

GEORGETOWN COUNTY AIRPORT

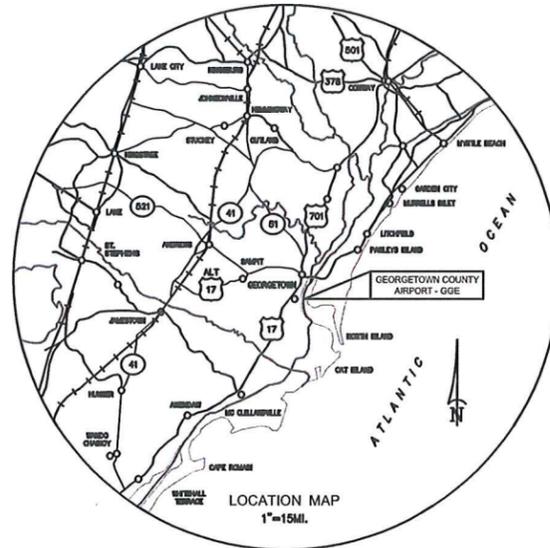
GEORGETOWN, SOUTH CAROLINA

CONSTRUCTION DRAWINGS FOR

ALLISON HANGAR APRON AND TAXILANE PAVING

SCAC PROJECT NO. XX-XXX

BID NO: 23-071



LOCATION MAP

DWG.	TITLE OF DRAWING	DATE
A1	TITLE SHEET	OCTOBER 2023
A2	SAFETY PLAN	OCTOBER 2023
A3	PHASING PLAN	OCTOBER 2023
EX1	EXISTING CONDITIONS AND REMOVAL PLAN	OCTOBER 2023
G1	GRADING AND PAVING PLAN	OCTOBER 2023
D1	TYPICAL SECTIONS AND MISCELLANEOUS DETAILS	OCTOBER 2023
P1	TAXIWAY PROFILE AND CROSS SECTION	OCTOBER 2023



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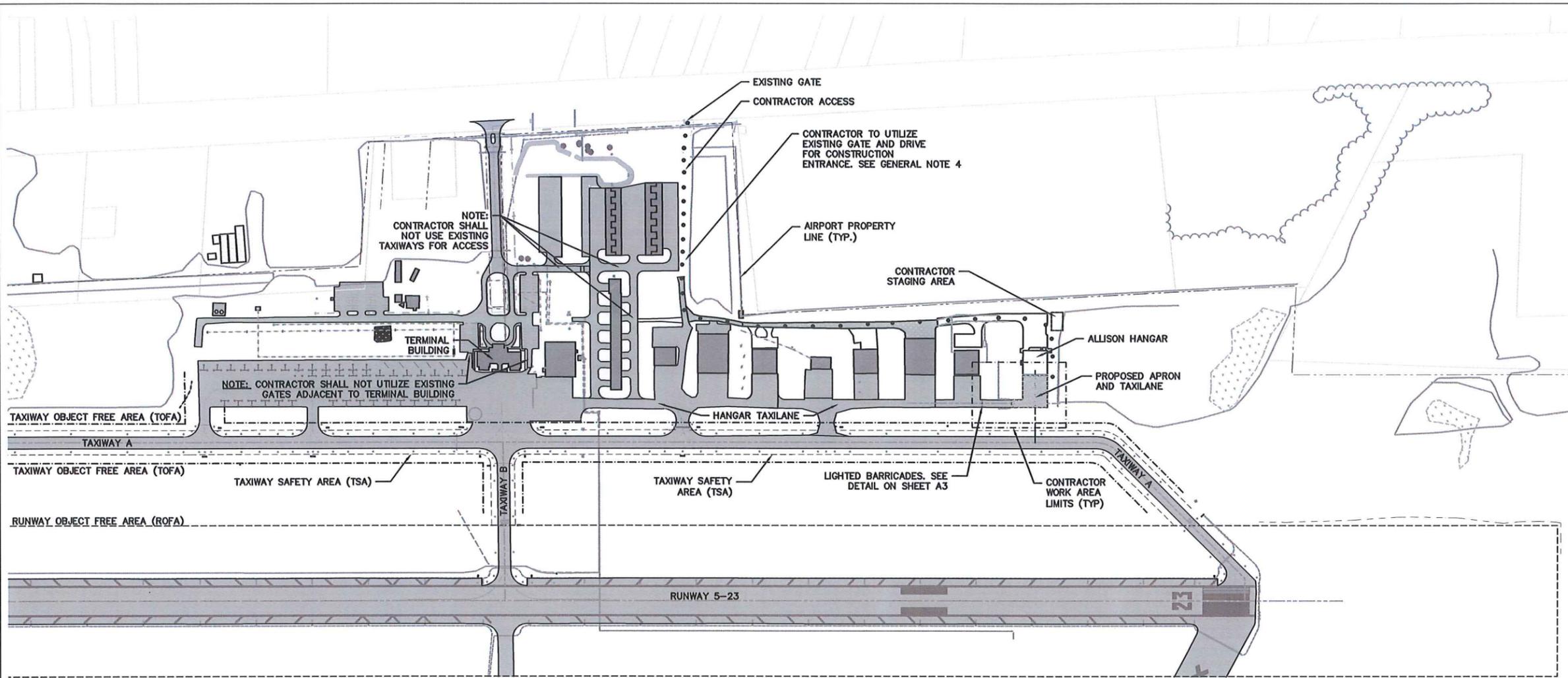
REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
ALLISON HANGAR APRON AND TAXILANE PAVING
TITLE SHEET



Date: OCTOBER 2023
Scale: NTS
Drawn: JDL
Checked: AMS
Project No.: 2601-2303
Sheet No.:

A1



SAFETY PLAN REQUIREMENTS

THE INTENT OF THIS PLAN IS TO ESTABLISH CERTAIN SAFETY REQUIREMENTS THAT MUST BE ADHERED TO BY THE CONTRACTOR DURING CONSTRUCTION OF THIS PROJECT. HANGAR TAXILANE, TAXIWAY 'A' AND RUNWAY 5-23 SHALL REMAIN OPEN AT ALL TIMES. THE AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. SEE SEQUENCE OF CONSTRUCTION ON SHEET A3.

