE DISTURBED AREAS ARE PERMANENTLY STABILIZED. REMOVE ALL CONSTRUCTION MATERIAL AND SEDIMENT, AND DISPOSE OF THEM. PROPERLY GRADE THE DISTURBED AREA TO THE ELEVATION OF THE DROP INLET CREST. STABILIZE ALL BARE AREAS IMMEDIATELY.

TEMPORARY INLET PROTECTION

NOT TO SCALE



GENERAL INSTALLATION DETAIL

NOT TO SCALE

NOTE:
SYNTHETIC MATTING TO BE AMERICAN EXCELSIOR COMPANY CURLEX I
EXCELSIOR EROSION CONTROL BLANKET OR ENGINEER APPROVED EQUAL.

EROSION CONTROL MATTING NOTES:

HORIZONTAL STAPLE SPACING MAY BE ALTERED IF NECESSARY TO ALLOW STAPLES TO SECURE THE CRITICAL POINTS ALONG THE CHANNEL SURFACE. REFER TO MANUFACTURE'S GENERAL STAPLE PATTERN GUIDE FOR CORRECT STAPLE RECOMMENDATIONS FOR CHANNELS.

- PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF LIME, FERTILIZER, AND SEED.
- BEGIN AT THE TOP OF THE CHANNEL BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
- ROLL CENTER BLANKET IN DIRECTION OF WATER FLOW ON BOTTOM OF CHANNEL.
- PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH A 6" OVERLAP. USE A DOUBLE ROW OF STAGGERED STAPLES 4" APART TO SECURE BLANKETS.
- BLANKETS ON SIDE SLOPES MUST BE OVERLAPPED 4" OVER THE CENTER BLANKET AND STAPLED.
- THE TERMINAL END OF THE BLANKETS MUST BE ANCHORED IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

MAINTENANCE:

INSPECT ALL MATTING PERIODICALLY, AND AFTER RAINSTORMS TO CHECK FOR RILL EROSION, DISLOCATION, OR FAILURE. WHERE EROSION IS OBSERVED, REPAIR EROSION AND MATTING IN ERODED AREA. IF WASHOUT OCCURS, REPAIR THE SLOPE GRADE, RESEED, AND REINSTALL MATTING. CONTINUE INSPECTIONS UNTIL VEGETATION IS FIRMLY ESTABLISHED.

EXCELSIOR MATTING DETAIL

NOT TO SCALE

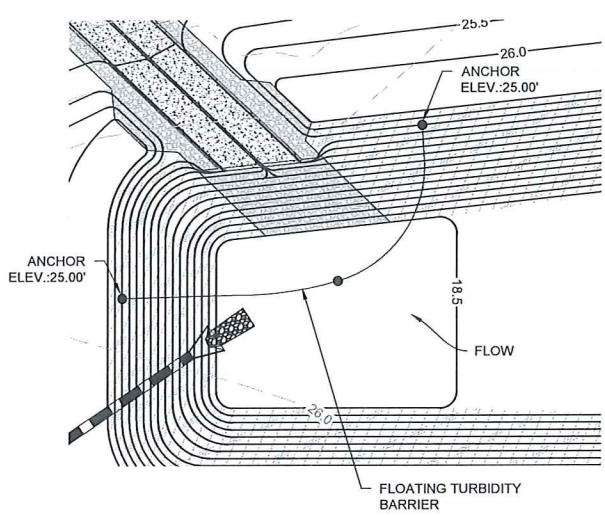


TABLE 1

TURBIDITY BARRIER NOTES:

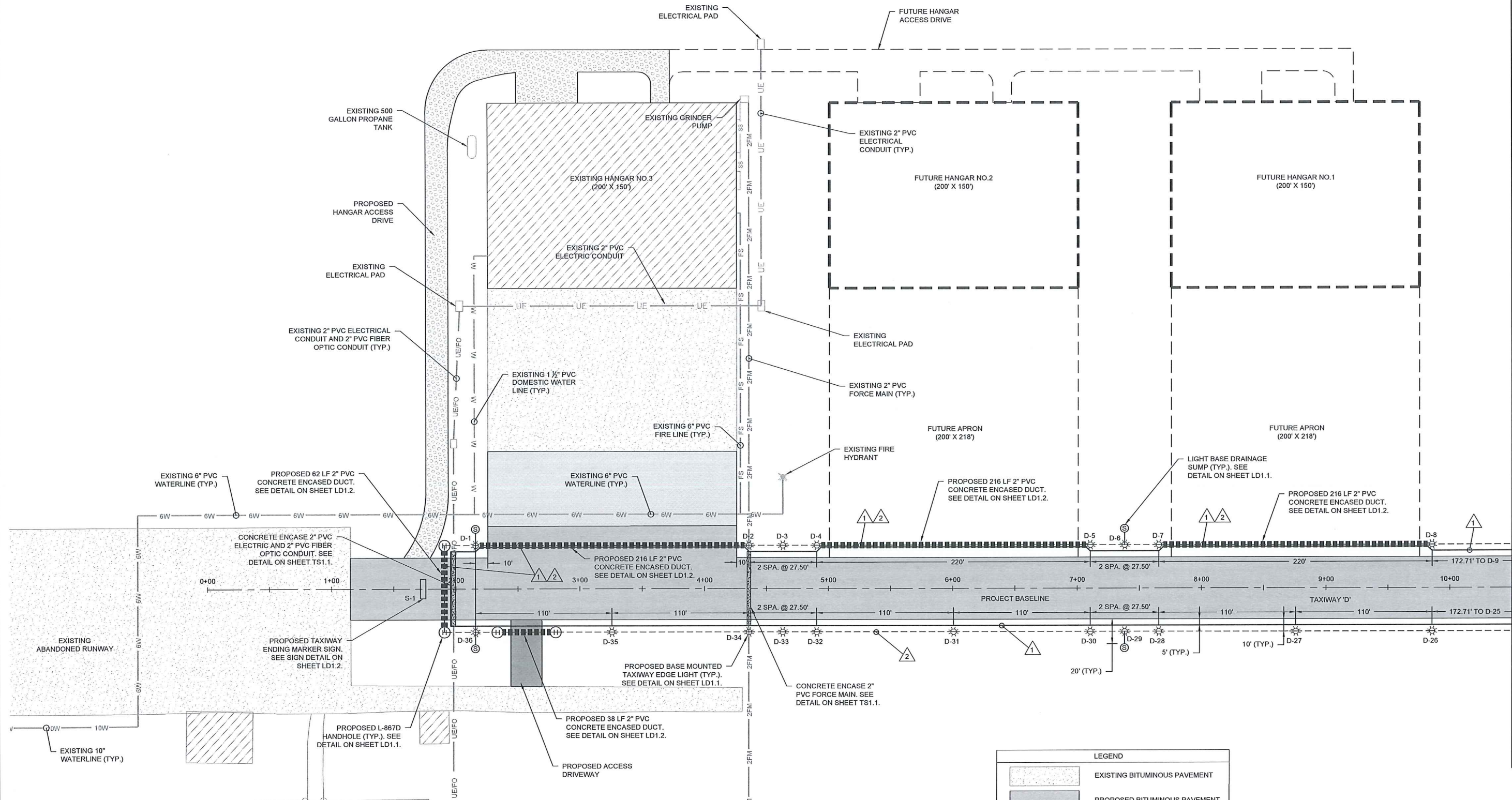
- THE PURPOSE OF THE FLOATING TURBIDITY BARRIER IS TO PROVIDE SEDIMENT PROTECTION CAUSED BY OVERLAND FLOW FROM PROJECT GRADING.
- MATERIALS USED IN THE FLOATING TURBIDITY BARRIER SHALL MEET TABLE 1 REQUIREMENTS.
- BOUYS USED IN CONJUNCTION WITH FLOATING TURBIDITY BARRIER SHALL BE INSTALLED AT 50' MAXIMUM SPACING AND COMPLY WITH SOUTH CAROLINA DEPARTMENT OF NATURAL RESOURCES LAW ENFORCEMENT BOUY SPECIFICATIONS.
- FLOATING TURBIDITY BARRIERS SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS, AND IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE END POINTS SHALL BE ANCHORED ON THE POND BANK AT CONTOUR ELEVATION 25' WITH SUFFICIENT SUPPORT TO SECURE THE BARRIER IN PLACE DURING TURBULENT CONDITIONS.

	MEDIUM DUTY
FABRIC - POLYESTER REINFORCED VINYL (OZ/SY)	22
FLOATATION (LB/FT)	22
TOP LOAD CABLE	---
STRESS PLATES	---
ROPE RETAINER	5/8" POLYPROPYLENE
GROMMETS	#4 BRASS
SEAMS HEAT WELDED	YES
BOTTOM LOAD CHAIN	5/8" GALVANIZED 0.95 LBS/FT (MIN)
CONNECTING HARDWARE	GALVANIZED STEEL
STANDARD DEPTH	5 FT. - 23 FT.
STANDARD LENGTH	50 & 100 FT.