1. THE PROJECT AREA IS LOCATED WITHIN THE AIRCRAFT OPERATIONS AREA (AOA). THIS IS A CLOSELY MAINTAINED SECURITY AREA WITH RESTRICTED ACCESS. THE CONTRACTOR WILL BE REQUIRED TO MEET ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THIS AREA AT ALL TIMES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH ALL REQUIREMENTS FOR ENTERING AND OPERATING IN THE AOA. FURTHER, IT WILL REMAIN THE CONTRACTOR'S RESPONSIBILITY TO KEEP HIMSELF ADVISED OF ANY CHANGES IN REQUIREMENTS AND TO ADHERE TO CURRENT REGULATIONS. CONTRACTOR SHALL ALSO HAVE PERSONNEL TRAINED TO OPERATE AND MONITOR AIRPORT SECURITY GATES USED DURING THE PROJECT.
 2. THE CONTRACTOR SHALL NOT BEGIN WORK UNLESS AND UNTIL 72 HOURS PRIOR NOTICE HAS BEEN GIVEN TO THE ENGINEER AND AIRPORT MANAGEMENT. CROSSING OF RUNWAYS OR TAXIWAYS IS NOT ALLOWED. CONTRACTOR IS PROHIBITED FROM ENTERING THE RUNWAY 5-23 SAFETY AREA AT ANY TIME. SEE GENERAL NOTE 1.
 3. IN AN EMERGENCY SITUATION THE CONTRACTOR SHALL CALL 911 AND NOTIFY THE AIRPORT MANAGEMENT IMMEDIATELY. THE AIRPORT MANAGER CAN BE REACHED BY PHONE AT 843-545-3638.
 4. SEE GENERAL NOTE 4 FOR AIRPORT ENTRY AND DEPARTURE PROCEDURES AND FOR VEHICLE MARKING REQUIREMENTS.
 5. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL DESIGNATE A REPRESENTATIVE AND ALTERNATE TO CONTACT ON A 24 HOUR BASIS SHOULD PROBLEMS ARISE. THE CONTRACTOR SHALL PROVIDE A CONTACT LIST FOR ALL SUPERVISORY PERSONNEL AND ALL SUBCONTRACTORS.
 6. UNDERGROUND UTILITIES ARE KNOWN TO BE LOCATED IN THE PROJECT AREAS. EXISTING UNDERGROUND UTILITIES INCLUDING BUT NOT LIMITED TO POWER CABLES AND OTHER UTILITIES MAY BE IN THE PATH OF CONSTRUCTION. LOCATIONS OF UTILITIES IF SHOWN ON THE PLANS ARE APPROXIMATE ONLY. ALL UTILITIES AND FACILITIES ARE NOT NECESSARILY INDICATED ON PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES FROM DAMAGE. SEE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGEMENT WHEN WORKING IN AREAS CONTAINING AIRFIELD LIGHTING OR NAVIAD CABLE.
- CONTACTS ARE:
 AIRPORT MR. JIM TAYLOR 843-545-3638
- FOR ADDITIONAL REQUIREMENTS RELATED TO PROTECTION OF EXISTING UTILITIES, INCLUDING CABLES AND CONTROLS. SEE PROJECT SPECIAL PROVISIONS.
7. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS IN REGARD TO CONSTRUCTION NOISE AND EROSION CONTROL DURING CONSTRUCTION.
 8. THE CONTRACTOR SHALL CLEAN ALL CONSTRUCTION AREAS OF LITTER, LOOSE PAPERS, DEBRIS, ETC. ON A DAILY BASIS, OR AS DIRECTED BY THE ENGINEER. PRIOR TO THE CLOSE OF DAILY OPERATIONS, CONTRACTOR SHALL INSPECT ALL ACTIVE AIR OPERATIONS AREAS AND CONSTRUCTION AREA FOR FOOD AND LITTER. ALL DEBRIS SHALL BE CLEANED UP AND PROPERLY DISPOSED OF PRIOR TO RELEASE OF CREWS FROM EACH SHIFT.
 9. MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL ARE NOT ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) OF AN OPEN RUNWAY OR THE TAXIWAY OBJECT FREE AREA (TOFA) (SEE GENERAL NOTE 1) AT ANY TIME. MEN, EQUIPMENT OR OTHER CONSTRUCTION-RELATED MATERIAL WILL NOT BE PERMITTED CLOSER THAN 150 FEET FROM THE EDGE OF THE RUNWAY OR 50 FEET FROM THE EDGE OF ANY TAXIWAY WITHOUT PRIOR PERMISSION FROM THE AIRPORT MANAGEMENT.
 10. DURING CONSTRUCTION, ADJACENT TAXIWAYS AND RUNWAY WILL BE OPEN TO AIRCRAFT UNLESS OTHERWISE NOTED. AIRCRAFT HAVE THE RIGHT OF WAY AT ALL TIMES. THE CONTRACTOR SHALL BE AWARE OF THE AIRCRAFT MOVEMENTS AND THE JETBLAST AND/OR PROP-WASH ASSOCIATED WITH THESE AIRCRAFT. THE CONTRACTOR SHALL SECURE LOOSE ITEMS AT ALL TIMES AND SHALL LOCATE STOCKPILES OF MATERIALS OR EQUIPMENT AWAY FROM AIRCRAFT OPERATION AREAS.
 11. INSPECTION - FREQUENT INSPECTIONS WILL BE MADE BY AIRPORT MANAGEMENT DURING CRITICAL PHASES OF THE WORK TO ENSURE THAT THE CONTRACTOR IS FOLLOWING THE RECOMMENDED AIRFIELD SAFETY PROCEDURES.
 12. EXCAVATIONS - CONTRACTOR MUST PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE AIRPORT, AND BACKFILL OR LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN 200 FEET OF THE RUNWAY CENTERLINE WHILE THE RUNWAY IS OPEN. IF THE RUNWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, COVER OR BACKFILL THE EXCAVATIONS APPROPRIATELY. COVERINGS FOR OPEN TRENCHES OR EXCAVATIONS MUST BE OF SUFFICIENT STRENGTH TO SUPPORT THE WEIGHT OF THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY.
 13. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA. NO WORK OR CONSTRUCTION ACTIVITY IS ALLOWED WITHIN THE TAXIWAY 'A' OBJECT FREE AREA (SEE GENERAL NOTE 1).

GENERAL NOTES:

1. IT IS THE INTENT OF THE OWNER THAT THE GEORGETOWN COUNTY AIRPORT WILL REMAIN OPEN TO AIR TRAFFIC AT ALL TIMES. THE RUNWAY SHALL NOT BE CLOSED. TAXIWAY "A" SHALL REMAIN OPEN AT ALL TIMES.
 2. PRIOR TO LEAVING WORK EACH DAY, CONTRACTOR SHALL RETURN HIS EQUIPMENT AND MATERIALS TO THE STAGING AREA IDENTIFIED ON THE PLANS.
 3. ALL CONTRACTOR PERSONNEL, INCLUDING BUT NOT LIMITED TO, GENERAL LABORERS, SUBCONTRACTORS, DRIVERS, AND JOURNEYMEN WORKING WITHIN ACTIVE AIR OPERATIONS AREAS MUST AT ALL TIMES REMAIN WITHIN VISUAL AND VOICE RANGE OF CONTRACTOR SUPERVISORY PERSONNEL. FOR THE PURPOSES OF THIS PROJECT, THE AIR OPERATIONS AREA (AOA) REFERS TO ALL AREAS WITHIN THE AIRPORT SECURITY FENCE.
 4. PRIOR TO ENTERING THE SECURED AOA OF THE AIRPORT EACH DAY, THE CONTRACTOR SHALL CHECK IN WITH THE AIRPORT. CLOSE COORDINATION FOR ACCESS TO WORK AREAS AND SCHEDULES BETWEEN THE CONTRACTOR, OTHER CONTRACTORS AND COUNTY STAFF WORKING IN THE PROJECT AREA, AND AIRPORT WILL BE REQUIRED THROUGHOUT THE PROJECT.
- THE CONTRACTOR SHALL COORDINATE INGRESS-EGRESS REQUIREMENTS WITH THE AIRPORT MANAGEMENT. ALL OPEN GATES TO SECURED AIRPORT AREAS SHALL BE MONITORED CONTINUOUSLY BY CONTRACTOR'S PERSONNEL TO CONTROL ACCESS TO SECURED AREA OR SHALL BE CLOSED AND LOCKED. CONTRACTOR PERSONNEL SHALL NOT ALLOW ANY UNAUTHORIZED PERSONNEL TO ENTER THROUGH THE CONSTRUCTION GATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING AND LOCKING ALL GATES WHEN NOT IN USE AND AT THE END OF EACH DAY'S OPERATIONS. CONTRACTOR SHALL INTERLOCK AT PADLOCKED GATES. CONTRACTOR SHALL PROVIDE A COPY OF ALL GATE KEYS TO THE AIRPORT. CONTRACTOR SHALL PROVIDE A LIST OF ALL KEY HOLDERS WHICH SHALL BE KEPT UPDATED THROUGHOUT THE PROJECT.
- ALL CONSTRUCTION VEHICLES MUST BE CLEARED FOR ACCESS BY THE AIRPORT MANAGEMENT. PERSONAL CARS SHALL BE PARKED IN STAGING AREA. ALL VEHICLES OPERATING IN THE AOA SHALL BE LIGHTED OR FLAGGED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F. COPIES OF THE ADVISORY CIRCULAR WILL BE MADE AVAILABLE UPON REQUEST.
5. THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN SUCH A MANNER AS TO ASSURE THAT SUCH OPERATIONS DO NOT IMPEDE ACCESS TO ANY AREA OF THE AIRFIELD AT ANY TIME FOR AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) VEHICLES AND OTHER EMERGENCY VEHICLES. EMERGENCY VEHICLE ACCESS SHALL BE A STANDING AGENDA ITEM FOR ALL PROGRESS MEETINGS. THE CONTRACTOR SHALL COOPERATE FULLY AND IMMEDIATELY WITH ANY DIRECTIVES ISSUED BY AIRPORT MANAGEMENT RELATIVE TO EMERGENCY ACCESS.
 6. ACCESS ROADS TO BE USED UNDER THIS CONTRACT SHALL BE THOSE DESIGNATED AND APPROVED BY THE AIRPORT. IN GENERAL, THE CONTRACTOR SHALL CONFINED HIS EQUIPMENT AND HAULING WHERE PRACTICAL TO EXISTING ROADS ON THE AIRPORT. IF EXISTING PAVEMENT OR ROAD SURFACE IS DAMAGED BY THE CONTRACTOR'S HAULING OPERATIONS, IT SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. HAUL ROADS ACROSS TURFED AREAS SHALL BE REPAIRED, SCARIFIED, SEEDED, MULCHED, AND FERTILIZED AT THE CONTRACTOR'S EXPENSE. METAL TRACK VEHICLES WILL NOT BE PERMITTED TO OPERATE ON OR ACROSS EXISTING PAVEMENT WITHOUT PROTECTIVE MATTING TO PREVENT MARRING OF THE PAVEMENT SURFACE. ACCESS ROADS SHALL BE CONSTRUCTED BY CONTRACTOR AS REQUIRED. ALL COSTS ASSOCIATED WITH SUPPLYING, CONSTRUCTING, MAINTAINING AND RESTORING TEMPORARY HAUL AND ACCESS ROADS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR "MOBILIZATION."

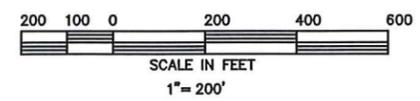
NOTAMS (NOTICE TO AIRMEN)

THE AIRPORT MANAGEMENT WILL ISSUE THE NECESSARY NOTAMS TO REFLECT HAZARDOUS CONDITIONS. IT IS IMPORTANT THAT NOTAMS BE KEPT CURRENT AND REFLECT THE ACTUAL CONDITIONS WITH RESPECT TO CONSTRUCTION SITUATIONS. ACTIVE NOTAMS SHALL BE REVIEWED PERIODICALLY AND REVISED TO REFLECT THE CURRENT CONDITIONS.

7. ALL EXISTING FACILITIES WILL BE CAREFULLY PROTECTED BY THE CONTRACTOR. ANY FACILITIES DAMAGED BY THE CONTRACTOR WILL BE REPAIRED IMMEDIATELY AND RESTORED TO ORIGINAL CONDITION AT CONTRACTOR'S COST.
8. CONTRACTOR WILL, BY WATERING, CHEMICALS, VEGETATION, OR OTHER MEANS, PREVENT THE OCCURRENCE OF DUST WHICH WILL BE OBJECTIONABLE TO THE HANGAR TENANTS OR AIRPORT MANAGEMENT OF THE AREA OR VIOLATE EXISTING LAWS OR REGULATION OR CAUSE HAZARDS TO AIR TRAFFIC.
9. CONTRACTOR MAY ENCOUNTER WET CONDITIONS DURING CONSTRUCTION. ALL COST FOR DEWATERING IS CONSIDERED INCIDENTAL TO COST OF ITEMS OF WORK BID UPON.
10. SEE PROJECT SPECIAL PROVISIONS FOR PROTECTION OF UTILITIES.
11. CONSTRUCTION ACTIVITIES MAY BE CONDUCTED BY MULTIPLE CONTRACTORS AND COUNTY FORCES SIMULTANEOUSLY. CONTRACTOR SHALL COORDINATE AND COOPERATE WITH ALL CONTRACTORS AND COUNTY FORCES THAT MAY BE WORKING ON SITE SO THAT ALL WORK ACTIVITIES ARE COMPLETED IN A TIMELY MANNER.

CONSTRUCTION CONTRACTOR'S RESPONSIBILITIES

1. CONTRACTOR SHALL HAVE AVAILABLE A COPY OF THE PROJECT SAFETY PLAN ON SITE AT ALL TIMES.
2. CONTRACTOR SHALL COMPLY WITH THE SAFETY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT.
3. CONTRACTOR SHALL PROVIDE A POINT OF CONTACT WHO WILL COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION-RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT.
4. CONTRACTOR SHALL PROVIDE A SAFETY/CONSTRUCTION INSPECTOR FAMILIAR WITH AIRPORT SAFETY TO MONITOR CONSTRUCTION ACTIVITIES.
5. CONTRACTOR SHALL RESTRICT MOVEMENT OF CONSTRUCTION VEHICLES TO CONSTRUCTION AREAS BY FLAGGING AND BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING FLAGMEN OR ESCORTS AS APPROPRIATE.
6. CONTRACTOR SHALL ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE ACTIVE AIR OPERATIONS AREAS (AOA) FROM THE CONSTRUCTION SITE UNLESS AUTHORIZED.



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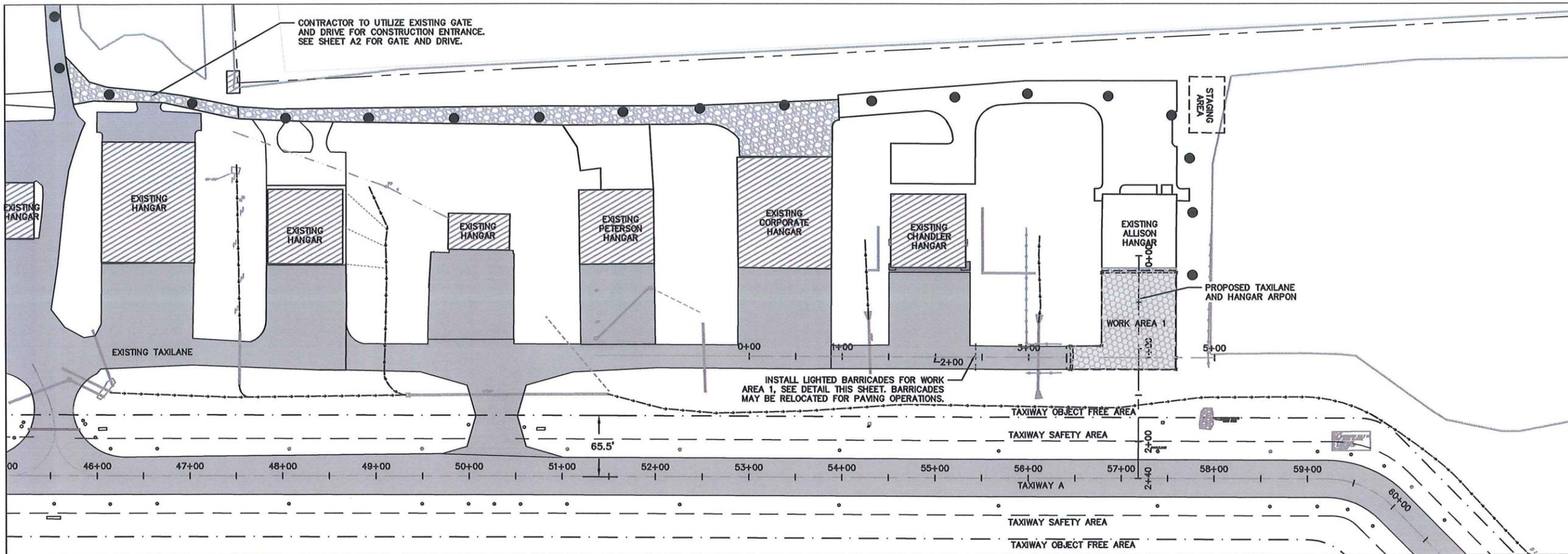
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REV. NO.	DESCRIPTION	DATE