FLOATING TURBIDITY BARRIER DETAIL

NOT TO SCALE

REV.	NO.	DESCRIPTION	DATE



GENERAL NOTES

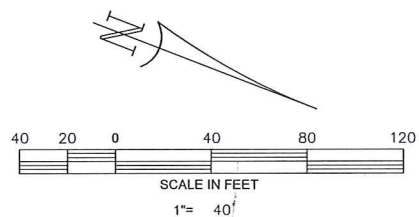
1. LOCATIONS OF EXISTING CIRCUITS/CONDUCTORS ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, HANDHOLES, AND PAPI ARE BASED ON AVAILABLE INFORMATION. LOCATIONS OF EXISTING UTILITIES PROVIDED BY THE OWNER. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. ANY EXISTING LIGHTING FACILITIES DAMAGED BY THE CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. SEE SHEET EX1.1 FOR ADDITIONAL INFORMATION.
2. THE EXISTING RUNWAY AND TAXIWAY LIGHTING SYSTEMS IN ACTIVE AIR OPERATIONS AREAS SHALL BE OPERATIONAL AT THE END OF EACH DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING ANY DAMAGE TO EXISTING LIGHTING SYSTEMS/CIRCUITS TO RESTORE LIGHTING OPERATIONS.

3. NEW INSTALLATIONS WHICH CROSS EXISTING INSTALLATIONS SHALL BE SEPARATED BY A MINIMUM OF 2 INCHES.
4. CONCRETE FOR BASE CAN INSTALLATION AND DUCT ENCASEMENT SHALL BE IN ACCORDANCE WITH SECTION P-610, STRUCTURAL PORTLAND CEMENT CONCRETE.
5. DUCT MARKERS SHALL BE INSTALLED AT THE ENDS OF ALL NEW DUCTS, AND WHERE THE NEW CABLE HOME RUN CHANGES DIRECTION AND EVERY 200' ALONG HOMERUN.
6. COPPER COUNTERPOISE GROUND WIRE SHALL BE CONNECTED TO 3/4" x 10' COPPER CLAD GROUND RODS SPACED AT MINIMUM 500' ALONG COUNTERPOISE TRENCH. WHERE DUCT RUNS CONTINUE BENEATH PAVEMENT, INSTALL THE COUNTERPOISE A MINIMUM OF 4" ABOVE DUCTS ALONG ENTIRE RUN. INSTALL GROUND RODS AT ENDS OF EACH DUCT. DO NOT INSTALL COUNTERPOISE THROUGH DUCT.
7. CONTRACTOR SHALL INSTALL NEW TAXIWAY LIGHTS AND CIRCUITS AS SHOWN. PROPOSED TAXIWAY LIGHTS SHALL BE INSTALLED 10' FROM PAVEMENT EDGE.
8. SEE SHEETS LD1.1 AND LD1.2 FOR LIGHTING INSTALLATION DETAILS.

CIRCUIT LEGEND

- 1 PROPOSED COUNTERPOISE
(1 - #6 AWG BARE COPPER COUNTERPOISE)
- 2 PROPOSED TAXIWAY CIRCUIT
(1-1/C, #8 AWG, 5KV, L-824C INSTALLED IN NEW 2" CONDUIT OR DUCT)

LEGEND	
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS OVERLAY
	PROPOSED TAXIWAY EDGE LIGHT
	PROPOSED HANDHOLE
	LIGHT BASE DRAINAGE SUMP
	PROPOSED DUCT
	PROPOSED TAXIWAY CIRCUIT
	PROPOSED COUNTERPOISE
	PROPOSED CONCRETE ENCASEMENT



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-5350
FAX: 910-762-5350
EMAIL: TBILLM@TBLM.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

REV.	NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

**HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK**

**LIGHTING LAYOUT AND CIRCUIT PLAN
(SHEET 1 OF 2)**

Date: FEBRUARY 2023

Scale: 1" = 40'

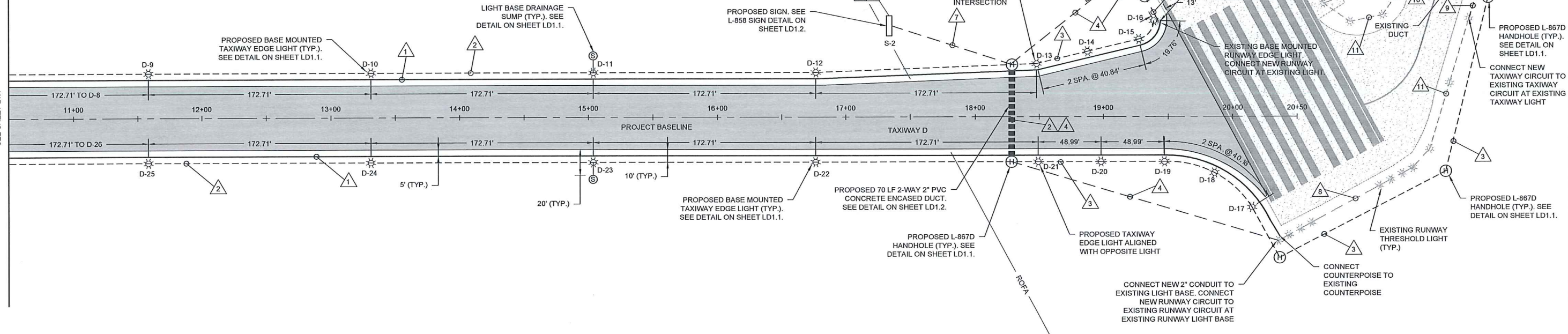
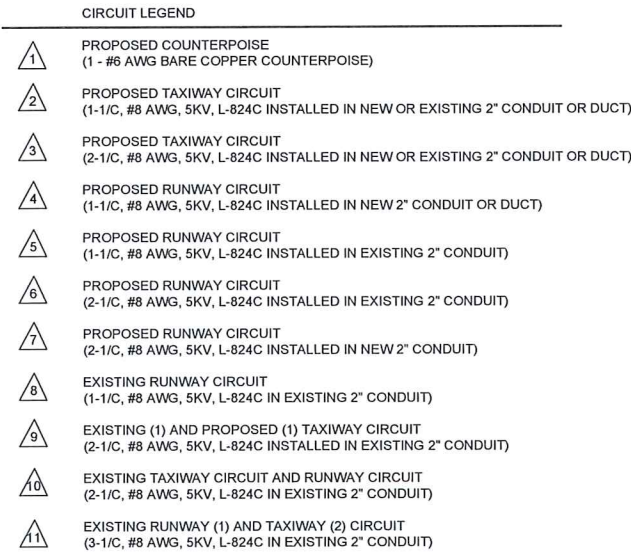
Drawn: NT

Checked: AMS

Project No: 2601-2101

Sheet No:

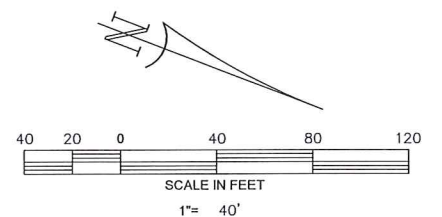
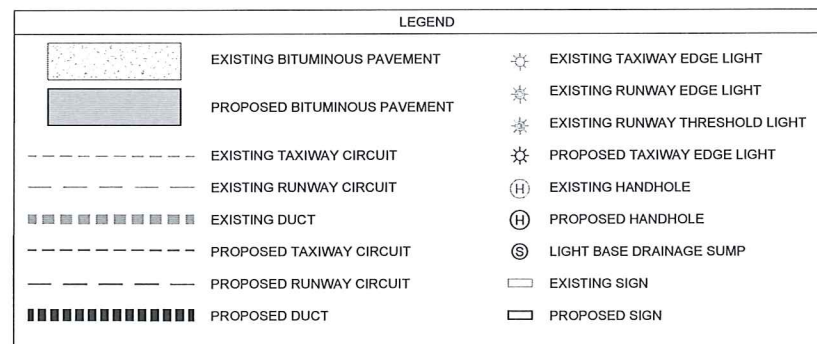
L1.1



GENERAL NOTES

1. LOCATIONS OF EXISTING CIRCUITS/CONDUCTORS ARE APPROXIMATE. LOCATIONS OF EXISTING LIGHTS, SIGNS, HANDHOLES, AND PAPI ARE BASED ON AVAILABLE INFORMATION. LOCATIONS OF EXISTING UTILITIES PROVIDED BY THE OWNER. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND CONDUITS, CIRCUITS, AND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND USE HAND EXCAVATION IN THE VICINITY OF EXISTING INSTALLATIONS TO REMAIN IN SERVICE. ANY EXISTING LIGHTING FACILITIES DAMAGED BY THE CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. SEE SHEET EX1.1 FOR ADDITIONAL INFORMATION.
2. THE EXISTING RUNWAY AND TAXIWAY LIGHTING SYSTEMS IN ACTIVE AIR OPERATIONS AREAS SHALL BE OPERATIONAL AT THE END OF EACH DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING ANY DAMAGE TO EXISTING LIGHTING SYSTEMS/CIRCUITS TO RESTORE LIGHTING OPERATIONS.
3. NEW INSTALLATIONS WHICH CROSS EXISTING INSTALLATIONS SHALL BE SEPARATED BY A MINIMUM OF 2 INCHES.
4. CONCRETE FOR BASE CAN INSTALLATION AND DUCT ENCASEMENT SHALL BE IN ACCORDANCE WITH SECTION P-610, STRUCTURAL PORTLAND CEMENT CONCRETE.

5. DUCT MARKERS SHALL BE INSTALLED AT THE ENDS OF ALL NEW DUCTS, AND WHERE THE NEW CABLE HOME RUN CHANGES DIRECTION AND EVERY 200' ALONG HOMERUN.
6. COPPER COUNTERPOISE GROUND WIRE SHALL BE CONNECTED TO $\frac{3}{4}$ " x 10' COPPER CLAD GROUND RODS SPACED AT MINIMUM 500' ALONG COUNTERPOISE TRENCH. WHERE DUCT RUNS CONTINUE BENEATH PAVEMENT, INSTALL THE COUNTERPOISE A MINIMUM OF 4" ABOVE DUCTS ALONG ENTIRE RUN. INSTALL GROUND RODS AT ENDS OF EACH DUCT. DO NOT INSTALL COUNTERPOISE THROUGH DUCT.
7. CONTRACTOR SHALL INSTALL NEW TAXIWAY LIGHTS AND CIRCUITS AS SHOWN. PROPOSED TAXIWAY LIGHTS SHALL BE INSTALLED 10' FROM PAVEMENT EDGE.
8. SEE SHEETS LD1.1 AND LD1.2 FOR LIGHTING INSTALLATION DETAILS.
9. CONTRACTOR TO INSTALL #8 AWG TEMPORARY JUMPER CABLE TO MAINTAIN EXISTING RUNWAY LIGHTING CIRCUIT AND TAXIWAY CIRCUIT IN OPERATION AT ALL TIMES.
10. WORK WITHIN THE PHASE 2 WORK AREA WILL REQUIRE CLOSURE OF RUNWAY 5-23 AND SHALL BE COMPLETED AT NIGHT BETWEEN 5:00 PM AND 5:00 AM. SEE PHASING PLAN AND CLOSURE REQUIREMENTS ON SHEET A1.6. WORK ELEMENTS WILL INCLUDE INSTALLATION OF COUNTERPOISE AND CONDUIT, CONCRETE ENCASED DUCT, LIGHTS, SIGNS, AND LIGHTING CIRCUIT. PRIOR TO REOPENING THERE SHALL BE NO HOLES OR TRENCHES LEFT OPEN.



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-762-4281
FAX: 910-763-5350

EMAIL: TBILL@TBILLM.COM
FENSE NO. C00366

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

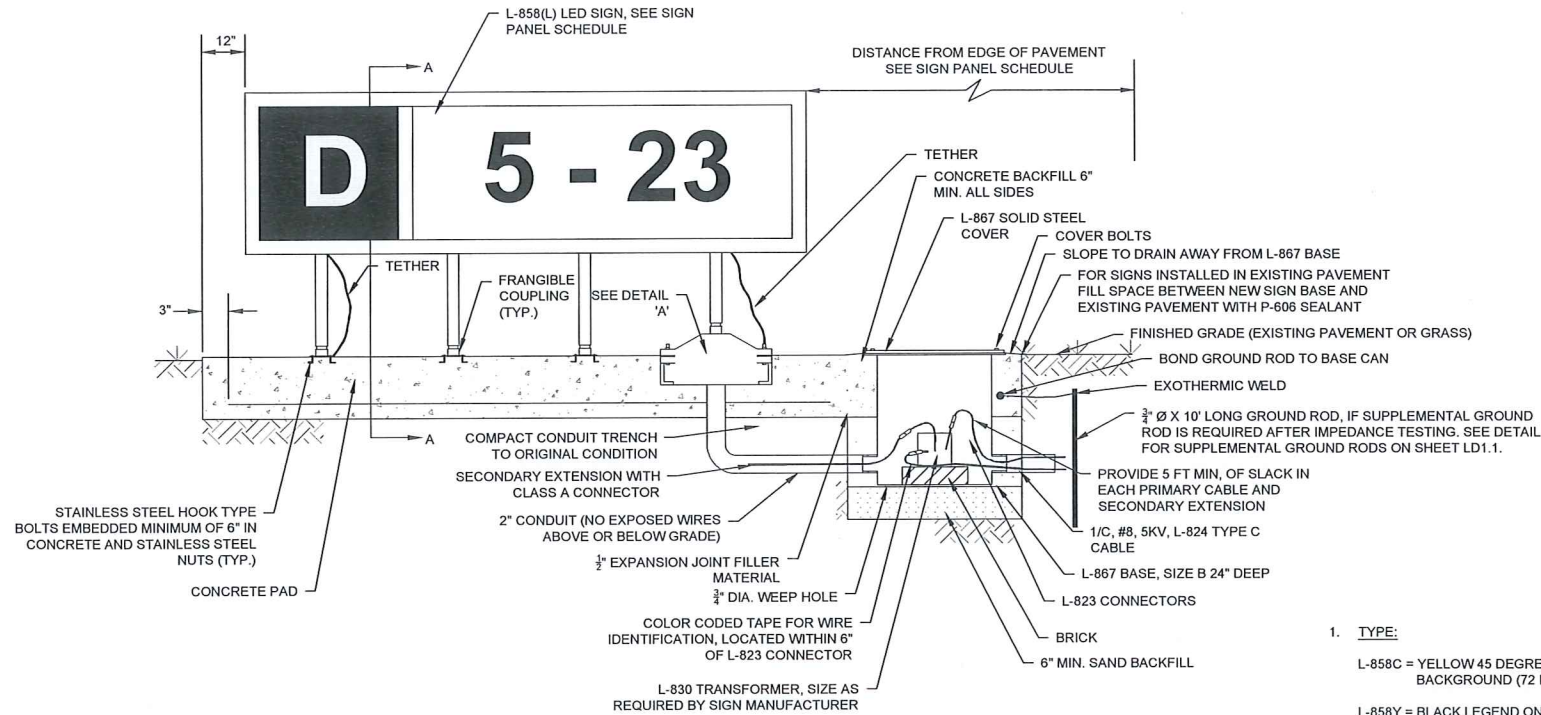
[illegible]

HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
LIGHTING LAYOUT AND CIRCUIT PLAN
(SHEET 2 OF 2)



note	FEBRUARY 2023
scale	1" = 40'
drawn	NT
checked	AMS
project No.	2601-2101
sheet No.	

L1.2



NOTES:

1. PROVIDE NEW SIGNS WITH ON/OFF SWITCH AND TETHERS.
2. NUMBER AND SPACING OF LEGS AS PER MANUFACTURER'S REQUIREMENTS.

1. TYPE:

L-858C = YELLOW 45 DEGREE DIAGONAL STRIPES ON A BLACK BACKGROUND (72 INCHES LENGTH)

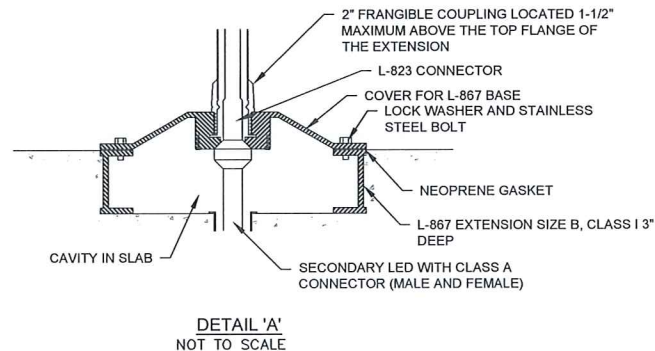
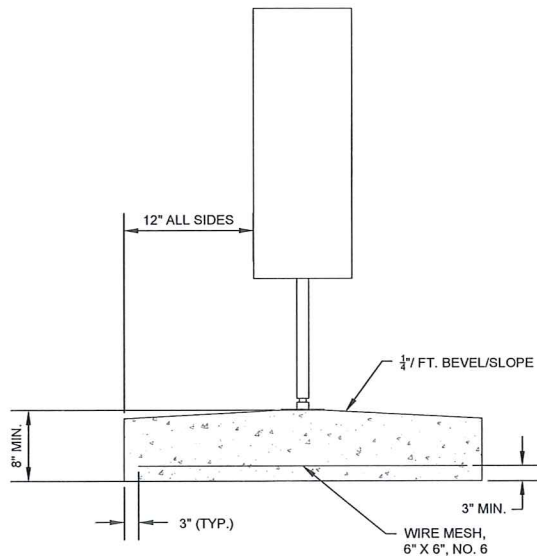
L-858Y = BLACK LEGEND ON A YELLOW BACKGROUND