SAFETY PLAN

ALLISON HANGAR APRON AND TAXILANE PAVING
GEORGETOWN, SOUTH CAROLINA

Date: OCT 2023
Scale: 1" = 200'
Drawn: JDL
Checked: AMS
Project No.: 2601-2303
Sheet No.: A2



THE SEQUENCE OF CONSTRUCTION FOR THIS PROJECT WILL FOLLOW TYPICAL PATTERN FOR PROJECTS OF THIS TYPE, INCLUDING ESTABLISHMENT OF STAGING AREA AND STOCKPILE AREA, INSTALLATION OF APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES, GRADING, PAVING, MARKING, AND SEEDING AND MULCHING.

THE FOLLOWING SEQUENCE OF CONSTRUCTION HAS BEEN DEVELOPED TO HELP THE CONTRACTOR UNDERSTAND THE OPERATIONAL NEEDS OF THE AIRPORT AND HELP ENSURE NO CLOSURE TIME TO TAXIWAY "A". IN ACCORDANCE WITH THE SPECIFICATIONS THE CONTRACTOR SHALL PROVIDE A DETAILED SCHEDULE OF CONSTRUCTION TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION. CONTRACTOR HAS 30 CALENDAR DAYS TO COMPLETE ALL WORK.

PRIOR TO BEGINNING PROJECT AND CONTRACT START TIME

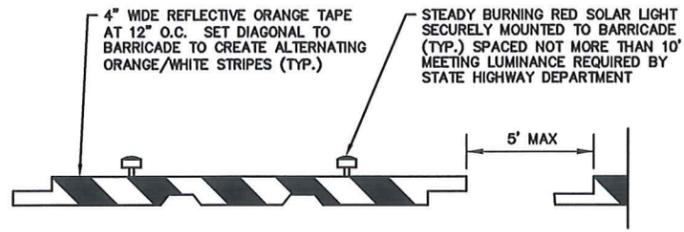
1. DEVELOP DETAILED SCHEDULE TO ENSURE CONSTRUCTION CAN BE COMPLETED FOR ALL WORK AREAS WITHIN CONSTRUCTION TIME ALLOTTED FOR THE PROJECT.
2. CONTRACTOR MAY COMPLETE REQUIRED SURVEY WORK ON PROJECT AREA PRIOR TO CONSTRUCTION START TIME. SCHEDULING FOR THE SURVEY WORK WILL BE REQUIRED TO BE APPROVED BY AIRPORT MANAGEMENT. SEE PROJECT SPECIAL PROVISIONS.

CONSTRUCTION SEQUENCE

1. MOBILIZE EQUIPMENT AND DEVELOP ACCESS ROAD AS REQUIRED AND ESTABLISH STAGING AREA. THE LOCATION OF THE STAGING AREA SHALL BE COORDINATED WITH THE AIRPORT.
2. INSTALL LIGHTED BARRICADES AT LOCATIONS SHOWN. CONTRACTOR WILL BE WORKING ADJACENT TO ACTIVE AIR OPERATIONS AND SHALL UTILIZE CAUTION AS REQUIRED BY THE SAFETY PLAN.
3. REMOVE EXISTING SILT FENCE AS REQUIRED. INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES.
4. COMPLETE EXCAVATION, GRADING OPERATIONS, AND FINE GRADING SUBGRADE. COMPACT SUBGRADE AS REQUIRED. ALL EXCESS OR WASTE MATERIAL SHALL BE DISPOSED OF OFFSITE.
5. COMPLETE PLACEMENT, GRADING, AND COMPACTION OF AGGREGATE BASE COURSE.
6. APPLY BITUMINOUS PRIME COAT IN ACCORDANCE WITH SPECIFICATIONS.
7. COMPLETE PAVING OPERATIONS ON THE TAXILANE AND HANGAR APRON. PAVEMENT SHALL BE PLACED IN ONE 2" LIFT.
8. COMPLETE PLACEMENT OF SHOULDER FILL, COMPACT AND FINE GRADE SHOULDERS IN ACCORDANCE WITH DETAILS SHOWN ON THE PLANS.
9. COMPLETE SEEDING AND MULCHING OPERATIONS AS REQUIRED. RESTORE ALL DISTURBED AREAS AND SEED AND MULCH ALL AREAS AS REQUIRED.
10. REMOVE ALL EQUIPMENT, MATERIALS, AND DEBRIS FROM WORK AREA 1 AND STAGING AREA.
11. REMOVE LIGHTED BARRICADES AND OPEN APRON IN WORK AREA 1 TO AIR OPERATIONS.
12. REMOVE REMAINING SILT FENCE AFTER SITE IS STABILIZED AS DIRECTED BY ENGINEER.

GENERAL NOTES:

1. SAFETY REQUIREMENTS AND LIGHTED BARRICADES SHOWN ARE TO BE UTILIZED DURING CONSTRUCTION. BARRICADES SHALL BE INSTALLED, REMOVED, OR RELOCATED AS REQUIRED FOR COMPLIANCE WITH SAFETY PLAN AND PHASING PLANS.
2. DURING WORK AREA 1 OPERATION, TAXIWAY "A" SHALL REMAIN OPEN. CONSTRUCTION PERSONNEL AND VEHICLES SHALL NOT ENTER ACTIVE AIR OPERATIONS AREAS WITHOUT PRIOR APPROVAL OF AIRPORT MANAGEMENT.
3. SEE SAFETY PLAN REQUIREMENTS, SHEET A2, AND PROJECT SPECIAL PROVISIONS FOR OTHER SAFETY RELATED ITEMS.
4. CONTRACTOR SHALL BE REQUIRED TO MAINTAIN A POWER BROOM ON SITE AT ALL TIMES FOR CLEANUP OF SPILLAGE. THE CONTRACTOR SHALL CLOSELY MONITOR ADJACENT ACTIVE AIR OPERATION AREAS FOR SPILLAGE AND/OR DEBRIS. ALL SPILLAGE AND/OR DEBRIS SHALL BE IMMEDIATELY CLEANED UP AND REMOVED FROM THE AIR OPERATIONS AREA.

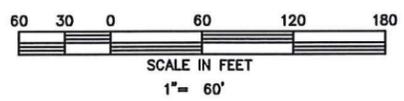


LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

AVIATION BARRICADE NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT. PAYMENT FOR LOW PROFILE LIGHTED BARRICADES SHALL BE INCLUDED IN ITEM "TEMPORARY CONSTRUCTION ITEMS".
2. LOW PROFILE LIGHTED BARRICADES SHALL BE MULTI-BARRIER AIRPORT RUNWAY BARRICADE A10X96 AND SOLAR LIGHTS SHALL BE PROVIDED FOR THE BARRICADES AS MANUFACTURED BY OFF THE WALL PRODUCTS, LLC. OR APPROVED EQUAL. THE CONTRACTOR SHALL PROVIDE WATER NECESSARY TO FILL THE BARRICADES AND ENSURE ADEQUATE WATER HAS BEEN PROVIDED TO ANCHOR THE BARRICADES IN PLACE.
3. CONTRACTOR SHALL CHECK LIGHTS DAILY TO VERIFY THAT THEY ARE IN WORKING CONDITION AND SHALL REPLACE LIGHTS AS REQUIRED.
4. CONTRACTOR SHALL INSTALL BARRICADES AT LOCATIONS SHOWN ON INDIVIDUAL PLAN SHEETS. BARRICADES SHALL BE MOVED AND RELOCATED AS REQUIRED. BARRICADES SHALL BE INSTALLED WITH MAXIMUM 5' SPACE BETWEEN BARRICADE AND CONTRACTOR SHALL LEAVE ONE 15' SPACE FOR EMERGENCY VEHICLES. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST.

LEGEND	
	WORK AREA LIMITS
	LIGHTED BARRICADE
	CONTRACTOR ACCESS
	EXISTING FENCE

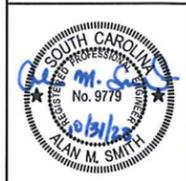


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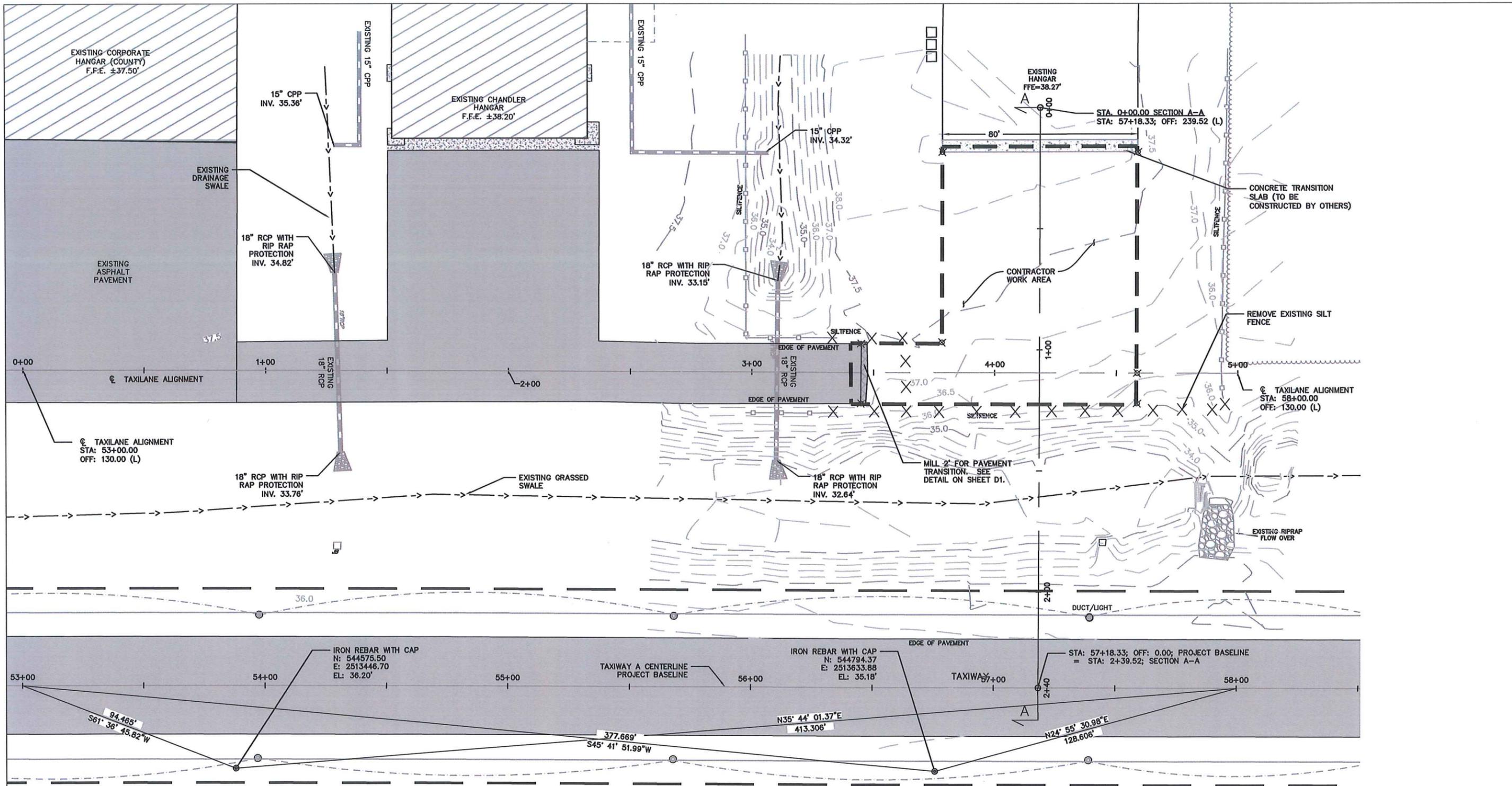
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
ALLISON HANGAR APRON AND TAXILANE PAVING
PHASING PLAN



Date	OCTOBER 2023
Scale	1" = 60'
Drawn	JDL
Checked	AMS
Project No.	2601-2303
Sheet No.	

A3



SURVEY NOTES:

- PROJECT BASE MAPPING AND CONTROL FROM PARKER LAND SURVEYING, LLC SURVEY DATED SEPTEMBER 29, 2023.
- THIS PROPERTY IS LOCATED IN FLOOD ZONE X, PER F.I.R.M. COMMUNITY PANEL 450085 0379 D, REVISED MARCH 16, 1989.

HORIZONTAL AND VERTICAL CONTROL NOTES

- CONTRACTOR SHALL USE SOUTH CAROLINA GEODETIC SURVEY MONUMENT 'USC & GS AERO AS MK 3' AS PRIMARY CONTROL POINT FOR THE PROJECT.
- ALL BEARINGS AND COORDINATES SHOWN HEREON ARE BASED ON SOUTH CAROLINA STATE PLANE COORDINATE SYSTEM 1983.
- ELEVATIONS SHOWN HEREON BASED ON VERTICAL DATUM NAVD '88.
- COORDINATE VALUES ARE SC GRID LOCALIZED ABOUT SCGS MONUMENT 'USC & GS AERO AS MK 3', THE COMBINED FACTOR IS 0.99981317.
- ALL DISTANCES ARE HORIZONTAL GROUND IN U.S. SURVEY FEET UNLESS OTHERWISE SHOWN.

LAYOUT PLAN NOTES:

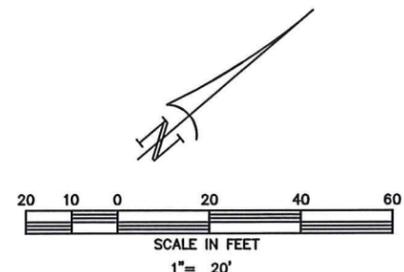
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON THIS SHEET. CONTRACTOR SHALL PROVIDE ADDITIONAL TEMPORARY BENCH MARKS FOR CONTROL OF CONSTRUCTION ACTIVITIES IF REQUIRED. CONTRACTOR SHALL COMPLETE NECESSARY LEVEL LOOPS TO VERIFY ACCURACY OF INSTALLED BENCH MARKS.
- FOR SEQUENCE OF CONSTRUCTION, SEE SHEETS A3.