L-858R = WHITE LEGEND ON A RED BACKGROUND

L-858L = YELLOW LEGEND ON A BLACK BACKGROUND

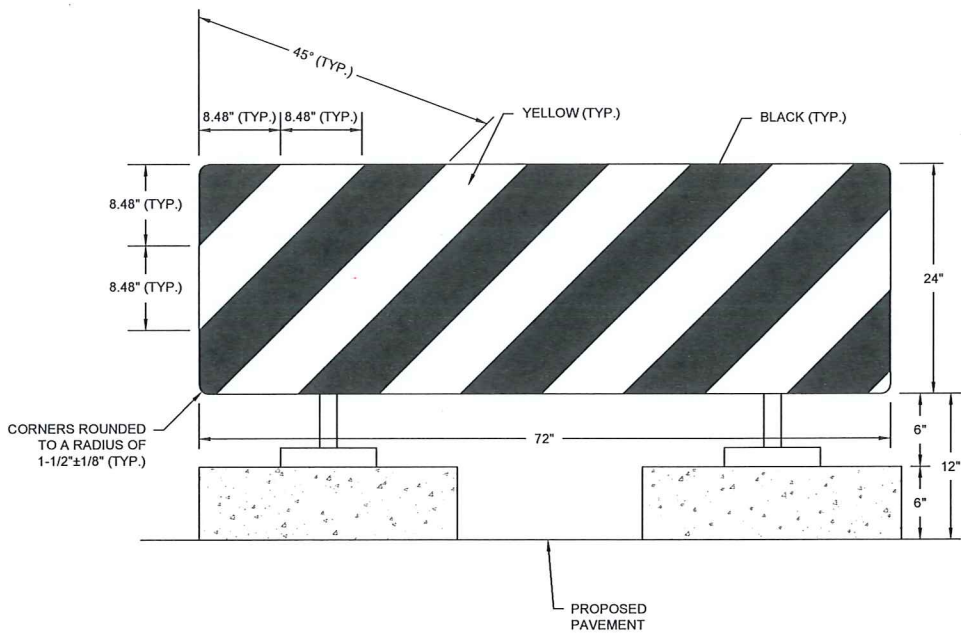
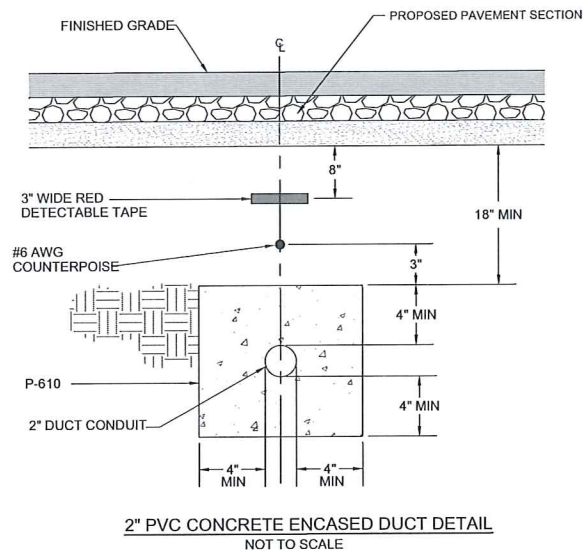
2. SIZE:

2 = 24" FACE, 15" LEGEND

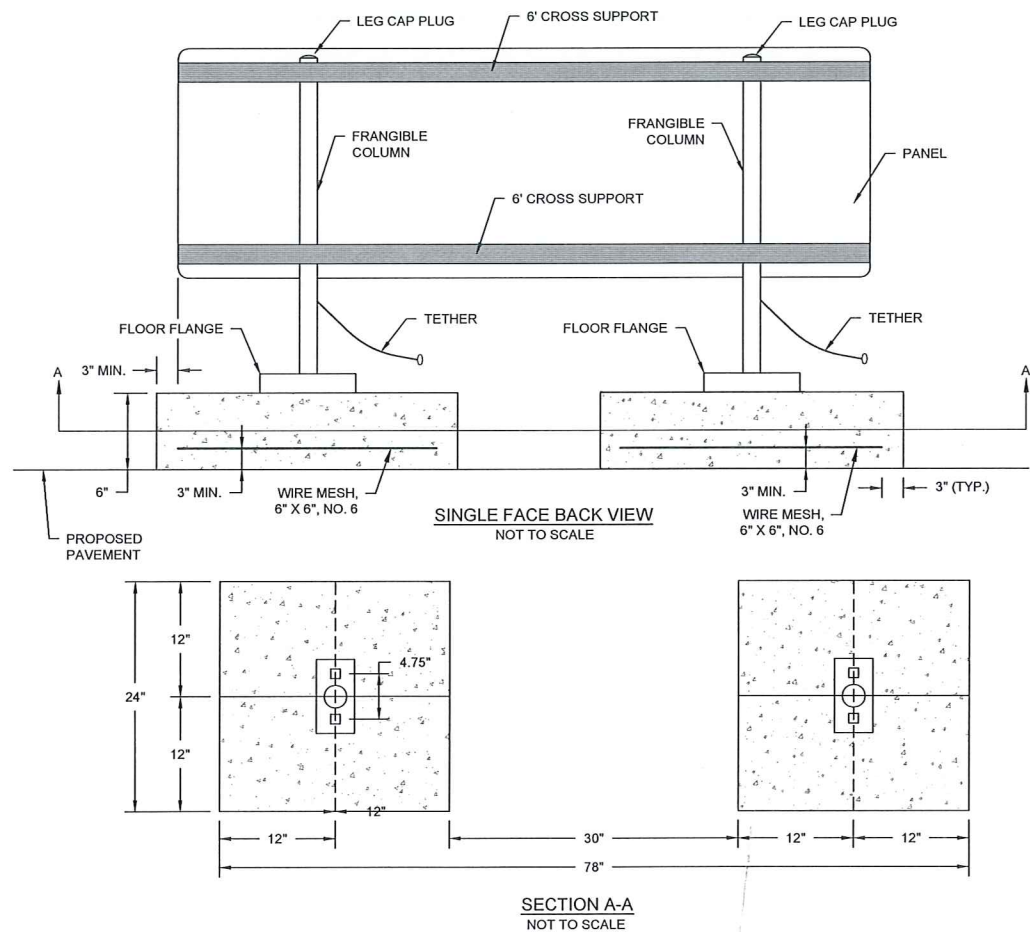


PROPOSED SIGN PANEL SCHEDULE								
SIGN NO.	FACE 1	FACE 2	SIGN TYPE	DISTANCE FROM RWY OR TWY EDGE AND NEAR SIDE OF SIGN	SIZE	STYLE/CLASS	MODE	NO. OF MODULES
S-1			L-858C	25'	2	4/-	2	-
S-2			L-858L L-858R	35'	2	2/2	2	2
S-3			L-858Y	35'	2	2/2	2	1