GENERAL NOTES:

- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL CONTROL FOR THE DURATION OF THE PROJECT. BENCHMARKS AND CONTROL POINTS ARE AS SHOWN ON THIS SHEET.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING UNDERGROUND CABLES. EXISTING UNDERGROUND POWER MAY EXIST WITHIN THE WORK AREA. PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR SHALL HAVE ALL UTILITIES WITHIN THE WORK AREA LOCATED. ANY ACTIVE UTILITY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- THE NEW CONCRETE SLAB, FOUNDATIONS AND TRANSITIONS SLAB CONSTRUCTED FOR THE NEW HANGAR HAVE BEEN COMPLETED BY OTHERS. HANGAR APRON AND TAXILANE CONTRACTOR SHALL UTILIZE EXTREME CAUTION WHEN WORKING ADJACENT TO THE EXISTING CONCRETE TRANSITION SLAB AND HANGAR. ANY DAMAGE TO THE HANGAR TRANSITION SLAB OR HANGAR CAUSED BY THE HANGAR APRON AND TAXILANE CONTRACTOR SHALL BE REPAIRED AT THEIR OWN EXPENSE.
- CONTRACTOR TO USE EXTREME CARE DURING REMOVAL OPERATIONS FOR EXISTING PAVEMENT AND PAVING OPERATIONS ADJACENT TO EXISTING PAVEMENT. ANY DAMAGE TO EXISTING PAVEMENT TO REMAIN SHALL BE REPAIRED AT CONTRACTORS EXPENSE.

LEGEND

	EXISTING CONCRETE PAVEMENT
	EXISTING BITUMINOUS PAVEMENT
	EXISTING PAVEMENT TO BE REMOVED
	CONTRACTOR WORK AREA LIMITS
	EXISTING TAXIWAY EDGE LIGHT
	EXISTING LIGHTING CONDUIT
	EXISTING SWALE
	EXISTING CONTOURS
	EXISTING STORM DRAIN

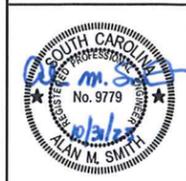


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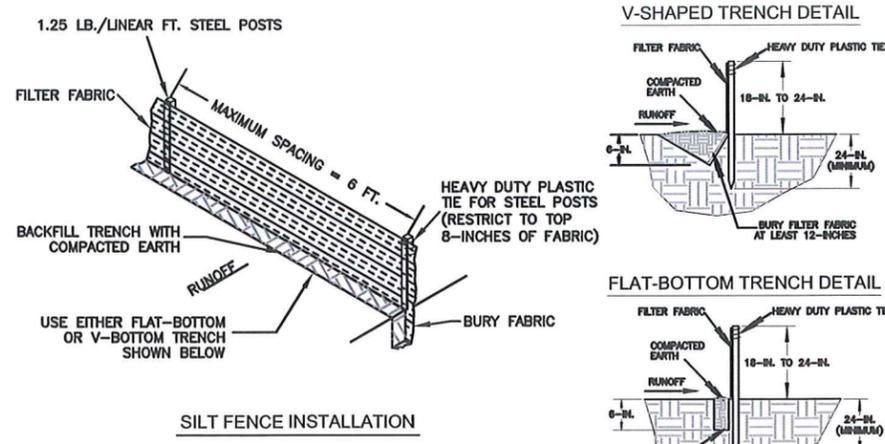
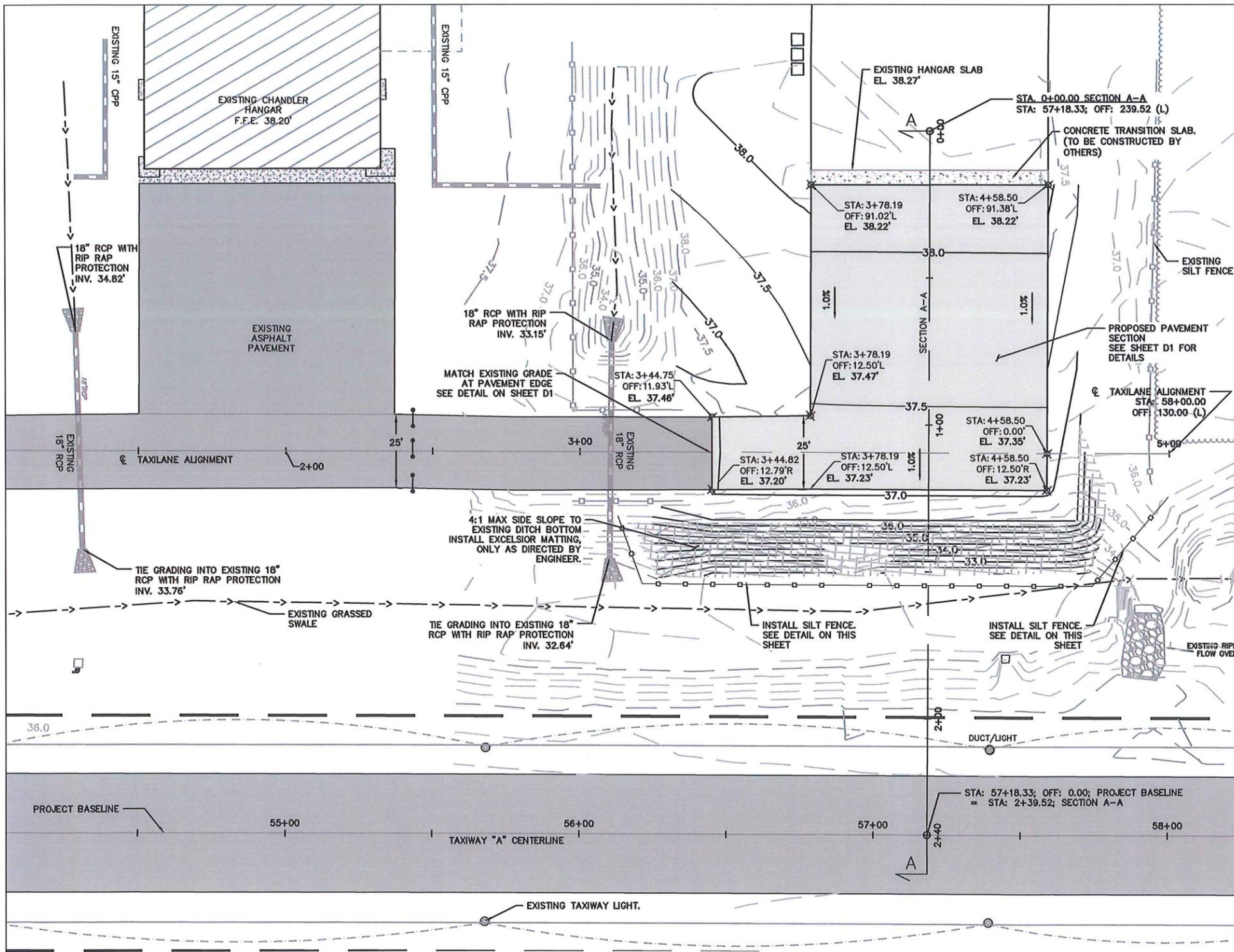
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REV. NO.	DESCRIPTION	DATE

GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
ALLISON HANGAR APRON AND TAXILANE PAVING
EXISTING CONDITIONS AND REMOVAL
PLAN



Date: OCTOBER 2023
Scale: 1" = 20'
Drawn: JDL
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Project No.: 2601-2303
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WHEN AND WHERE TO USE IT

DO NOT PLACE SILT FENCE ACROSS CHANNELS OR IN OTHER AREAS SUBJECT TO CONCENTRATED FLOWS. SILT FENCE SHOULD NOT BE USED AS A VELOCITY CONTROL BMP. CONCENTRATED FLOWS ARE ANY FLOWS GREATER THAN 0.5 CFS. MAXIMUM SHEET OR OVERLAND FLOW PATH LENGTH TO THE SILT FENCE SHALL BE 100-FEET. MAXIMUM SLOPE STEEPNESS (NORMAL [PERPENDICULAR] TO THE FENCE LINE) SHALL BE 2:1. SILT FENCE JOINTS, WHEN NECESSARY, SHALL BE COMPLETED BY ONE OF THE FOLLOWING OPTIONS: WRAP EACH FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 1-FOOT MINIMUM OVERLAP; OVERLAP SILT FENCE BY INSTALLING 3-FEET PASSED THE SUPPORT POST TO WHICH THE NEW SILT FENCE ROLL IS ATTACHED. ATTACH OLD ROLL TO NEW ROLL WITH HEAVY-DUTY PLASTIC TIES; OR, OVERLAP ENTIRE WIDTH OF EACH SILT FENCE ROLL FROM ONE SUPPORT POST TO THE NEXT SUPPORT POST.