LED SIGN INSTALLATION ON NEW BASE
NOT TO SCALE

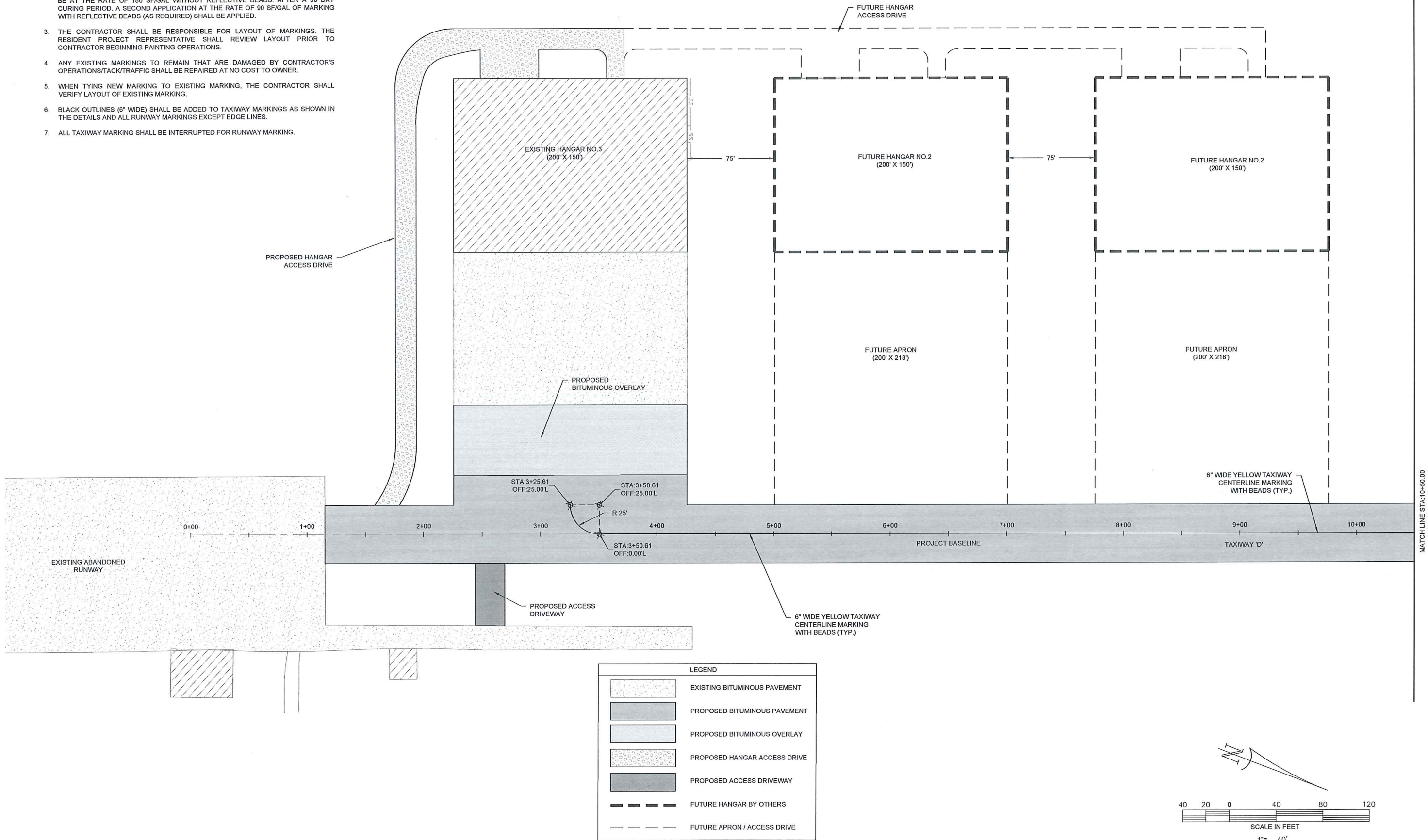


L-858C TAXIWAY ENDING MARKER SIGN DETAIL
NOT TO SCALE



GENERAL NOTES:

1. RUNWAY MARKINGS SHALL BE WHITE. TAXIWAY CENTERLINE MARKINGS SHALL BE YELLOW. ALL MARKINGS (EXCEPT BLACK) REQUIRE REFLECTIVE BEADS. BEADS SHALL BE APPLIED DURING SECOND APPLICATION.
2. MARKING WILL BE APPLIED IN TWO APPLICATIONS. THE FIRST APPLICATION SHALL BE AT THE RATE OF 180 SF/GAL WITHOUT REFLECTIVE BEADS. AFTER A 30 DAY CURING PERIOD. A SECOND APPLICATION AT THE RATE OF 90 SF/GAL OF MARKING WITH REFLECTIVE BEADS (AS REQUIRED) SHALL BE APPLIED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT OF MARKINGS. THE RESIDENT PROJECT REPRESENTATIVE SHALL REVIEW LAYOUT PRIOR TO CONTRACTOR BEGINNING PAINTING OPERATIONS.
4. ANY EXISTING MARKINGS TO REMAIN THAT ARE DAMAGED BY CONTRACTOR'S OPERATIONS/TACK/TRAFFIC SHALL BE REPAIRED AT NO COST TO OWNER.
5. WHEN TYING NEW MARKING TO EXISTING MARKING, THE CONTRACTOR SHALL VERIFY LAYOUT OF EXISTING MARKING.
6. BLACK OUTLINES (6" WIDE) SHALL BE ADDED TO TAXIWAY MARKINGS AS SHOWN IN THE DETAILS AND ALL RUNWAY MARKINGS EXCEPT EDGE LINES.
7. ALL TAXIWAY MARKING SHALL BE INTERRUPTED FOR RUNWAY MARKING.



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281
EMAIL: TBILL@TBILM.COM

SC LICENSE NO. 000366

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

REV.	NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
MARKING PLAN
(SHEET 1 OF 2)

REGISTERED PROFESSIONAL ENGINEER
No. 9779
ALAN M. SMITH

Date
FEBRUARY 2023

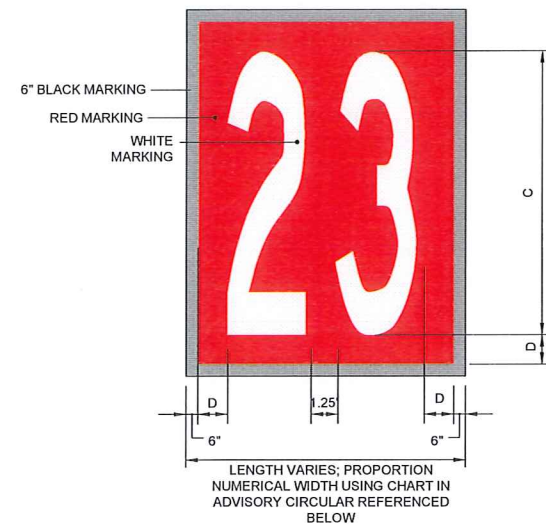
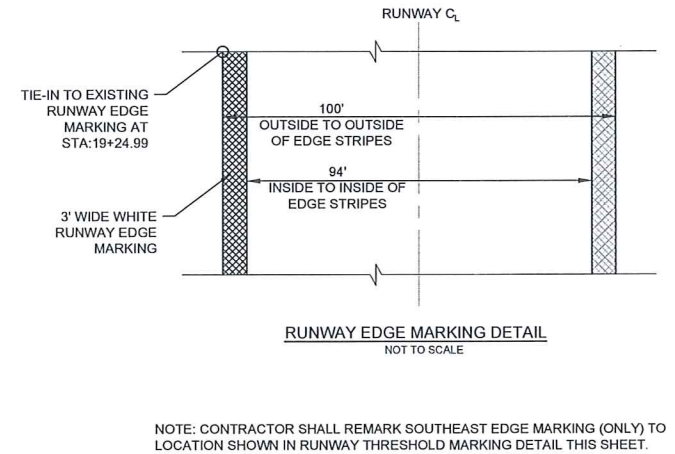
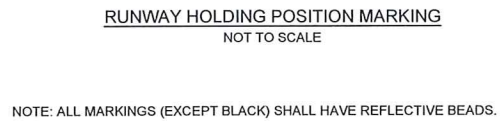
Scale
1" = 40'

Drawn
NT

Checked
AMS

Project No.
2601-2101

Sheet No.
M1.1



DIMENSION LETTER	DIMENSION	NOTES
A	4'	ALL SURFACE PAINTED HOLDING POSITION SIGN MARKINGS SHALL HAVE 6" BLACK BORDER.
B	5'	
C	12'	
D	1.25'	

- NOTES:
1. INSCRIPTIONS SHALL FOLLOW THE INSCRIPTION CRITERIA FOUND IN FAA ADVISORY CIRCULAR 150/5340-1M.
 2. OUTER EDGE OF SIGNS SHALL BE A MINIMUM OF 2' FROM EDGE OF TAXIWAY PAVEMENT. CONTRACTOR TO VERIFY DURING LAYOUT.
 3. BLACK PAINT MARKINGS SHALL BE NON-REFLECTIVE. WHITE AND RED PAINT MARKING SHALL BE REFLECTIVE. SEE SPECIFICATION P-620 FOR TYPE AND APPLICATION RATE.

SURFACE PAINTED HOLDING POSITION SIGN MARKING
NOT TO SCALE

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281

ENSE NO. 000386
EMAIL: TBILL@TBILLM.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

[illegible]