ATTACH FILTER FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED WITHIN THE TOP 8-INCHES OF THE FABRIC. INSTALL THE SILT FENCE PERPENDICULAR TO THE DIRECTION OF THE STORMWATER FLOW AND PLACE THE SILT FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT. INSTALL SILT FENCE CHECKS (TIE-BACKS) EVERY 50-100 FEET, DEPENDENT ON SLOPE, ALONG SILT FENCE THAT IS INSTALLED WITH SLOPE AND WHERE CONCENTRATED FLOWS ARE EXPECTED OR ARE DOCUMENTED ALONG THE PROPOSED/INSTALLED SILT FENCE.

MATERIALS

STEEL POSTS
USE 48-INCH LONG STEEL POSTS THAT MEET THE FOLLOWING MINIMUM PHYSICAL REQUIREMENTS: COMPOSED OF HIGH STRENGTH STEEL WITH MINIMUM YIELD STRENGTH OF 50,000 PSI. HAVE A STANDARD "T" SECTION WITH A NOMINAL FACE WIDTH OF 1.38-INCHES AND NOMINAL "T" LENGTH OF 1.48-INCHES. WEIGH 1.25 POUNDS PER FOOT (± 8%). HAVE A SOIL STABILIZATION PLATE WITH A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES ATTACHED TO THE STEEL POSTS. PAINTED WITH A WATER BASED BAKED ENAMEL PAINT.

USE STEEL POSTS WITH A MINIMUM LENGTH OF 48 INCHES, WEIGHING 1.25 POUNDS PER LINEAR FOOT (± 8%) WITH PROJECTIONS TO AID IN FASTENING THE FABRIC. WHEN HEAVY CLAY SOILS ARE PRESENT ON SITE, STEEL POSTS WILL HAVE A METAL SOIL STABILIZATION PLATE WELDED TO THE BOTTOM SUCH THAT WHEN THE POST IS DRIVEN TO THE PROPER DEPTH, THE PLATE WILL BE BELOW THE GROUND LEVEL FOR ADDED STABILITY. THE SOIL PLATES SHOULD HAVE THE FOLLOWING CHARACTERISTICS: BE COMPOSED OF MINIMUM 15 GAUGE STEEL AND HAVE A MINIMUM CROSS SECTION AREA OF 17-SQUARE INCHES.

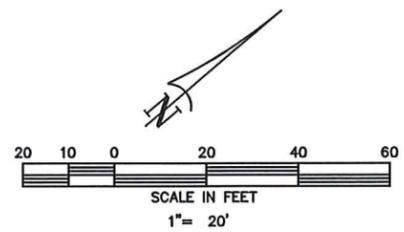
GEOTEXTILE FILTER FABRIC
SILT FENCE MUST BE COMPOSED OF WOVEN GEOTEXTILE FILTER FABRIC THAT CONSISTS OF THE FOLLOWING REQUIREMENTS: COMPOSED OF FIBERS CONSISTING OF LONG CHAIN SYNTHETIC POLYMERS OF AT LEAST 85% BY WEIGHT OF POLYESTERS, POLYESTERS, OR POLYAMIDES THAT ARE BLENDED WITH A NETWORK SUCH THAT THE FILAMENTS RETAIN DIMENSIONAL STABILITY RELATIVE TO EACH OTHER; FREE OF ANY TREATMENT OR COATING WHICH MIGHT ADVERSELY ALTER ITS PHYSICAL PROPERTIES AFTER INSTALLATION; FREE OF ANY DEFECTS OR FLAWS THAT SIGNIFICANTLY AFFECT ITS PHYSICAL AND/OR FILTERING PROPERTIES; AND HAVE A MINIMUM WIDTH OF 36-INCHES.

USE ONLY FABRIC APPEARING ON SC DOT'S QUALIFIED PRODUCTS LISTING (QPL), APPROVAL SHEET #34, MEETING THE REQUIREMENTS OF THE MOST CURRENT EDITION OF THE SC DOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION. 12-INCHES OF THE FABRIC SHOULD BE PLACED WITHIN EXCAVATED TRENCH AND TOED IN WHEN THE TRENCH IS BACKFILLED. FILTER FABRIC SHALL BE PURCHASED IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS. FILTER FABRIC SHALL BE INSTALLED AT A MINIMUM OF 24-INCHES ABOVE THE GROUND.

INSTALLATION
EXCAVATE A TRENCH APPROXIMATELY 6-INCHES WIDE AND 6-INCHES DEEP WHEN PLACING FABRIC BY HAND. PLACE 12-INCHES OF GEOTEXTILE FABRIC INTO THE 6-INCH DEEP TRENCH, EXTENDING THE REMAINING 6-INCHES TOWARDS THE UPSLOPE SIDE OF THE TRENCH. BACKFILL THE TRENCH WITH SOIL OR GRAVEL AND COMPACT. BURY 12-INCHES OF FABRIC INTO THE GROUND WHEN PNEUMATICALLY INSTALLING SILT FENCE WITH A SLICING METHOD. PURCHASE FABRIC IN CONTINUOUS ROLLS AND CUT TO THE LENGTH OF THE BARRIER TO AVOID JOINTS. WHEN JOINTS ARE NECESSARY, WRAPPED THE FABRIC TOGETHER AT A SUPPORT POST WITH BOTH ENDS FASTENED TO THE POST, WITH A 6-INCH MINIMUM OVERLAP. INSTALL POSTS TO A MINIMUM DEPTH OF 24-INCHES. INSTALL POSTS TO A MINIMUM OF 1 TO 2 INCHES ABOVE THE FABRIC, WITH NO MORE THAN 3-FEET OF THE POST ABOVE THE GROUND. SPACE POSTS TO MAXIMUM 6-FEET CENTERS. ATTACH FABRIC TO THE STEEL POSTS USING HEAVY-DUTY PLASTIC TIES THAT ARE EVENLY SPACED AND PLACED IN A MANNER TO PREVENT SAGGING OR TEARING OF THE FABRIC. IN ALL CASES, TIES SHOULD BE AFFIXED IN NO LESS THAN 4 PLACES. INSTALL THE FABRIC A MINIMUM OF 24-INCHES ABOVE THE GROUND. WHEN NECESSARY, THE HEIGHT OF THE FENCE ABOVE GROUND MAY BE GREATER THAN 24-INCHES. IN TIDAL AREAS, EXTRA SILT FENCE HEIGHT MAY BE REQUIRED. THE POST HEIGHT WILL BE TWICE THE EXPOSED POST HEIGHT. POST SPACING WILL REMAIN THE SAME AND EXTRA HEIGHT FABRIC WILL BE 4-, 5-, OR 6- FEET TALL. LOCATE SILT FENCE CHECKS EVERY 100 FEET MAXIMUM AND AT LOW POINTS. INSTALL THE FENCE PERPENDICULAR TO THE DIRECTION OF FLOW AND PLACE THE FENCE THE PROPER DISTANCE FROM THE TOE OF STEEP SLOPES TO PROVIDE SEDIMENT STORAGE AND ACCESS FOR MAINTENANCE AND CLEANOUT.

INSPECTION AND MAINTENANCE
THE KEY TO FUNCTIONAL SILT FENCE IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL. REGULAR INSPECTIONS OF SILT FENCE SHALL BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24-HOURS AFTER EACH RAINFALL EVENT THAT PRODUCES 1/2-INCH OR MORE OF PRECIPITATION. ATTENTION TO SEDIMENT ACCUMULATIONS ALONG THE SILT FENCE IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED AND REMOVED WHEN NECESSARY. REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 1/3 THE HEIGHT OF THE FENCE. SEDIMENT REMOVED SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED. CHECK FOR AREAS WHERE STORMWATER RUNOFF HAS ERODED A CHANNEL BENEATH THE SILT FENCE, OR WHERE THE FENCE HAS SAGGED OR COLLAPSED DUE TO RUNOFF OVERTOPPING THE SILT FENCE. INSTALL CHECKS/TIE-BACKS AND/OR REINSTALL SILT FENCE, AS NECESSARY. CHECK FOR TEARS WITHIN THE SILT FENCE. AREAS WHERE SILT FENCE HAS BEGUN TO DECOMPOSE AND FOR ANY OTHER CIRCUMSTANCE THAT MAY RENDER THE SILT FENCE INEFFECTIVE. REMOVED DAMAGED SILT FENCE AND REINSTALL NEW SILT FENCE IMMEDIATELY. SILT FENCE SHOULD BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED AND ONCE IT IS REMOVED, THE RESULTING DISTURBED AREA SHALL BE PERMANENTLY STABILIZED.

TEMPORARY SILT FENCE
NOT TO SCALE



NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR COMPLETION OF AS-BUILT SURVEY. SEE SECTION PSP-28 OF THE PROJECT SPECIFICATIONS FOR AS-BUILT SURVEY REQUIREMENTS.

GRADING NOTES:

1. PROPOSED CONTOURS SHOWN ON THIS SHEET ARE THE FINISHED GRADE CONTOURS AFTER COMPLETION OF ALL PAVING.
2. FOR TYPICAL PAVEMENT SECTION AND DETAILS SEE SHEET D1.

LEGEND	
	EXISTING CONCRETE PAVEMENT
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED PAVEMENT
	PROPOSED EXCELSIOR MATTING
	EXISTING CONTOUR
	EXISTING TREELINE
	EXISTING GRASSED SWALE
	EXISTING TAXIWAY LIGHT (BASE MOUNTED)
	EXISTING ELECTRICAL DUCT
	EXISTING STORM DRAIN
	PROPOSED CONTOUR

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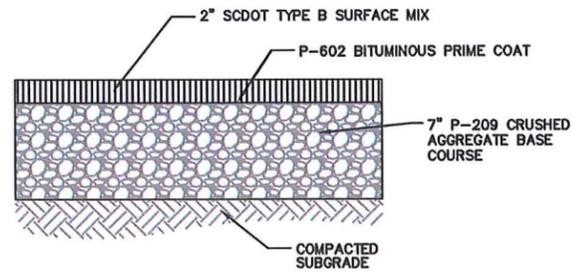
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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
ALLISON HANGAR APRON AND TAXILANE PAVING
GRADING AND PAVING PLAN



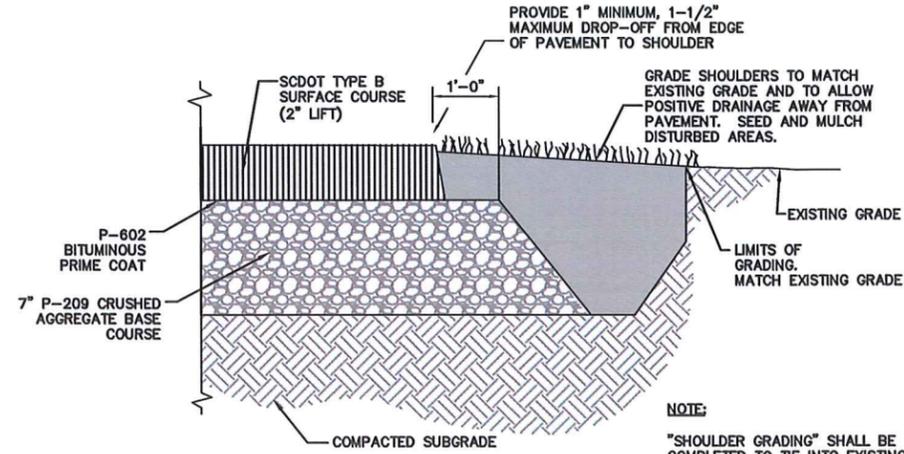
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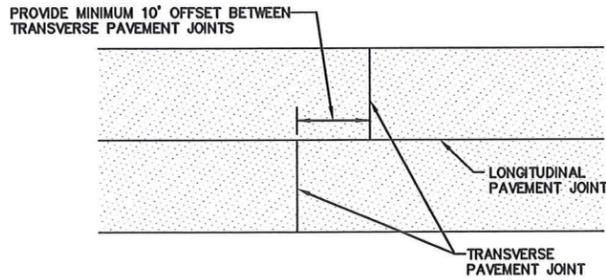
APRON AND TAXILANE PAVEMENT SECTION
NOT TO SCALE

NOTE:
WHERE REQUIRED, FILL MATERIAL PLACED UNDER THE PROPOSED PAVEMENT SECTION SHALL BE CONSTRUCTED AND COMPACTED IN ACCORDANCE WITH SPECIFICATION P-152 REQUIREMENTS.
WHERE PAVEMENT SECTION IS CONSTRUCTED DIRECTLY ON EXISTING SUBGRADE, THE EXPOSED SUBGRADE SHALL BE COMPACTED IN ACCORDANCE WITH SPECIFICATION SECTION P-152.

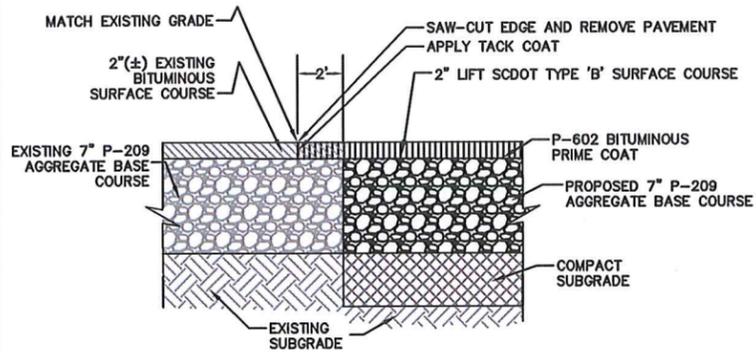


TAXILANE PAVEMENT EDGE DETAIL
NOT TO SCALE

NOTE:
"SHOULDER GRADING" SHALL BE COMPLETED TO TIE INTO EXISTING GRADE OR A MAXIMUM OF 10'.
GRADE SHOULDERS TO MATCH EXISTING GRADE AND TO ALLOW POSITIVE DRAINAGE AWAY FROM PAVEMENT. SEED AND MULCH DISTURBED AREAS.



TRANSVERSE PAVEMENT JOINTS
NOT TO SCALE



LEGEND

- SCDOT TYPE 'B' SURFACE COURSE
- P-209 CRUSHED AGGREGATE BASE COURSE
- COMPACTED SUBGRADE
- EXISTING SUBGRADE

PAVEMENT TRANSITION DETAIL AT EXISTING TAXILANE
NOT TO SCALE

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GEORGETOWN COUNTY AIRPORT
GEORGETOWN, SOUTH CAROLINA
ALLISON HANGAR APRON AND TAXILANE PAVING
TYPICAL SECTIONS AND MISCELLANEOUS DETAILS



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