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

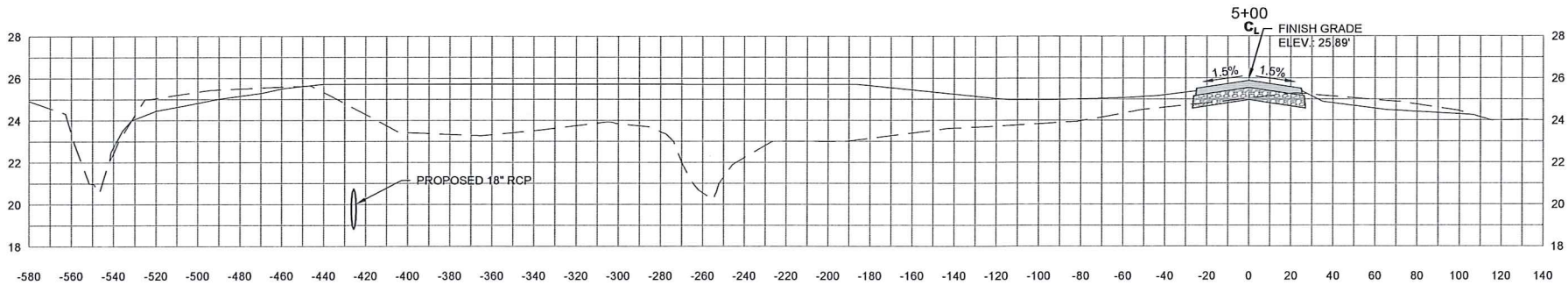
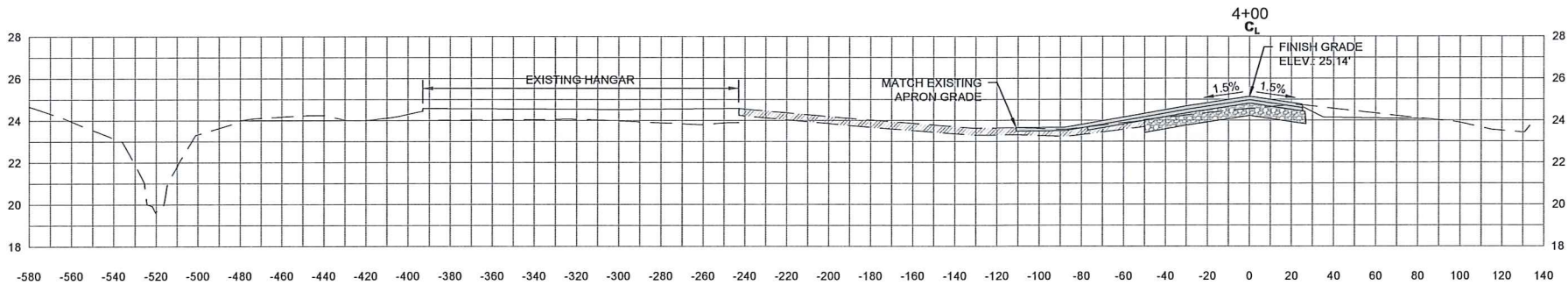
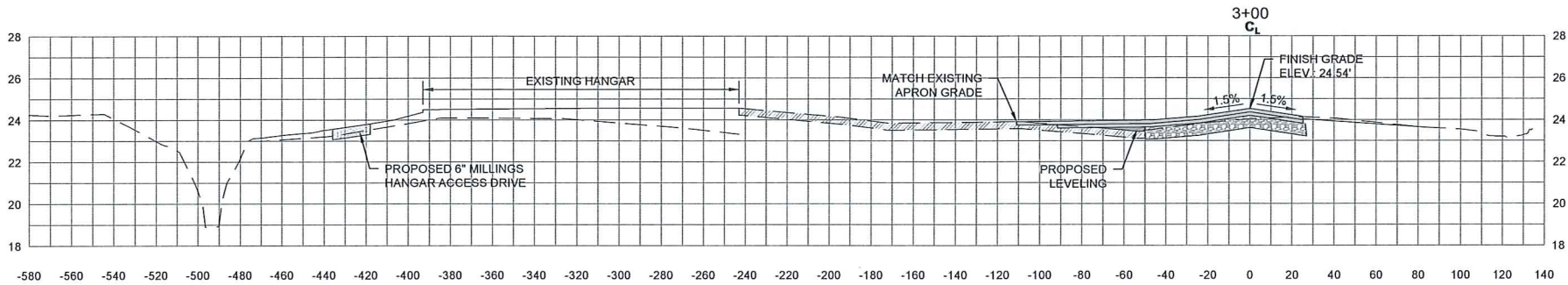
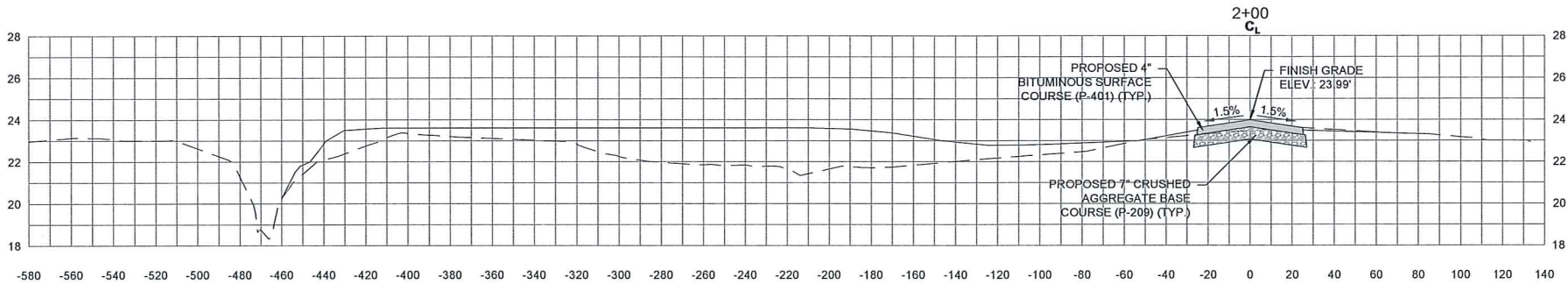
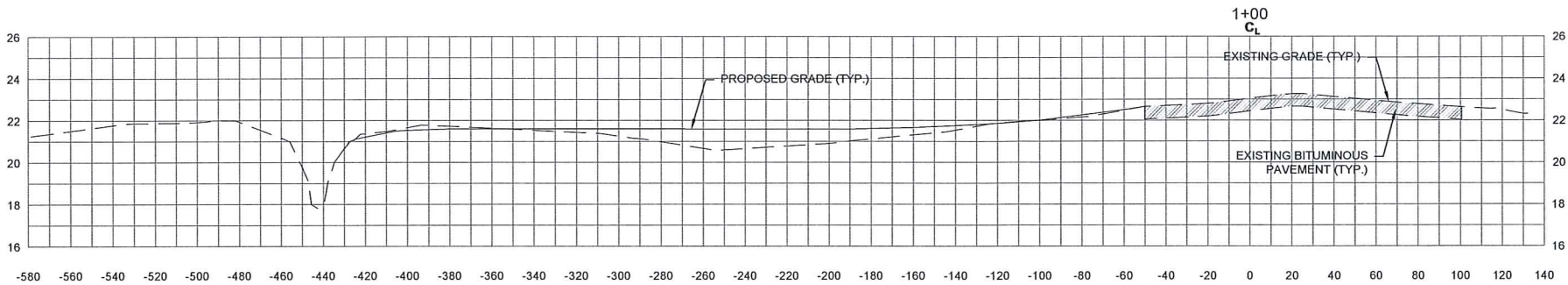
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK

MARKING DETAILS



Date	FEBRUARY 2023
Scale	NTS
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

M1.3



LEGEND	
	PROPOSED 4" BITUMINOUS SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE
	PROPOSED 6" ASPHALT MILLINGS
	EXISTING BITUMINOUS PAVEMENT

NOTE: FOR BASELINE INFORMATION
SEE SHEETS SL1.1 AND SL1.2.

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.
TALBERT & BRIGHT, INC.
© 2023

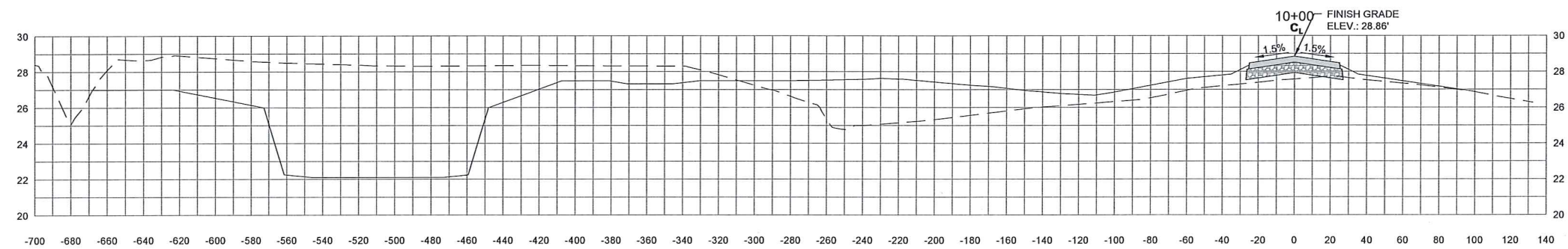
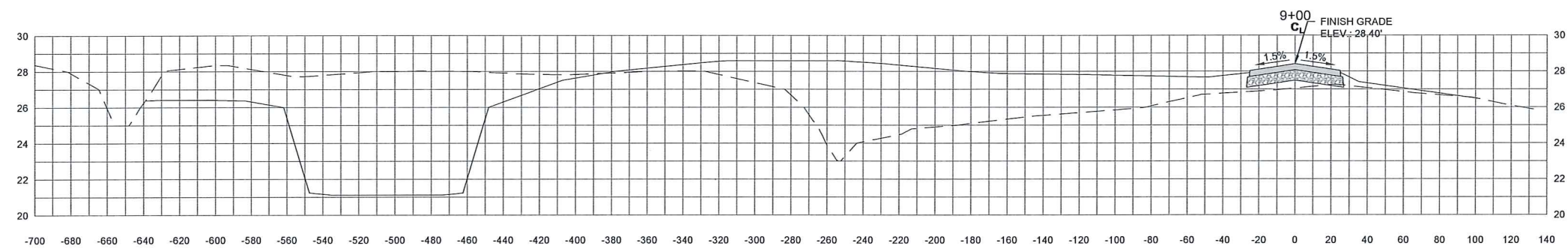
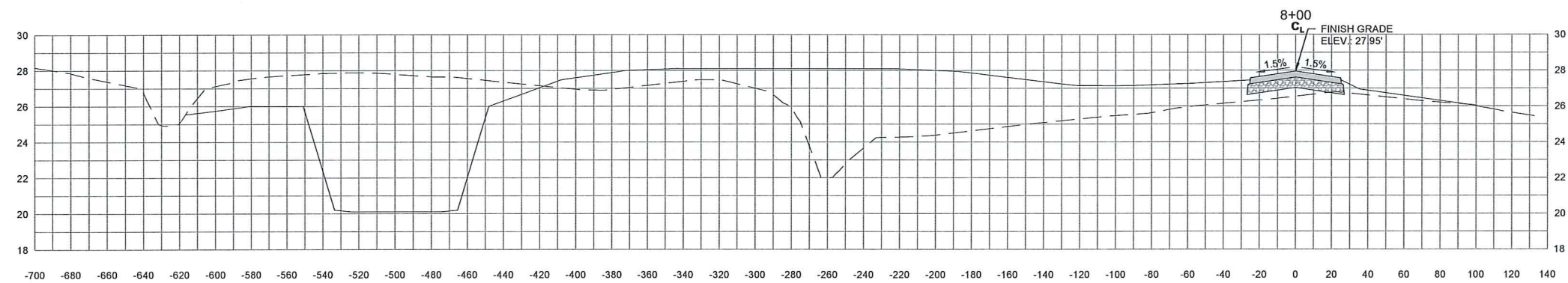
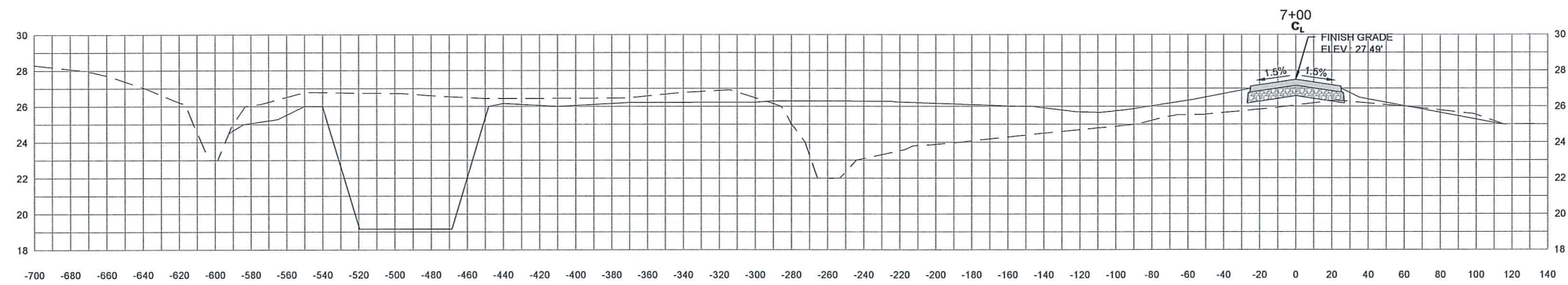
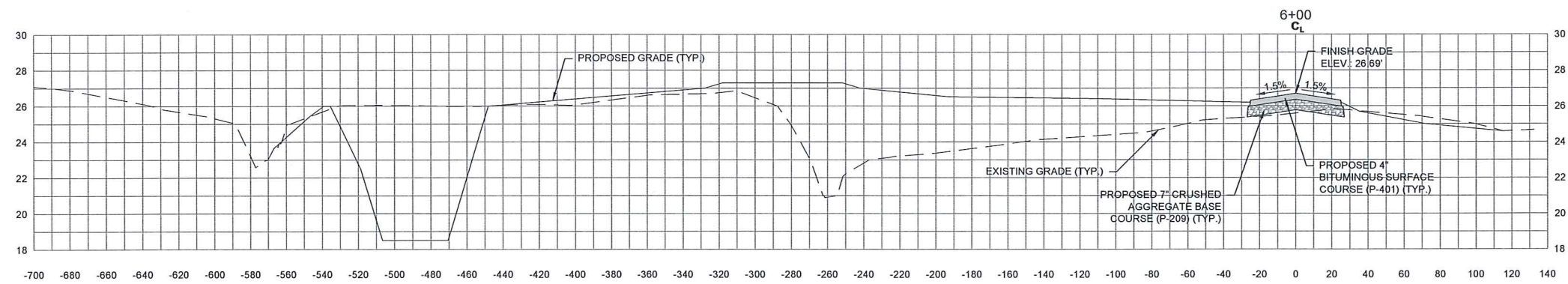
REV. NO.	DESCRIPTION	DATE



GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK
CROSS SECTIONS
(SHEET 1 OF 4)



Date	FEBRUARY 2023
Scale	V: 1" = 4' H: 1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

X1.1



LEGEND	
	PROPOSED 4" BITUMINOUS SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE

**NOTE: FOR BASELINE INFORMATION
SEE SHEETS SL1.1 AND SL1.2.**

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
PHONE: 910-763-5350
FAX: 910-762-6281

SC LICENSE NO. C00386

EMAIL: TBIILM@TBIILM.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023[illegible]

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

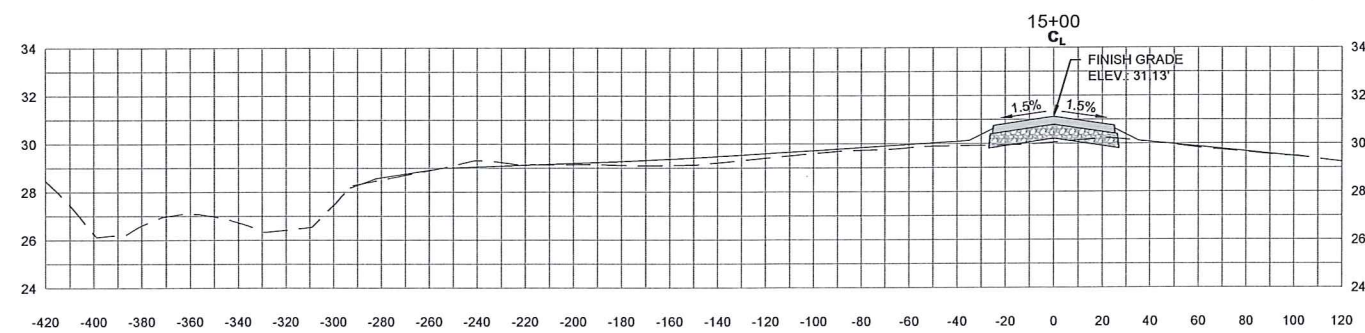
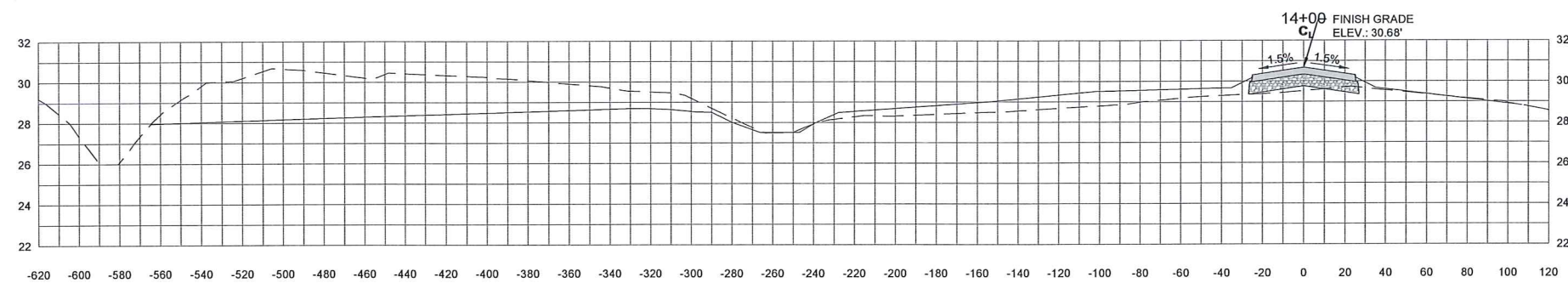
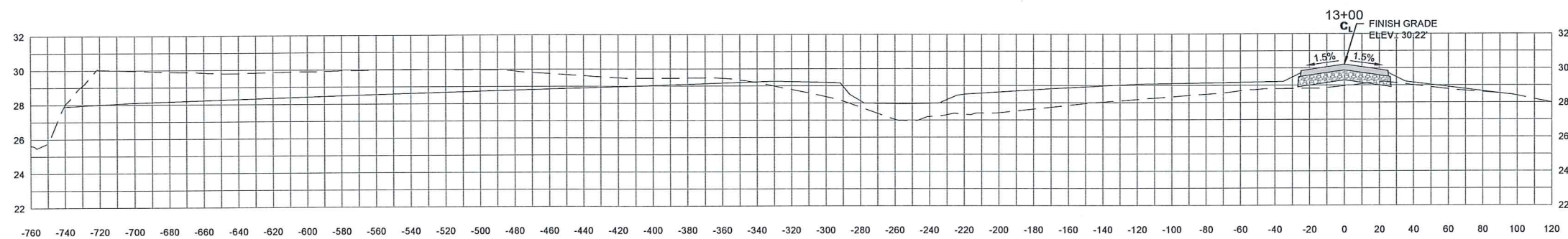
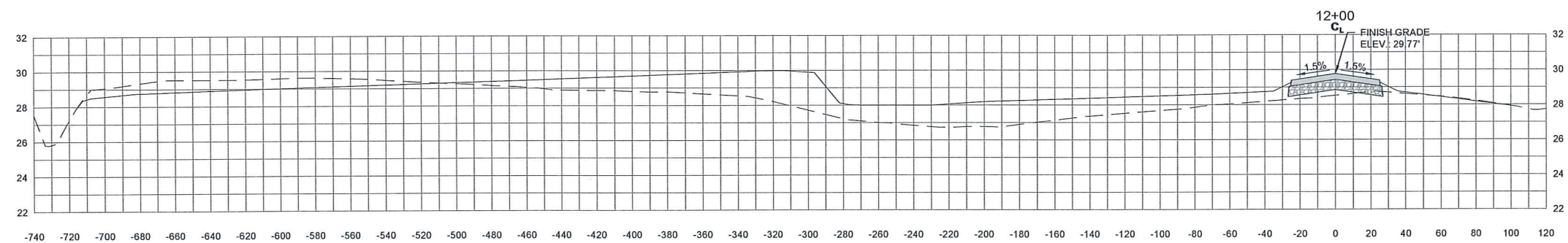
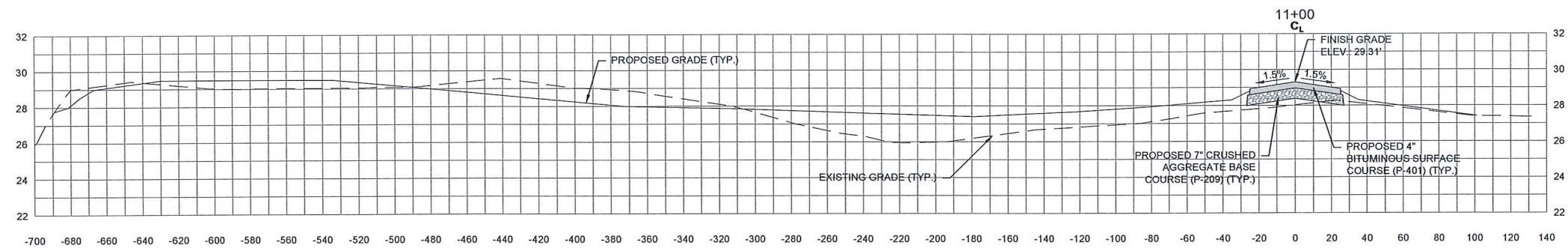
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK

CROSS SECTIONS
(SHEET 2 OF 4)



Date	FEBRUARY 2023
Scale	V: 1" = 4' H: 1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

X1.2



LEGEND	
	PROPOSED 4" BITUMINOUS SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE

**NOTE: FOR BASELINE INFORMATION
SEE SHEETS SL1.1 AND SL1.2.**

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
FAX: 910-762-6281
WILMINGTON, NC 28405
PHONE: 910-763-5350

4810 SHELLEY DRIVE
FAX: 910-762-6281

SC LICENSE NO. C00386

EMAIL: TBIILM@TBIILM.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.

[illegible]

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

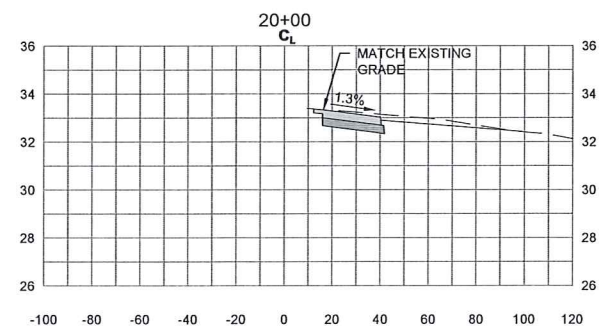
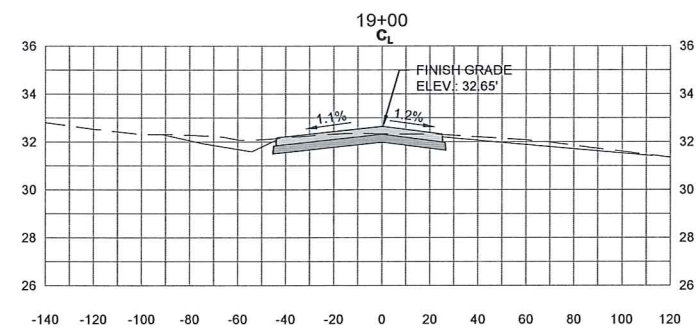
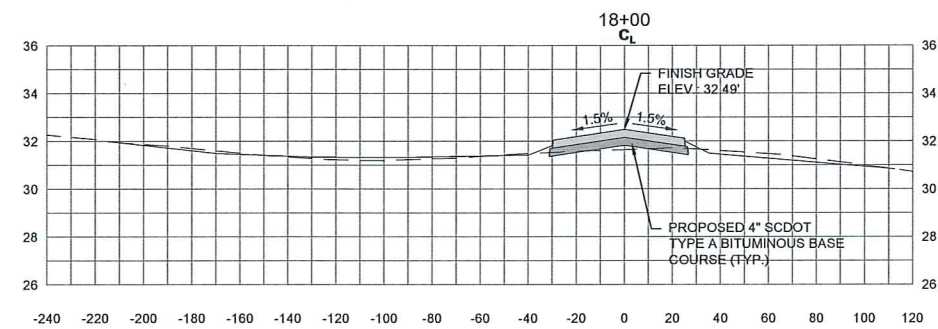
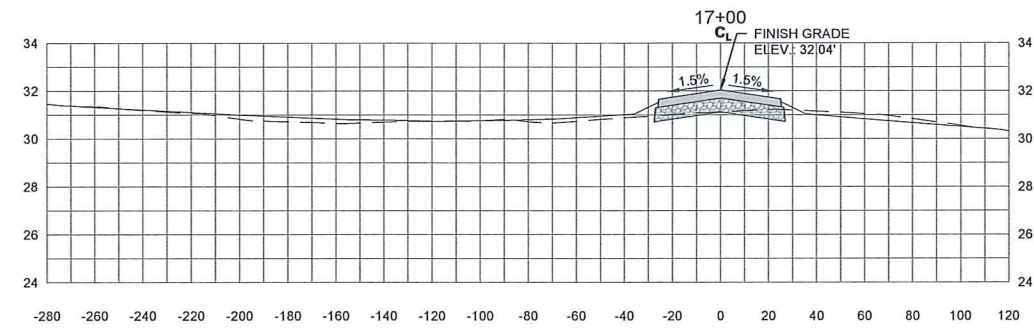
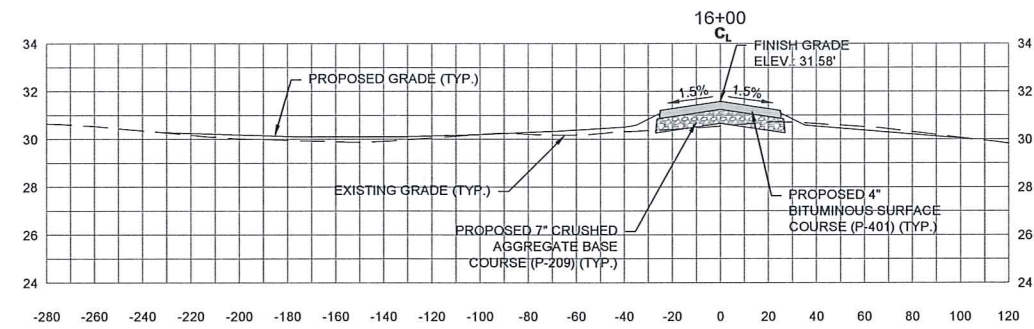
HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK




CROSS SECTIONS
(SHEET 3 OF 4)



Date	FEBRUARY 2023
Scale	V:1" = 4' H:1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

X1.3



LEGEND	
	PROPOSED 4" BITUMINOUS SURFACE COURSE
	PROPOSED 7" CRUSHED AGGREGATE BASE COURSE
	PROPOSED 4" SCDOT TYPE A BITUMINOUS BASE COURSE

NOTE: FOR BASELINE INFORMATION
SEE SHEETS SL1.1 AND SL1.2.

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS

4810 SHELLEY DRIVE
WILMINGTON, NC 28405
FAX: 910-762-6281
PHONE: 910-762-5350

SC LICENSE NO. 000386
E-MAIL: TBILLI@TBLM.COM

THIS DRAWING AND THE DESIGN SHOWN IS THE PROPERTY OF TALBERT & BRIGHT, INC. THE REPRODUCTION, COPY OR USE OF THIS DRAWING WITHOUT THEIR WRITTEN CONSENT IS PROHIBITED.

TALBERT & BRIGHT, INC.
© 2023

[illegible]

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA

HANGAR TAXILANE IMPROVEMENTS
AND HANGAR SITEWORK

CROSS SECTIONS
(SHEET 4 OF 4)



Date	FEBRUARY 2023
Scale	V: 1" = 4' H: 1" = 40'
Drawn	NT
Checked	AMS
Project No.	2601-2101
Sheet No.	

X1.4